



Brent Spence Bridge Corridor Project Friends of Peaseburg(FOPNA) Neighborhood Association Meeting Summary November 28, 2022

Introduction

The Friends of Peaseburg Neighborhood Association (FOPNA) Neighborhood Meeting was held on November 28, 2022 from 6:00 pm to 8:00 pm. FOPNA conducted their regularly-scheduled neighborhood meeting from 6:00 pm to 7:00 pm. The Brent Spence Bridge (BSB) Corridor Project team presented and answered questions from 7:00 pm to 8:00 pm. The meeting was held at the Covington Firefighter's Hall located at 2232 Howell Street in Covington, Kentucky. It was publicized through the FOPNA Facebook page, the FOPNA email list, and flyers were posted at three local stores, two schools, and a church (see Attachment 1). Attendees at the meeting included the City of Covington, the project team, and members of the public. In total, 22 people attended the meeting, including 6 individuals from the city and the project team (see Attachment 2). The meeting was also streamed via Facebook Live with a maximum of five viewers. Photographs are included in Attachment 3.

The meeting format included introductions by the Mayor of Covington, Joseph U. Meyer and a formal presentation by Stacey Hans (KYTC) (see Attachment 4). During the presentation, the project team responded to questions and comments offered by those present. The mayor concluded the meeting and noted there is an Aesthetics Committee working to support the project. He also noted that the biggest priorities of noise walls and flooding issues are being addressed. He commended KYTC on the way they have managed the project today and when the project will be done. The FOPNA president also commended the presentation and said it was insightful.

Exhibits showing noise wall renderings and lane configurations were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit PublicInput.com/bsbc to review project materials and offer additional feedback were also provided. Six (6) comment forms and ten (10) demographic surveys were returned at the meeting.



Demographics

The following is a breakdown of the responses received from the demographic survey.



A write in space was available to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. Comments included:

- Continue meetings throughout our neighborhoods.
- I think you're already going above and beyond on this project.

Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Clarification / understanding of local versus through traffic and ramp connections.
- Concern over truck traffic and noise.
- Clarification / understanding of the process for noise evaluation and discussion on impacts with / without noise wall.
- Drainage for low lying areas, particularly neighborhoods.
- Clarification of timeline and staging of construction.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website (www.brentspencebridgecorridor.com) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.

Comments noted on the survey forms returned at the meeting include:

- Noise / sound walls need to extend south beyond Pike – the current proposed noise walls that extend to Kyles Ln are much needed and wanted.
- Will there be people uprooted? If so, are they getting a fair price for their property? How long will the project take and what about the traffic? Will there be long delays?
- The updated designs are much better than the original plans. We appreciate the effort to retain so many existing homes and businesses. We also appreciate the adjacent infrastructure plans such as storm sewers and other utilities. Thanks for all the extra outreach.
- Great presentation and presenter Stacey; Extremely valuable information and insight; Noise walls area must for Peasenburg residents (thank you); Eliminating Willow Run combined water flow will truly assist in deterring flooding our neighborhood; Timeline seems reasonable; Keep project transparency.



Table 1: Q & A from Session

Question	Project Team Response
What have neighbors said about traffic?	The project team is holding meetings in the neighborhoods adjacent to the project to solicit input.
When do you make the decision to go I-71 or I-75 NB?	It will be just past the bridge NB on the Ohio side and will be the only decision to make on the companion bridge. Travelers on the local bridge would have the choice to get on I-71 and can continue on the collector-distributor system to reach I-75.
12 th Street will be accessible to both?	All local movements to and from the highway will occur through the collector-distributor system. Local traffic will function similar to how it does today.
Does it affect neighborhoods or Goebel Park?	Some land will be required from Goebel Park, but no residences will be impacted on the east side of the highway.
About how much of the park are you taking?	About 3 acres will be required from Goebel Park, but KYTC will give back 2 acres once the West 5 th Street ramp is relocated.
Is the collector-distributor where the storm / sewer line will go?	The presentation provided a map of the storm trunk line.
Will the walking trail through the park be lost? Concerned about trail by Toyota to hospital.	A portion of the walking trail through Goebel Park will be impacted, but it will be replaced. KYTC is committed to providing bike and pedestrian facilities in addition to vehicular traffic.
Question about taking away flood basins. Will they be replaced? We have flooding issues.	Mapping showing flood basins was provided and explained during the presentation.
Where are the residential relocations?	Three residential properties in Covington and one property further to the south will be relocated. The three in Covington are all on the west side of the interstate. The one to the south is on the east side of the interstate. Other properties may have strip right-of-way acquisition. KYTC is currently acquiring right-of-way. Commercial relocations are primarily on the west side of the highway. Billboards are technically considered commercial property.
Jake Braking? Enforcement?	KYTC cannot prohibit jake braking on state-maintained roadways since it is a safety function.
Will the expanded interstate be noisier?	Ideally adding capacity will allow traffic to flow more freely and possibly slightly reduce noise.



Question	Project Team Response
Are Crown Pointe apartments protected / affected?	The Crown Pointe apartments are further north. Interstate may be pulling further away, so noise walls will not be as effective.
Specifically point out where the noise wall starts and stops and where Highland Avenue is.	Noise wall locations were clarified during the presentation.
Round of applause for noise walls!	Comment noted.
Will this help with flooding on 27 th and Russell?	Flooding issues in that location are beyond what KYTC can control with the BSB Corridor Project. The local government would need to be contacted about other flooding issues.
Where does the riverfront walkway go? Under / through bridge?	The riverfront walkway has been built under the BSB along the Ohio River. The BSB Corridor Project will connect shared use paths in Goebel Park to this trail.
How will vehicles access Covington going north or south?	All access to and from Covington will be off the collector-distributor.
How much of that time is “no bridge”?	It is anticipated the companion bridge will be built first. Once traffic is transferred to the companion bridge, repair work to the existing bridge will occur. KYTC does not anticipate any full closures of traffic across the river unless a major incident occurs. The project will establish a maintenance of traffic and incident management plan during construction.
Will the Roebling Bridge be protected?	Yes.



Attachment 1: Advertising Materials

Seth Turner

From: FOPNA Peaselburg
Sent: Tuesday, November 1, 2022 12:02 PM
To: Sherry Kish
Cc: Seth Turner
Subject: Re: Brent Spence Bridge Presentation on 11/28/22

Follow Up Flag: Follow up
Flag Status: Completed

Sherry and Seth,

Thank you for reaching out to me regarding spreading the word about our upcoming meeting.

I already informed our residents at our recent meeting of this presentation opportunity.
I have marked below the areas that I will be happy to share the information.
I also think we can begin your presentation at 7pm....would you kindly update the flyer and resend.

Thanks in advance,

Susan

On Fri, Oct 28, 2022 at 3:54 PM Sherry Kish wrote: [_____](#)

Susan,

Thank you so much for allowing the Brent Spence Bridge project team to attend the Friends of Peaselburg neighborhood meeting you are setting up on November 28th. As you know, the project team wants to share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

Our goal is to be sure all notifications about meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous neighborhood meeting?	X	10/24/22

Email list	Can you email a neighborhood wide or citywide email list with information about BSB presentation at the upcoming meeting?	X I WILL EMAIL BY 11/4/22	
Facebook (private to neighborhood)	Can you share a Facebook message about BSB presentation?	X I WILL POST ON BOTH FOPNA AND THE COVINGTON NEIGHBORHOOD COLLABORATIVE FB PAGES WITH ABILITY TO SHARE	
Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?	X WE HAVE 3 LOCAL STORES I WILL HANG FLYERS	
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?	X WE HAVE TWO ELEMENTARY SCHOOLS AND A CHURCH THAT I WILL SHARE THE FLYER WITH AND ASK THAT THEY SHARE WITH PARENTS.	
Other	Is there another outlet you have available to get the word out?	NA	
Do you need assistance from us with advertising?	We made a flyer for your consideration (attached to this email).	I WILL SHARE VIA EMAIL, FACEBOOK AND HANG LOCALLY.	

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!
Sherry

Sherry R. Kish

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Susan Barnett
FOPNA Chairperson

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Seth Turner

From: FOPNA Peaselburg
Sent: Monday, November 28, 2022 1:48 PM
To: Seth Turner
Subject: Fwd: Brent Spence Bridge Project Meeting hosted by FOPNA 11-28-22
Attachments: KYTC -ODOT - FOPNA Bridge Flyer.pdf

Hello Seth,

Attached is the email that I sent to our neighbors....it was sent to about 50 residents. We also posted it on our FaceBook page - Friends of Peaselburg. In addition, I hung a couple of the flyers in our local businesses.

If you have any additional questions, please just let me know.

Susan

----- Forwarded message -----

From: **FOPNA Peaselburg** [_____](#)
Date: Fri, Nov 4, 2022 at 12:41 PM
Subject: Fwd: Brent Spence Bridge Project Meeting hosted by FOPNA 11-28-22
To:

Hello Neighbors,

As mentioned at our recent FOPNA meeting, I was contacted by the HNTB Corporation which has been contracted by the Kentucky Transportation Cabinet (KTC) and the Ohio Department of Transportation (ODOT) regarding the Brent Spence Bridge Project. Representatives from the Bridge Project Team requested a meeting with FOPNA so they can discuss and share project updates.

Therefore, we have invited them to provide a presentation after our November FOPNA meeting on November 28th. *Our November 28th FOPNA meeting will begin at 6pm and the Brent Spence Bridge presentation will begin at 7pm.*

Here are some details and links for further information prior to the meeting:

The presentation should last about 20-30 minutes with the goal to share a little about the project and then answer any questions we may have.

Information boards will be up for display so it's easier to see the proposed changes. Team members will stay after the meeting to discuss, if necessary.

FOPNA hopes to be able to stream this meeting on FaceBook Live via our Friends of Peaselburg Page.

The team is sharing website information in advance to allow our neighbors to formulate questions in advance.

Below is some information and links to website/webpages to allow you to formulate questions in advance:

- The project team has submitted applications for two federal grants.
- Project website link. <https://brentspencebridgecorridor.com/>
- News and past newsletters are linked on this page: <https://brentspencebridgecorridor.com/news/>
- At the bottom of each page of the website is a sign-up form for newsletters and a comment/question form in case someone has a question before the meeting.

We hope you will join us! Please share this information with your neighbors so we are all informed about this important and informative meeting that will effect our neighborhood and city.

Thanks in advance,

Susan

Susan Barnett
FOPNA Chairperson


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FOPNA Facebook Page

Facebook.com



Friends of Peaselburg Neighborhood Association

Intro

Peaselburg is nestled in the hills and valleys on the west side of the city of Covington in Northern

Page · Community

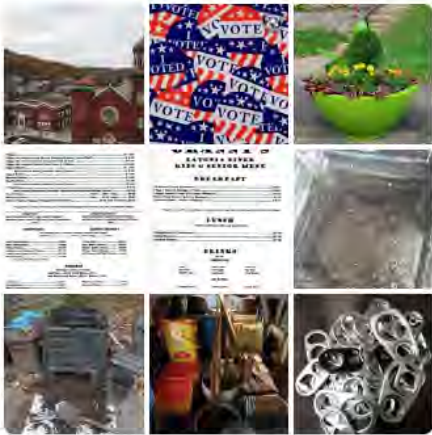
Peaselburg, KY


(859) 547-5563

friendsofpeaselburg@gmail.com

Photos

[See all photos](#)





Friends of Peaselburg Neighborhood Association

November 8 at 2:48 PM · 🌐

Brent Spence Bridge Project Discussion 11-28-22

FOPNA was contacted by the HNTB Corporation which has been contracted by the Kentucky Transportation Cabinet (KTC) and the Ohio Department of Transportation (ODOT) regarding the Brent Spence Bridge Project to set a neighborhood meeting to discuss the bridge plan and share updates to the project.

Therefore, we will be hosting them to provide a presentation after our November FOPNA meeting on November 28th.

- FOPNA meeting at 6pm
- Brent Spence Bridge presentation at 7pm.

Here are some details and links for further information prior to the meeting:

The presentation should last about 20-30 minutes to share a little about the project and then answer any questions we may have. Information boards will be up for display so it's easier to see the proposed changes.

FOPNA is planning to stream this meeting on FaceBook Live via this Friends of Peaselburg Page.

Below is information and links to website/webpages to allow you to formulate questions in advance:

- *The project team has submitted applications for two federal grants
- *Project website link. <https://brentspencebridgecorridor.com/>
- *News and past newsletters are linked on this page: <https://brentspencebridgecorridor.com/news/>

NOTE: At the bottom of each page of the website is a sign-up form for newsletters and a comment/question form in case someone has a question before the meeting.

We hope you will join us! Please share this information with your neighbors so we are all informed about this important and informative meeting that will effect our neighborhood and city.



Attachment 2: Sign-In Sheet

SIGN-IN SHEET



BRENT SPENCE BRIDGE CORRIDOR PROJECT
Neighborhood Outreach Meeting — November 28, 2022

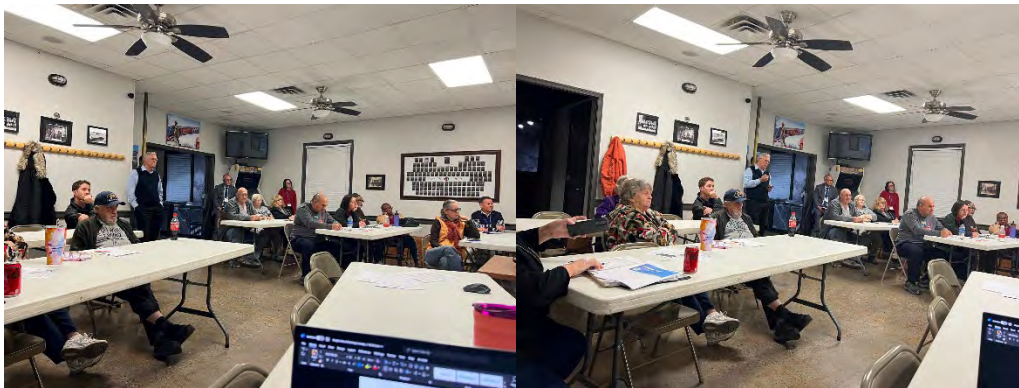
Name	Representing	Address	Phone	Email
1. Susan Barrett	Friends of Peaseburg			
2. JANITA WINDLE	Friends of Peaseburg			
3. Betty Schumacher	"			
4. MIKE TORRE	"			
5. Yola Reagon	"			
6. Larry Kibbgood	"			
7. Randy Fitzwater	"			
8. Deanna Fitzwater	"			
9. DANAN DUMALLIS	FOPNA			
10. Heidi Buchman	FOPNA			
11. RHONDA CLAWNER	FOPNA			
12. Pete Neroni	FOPNA			
13. Jill Corwin	FOPNA			
14. Clay Tenberge	FOPNA			
15. Sheila Gray	FOPNA			
16. Judi HeFlig	HNTB			
17. Mark Behreer	HNTB			
18. Sharré Hobb	KYTC			
19. Gary Galister	KYTC			
20. Lindsay Lumbel	HMB			

page _____ of _____
KYTC Item No. 6-17 | ODOT PID 89068

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



Attachment 3: Photographs





Attachment 4: Presentation

BRENT SPENCE
BRIDGE CORRIDOR



Peaseburg Neighborhood Meeting (KY)

November 28, 2022



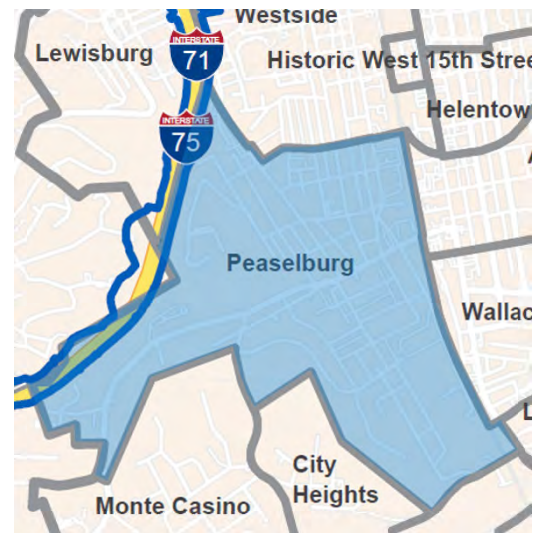
INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.
brentspencebridgecorridor.com



The meeting will open with introductions of the Project Team in attendance (2 min).

Welcome

- Meeting purpose
 - Share updates on the Brent Spence Bridge (BSB) Corridor Project
 - Offer residents in the Peasenburg neighborhood the opportunity to share feedback with the Project Team
- Agenda
 - General project overview
 - Project specifics in the Peasenburg area
 - Discussion/feedback from Peasenburg residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Peasenburg neighborhood. Most importantly, tonight we're focusing on hearing from the residents of Peasenburg. We're here to answer your questions and to listen to your feedback about the project.

Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, KYTC has begun purchasing the land needed to build the project.

Project Description

Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E

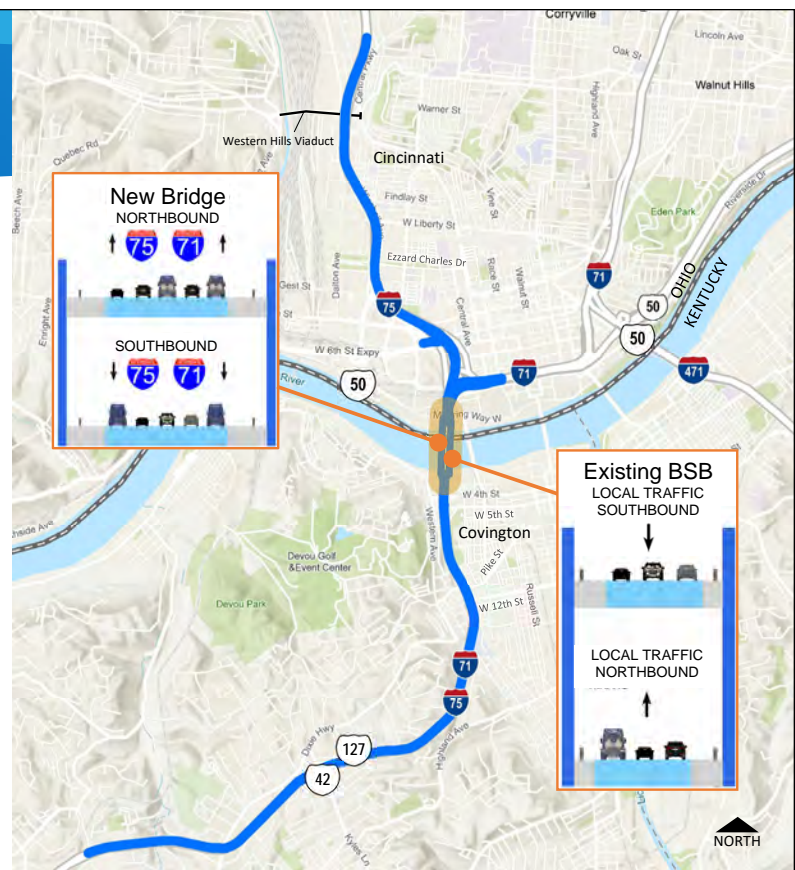


In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

Project Description

Brent Spence Bridge

- New double-decker companion bridge
 - 5 lanes each deck
 - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
 - Three lanes each deck
 - Increased inside/outside shoulders
 - Carry local traffic



Note: Bridge details will come in with click (marked by #).

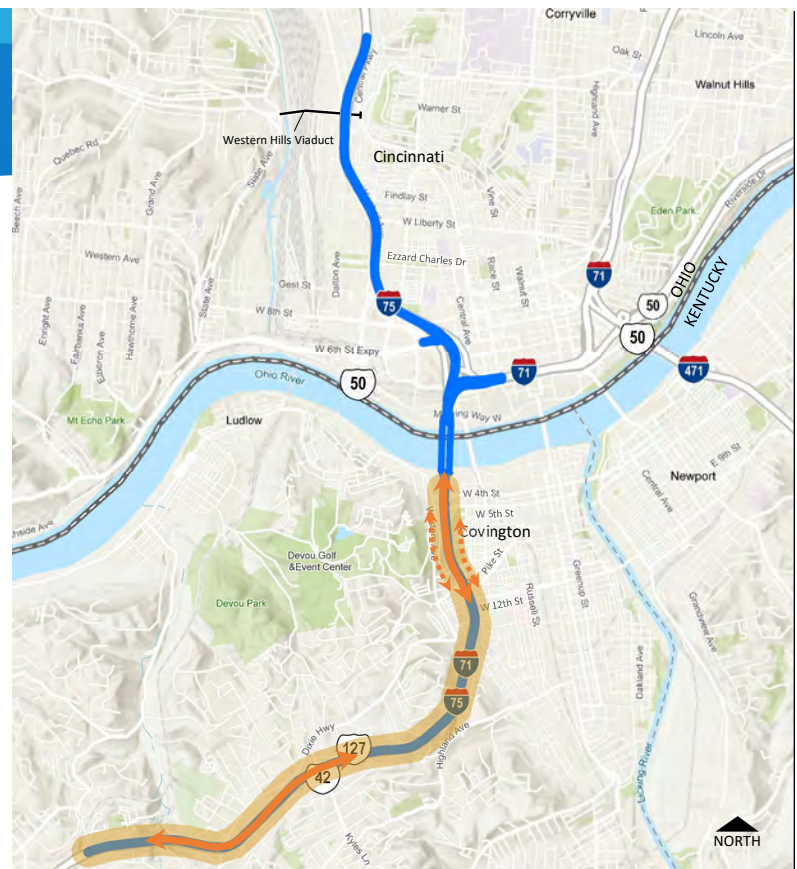
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

Project Description

Kentucky

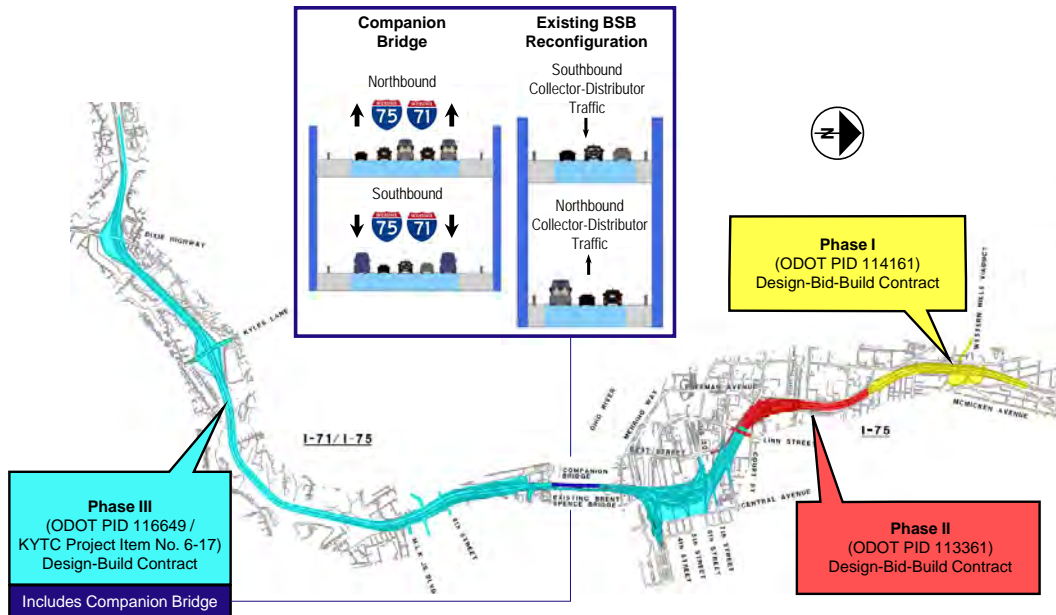
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

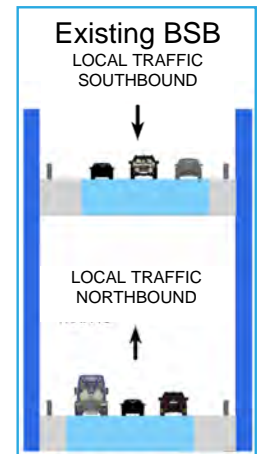
Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

What Has Changed?

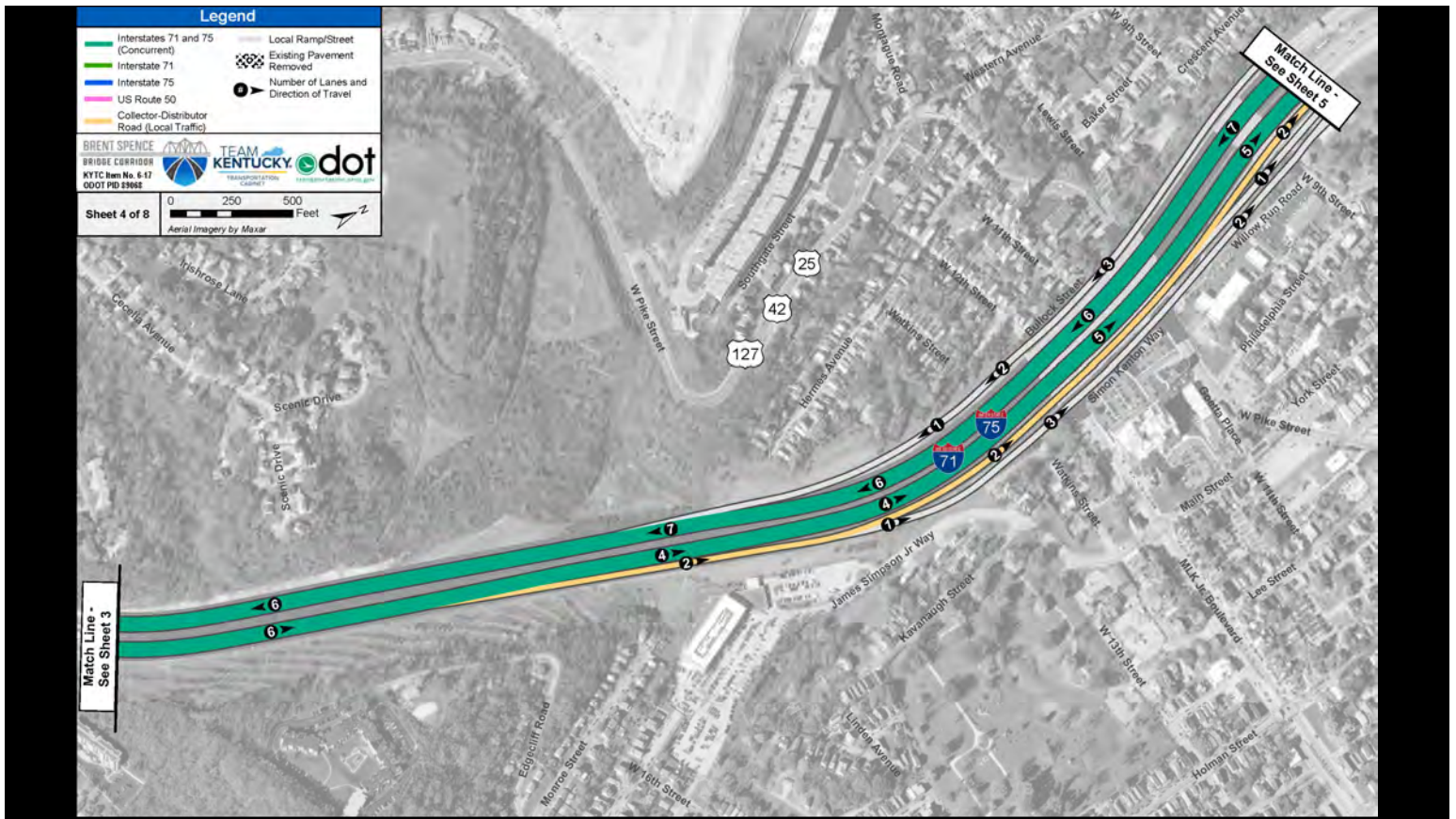
- Reconfigured how traffic travels across the Ohio River
 - Companion bridge carries through (interstate) traffic
 - Existing bridge carries local traffic
 - All northbound and southbound traffic on one deck
 - Width of companion bridge substantially reduced



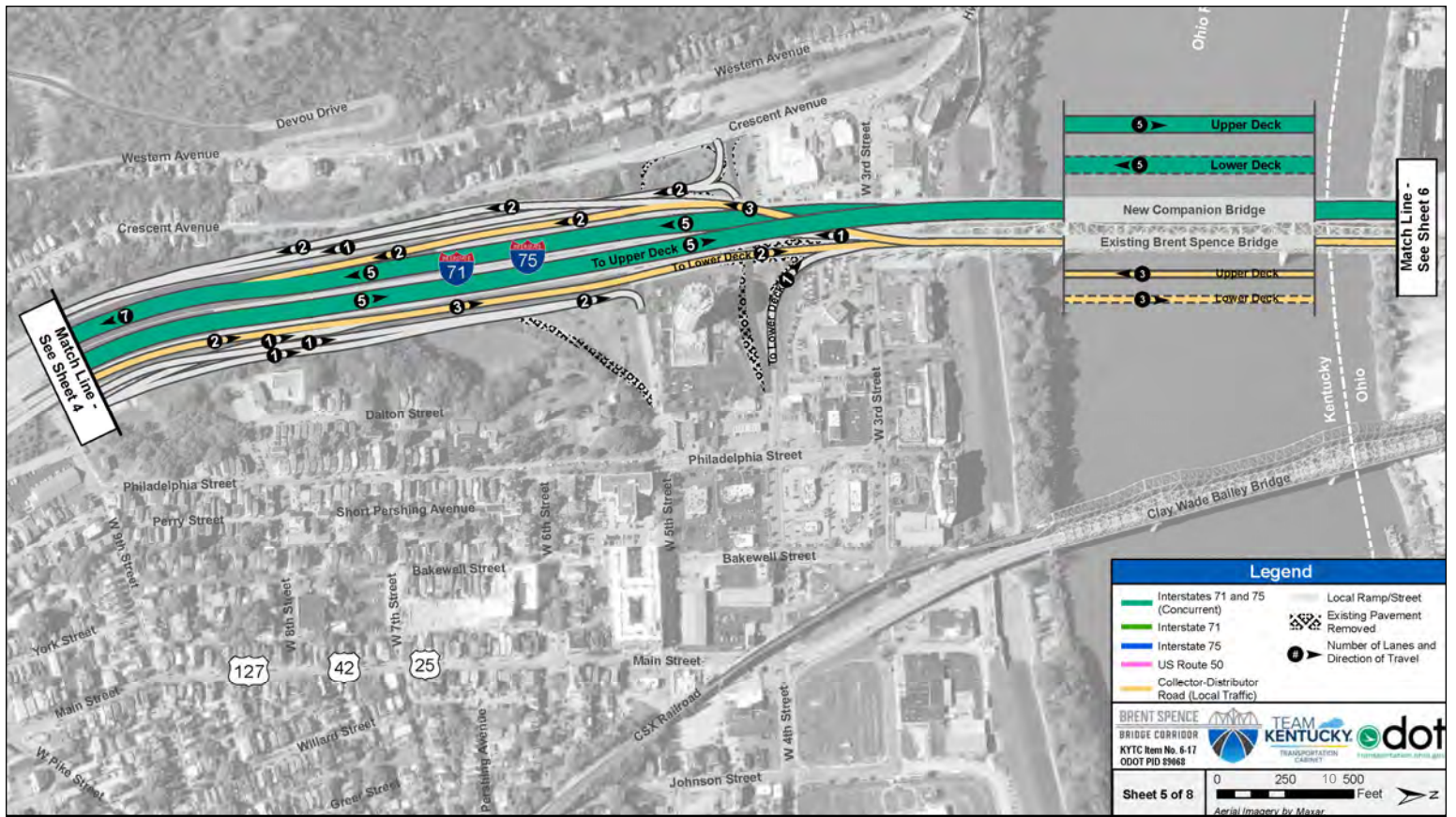
8

As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

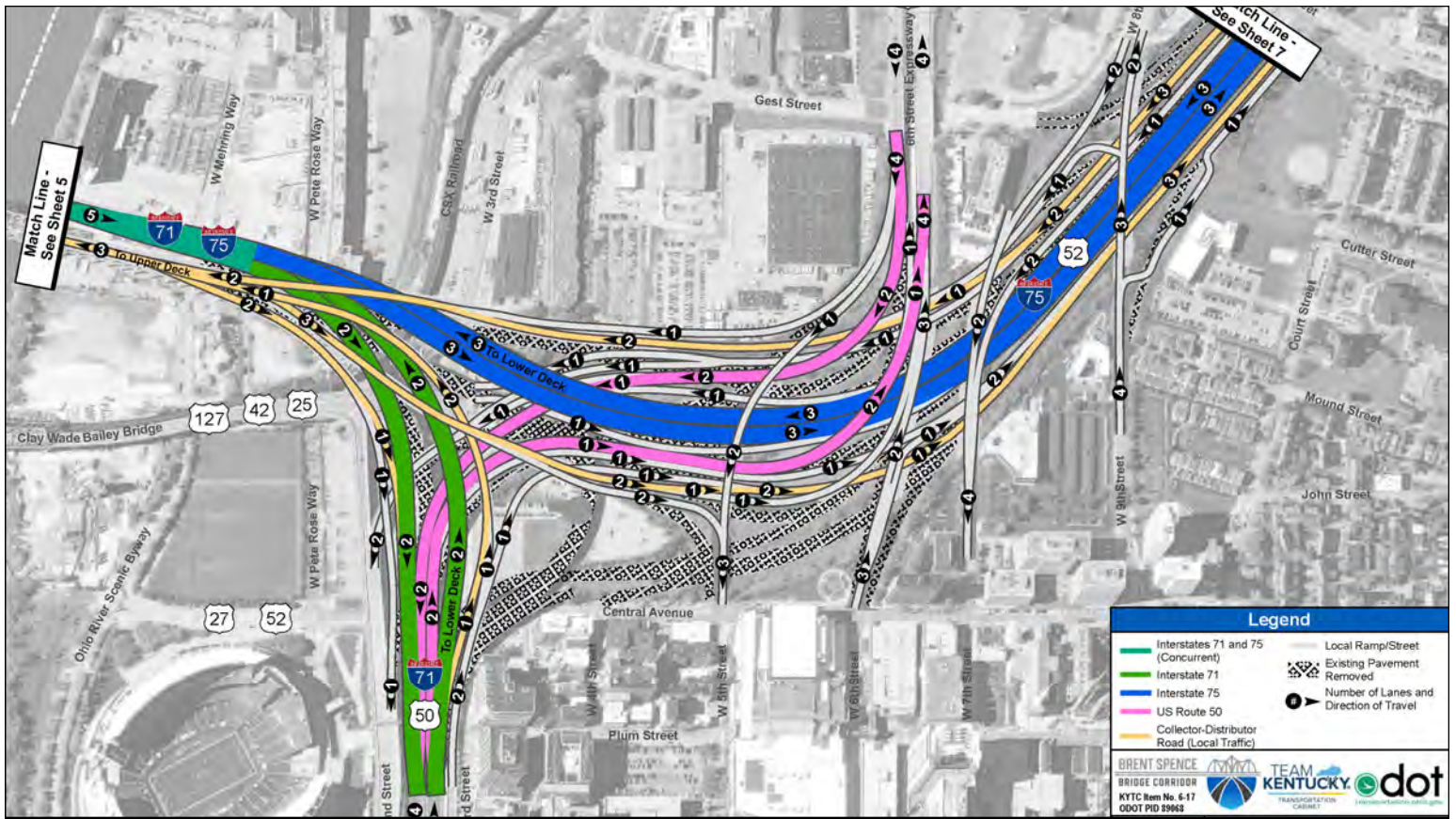
Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



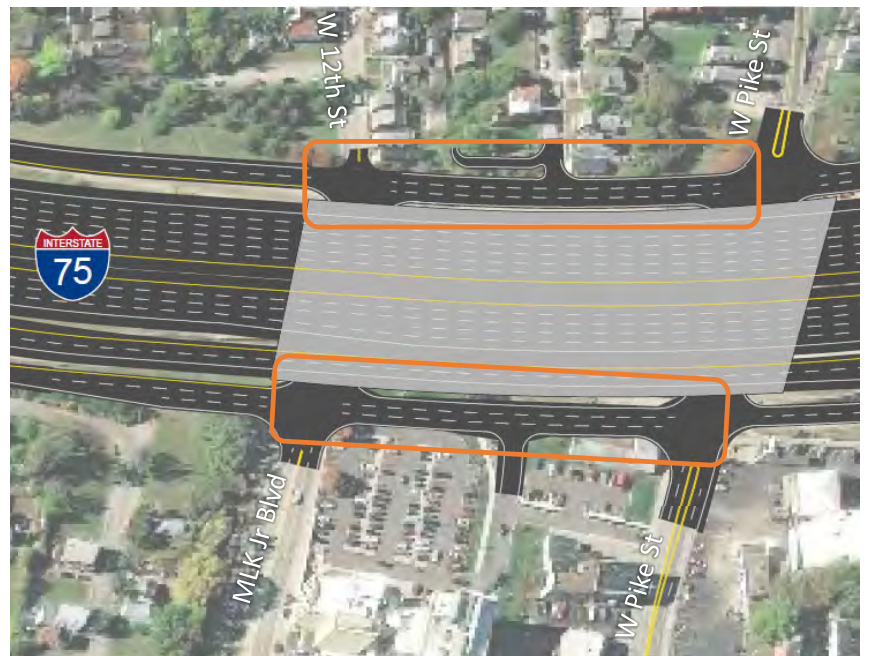
This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.



Here's another view from Covington about ¾ mile north of Peaselburg.

What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
 - Residential | 4 vs. 40+
 - Commercial | 5 vs. 6



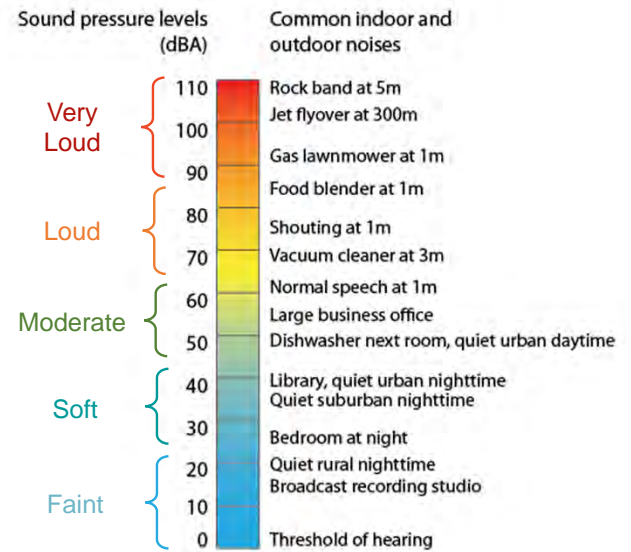
Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
 - Change in 1 dBA = barely perceptible
 - Change in 10 dBA = doubling or halving of sound



Source: Minnesota Pollution Control Agency

15

Now let's dig into how the project will impact Peaselburg, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

Noise

- KYTC Noise Policy
- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
 - Are areas of frequent human exterior use present?
 - Does the noise level exceed certain established thresholds?
 - Does the noise level substantially increase?
 - Can a barrier effectively block noise?
 - Does a barrier provide enough noise reduction to justify cost?
 - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.

Kentucky Transportation Cabinet
Department of Highways
Division of Environmental Analysis



NOISE ANALYSIS AND ABATEMENT POLICY

APPROVED BY:

Todd Jeter, Division Administrator
Federal Highway Administration

Date

James Gray, Secretary
Kentucky Transportation Cabinet

Date

Revised August 1, 2022
EFFECTIVE DATE JULY 1, 2020
Revised July 1, 2010
Original July 2011

16

KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dbA. We also determine if there is a substantial increase – specifically a 10 dbA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dbA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

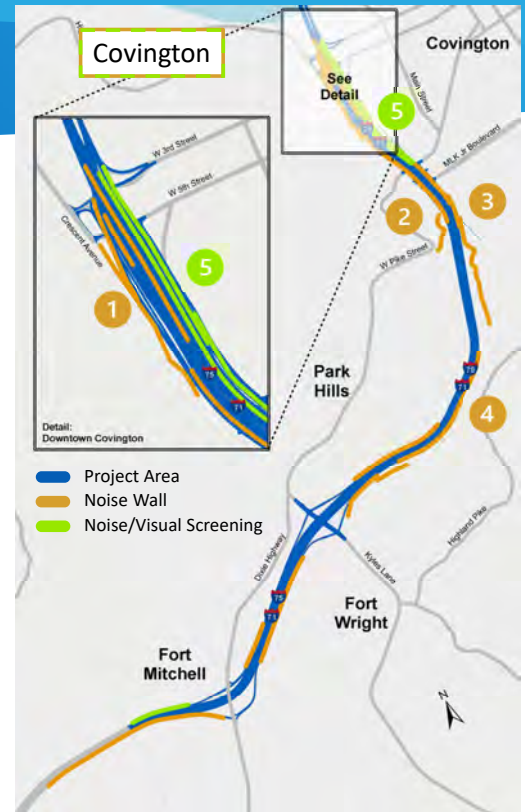
Noise

Noise walls reasonable/feasible per KYTC Noise Policy

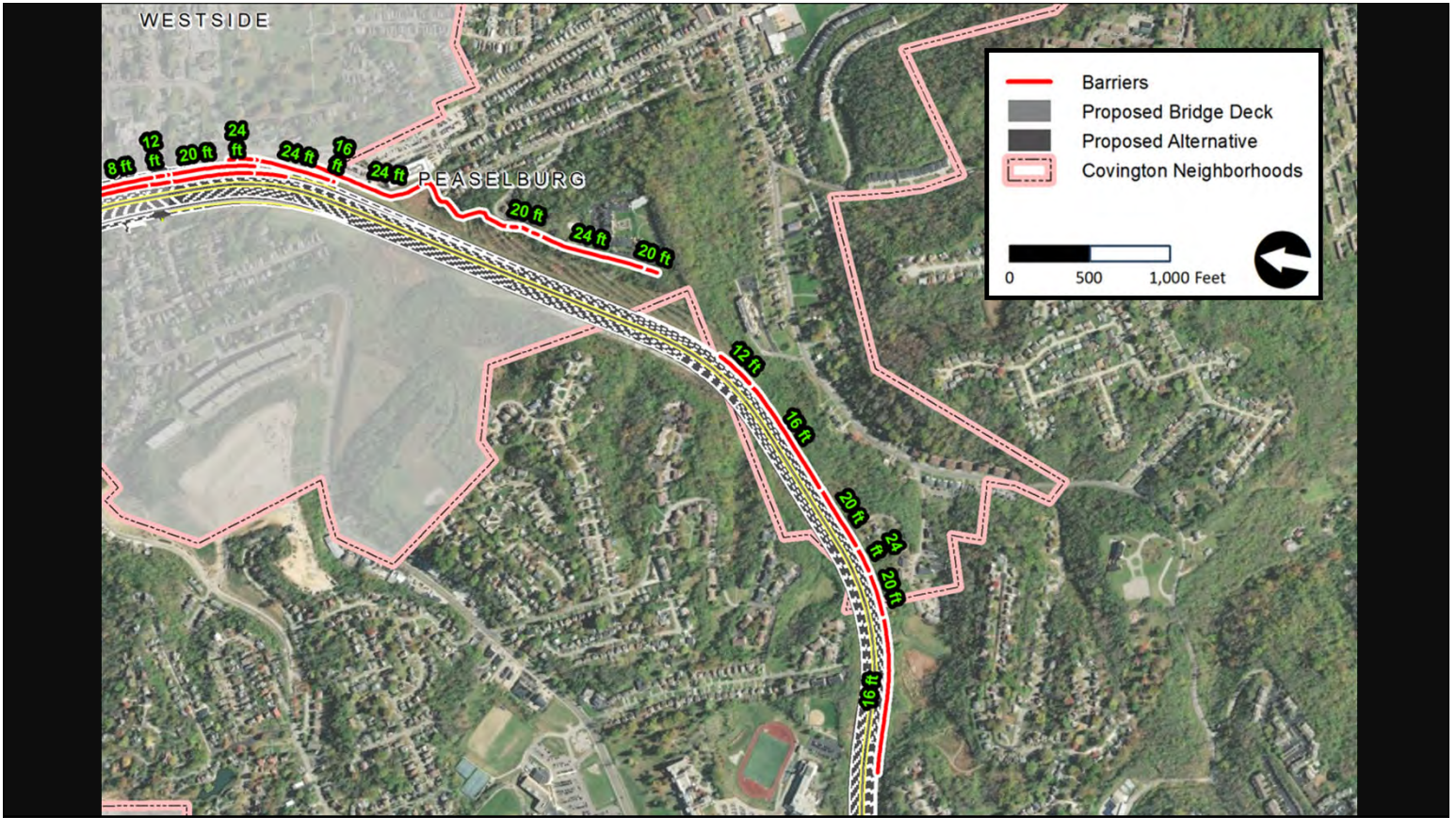
- 3 • Pike St to Edgecliff Rd (Westside/Peaselburg)
 - Without noise wall = 71 dBA
 - With noise wall = 64 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m
	Large business office

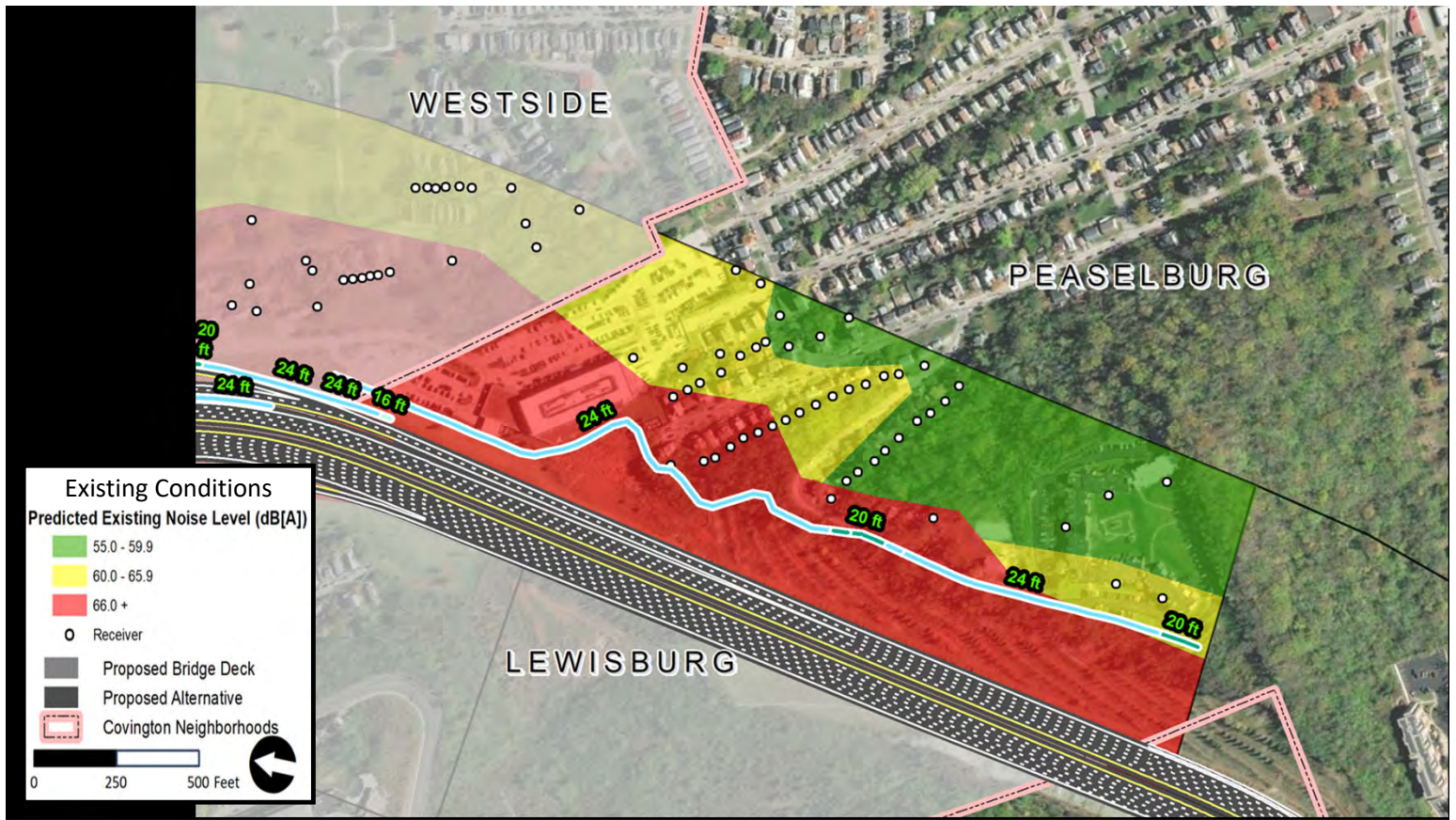
Note: Sound levels represent the average for the area benefitted by the wall.



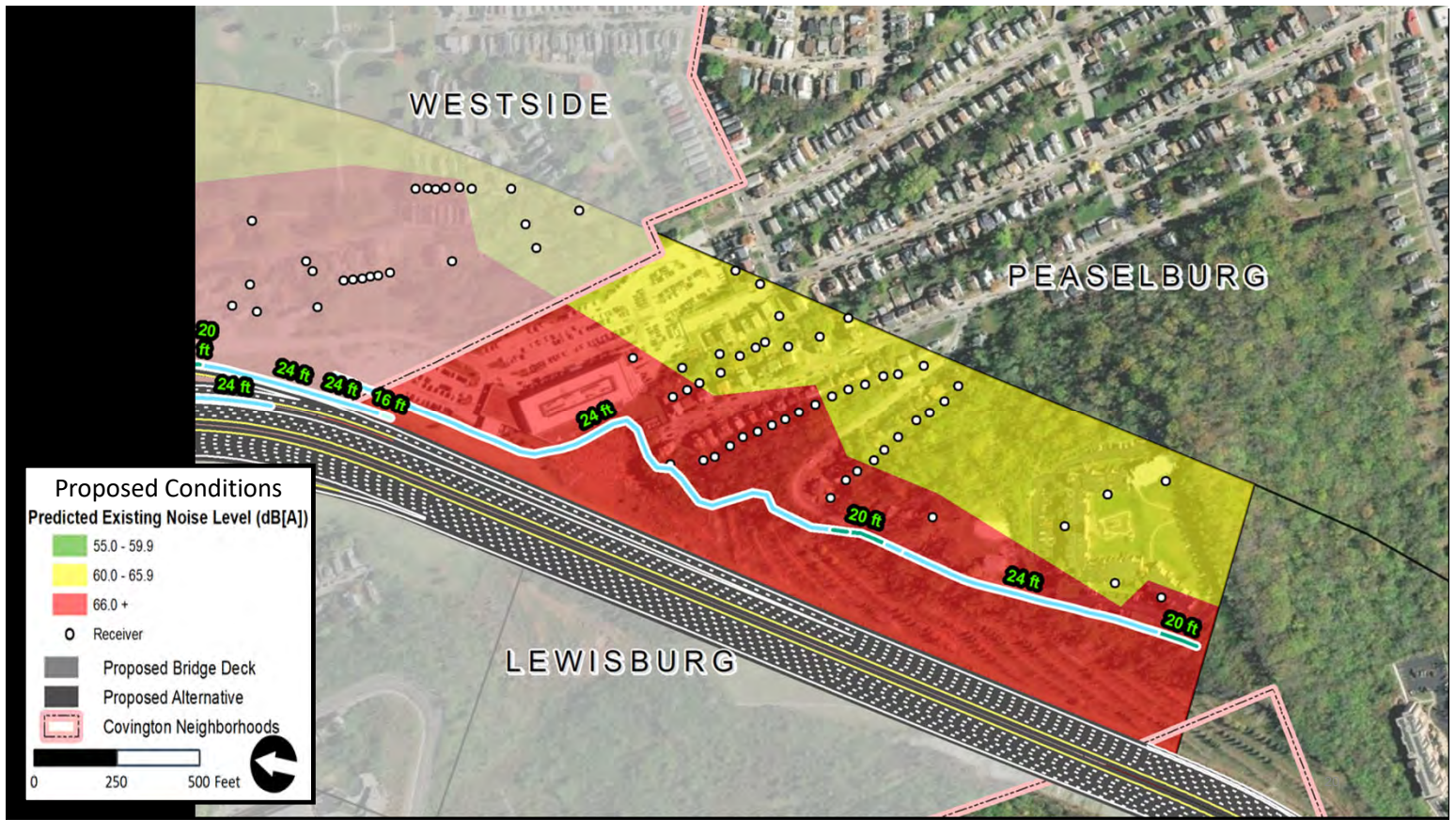
In Peaselburg, a noise wall system extending from Pike Street to Edgecliff Road was found to be reasonable and feasible (shown by the number 3). In this area, average noise levels are predicted to be 71 decibels without a noise wall and 64 decibels with a noise wall. For reference, the average exterior sound levels with noise walls in Covington are predicted to be near what you'd experience in normal conversation.



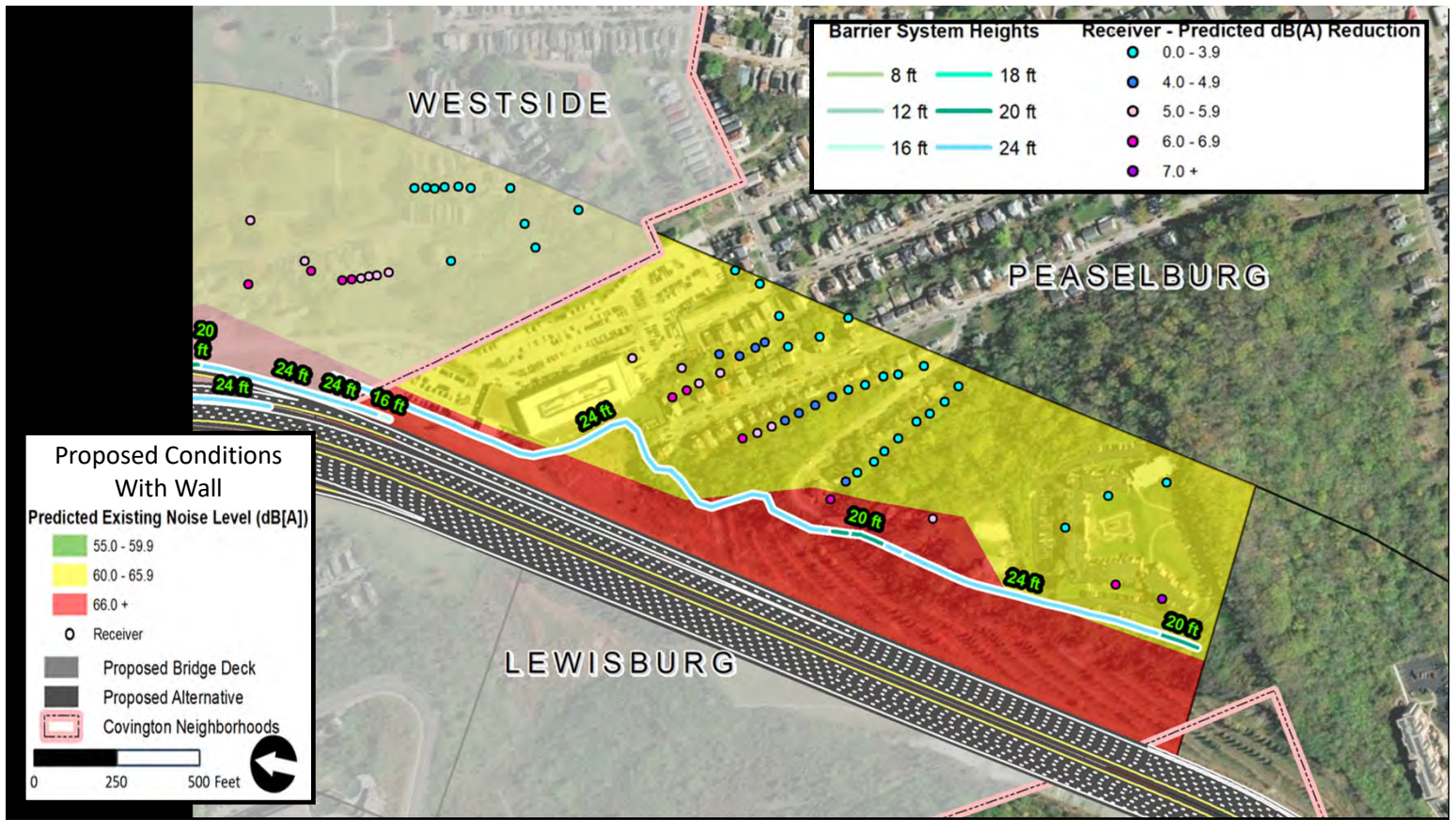
The red lines on this slide show noise walls that were evaluated as part of the noise study for northern Peaseburg. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis for northern Peaseburg. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

Noise



Pike Street Without Noise Wall (looking east)
71 dBA (average)



Pike Street With Noise Wall (looking east)
64 dBA (average)

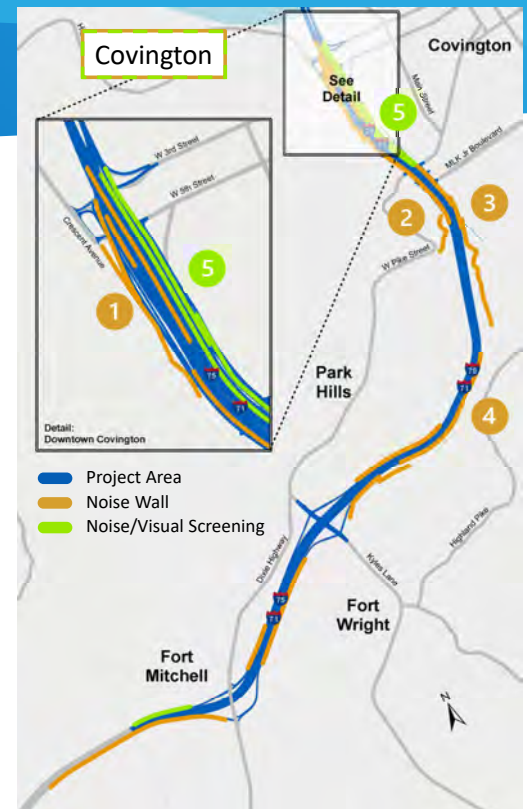
To the left is a drawing showing what the finished project might look like at Pike Street if a noise wall is not built. To the right is a drawing showing what Pike Street might look like with a noise wall. These photos are looking east on Pike Street.

Noise – Covington (KY)

Noise walls reasonable/feasible per KYTC Noise Policy

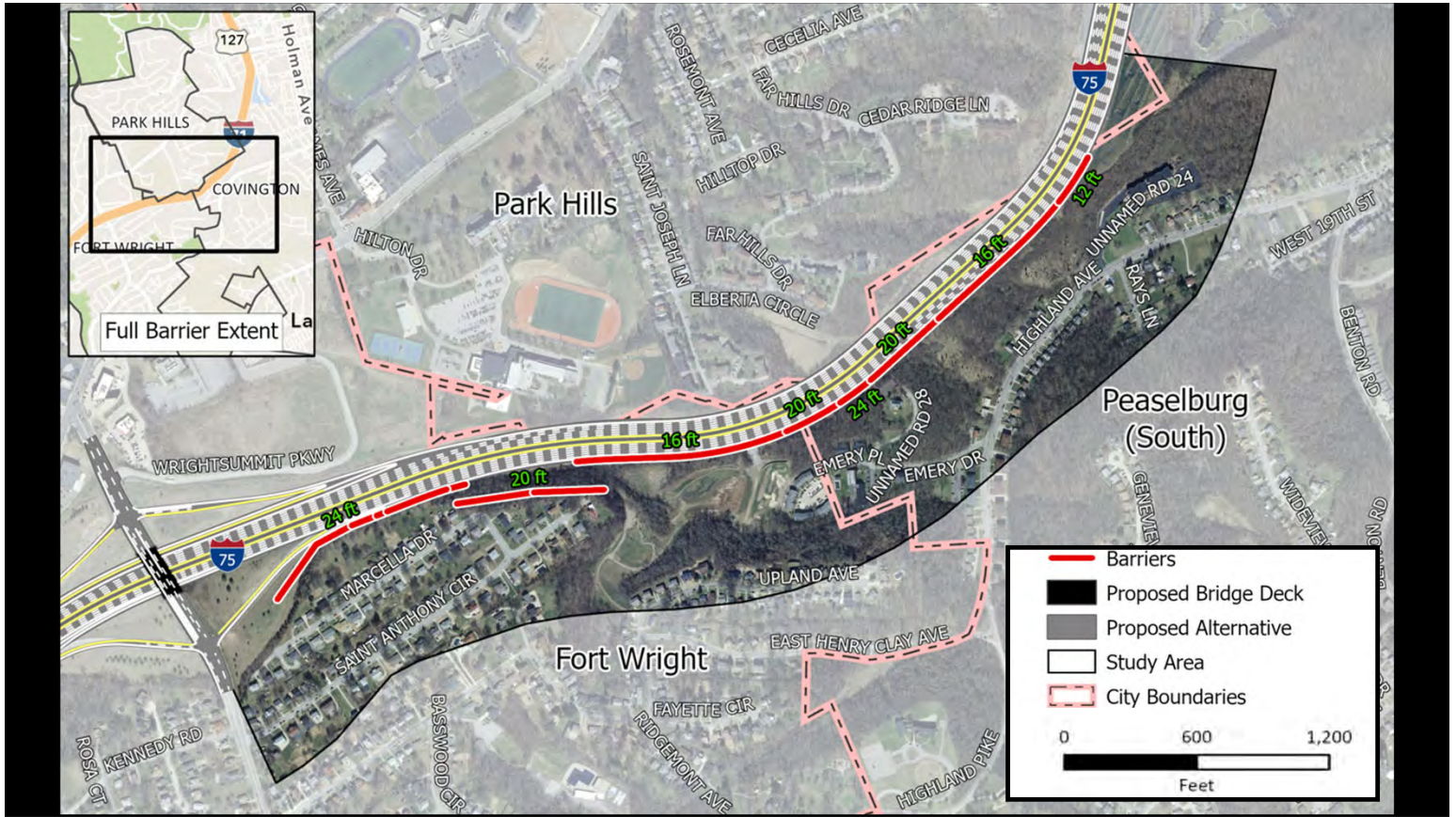
- 4 • Highland Pike to Kyles Lane (Peaselburg)
 - Without noise wall = 64 dBA
 - With noise wall = 57 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office

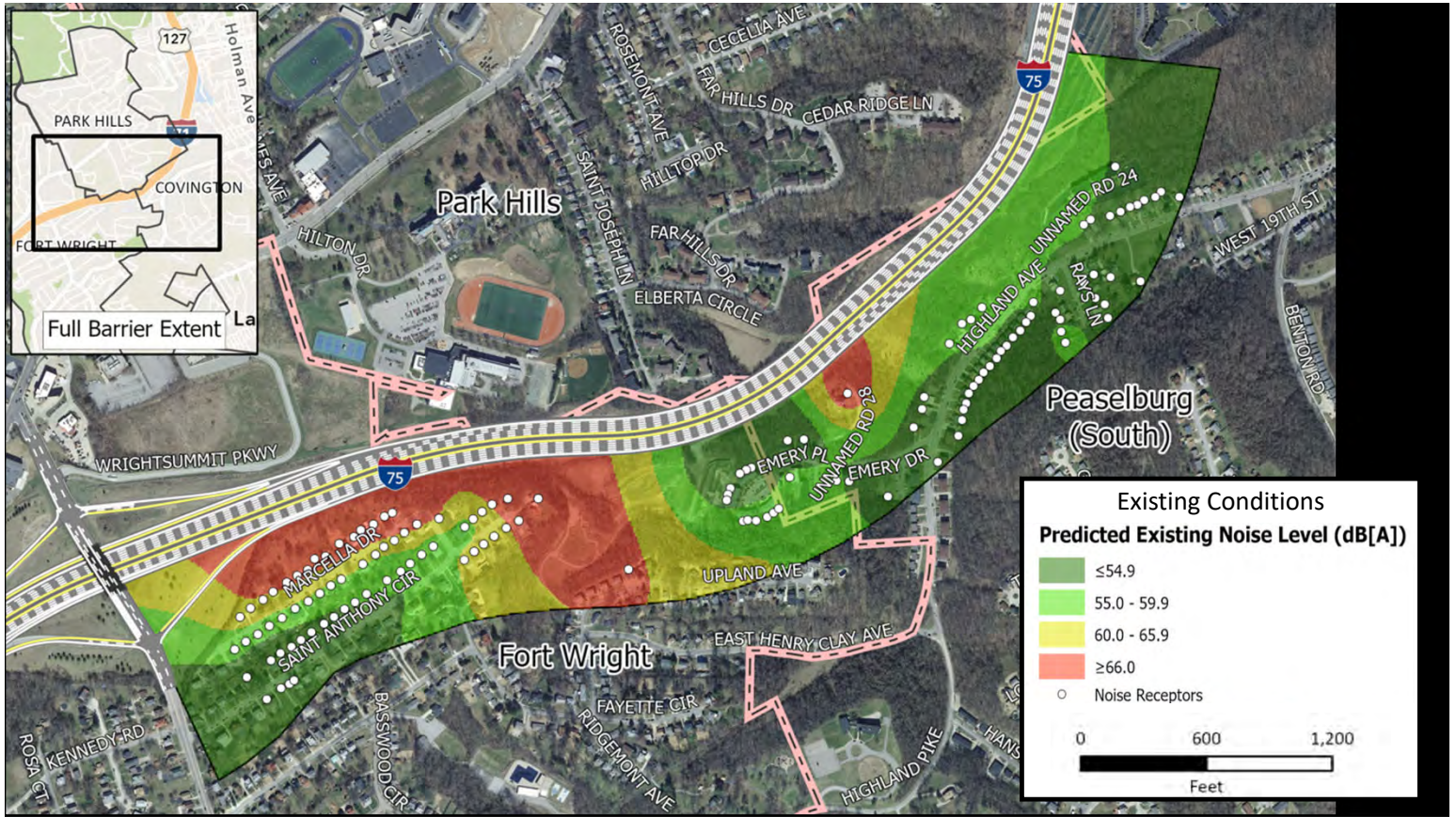


Note: Sound levels represent the average for the area benefitted by the wall in Peaselburg.

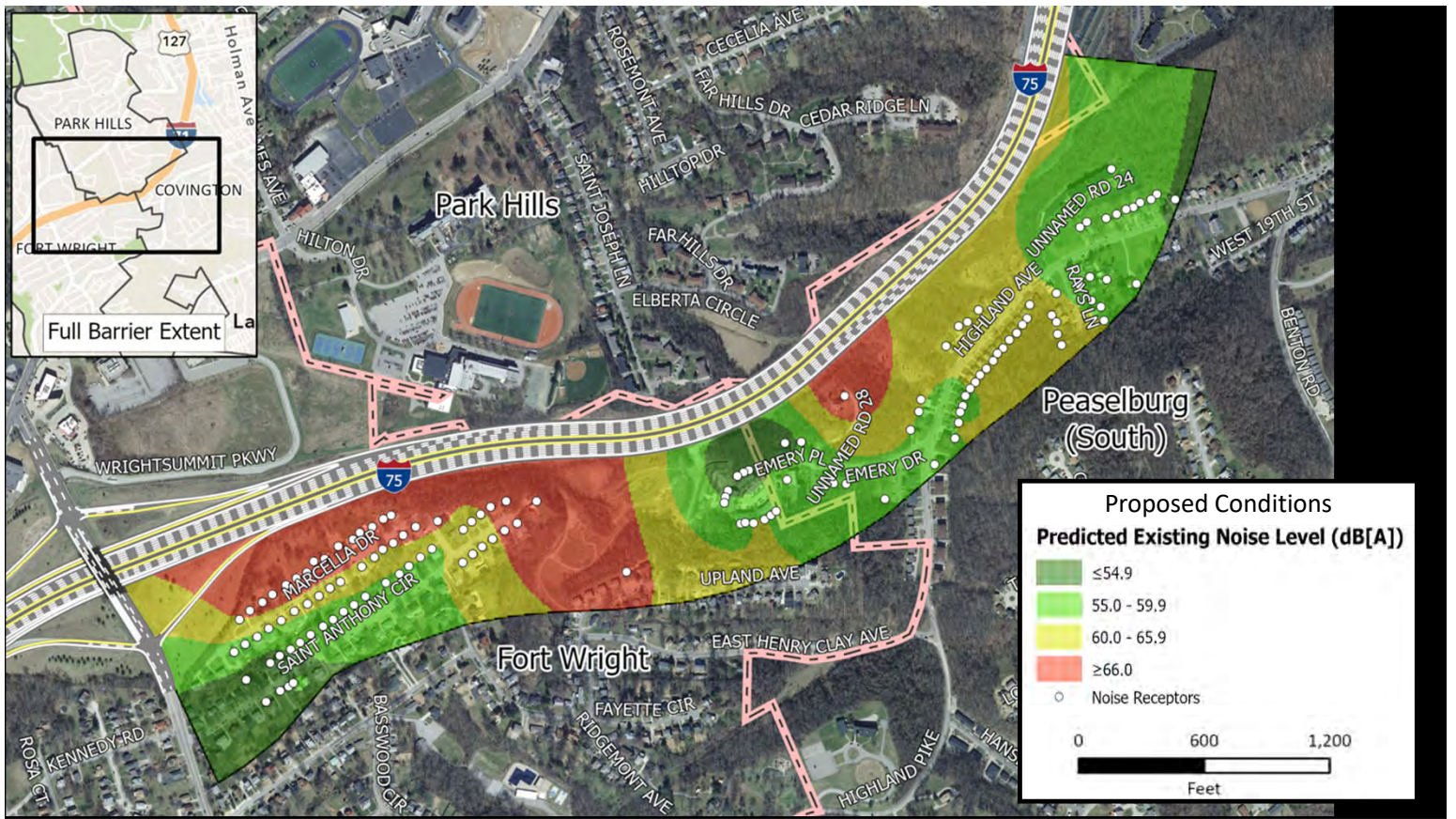
In southern Peaselburg, a noise wall system extending from Highland Pike to Kyles Lane was also found to be reasonable and feasible (shown by the number 4). In this area, average exterior noise levels are predicted to be 64 decibels without a noise wall and 57 decibels with a noise wall. The sound levels will vary depending on where each receptor is located. For reference, the average exterior sound levels with noise walls in southern Peaselburg are predicted to be near what you'd experience in normal conversation.



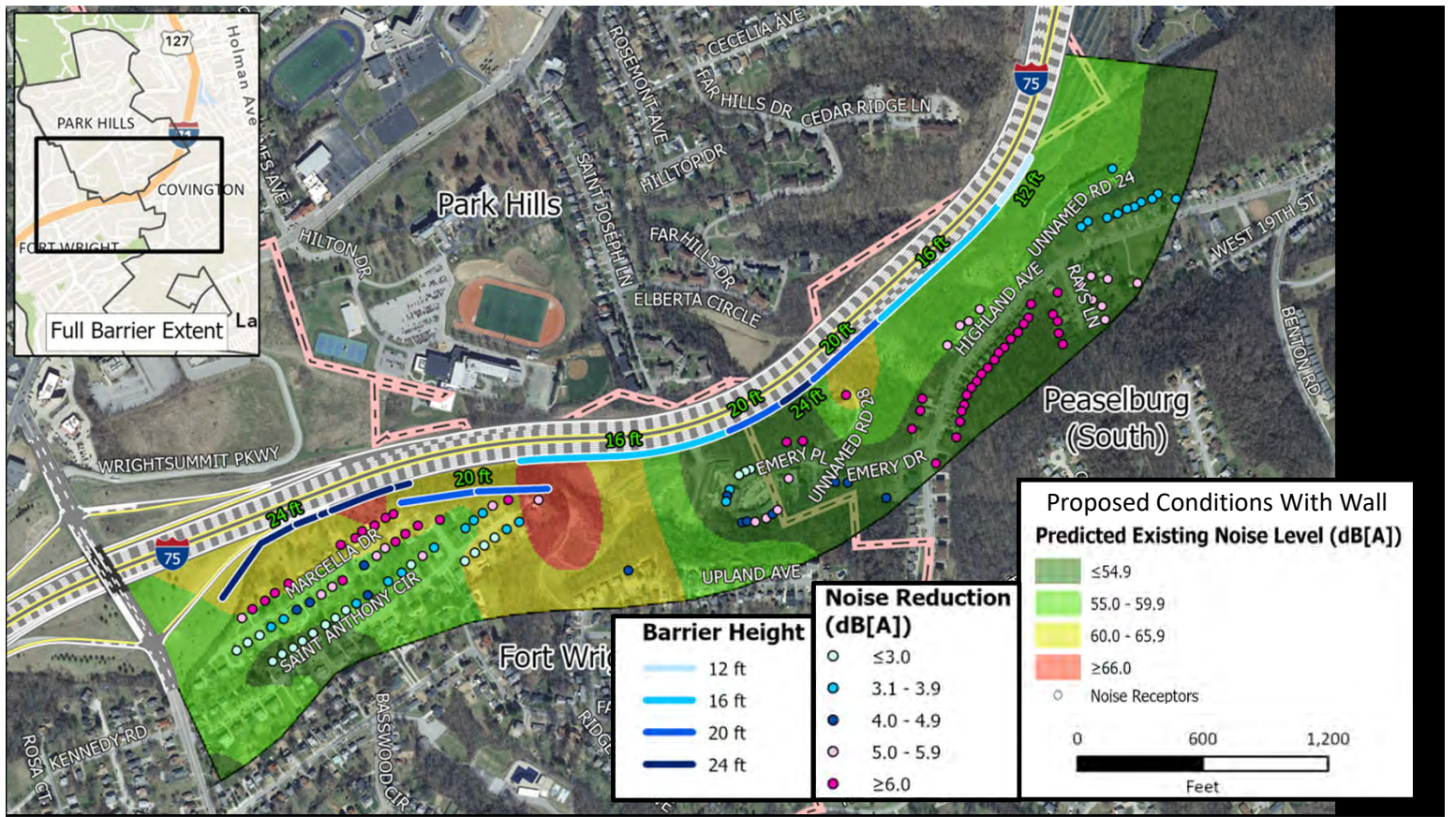
The red lines on this slide show noise walls that were evaluated as part of the noise study for northern Peasenburg and extending into Fort Wright. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.

Willow Run Watershed Drainage Improvements

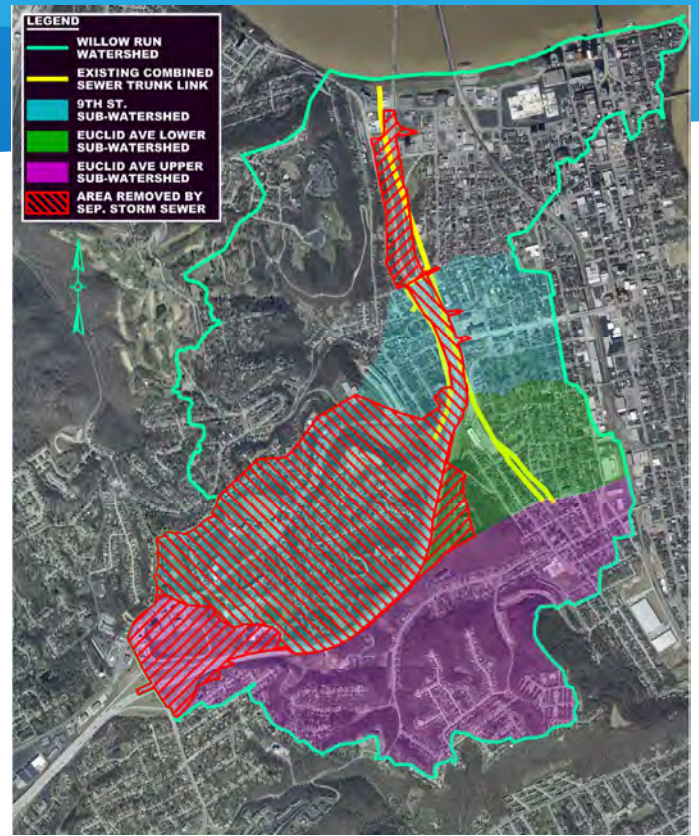
- Project team working with the City of Covington and Sanitation District No. 1
- Euclid Avenue drainage/flooding issues
- Goal
 - Construct a new interstate storm sewer system to remove stormwater runoff from the Willow Run combined sewer system
- Benefits
 - Reduce flooding problems in the Euclid Avenue area
 - Reduce combined sewer overflows



KYTC is working with the City of Covington and Sanitation District No. 1 to address drainage issue in the project area. Existing highway runoff flows in the combined sewer system in Covington, and overflow events negatively affect residents, particularly in the Euclid Avenue area. KYTC is committed to separating the highway drainage from the combined sewer systems as part of the project.

Willow Run Watershed Drainage Improvements

- New separated storm sewer system along interstate
- Removes drainage areas from the combined Willow Run sewer in the following sub-basins:
 - Euclid Avenue upper basin – 70 acres
 - Euclid Avenue lower basin – 22 acres
 - West 9th Street basin – 375 acres
- Replace portion of Willow Run trunk line with new 120-inch pipe from hospital to W. 9th Street
- Modeling of the system shows reduction in flooding events in the Euclid Avenue area



(Note: Project details will come in with click (marked by #))

As part of the Brent Spence Bridge Corridor Project, KYTC will be a new separated storm sewer system along the reconstructed interstate. We are not just stiving to separate additional runoff from the combined sewer systems, but we are working to separate all runoff in in Covington, which will substantially reduce overflow events.

The yellow line shows the existing combined storm sewer, and the teal line shows the Willow Run watershed which feeds the sewer. (#) The blue, green, and purple show the Euclid Avenue upper and lower and the West 9th Street sub-basins, which feed the Willow Run sewer. (#) When the new storm sewer system is built, it will the red hatched area from the combined Willow Run system.

In addition, portions of the Willow Run trunk line will be replaced with a larger 120-inch pipe from Saint Elizabeth Hospital to West 9th Street. Modeling of the system shows a reduction in flooding events in the Euclid Avenue area.

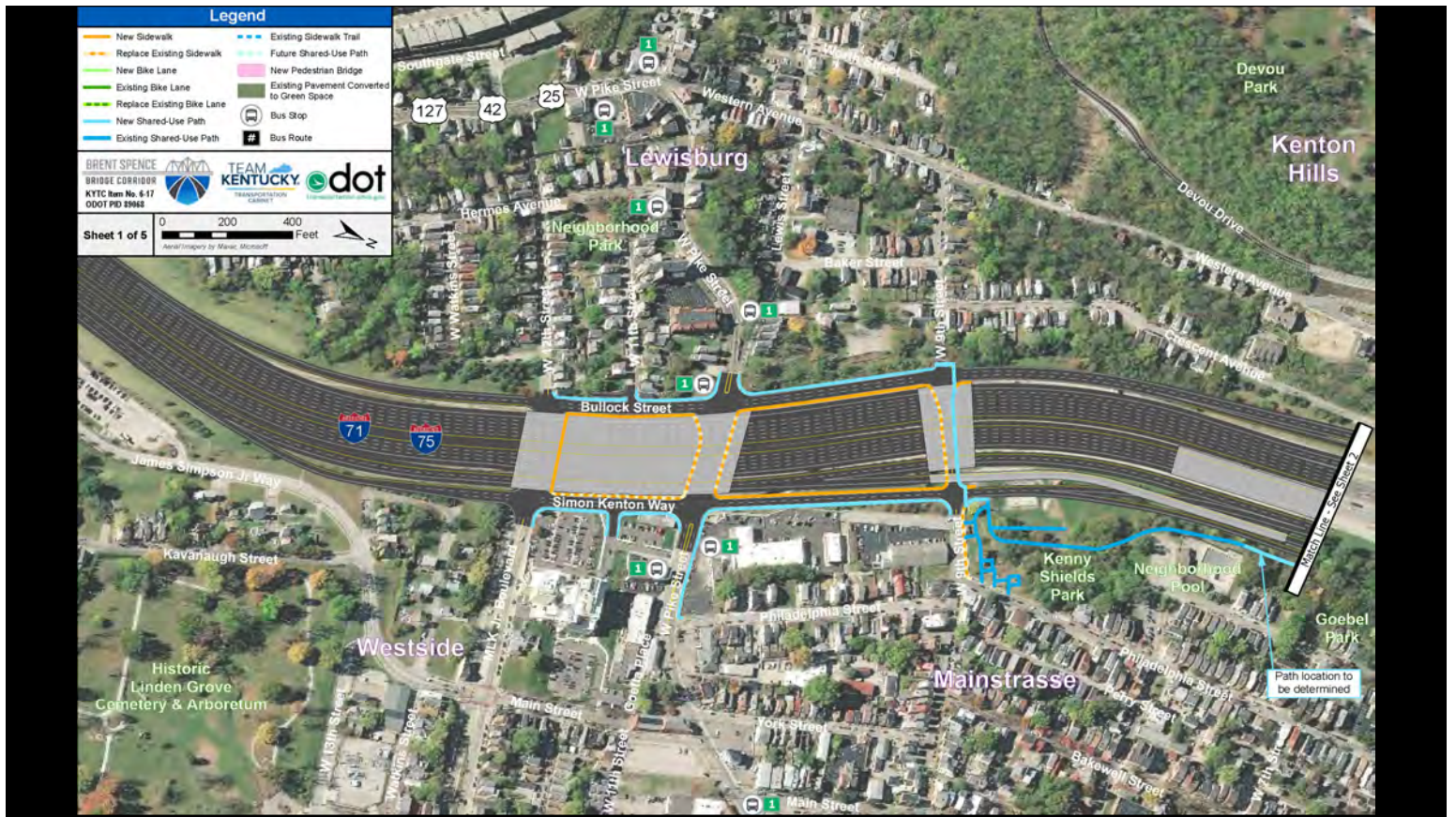
Enhancement Measures

- Working with Covington to develop aesthetic guidelines
 - Landscaping
 - Streetscapes
 - Gateways
 - Treatments for piers, abutments, retaining walls, and pedestrian paths
- Pedestrians and Bicycles
 - Connections across I-71 and I-75
 - New shared use paths
 - New and rebuilt sidewalks

31

In addition to coordinating work in Goebel Park, KYTC is also working with Covington to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.



This exhibit shows some of the planned pedestrian and bicycle facilities near Peasenburg. Note the new shared use paths and sidewalks along the frontage roads. Also notice the new and rebuilt sidewalks under the MLK/West 12th, Pike, and West 9th Street bridges. A new shared use path will be built under the West 9th Street bridge, which will tie into the shared use paths in Goebel Park.



Moving north, note the new shared use path in Goebel Park and under the West 5th Street bridge. The shared use path will be extended along Crescent Avenue to connect to the existing shared use path along the river. Also notice the new and rebuilt sidewalks along West 5th and West 3rd streets. Throughout Covington, the shared use paths and sidewalks will improve connections to existing transit stops.



This view shows how the finished project might appear when looking across I-71 and I-75 toward Peasenburg.



This view shows what the project might look like at Pike Street. Notice the potential for aesthetic treatments on the bridge walls and piers. Also notice the sidewalks and shared use paths.



THANK YOU!

For more detailed information or to provide feedback visit:
www.PublicInput.com/bsbc



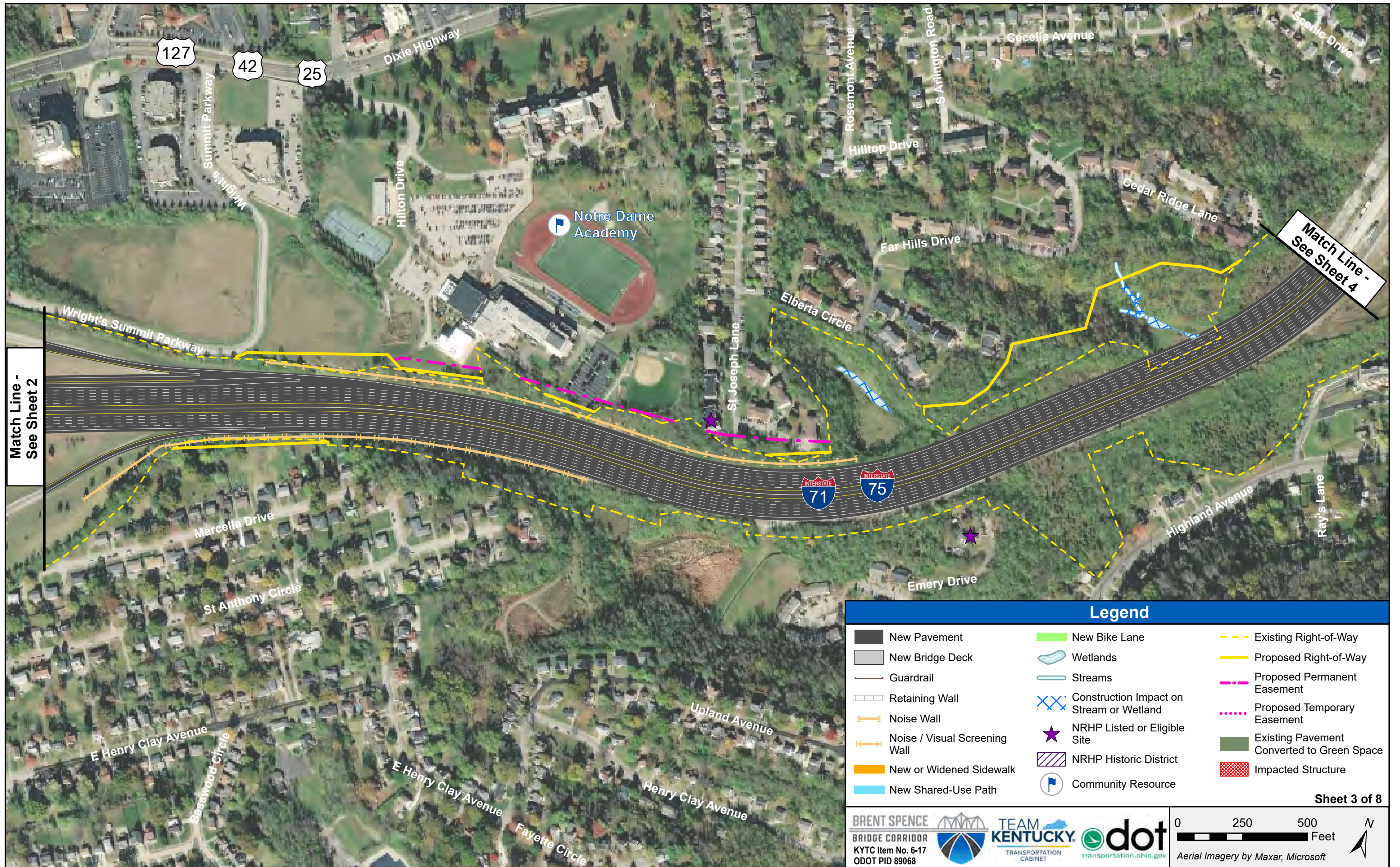
BRENTSPENCEBRIDGECORRIDOR.COM



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a PublicInput.com to review information about the project and provide your feedback.



[Attachment 5: Exhibits](#)



Match Line -
See Sheet 2

Match Line -
See Sheet 4

Legend

- | | | | | | |
|--|-------------------------------|--|--|--|--|
| | New Pavement | | New Bike Lane | | Existing Right-of-Way |
| | New Bridge Deck | | Wetlands | | Proposed Right-of-Way |
| | Guardrail | | Streams | | Proposed Permanent Easement |
| | Retaining Wall | | Construction Impact on Stream or Wetland | | Proposed Temporary Easement |
| | Noise Wall | | NRHP Listed or Eligible Site | | Existing Pavement Converted to Green Space |
| | Noise / Visual Screening Wall | | NRHP Historic District | | Impacted Structure |
| | New or Widened Sidewalk | | Community Resource | | |
| | New Shared-Use Path | | | | |

KYTC Item No. 6-17
 ODOT PID 89068

transportation.kentucky.gov
transportation.ohio.gov

0 250 500 Feet

Aerial Imagery by Maxar, Microsoft

Legend

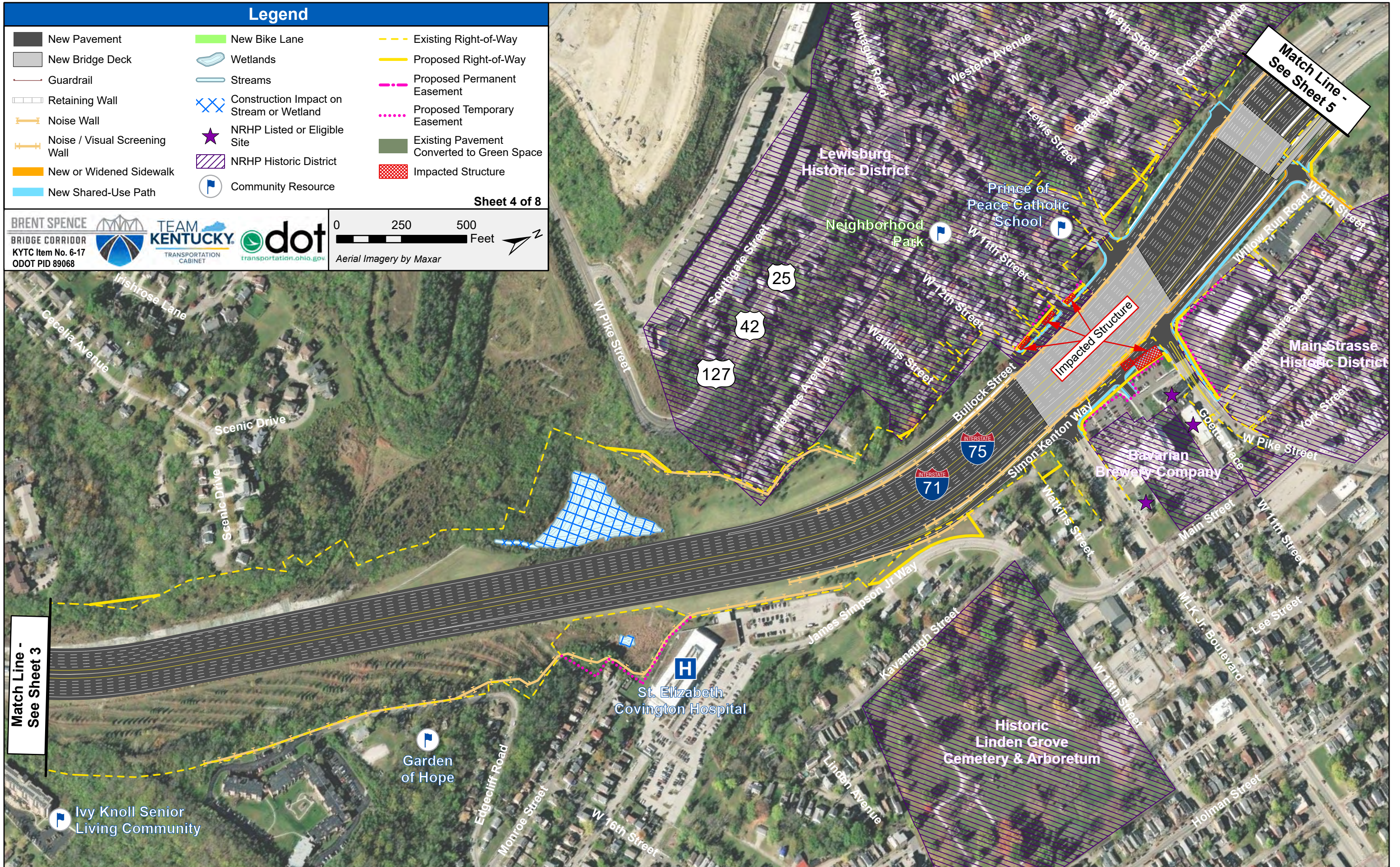
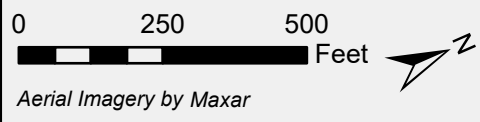
- | | | | | | |
|--|-------------------------------|--|--|--|--|
| | New Pavement | | New Bike Lane | | Existing Right-of-Way |
| | New Bridge Deck | | Wetlands | | Proposed Right-of-Way |
| | Guardrail | | Streams | | Proposed Permanent Easement |
| | Retaining Wall | | Construction Impact on Stream or Wetland | | Proposed Temporary Easement |
| | Noise Wall | | NRHP Listed or Eligible Site | | Existing Pavement Converted to Green Space |
| | Noise / Visual Screening Wall | | NRHP Historic District | | Impacted Structure |
| | New or Widened Sidewalk | | Community Resource | | |
| | New Shared-Use Path | | | | |

Sheet 4 of 8

BRENT SPENCE
BRIDGE CORRIDOR
KYTC Item No. 6-17
ODOT PID 89068

TEAM KENTUCKY
TRANSPORTATION CABINET

dot
transportation.ohio.gov





Brent Spence Bridge Corridor (Looking Northwest from Kentucky)



Brent Spence Bridge Corridor - Kentucky (Looking Southeast)





Pike Street With Noise Walls (Looking Northwest) - Kentucky



Pike Street Without Noise Wall (Looking West) - Kentucky



Pike Street With Noise Wall (Looking West) - Kentucky



Attachment 6: Comment Forms

COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: _____

Mailing address (or nearest cross streets): _____

Email address: _____ (Phone: _____)

Business/Organization Name: _____

Business/Organization Address: _____

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: _____

General Comments (Please attach additional pages, if needed):

Noise / Sound walls need to extend south beyond Pike - the current proposed noise walls that extend to Kyles Ln are much needed and wanted

**COMMENTS DUE BY
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
 - By email at Keith.Smith@dot.ohio.gov
 - By telephone at 1-800-831-2142
 - Online at PublicInput.com/bsbc
 - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: _____

Mailing address (or nearest cross streets): _____

Email address: _____ Phone: _____

Business/Organization Name: NONE

Business/Organization Address: NONE

What is your interest in the proposed project? (Select all that apply.)

- Area Resident Area business owner or employee Commuter
 Other: _____

General Comments (Please attach additional pages, if needed):

**COMMENTS DUE BY
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
 - By email at Keith.Smith@dot.ohio.gov
 - By telephone at 1-800-831-2142
 - Online at [PublicInput.com/bsbc](https://www.PublicInput.com/bsbc)
 - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: _____

Mailing address (or nearest cross streets): _____

Email address: _____ Phone: _____

Business/Organization Name: FONA

Business/Organization Address: _____

What is your interest in the proposed project? (Select all that apply.)

- Area Resident Area business owner or employee Commuter
 Other: _____

General Comments (Please attach additional pages, if needed):

Will there be people uprooted?
If so, are they getting a fair price
for their property? How long will the
project take and what about the traffic?
Will ^{there} be long delays?

**COMMENTS DUE BY
January 5, 2023**

- Comments may be submitted:
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 - By email at Keith.Smith@dot.ohio.gov
 - By telephone at 1-800-831-2142
 - Online at PublicInput.com/bsbc
 - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: _____

Mailing address (or nearest cross streets): _____

314

Email address: _____ Phone: _____

Business/Organization Name: Friends of Painesburg Neighborhood Association

Business/Organization Address: _____

What is your interest in the proposed project? (Select all that apply.)

- Area Resident Area business owner or employee Commuter
 Other: _____

General Comments (Please attach additional pages, if needed):

The updated designs are much better than the original plans. We appreciate the effort to retain so many existing homes and businesses. We also appreciate the adjacent infrastructure plans such as storm sewers and other utilities. Thanks for all the extra outreach.

**COMMENTS DUE BY
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
 - By email at Keith.Smith@dot.ohio.gov
 - By telephone at 1-800-831-2142
 - Online at PublicInput.com/bsbc
 - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: _____

Mailing address (or nearest cross streets): _____

Email address: _____ Phone: _____

Business/Organization Name: _____

Business/Organization Address: _____

What is your interest in the proposed project? (Select all that apply.)

- Area Resident Area business owner or employee Commuter
- Other: _____

General Comments (Please attach additional pages, if needed):

COMMENTS DUE BY
January 5, 2023

Comments may be submitted:

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- Online at PublicInput.com/bsbc
- By mail using address on back



COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: _____

Mailing address (or nearest cross streets): _____

Email address: _____ Phone: _____

Business/Organization Name: Friends of Peaselburg Neighborhood Assn.

Business/Organization Address: same

What is your interest in the proposed project? (Select all that apply.)

- Area Resident Area business owner or employee Commuter
 Other: Chairperson of Friends of Peaselburg NA

General Comments (Please attach additional pages, if needed):

- Great presentation + presenter grace
- Extremely valuable information & insight
- Noise walls are a must for Peaselburg residents (Thank you)
- Eliminating Willow Run combined water flow will truly assist in deterring flooding in our neighborhood.
- Timeline seems reasonable
- Keep project transparency!

**COMMENTS DUE BY
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
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 - By telephone at 1-800-831-2142
 - Online at PublicInput.com/bsbc
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KYTC Item No. 6-17 | ODOT PID 89068



DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

What is your race or ethnicity?

- Black or African American American Indian or Alaskan Native Asian Hispanic or Latino
 White Native Hawaiian or Other Pacific Islander
 I prefer to self-describe: _____

What is the primary language spoken in your home?

- English Spanish Other (Please specify): _____

Was project information translated into other languages appropriately?

- Yes No Not Applicable

How many people live in your household?

- 1 - 2 3 - 5 6 +

What are the age ranges of those living in your household? (Check all that apply.)

- Under 18 19 - 44 45 - 64 65 +

What is your annual household income?

- Less than \$10,000 \$10,000 - \$24,999 \$25,000 - \$49,999 \$50,000 - \$74,999
 \$75,000 - \$99,999 \$100,000 - \$149,999 \$150,000 +

What is the highest level of education completed by members of your household?

- No Elementary school Middle school High school College/university
 Other: _____

Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Continue meetings throughout our neighborhoods

Thank you for helping ODOT and
KYTC improve their public
involvement practices!

Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov
Mail to: Ohio Department of
Transportation, District 8
ATTN: Keith Smith, P.E.
505 South State Route 741
Lebanon, OH 45036-9518

DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: _____

What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): _____

Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: _____

Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Thank you for helping ODOT and KYTC improve their public involvement practices!

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Email to: Keith.Smith@dot.ohio.gov
Mail to: Ohio Department of Transportation, District 8
ATTN: Keith Smith, P.E.
505 South State Route 741
Lebanon, OH 45036-9518



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What is your race or ethnicity?

- Black or African American American Indian or Alaskan Native Asian Hispanic or Latino
 White Native Hawaiian or Other Pacific Islander
 I prefer to self-describe: _____

What is the primary language spoken in your home?

- English Spanish Other (Please specify): _____

Was project information translated into other languages appropriately?

- Yes No Not Applicable

How many people live in your household?

- 1 - 2 3 - 5 6 +

What are the age ranges of those living in your household? (Check all that apply.)

- Under 18 19 - 44 45 - 64 65 +

What is your annual household income?

- Less than \$10,000 \$10,000 - \$24,999 \$25,000 - \$49,999 \$50,000 - \$74,999
 \$75,000 - \$99,999 \$100,000 - \$149,999 \$150,000 +

What is the highest level of education completed by members of your household?

- No Elementary school Middle school High school College/university
 Other: _____

Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Thank you for helping ODOT and
KYTC improve their public
involvement practices!

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Email to: Keith.Smith@dot.ohio.gov
Mail to: Ohio Department of
Transportation, District 8
ATTN: Keith Smith, P.E.
505 South State Route 741
Lebanon, OH 45036-9518

DEMOGRAPHICS FORM



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What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: _____

What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): _____

Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: _____

Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

I think you're already going above and beyond
on this project.

Thank you for helping ODOT and KYTC improve their public involvement practices!

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