

Brent Spence Bridge Corridor Project Westside Covington Neighborhood Meeting Summary November 29, 2022

Introduction

The Westside Covington Neighborhood Meeting was held on November 29, 2022 from 6:30 pm to 7:40 pm. The meeting was held at the Center for Great Neighborhoods located at 321 West 12th Street in Covington, Kentucky. It was publicized through the West Covington Facebook page and an announcement was posted at the meeting location (see Attachment 1). Attendees at the meeting included the Covington police, the project team, and members of the public. In total, 20 people attended the meeting, including 7 individuals from the project team (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). During the presentation, the project team responded to questions and comments offered by those present.

Exhibits showing noise wall renderings, lane configurations, and a rendering of Goebel Park were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](https://www.PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Six (6) comment forms and nine (9) demographic surveys were returned at the meeting.

Demographics

The following is a breakdown of the responses received from the demographic survey.





A write in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. Comments included:

- Keep involving westside residents including residents on planning committees for projects.

Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Clarification / understanding of local versus through traffic and ramp connections.
- Concern over relocations / property impacts.
- Clarification / understanding of the process for noise evaluation and discussion of impacts with / without noise wall.
- Clarification of timeline and staging of construction.
- Information of cost / funding.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website (www.brentspencebridgecorridor.com) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.

Comments noted on the survey forms returned at the meeting include:

- Sound barrier essential.
- I personally like the noise wall.
- Great presentation – thanks for this opportunity!

Table 1: Q & A from Session

Question	Project Team Response
On the south side of the bridge, will all construction be on the west?	Not completely. There will be some construction on the east side to build the local network.
Will most of buildings that will be affected be on the west side?	The majority of the relocations will be on the west side, although one business is on east side.
Will park lands will be lost?	Some land from Goebel Park will be used, but KYTC will return some area to the park by reconfiguring the West 5 th Street exit ramp.
Will the Texas turnaround be part of this project?	The 4 th Street ramp that is going to be restricted to emergency vehicle access only will be reopened for this project. The Texas turnaround will be eliminated once the project is built, but it will be helpful for maintenance of traffic.



Question	Project Team Response
Will there be emergency lanes on the existing (local) bridge?	There will be 8-foot shoulders.
If you are going north and want to go to I-71, do you have to take the local bridge?	I-71 can be accessed from both the companion (interstate) and the existing (local traffic) bridges. The only decision on the Ohio side of the companion bridge will be whether to stay on I-75 or to take I-71.
Is existing 5 th Street still going to go under the interstate?	Yes.
Once it gets into Covington will it be a double decker?	Both the companion and existing bridges will be double-decker structures. In Covington, the southbound lanes from the existing bridge will weave between the lanes for the upper and lower decks of the companion bridge.
What will be the impact in 20-30 years? Does reducing the project footprint now have ramifications 20-30 years down the road?	Most of the reductions in the project footprint are due to the reconfiguration of lanes on the companion bridge and reductions on the local street network. Traffic analyses consider traffic operations 20 years into the future.
If you end up in interstate / express lanes where is your next decision point going southbound once you've crossed the bridge?	Kyle's Lane will be the next opportunity to exit going southbound in the interstate lanes.
Has there been an economic study to determine if there will be negative impacts on businesses with the express lanes?	The concept of express / local lanes is not unique and is in use throughout the country. KYTC has already developed conceptual signing plans to help address concerns about wayfinding and access to local businesses.
Some communities want the wall and some don't? How does that work?	Those that benefit from the wall will be given more say in whether or not walls will be built.
Will there be walls on the other side of the interstate? Will noise reflect off the walls and make it louder on the other side?	These conditions are considered in the noise model.
What do you mean by enhanced sidewalks?	West 9 th Street was used as an example. Today, the walks are narrow, and it's dark under the bridge. The walks can be made wider to accommodate multi-use and there are opportunities for lighting under bridges and improved crossings.
Would the vegetative median be a rain garden?	Treatments for medians have not been specifically decided yet. The aesthetics subcommittee is exploring options.



Question	Project Team Response
Is there a projected start date?	KYTC and ODOT have applied for Megaproject and Bridges Grants established by the Bipartisan infrastructure law. The announcement of grant recipients will likely occur by the end of year. Some construction could begin in late 2023, but heavy construction will not begin until 2024.
Is the project being funded with City or Federal dollars?	The project has a significant price tag. Federal funding is required. Neither state can fund the project independently.
What is estimated cost?	The current cost estimate is over \$3.2 billion, but prices can / will fluctuate due to inflation. This is one of the reasons the project team decided to use progressive design / build.
What is the bridge type? Cable stay rendering? Will this look like the arch bridge?	Do not have current renderings of both bridge types. Renderings from 2012 are available on PublicInput.com/bsbc .



Attachment 1: Advertising Materials

Westside Facebook Page

Facebook.com



Westside Covington Neighborhood



Reyna Larie

November 5 at 9:52 AM · 🌐



This event is intended for any westside resident..

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of neighborhood outreach meetings for the Brent Spence Bridge (BSB) Corridor Project

Purpose: To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team ... [See more](#)



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4 Shares

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Submit your first comment...





Attachment 2: Sign-In Sheet

BRENT SPENCE BRIDGE CORRIDOR
TEAM KENTUCKY

BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – November 29, 2022

Name	Representing	Address
1. <i>David Arnold</i>	<i>Westside</i>	
2. <i>Aaron Winters</i>		
3. <i>Ryan Smith</i>	<i>Westside</i>	
4. <i>Bill Wells</i>	<i>CCC</i>	
5. <i>Michael Campbell</i>	<i>Westside</i>	
6. <i>Heather Butler</i>	<i>Westside</i>	
7. <i>Susan Allen</i>	<i>Neighborhood</i>	
8. <i>Lindsay Allen</i>		
9. <i>Dave Gordon</i>	<i>Westside</i>	
10. <i>Mike Nesbitt</i>	<i>Westside</i>	
11. <i>Mike Spivey</i>	<i>Westside</i>	
12. <i>Mike Spivey</i>	<i>Westside</i>	
13. <i>Sam Fekert</i>	<i>West Side</i>	
14. <i>Rechal White</i>	<i>Covington Police</i>	
15. <i>Miss Burrell</i>	<i>Covington Police</i>	
16. <i>Heather Patten</i>		
17. <i>John Hill</i>	<i>HUB</i>	
18. <i>Steve Hays</i>	<i>KYC</i>	
19. <i>Greg Vahlsing</i>	<i>KYC</i>	
20. <i>Lindsay Walker</i>	<i>HUB</i>	

Please print clearly. Contact information is optional but must be included if you wish to receive updates.
KYTC Item No. 6-17 (08/17) (08/17) (08/17)



Attachment 3: Photographs





[Attachment 4: Presentation](#)

BRENT SPENCE
BRIDGE CORRIDOR



Westside Covington Neighborhood Meeting (KY)

November 29, 2022



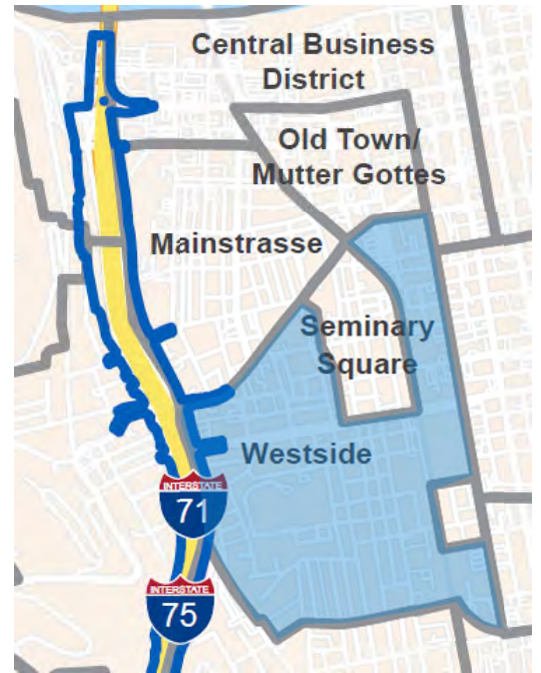
INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.
brentspencebridgecorridor.com



The meeting will open with introductions of the Project Team in attendance (2 min).

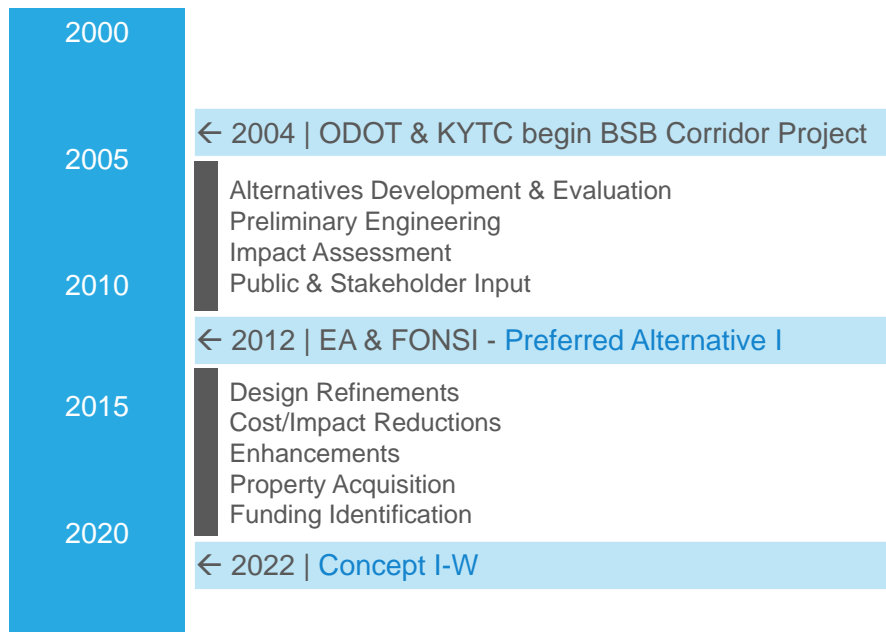
Welcome

- Meeting purpose
 - Share updates on the Brent Spence Bridge (BSB) Corridor Project
 - Offer residents in the Westside Covington neighborhood the opportunity to share feedback with the Project Team
- Agenda
 - General project overview
 - Project specifics in the Westside area
 - Discussion/feedback from Westside residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Westside Covington neighborhood. Most importantly, tonight we're focusing on hearing from the residents of Westside. We're here to answer your questions and to listen to your feedback about the project.

Project History



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For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, KYTC has begun purchasing the land needed to build the project.

Project Description

Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

Project Description

Brent Spence Bridge

- New double-decker companion bridge
 - 5 lanes each deck
 - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
 - Three lanes each deck
 - Increased inside/outside shoulders
 - Carry local traffic



Note: Bridge details will come in with click (marked by #).

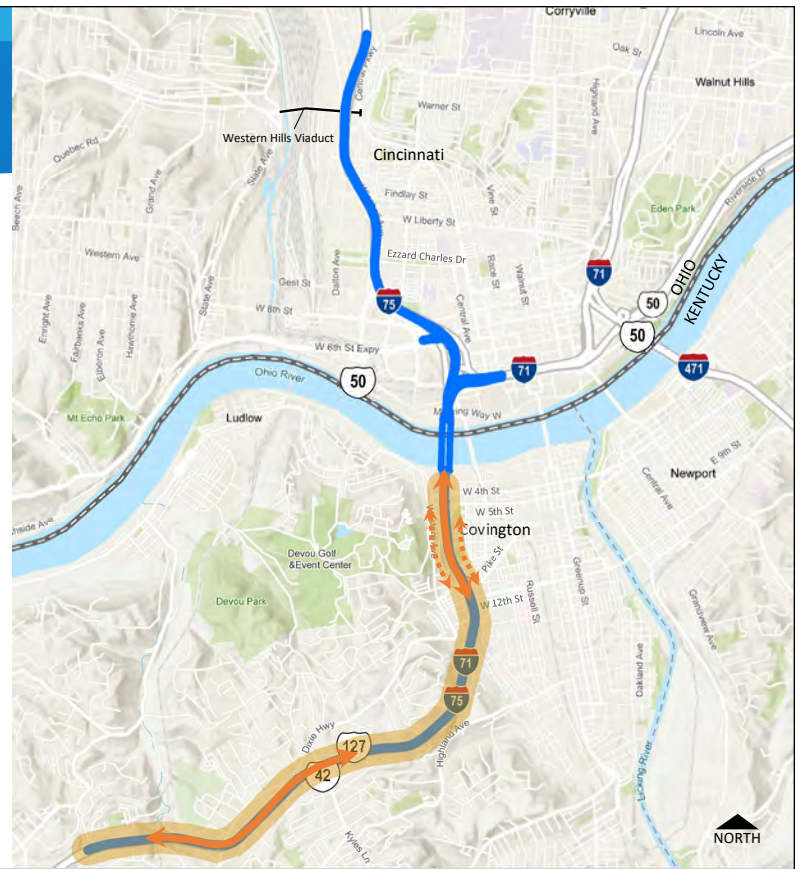
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

Project Description

Kentucky

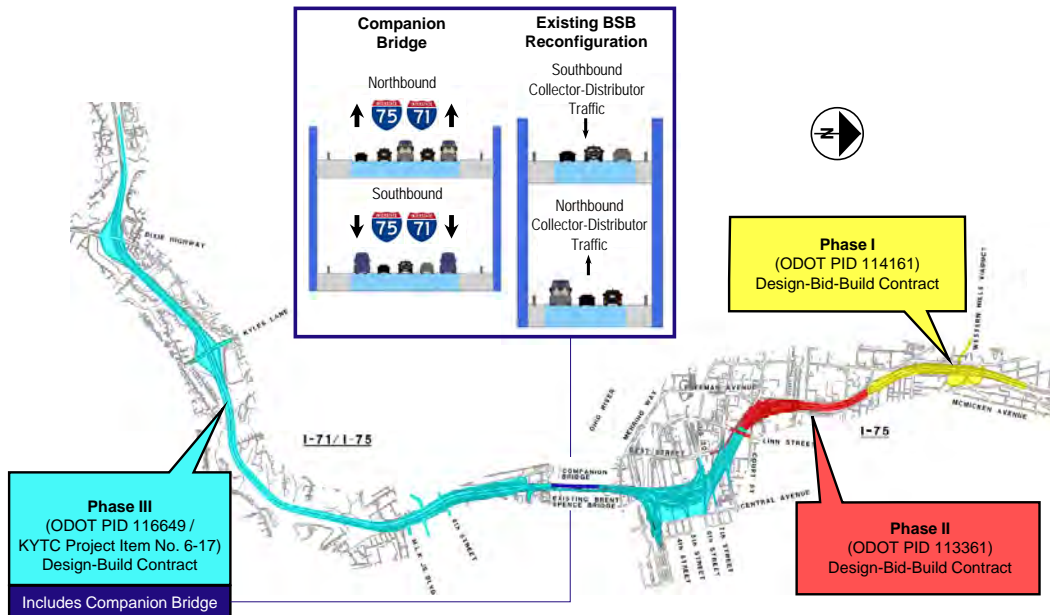
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

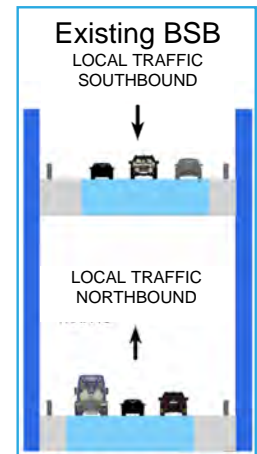
Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

What Has Changed?

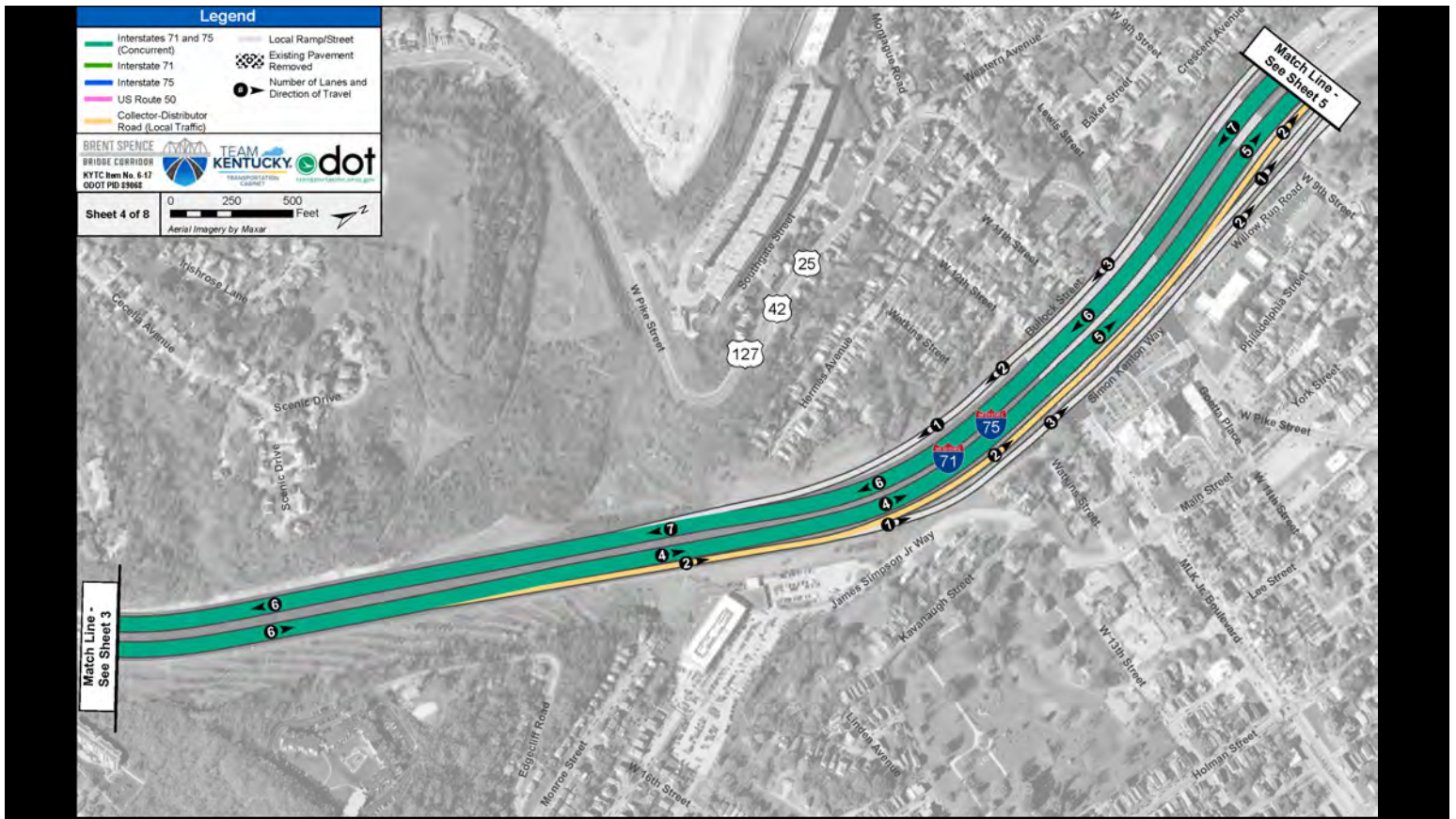
- Reconfigured how traffic travels across the Ohio River
 - Companion bridge carries through (interstate) traffic
 - Existing bridge carries local traffic
 - All northbound and southbound traffic on one deck
 - Width of companion bridge substantially reduced



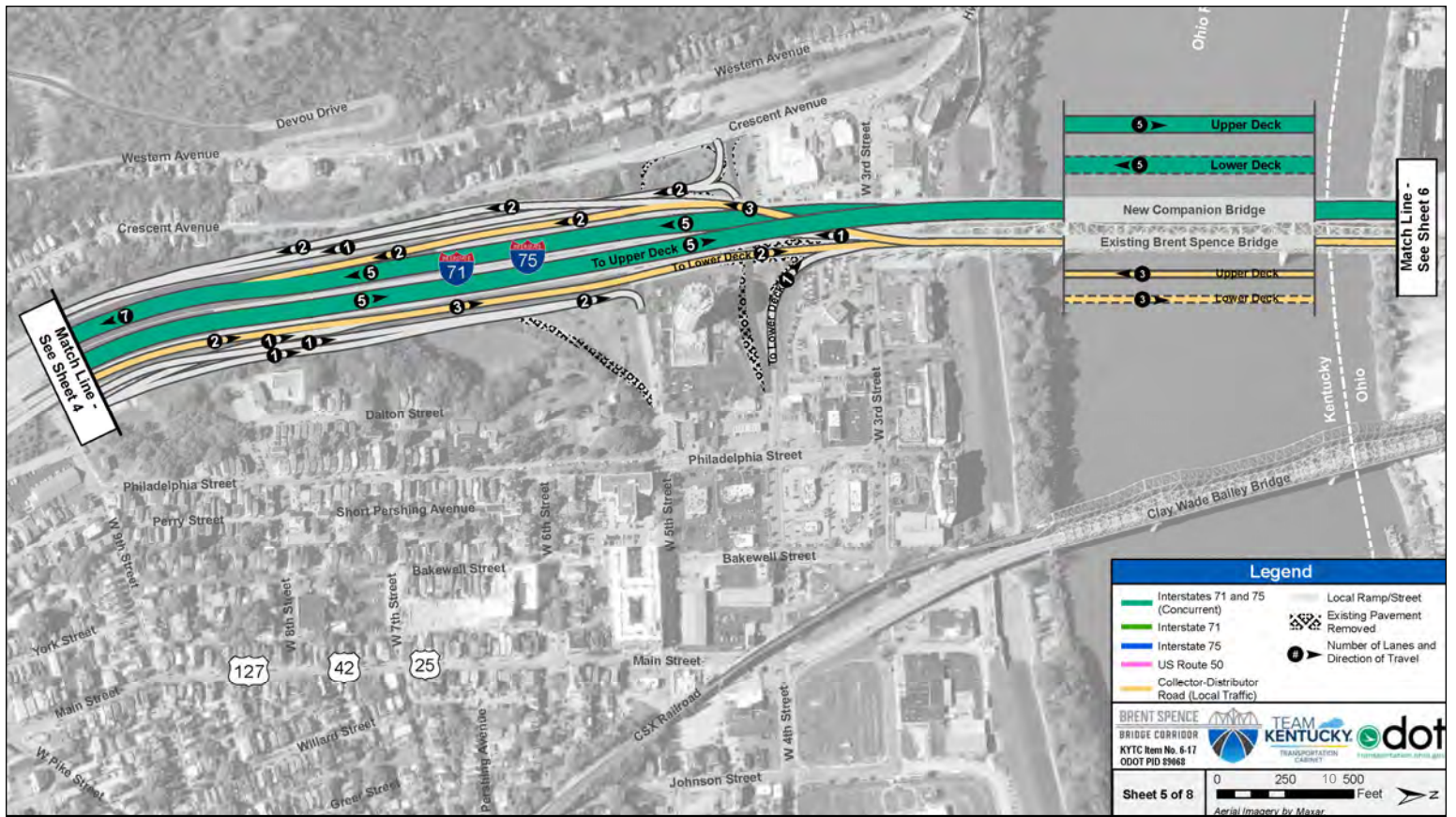
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As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

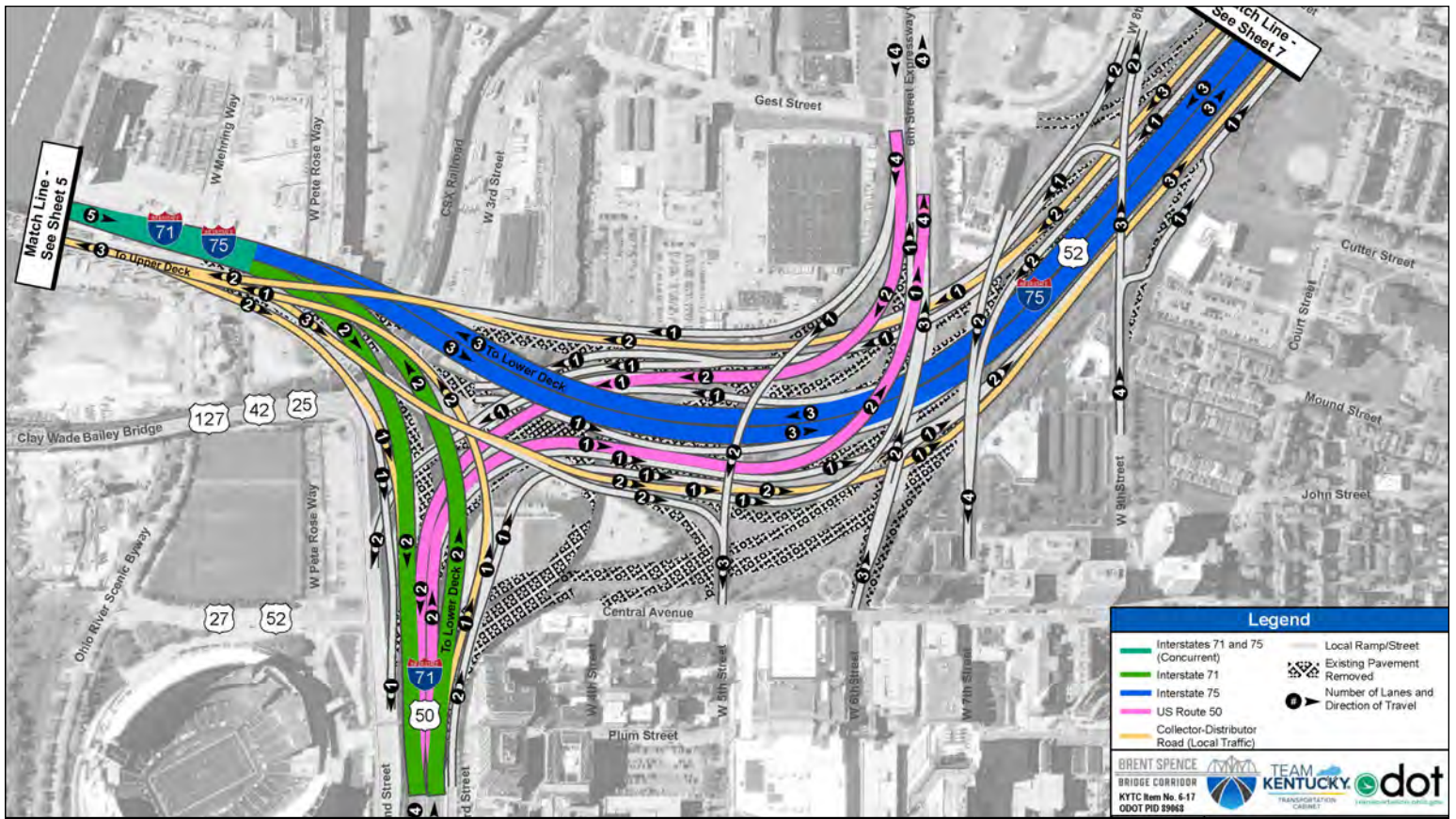
Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



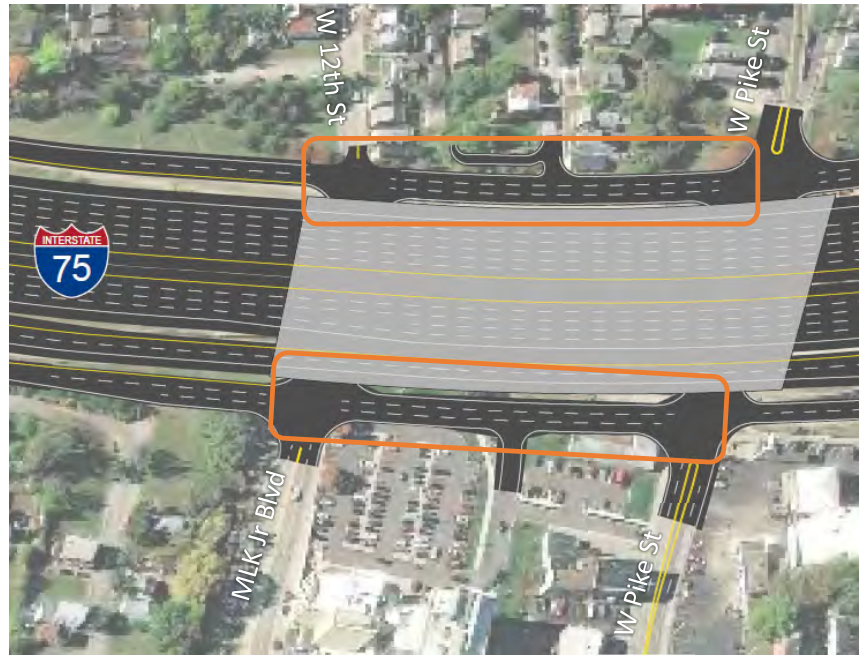
This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.



Here's another view from Covington north of Westside.

What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
 - Residential | 40+ then vs. 4 now
 - Commercial | 6 then vs. 5 now



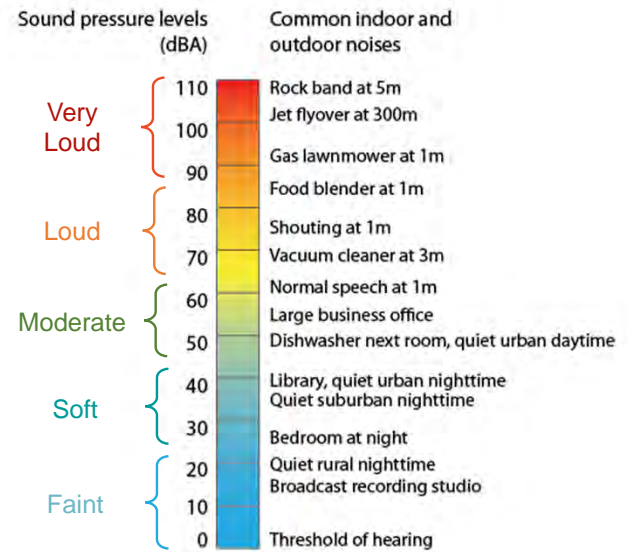
Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
 - Change in 1 dBA = barely perceptible
 - Change in 10 dBA = doubling or halving of sound



Source: Minnesota Pollution Control Agency

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Now let's dig into how the project will impact Westside Covington, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

Noise

- KYTC Noise Policy
- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
 - Are areas of frequent human exterior use present?
 - Does the noise level exceed certain established thresholds?
 - Does the noise level substantially increase?
 - Can a barrier effectively block noise?
 - Does a barrier provide enough noise reduction to justify cost?
 - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.

Kentucky Transportation Cabinet
Department of Highways
Division of Environmental Analysis



NOISE ANALYSIS AND ABATEMENT POLICY

APPROVED BY:

Todd Jeter, Division Administrator
Federal Highway Administration

Date

James Gray, Secretary
Kentucky Transportation Cabinet

Date

Revised August 1, 2022
EFFECTIVE DATE JULY 1, 2020
Revised July 1, 2015
Original July 2011

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KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dBA, while the threshold for a house receptor is 67 dBA. We also determine if there is a substantial increase – specifically a 10 dBA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dBA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

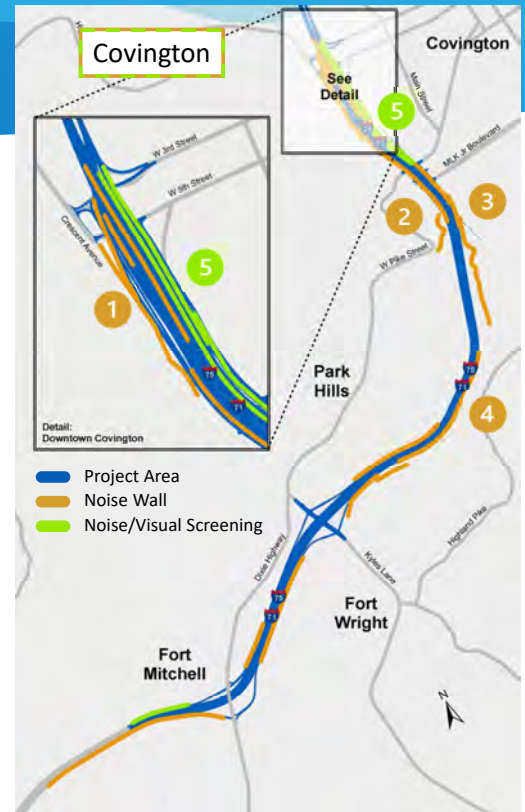
If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

Noise

Noise walls reasonable/feasible per KYTC Noise Policy

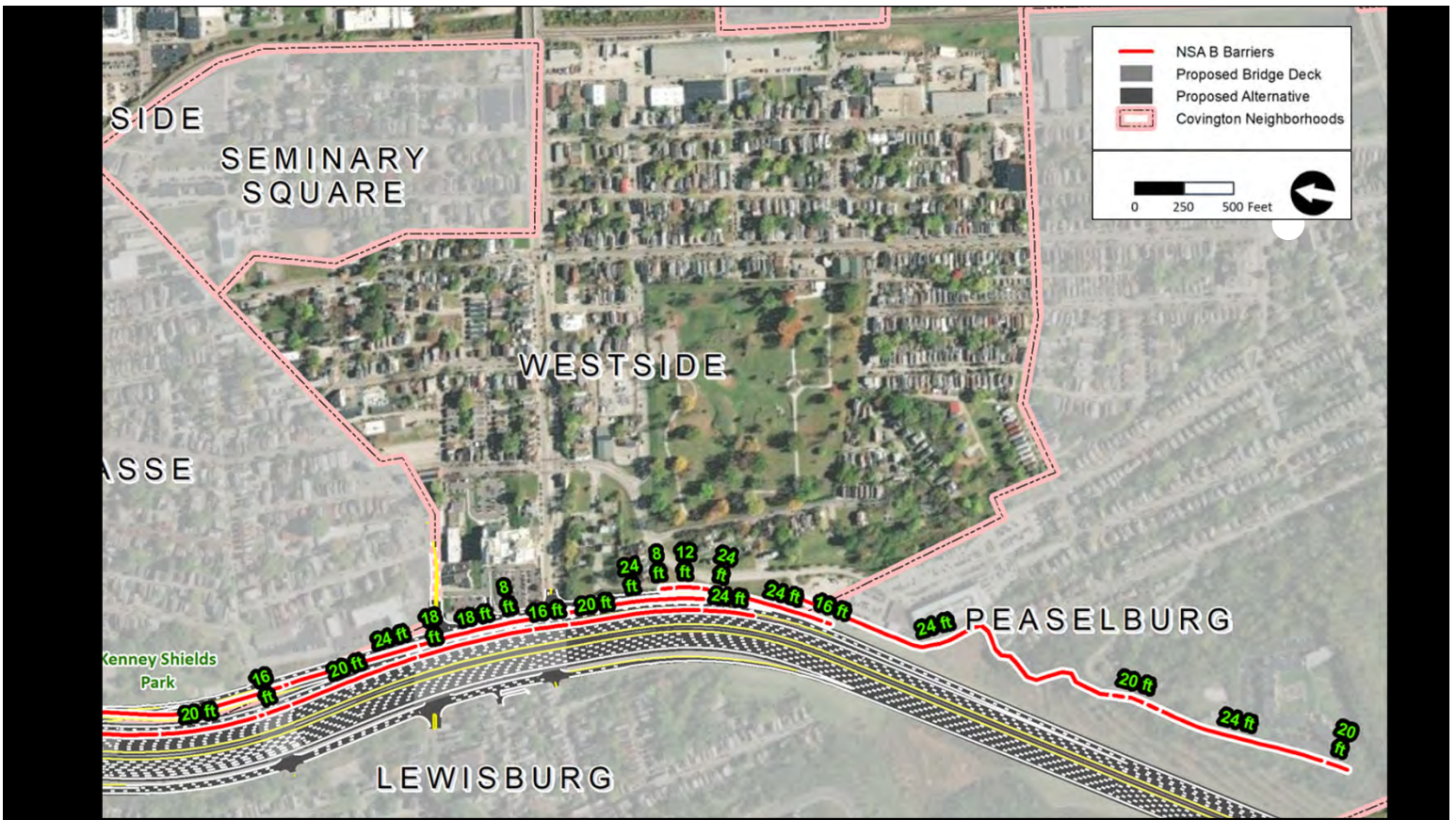
- 3 • Pike St to Edgecliff Rd (Westside/Peaselburg)
 - Without noise wall = 71 dBA
 - With noise wall = 64 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office

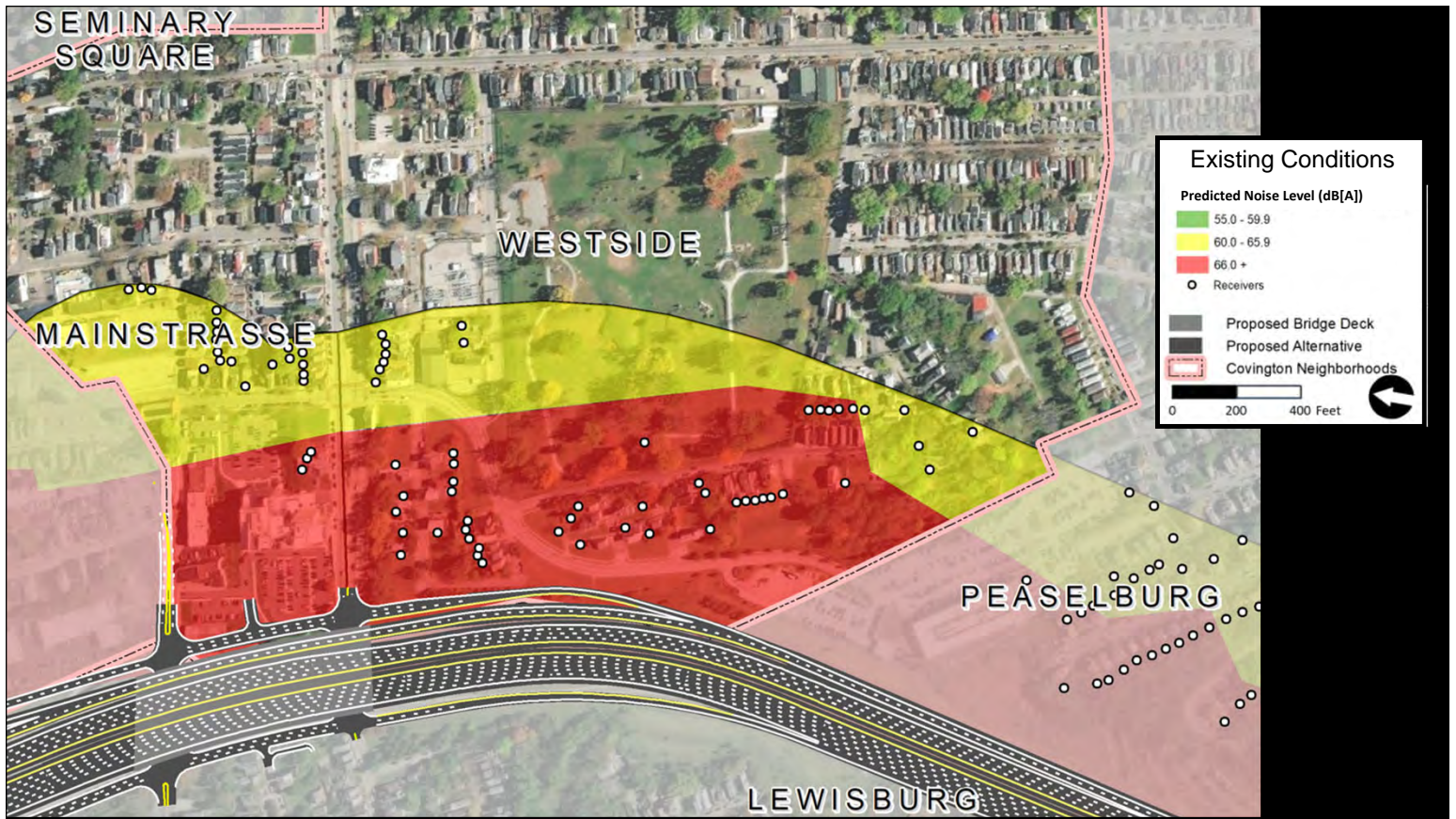


Note: Sound levels represent the average for the area benefitted by the wall.

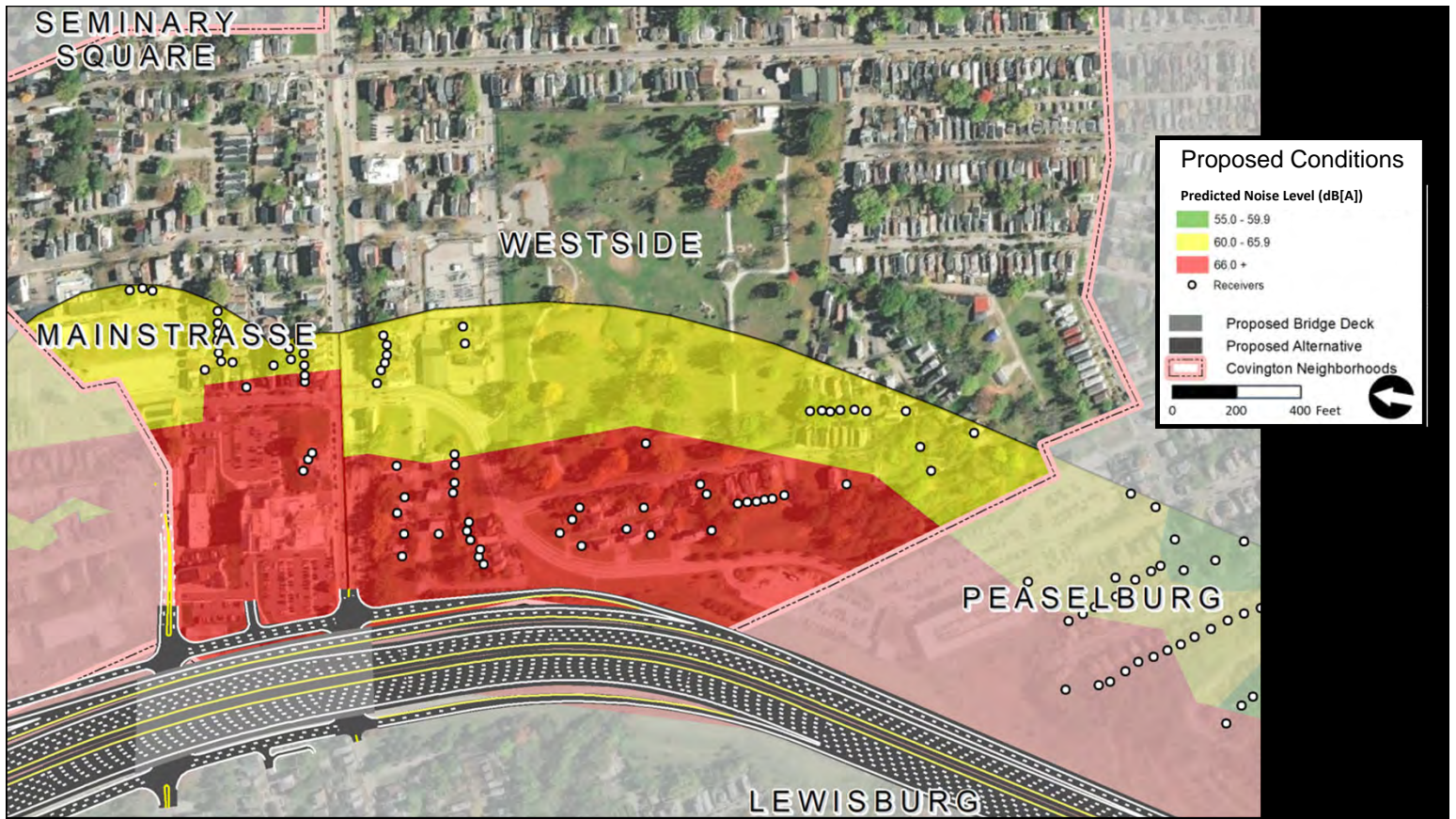
In Westside Covington, a noise wall system extending from Pike Street to Edgecliff Road was found to be reasonable and feasible (shown by the number 3). In this area, average noise levels are predicted to be 71 decibels without a noise wall and 64 decibels with a noise wall. For reference, the average exterior sound levels with noise walls in Westside are predicted to be near what you'd experience in normal conversation.



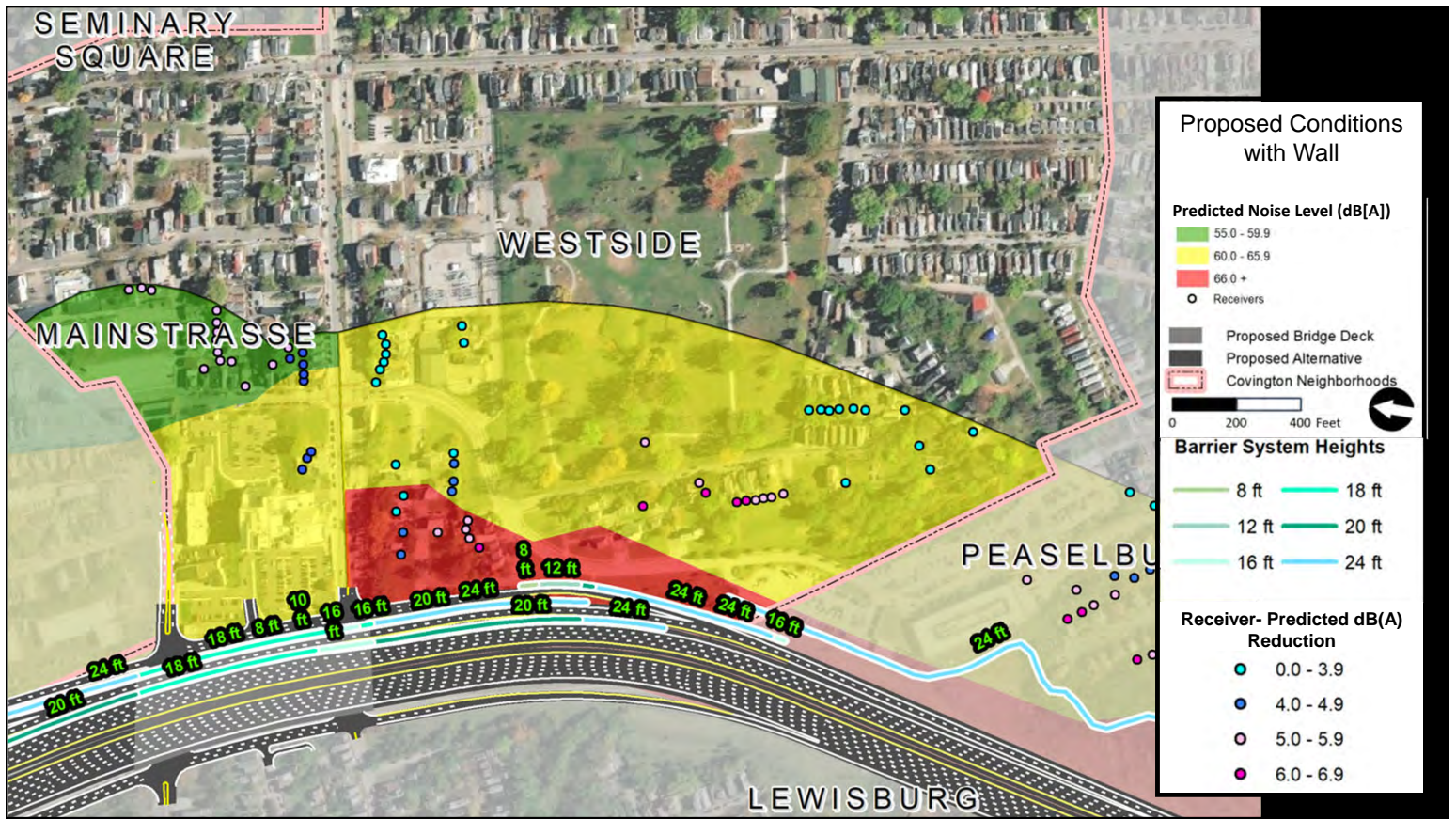
The red lines on this slide show noise walls that were evaluated as part of the noise study for in the Westside Covington area. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a noise wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

Noise



Pike Street Without Noise Wall (looking east)
71 dBA (average)



Pike Street With Noise Wall (looking east)
64 dBA (average)

To the left is a drawing showing what the finished project might look like at Pike Street if a noise wall is not built. To the right is a drawing showing what Pike Street might look like with a noise wall. These photos are looking east on Pike Street.

Noise



Pike Street Without Noise Wall (looking west)
71 dBA (average)



Pike Street With Noise Wall (looking west)
64 dBA (average)

These views are looking west on Pike Street. Again, you can see what the project would look like with and without noise walls.

Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.

Enhancement Measures

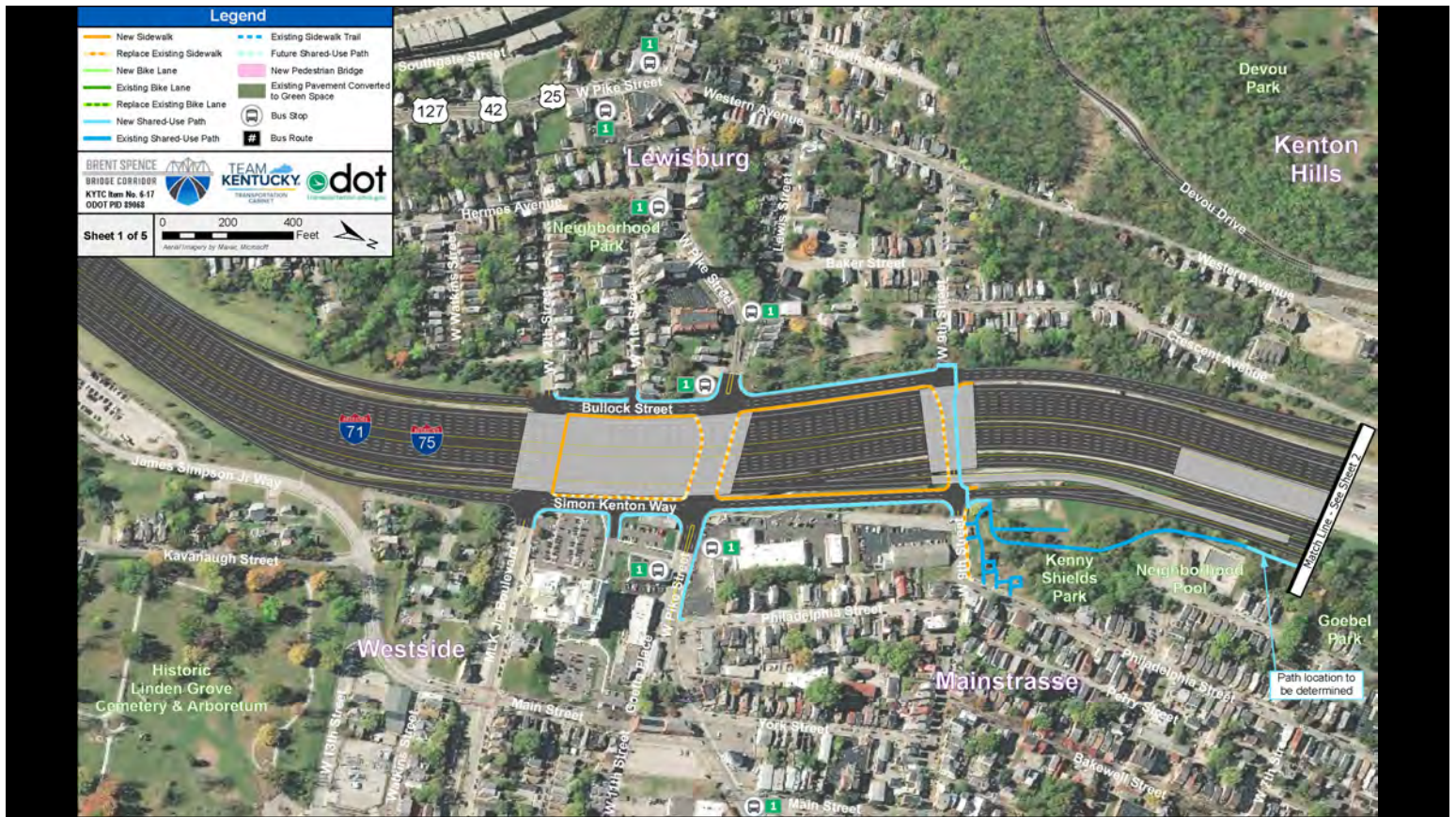
- Working with Covington to develop aesthetic guidelines
 - Landscaping
 - Streetscapes
 - Gateways
 - Treatments for piers, abutments, retaining walls, and pedestrian paths
- Pedestrians and Bicycles
 - Connections across I-71 and I-75
 - New shared use paths
 - New and rebuilt sidewalks
- Stormwater
 - Separate highway runoff from combined sewer system
 - Reduce overflow event
 - Working with the City of Covington and Sanitation District No. 1

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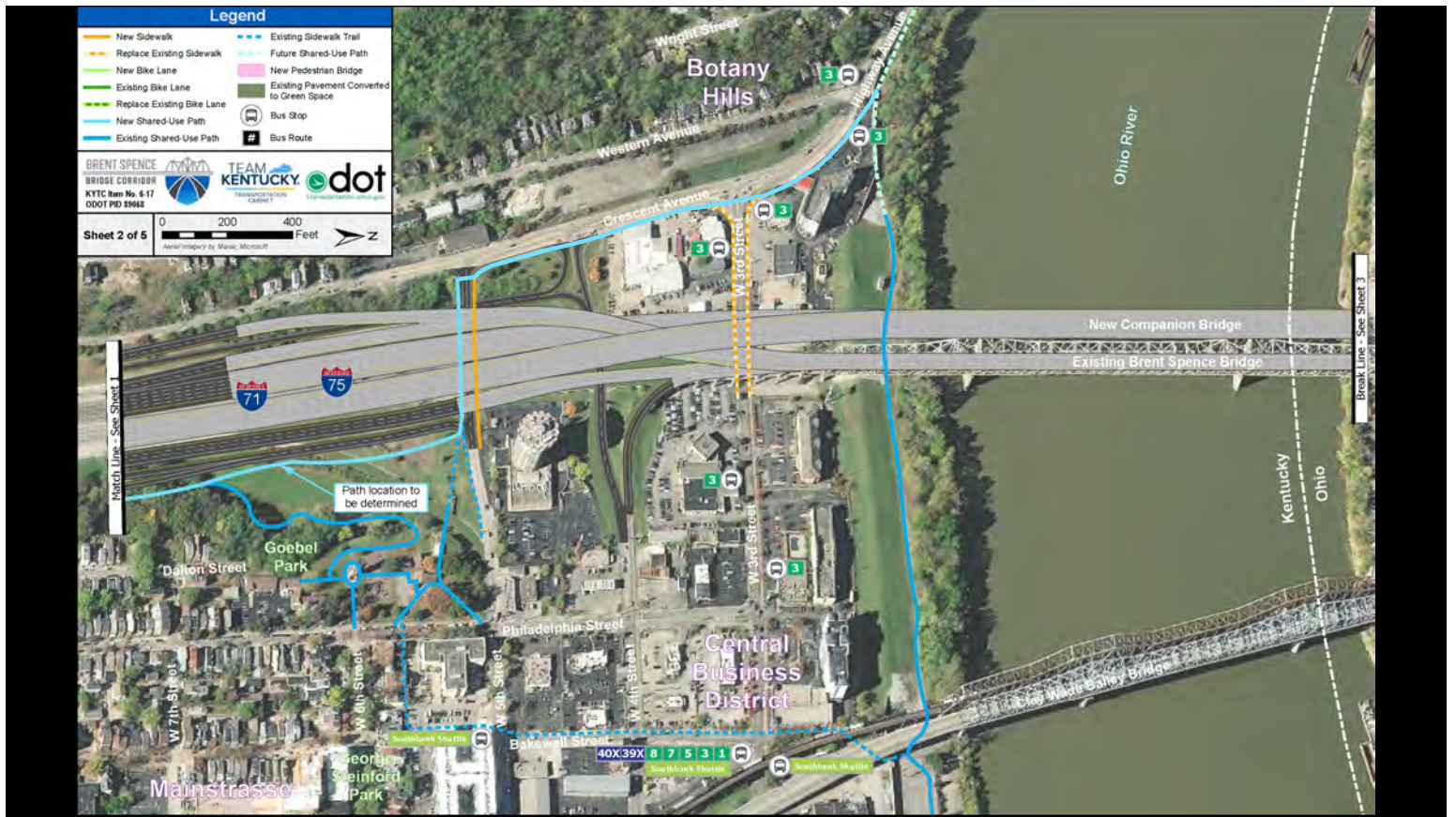
In addition to coordinating work in Goebel Park, KYTC is also working with Covington to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.

Existing highway runoff flows in the combined sewer system in Covington, and overflow events negatively affect area residents. KYTC is committed to separating the highway drainage from the combined sewer systems as part of the project. It should be noted that we are not just striving to separate additional runoff from the MS4 systems, but we are working to separate all runoff in in Covington, which will substantially reduce overflow events.



This exhibit shows some of the planned pedestrian and bicycle facilities near Westside. Note the new shared use paths and sidewalks along the frontage roads. Also notice the new and rebuilt sidewalks under the MLK/West 12th, Pike, and West 9th Street bridges. A new shared use path will be built under the West 9th Street bridge, which will tie into the shared use paths in Goebel Park.



Moving north, note the new shared use path in Goebel Park and under the West 5th Street bridge. The shared use path will be extended along Crescent Avenue to connect to the existing shared use path along the river. Also notice the new and rebuilt sidewalks along West 5th and West 3rd streets. Throughout Covington, the shared use paths and sidewalks will improve connections to existing transit stops.



This is a view of what Goebel Park might look like once the highway improvements are constructed, the new shared use paths are built, and agreed upon landscaping is in place. The project's efforts to separate highway runoff from the existing MS4 system will also reduce the number of overflow events affecting Goebel Park.



This view shows how the finished project might appear when looking across I-71 and I-75 with Westside in the distance.



This view shows what the project might look like at Pike Street. Notice the potential for aesthetic treatments on the bridge walls and piers. Also notice the sidewalks and shared use paths.



THANK YOU!

For more detailed information or to provide feedback visit:
www.PublicInput.com/bsbc



BRENTSPENCEBRIDGECORRIDOR.COM



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a PublicInput.com to review information about the project and provide your feedback.



[Attachment 5: Exhibits](#)

Legend

- New Sidewalk
- - - Replace Existing Sidewalk
- New Bike Lane
- - - Existing Bike Lane
- - - Replace Existing Bike Lane
- New Shared-Use Path
- - - Existing Shared-Use Path
- - - Existing Sidewalk Trail
- - - Future Shared-Use Path
- New Pedestrian Bridge
- Existing Pavement Converted to Green Space
- Bus Stop
- # Bus Route

KYTC Item No. 6-17
 ODOT PID 89068

Sheet 1 of 5

0 200 400
 Feet

Aerial Imagery by Maxar, Microsoft



Legend

- New Sidewalk
- - - Replace Existing Sidewalk
- New Bike Lane
- - - Existing Bike Lane
- - - Replace Existing Bike Lane
- New Shared-Use Path
- - - Existing Shared-Use Path
- - - Existing Sidewalk Trail
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- Bus Stop
- # Bus Route


BRENT SPENCE BRIDGE CORRIDOR
 KYTC Item No. 6-17
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Sheet 2 of 5

0 200 400 Feet

Aerial Imagery by Maxar, Microsoft




Match Line - See Sheet 1

Path location to be determined

Break Line - See Sheet 3



Brent Spence Bridge Corridor (Looking Northwest from Kentucky)



Brent Spence Bridge Corridor - Kentucky (Looking Southeast)





Pike Street With Noise Walls (Looking Northwest) - Kentucky



Pike Street Without Noise Wall (Looking East) - Kentucky



Pike Street With Noise Wall (Looking East) - Kentucky



Goebel Park (Looking North) - Kentucky



Attachment 6: Comment Forms

DEMOGRAPHICS FORM

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What is your race or ethnicity?

- Black or African American American Indian or Alaskan Native Asian Hispanic or Latino
 White Native Hawaiian or Other Pacific Islander
 I prefer to self-describe: _____

What is the primary language spoken in your home?

- English Spanish Other (Please specify): _____

Was project information translated into other languages appropriately?

- Yes No Not Applicable

How many people live in your household?

- 1 - 2 3 - 5 6 +

What are the age ranges of those living in your household? (Check all that apply.)

- Under 18 19 - 44 45 - 64 65 +

What is your annual household income?

- Less than \$10,000 \$10,000 - \$24,999 \$25,000 - \$49,999 \$50,000 - \$74,999
 \$75,000 - \$99,999 \$100,000 - \$149,999 \$150,000 +

What is the highest level of education completed by members of your household?

- No Elementary school Middle school High school College/university
 Other: _____

Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Thank you for helping ODOT and
KYTC improve their public
involvement practices!

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- Under 18 19 - 44 45 - 64 65 +

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 \$75,000 - \$99,999 \$100,000 - \$149,999 \$150,000 +

What is the highest level of education completed by members of your household?

- No Elementary school Middle school High school College/university

Other: Graduate degrees

Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes No

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- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: _____

What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): _____

Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: _____

Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
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Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

• Keep involving westside residents including residents on planning committees for project

Thank you for helping ODOT and KYTC improve their public involvement practices!

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Thank you for helping ODOT and
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TEAM
KENTUCKY
TRANSPORTATION
CABINET


transportation.ohio.gov

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COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: _____

Mailing address (or nearest cross streets): _____

Email address: _____ Phone: _____

Business/Organization Name: _____

Business/Organization Address: _____

What is your interest in the proposed project? (Select all that apply.)

- Area Resident Area business owner or employee Commuter
 Other: _____

General Comments (Please attach additional pages, if needed):

**COMMENTS DUE BY
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
 - By email at Keith.Smith@dot.ohio.gov
 - By telephone at 1-800-831-2142
 - Online at PublicInput.com/bsbc
 - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



COMMENT FORM

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What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
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General Comments (Please attach additional pages, if needed):

SOUND BARRIER ESSENTIAL

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Business/Organization Name: _____

Business/Organization Address: _____

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- Area Resident Area business owner or employee Commuter
 Other: _____

General Comments (Please attach additional pages, if needed):

I personally like the noise wall -

COMMENTS DUE BY
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Email address: _____

(Phone: COV.)

Business/Organization Name: _____

Business/Organization Address: _____

What is your interest in the proposed project? (Select all that apply.)

- Area Resident Area business owner or employee Commuter
 Other: _____

General Comments (Please attach additional pages, if needed):

Great presentation -
thanks for this opportunity!

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