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# Brent Spence Bridge Corridor Project Mutter Gottes / CBD Meeting Summary December 5, 2022

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## Introduction

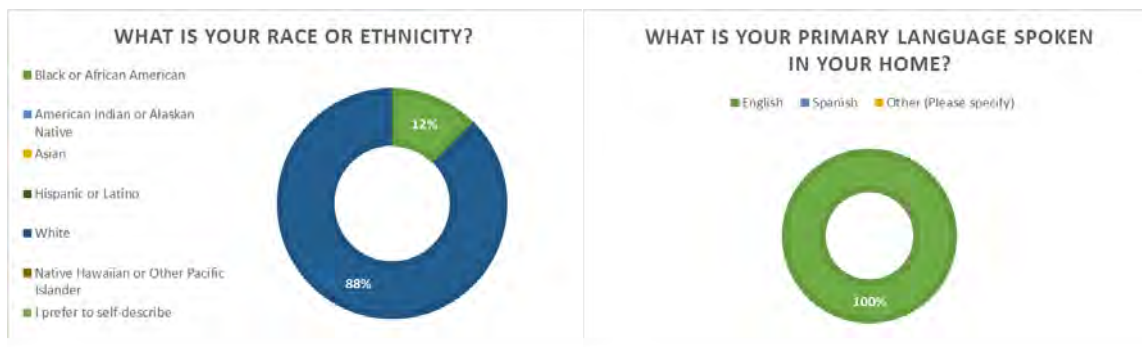
The Mutter Gottes and Covinton Central Business District (CBD) Neighborhood Meeting was held on December 5, 2022 from 7:00 pm to 8:15 pm. The meeting was held at the Kenton County Public Library – Covington Branch located at 502 Scott Street in Covington, Kentucky. It was publicized through the Mutter Gottes Facebook page (private group) and email list. The neighborhood group also distributed 300 flyers (see Attachment 1). Attendees at the meeting included the project team and members of the public. In addition to residents of Mutter Gottes, residents of Edgewood, Mainstrasse, and downtown Cincinnati were also in attendance. In total, 22 people attended the meeting, including 5 individuals from the project team (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). Following the presentation, the project team responded to questions and comments offered by those present.

Exhibits showing project renderings and lane configurations were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Five (5) comment forms and eight (8) demographic surveys were returned at the meeting.

## Demographics

The following is a breakdown of the responses received from the demographic survey.





A write in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. Comments included:

- All Covington neighborhoods are affected. Maybe reach out to other neighborhood groups.
- Post invites in neighborhood Facebook groups to the [sic] public meetings.



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## Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Clarification / understanding of local versus through traffic and ramp connections.
- Concern over traffic congestion / backup in Covington during bridge closures and / or incidents requiring closure along with concern over access response time by emergency responders.
- Clarification / understanding of the process for noise evaluation and discussion on impacts with / without noise wall.
- Routing of truck traffic and sound / noise associated with truck traffic.
- Clarification of timeline and staging of construction.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. The project team will send responses to individuals who provided contact information.

Comments noted on the survey forms returned at the meeting include:

- Interested in historic preservation.
- Nice presentation.
- Community engagement is critical. As much as practical will be key to adoption. What are projected project cost and tax impacts? If you wish to get in touch with Mutter Gottes neighborhood (MOGNA.cov.mail.com).
- Please protect exits and entrances to and from north Covington, especially at 4<sup>th</sup> and 5<sup>th</sup> Streets.
- Definitely build the sound walls!!! Thx. Is it possible to increase local bridge traffic to 55 mph? 45 mph seems to slow for the bridge. 9<sup>th</sup> Street already has a speed problem especially at 9<sup>th</sup> and Bakewell and 9<sup>th</sup> and Willard. Can you add stop signs at each intersection to help with the added traffic? I'm worried about pedestrian safety on 9<sup>th</sup>. Will the combined Kyles and Dixie exit (south) ramp have 2 lanes? Kyles exit is already very slow, but maybe that's because of the trucks?



**Table 1: Q & A from Session**

Question	Project Team Response
Will there be lights or signals at the project intersections?	The traffic control at each intersection will be decided in the design / build phase.
Will the collector-distributor be northbound or southbound?	The collector-distributor will be in both directions, one on each side of the interstate.
Will there be a barrier between the interstate and collector-distributor road?	Yes.
What is the first exit going south on the collector-distributor road?	It is a left-hand exit for 5 <sup>th</sup> Street.
Can jake braking be prohibited?	KYTC cannot prohibit jake braking on interstate highways because it is a safety feature for trucks.
What if an incident occurs on the interstate?	The new companion bridge provides redundancy in the system. If there is an incident on the interstate, traffic can divert to the existing Brent Spence Bridge rather than the Covington street network.
What are the exits for the collector-distributor road in Cincinnati?	Going northbound, an exit to I-71/2 <sup>nd</sup> Street is first, then 5 <sup>th</sup> Street, 6 <sup>th</sup> Street, and Winchell Drive.
How do you go about determining noise levels?	Noise levels are determined during the highest peak period for traffic volumes. A model is used to determine noise levels, but it is field verified for existing noise levels to make sure it is modeling conditions correctly before it is used to project future noise levels.
Please explain the cost-effective criteria relative to noise walls.	The cost of a wall is divided by the number of people who will benefit from the wall. KYTC has set a threshold for cost per benefitted receptor that determines if a noise wall is cost-effective.
Do noise walls redirect noise upward instead of outward?	A portion of the noise is absorbed by the textures on the wall.
With current conditions and narrow lanes, there may be some trucks that avoid the Brent Spence Bridge. Will there be anything to prohibit / reduce trucks when the project is built?	The additional capacity across the river and splitting through and local traffic will help to improve traffic flow and safety. Trucks will not be restricted or prohibited in the project corridor.
How far south does the corridor extend? Could there be more trucks? I have concern over induced demand.	The project ties back into the existing interstate south of Dixie Highway. When in Covington and Cincinnati, through-traveling trucks will be separated from local traffic. The project has been designed to meet the demand that is anticipated over the next 25 years.



Question	Project Team Response
Did you look at anything like planting trees for noise reduction?	Trees are not considered a viable method of noise reduction in Kentucky. However, KYTC is looking at opportunities for enhancements that could include planting trees in some areas, such as along the shared-use path in Goebel Park.
If you eliminate the local 9 <sup>th</sup> Street to 5 <sup>th</sup> Street connector would there be an exit to 5 <sup>th</sup> Street?	Yes.
Will you have bicycle ramps on / along I-71?	No. KYTC will focus on opportunities to improve east-west movements for bicycles in the project area.
Can anything be done to reduce speeds and improve safety for pedestrians at the end of Bakewell and Willard?	The project team can discuss this further one-on-one after the presentation.
Separating the storm and sanitary is a huge improvement, especially for Peaselburg.	KYTC has been working with neighborhoods to design a project that benefits the communities. This is a concern that has been raised that KYTC has been able to incorporate into the design.
Will it be more complex for local law enforcement and response after the project is built?	KYTC will collaborate with local emergency response and first responders (police / fire / EMT) for all municipalities in the project corridor to develop an Incident Management Plan.
How much of what you are describing is absolute versus what is being tweaked? What are you signing off on? What might change during the design / build phase?	One example of an absolute is the number of lanes. Opportunities for modifications or refinements include pedestrian facilities, landscape design, and others. One of the biggest decisions during the design / build phase could be selecting a bridge type.



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## Attachment 1: Advertising Materials

## Seth Turner

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**From:** Brandon Galeas  
**Sent:** Friday, November 11, 2022 9:51 AM  
**To:** Sherry Kish; Ginger Dawson  
**Cc:** Jodi Heflin; Seth Turner  
**Subject:** RE: Brent Spence Bridge Presentation on December 5th

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

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**From:** Sherry Kish  
**Sent:** Wednesday, November 09, 2022 5:02 PM  
**To:** Brandon Galeas; Ginger Dawson  
**Cc:** Jodi Heflin; Seth Turner  
**Subject:** Brent Spence Bridge Presentation on December 5th

Brandon and Ginger,

Thank you so much for working with me to set up a meeting to invite the residents of Mutter Gottes to hear from the Brent Spence Bridge (BSB) project team. The project team will share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

Our goal is to be sure all notifications about meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous neighborhood meeting?		X – 11/2 though we did say it was in the works and shooting for this month which is now not in the cards
Email list	Can you email a neighborhoodwide or citywide email list with information about BSB presentation at the upcoming meeting?	X	
Facebook (private to neighborhood)	Can you share a Facebook message about BSB presentation?	X	

Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?	X – There are some hot spots for businesses in our neighborhood, though there is a lot of cross-pollination exposure of residents as a result of our city’s walkability	
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?		
Other	Is there another outlet you have available to get the word out?		
Do you need assistance from us with advertising?	We have created a flyer that could be helpful (attached).	X – if you can print flyers our distribution team can pass them out to every home	

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!  
Sherry

**Sherry R. Kish**

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*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*



## Seth Turner

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**From:** Sherry Kish  
**Sent:** Monday, November 21, 2022 11:32 AM  
**To:** Seth Turner; Jodi Heflin  
**Subject:** FW: MGNA Brent Spence Bridge Presentation  
**Attachments:** 11-08-2022\_Invitation Flyer\_Mutter Gottes.pdf

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

FYI

**Sherry R. Kish**  
Manager Business Development

**HNTB CORPORATION**  
88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

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**From:** Brandon Galeas  
**Sent:** Sunday, November 20, 2022 2:55 PM  
**To:** Sherry Kish  
**Subject:** FW: MGNA Brent Spence Bridge Presentation

Sherry,

Here is our email notification for the event. A facebook post was also posted to our page but it's a private group with 423 members from the local community.

Brandon Galeas

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**From:** Mutter Gottes Neighborhood Association  
**Sent:** Sunday, November 20, 2022 2:52 PM  
**Subject:** MGNA Brent Spence Bridge Presentation

The Kentucky Transportation Cabinet and Ohio Department of Transportation are conducting a series of neighborhood outreach meetings for the Brent Spence Bridge Corridor project. The project team will share updates, answer questions and give the opportunity for residents to share feedback.

Consider joining the Mutter Gottes community meeting on **December 5, 2022 at 7 p.m.** in the **Kenton County Public Library – Covington Branch** Meeting Rooms, 502 Scott Street.



Attachment 2: Sign-In Sheet

**SIGN-IN SHEET**

**BRENT SPENCE BRIDGE CORRIDOR PROJECT**

Neighborhood Outreach Meeting – December 5, 2022

Name	Representing	Address	Phone	Email
1. Raymond Senkowitz	SELF			
2. John Paul	Self			
3. Steve Frank	REALTY			
4. Dennis Bullock	Self			
5. Ray Burr	MGNA			
6. Shannon Givens	MGNA			
7. Anne Mitchell	MGNA			
8. GERRI KING	MGNA			
9. PAULY NAVIN	MGNA			
10. Ginger Dawson	MGNA			
11. JeanneHouck Trifilio	Self			
12. David Averdich	Self			
13. Alex Njor	Self			
14. Bridget Cursiorowski	Self			
15. JIAN LUK	SELF			
16. MAREN BECHMAN	HWFB			
17. Catherine Clark	Self			
18. Jamir Davis	Self			
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



### Attachment 3: Photographs





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Attachment 4: Presentation

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



# Mutter Gottes/CBD Neighborhood Meeting (KY)

December 5, 2022



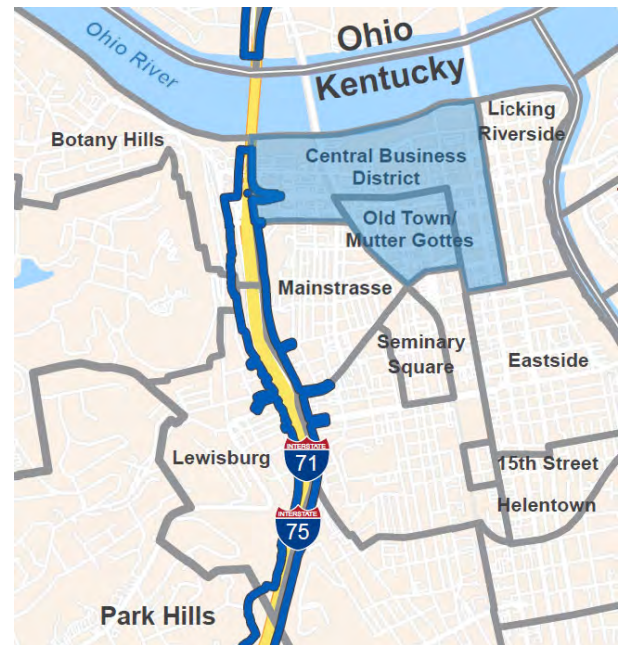
INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with remarks by the City of Covington (5 min) and introductions of the Project Team in attendance (2 min).

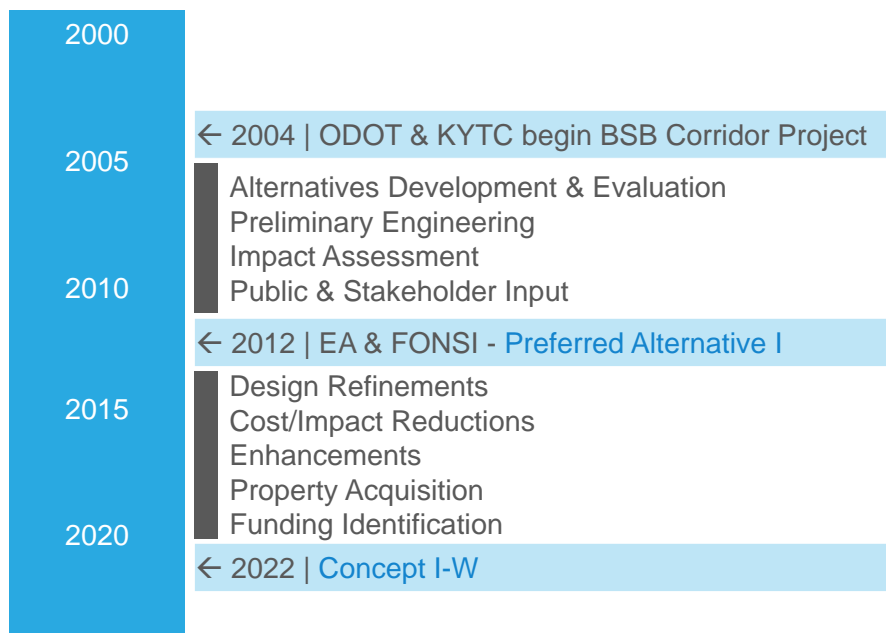
# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the Mutter Gottes / CBD neighborhoods the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Mutter Gottes / CBD area
  - Discussion/feedback from Mutter Gottes / CBD residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Mutter Gottes / CBD neighborhoods. Most importantly, tonight we're focusing on hearing from the residents of Mutter Gottes and the CBD. We're here to answer your questions and to listen to your feedback about the project.

# Project History



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For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2022, KYTC has begun purchasing the land needed to build the project.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



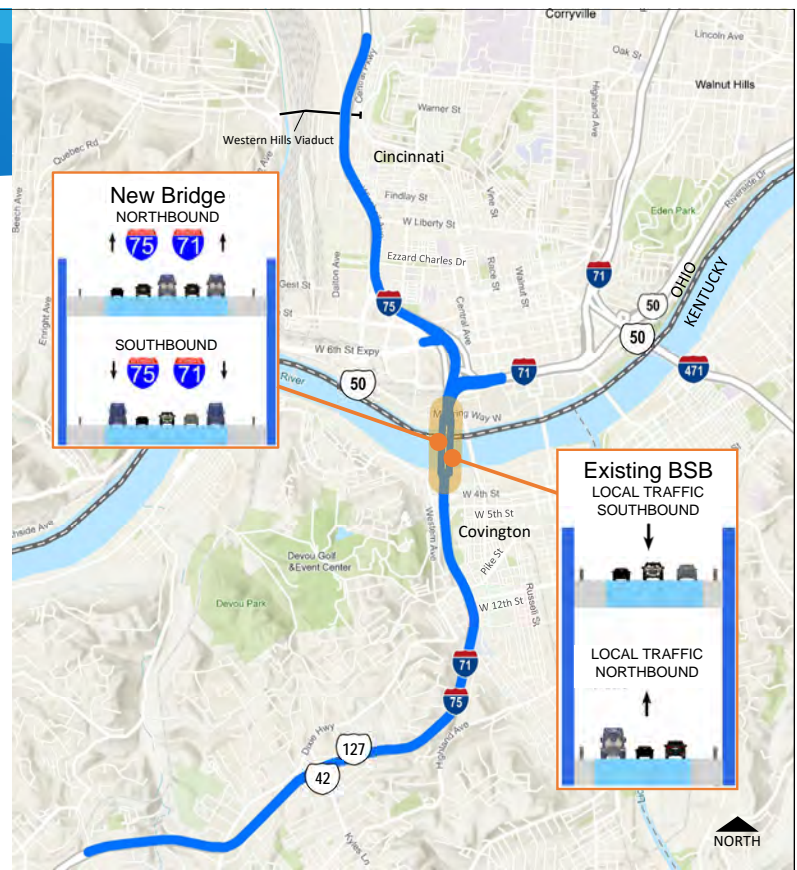
In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.



# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

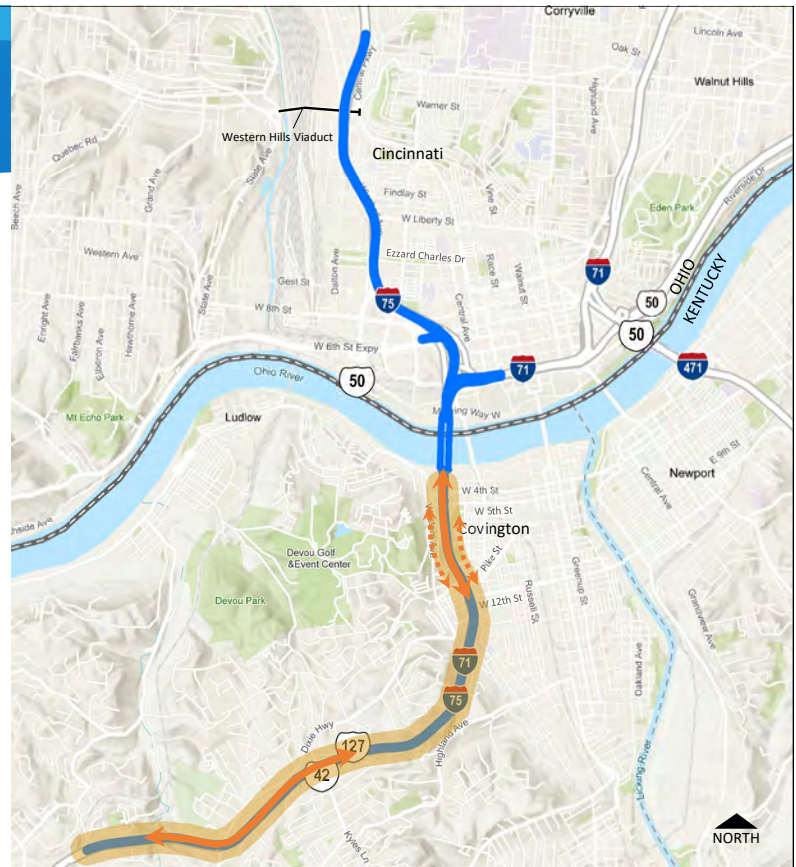
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

# Project Description

## Kentucky

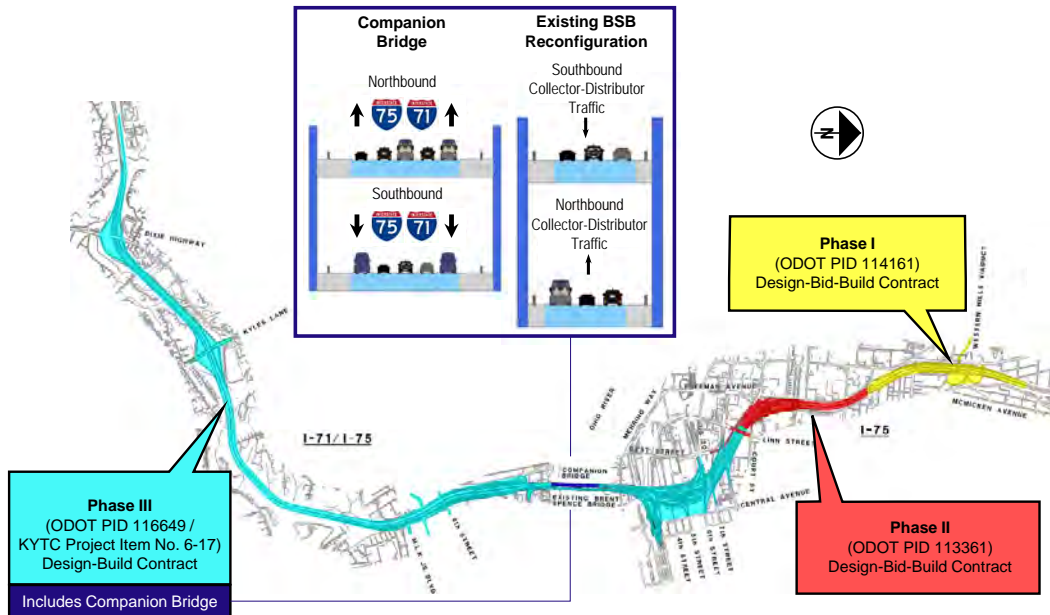
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles Lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

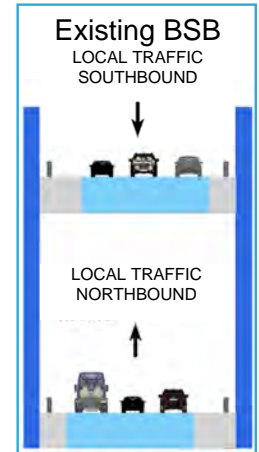
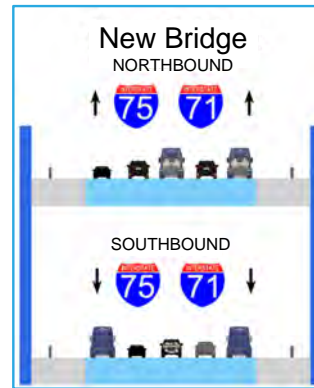
# Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

# What Has Changed?

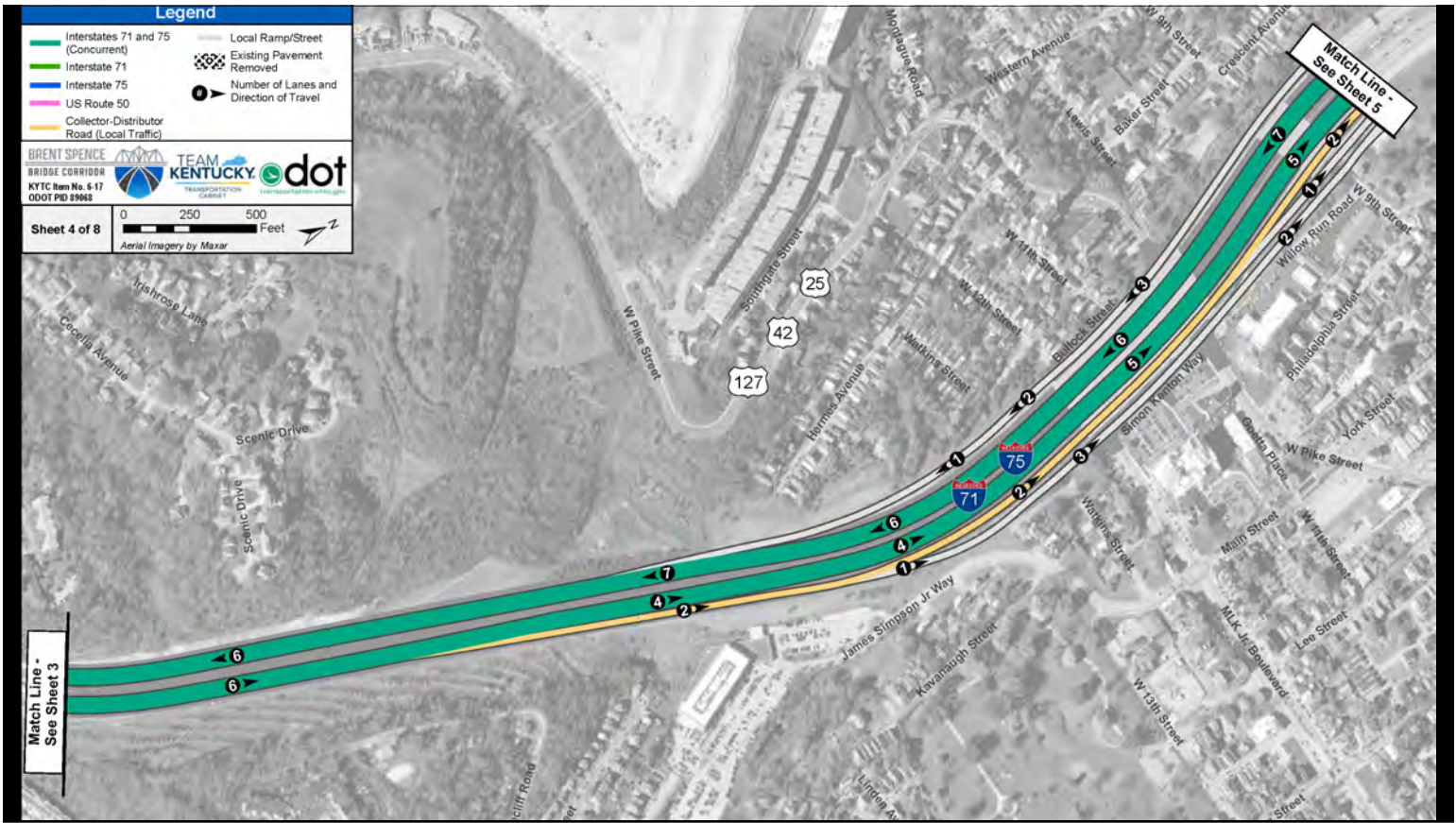
- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



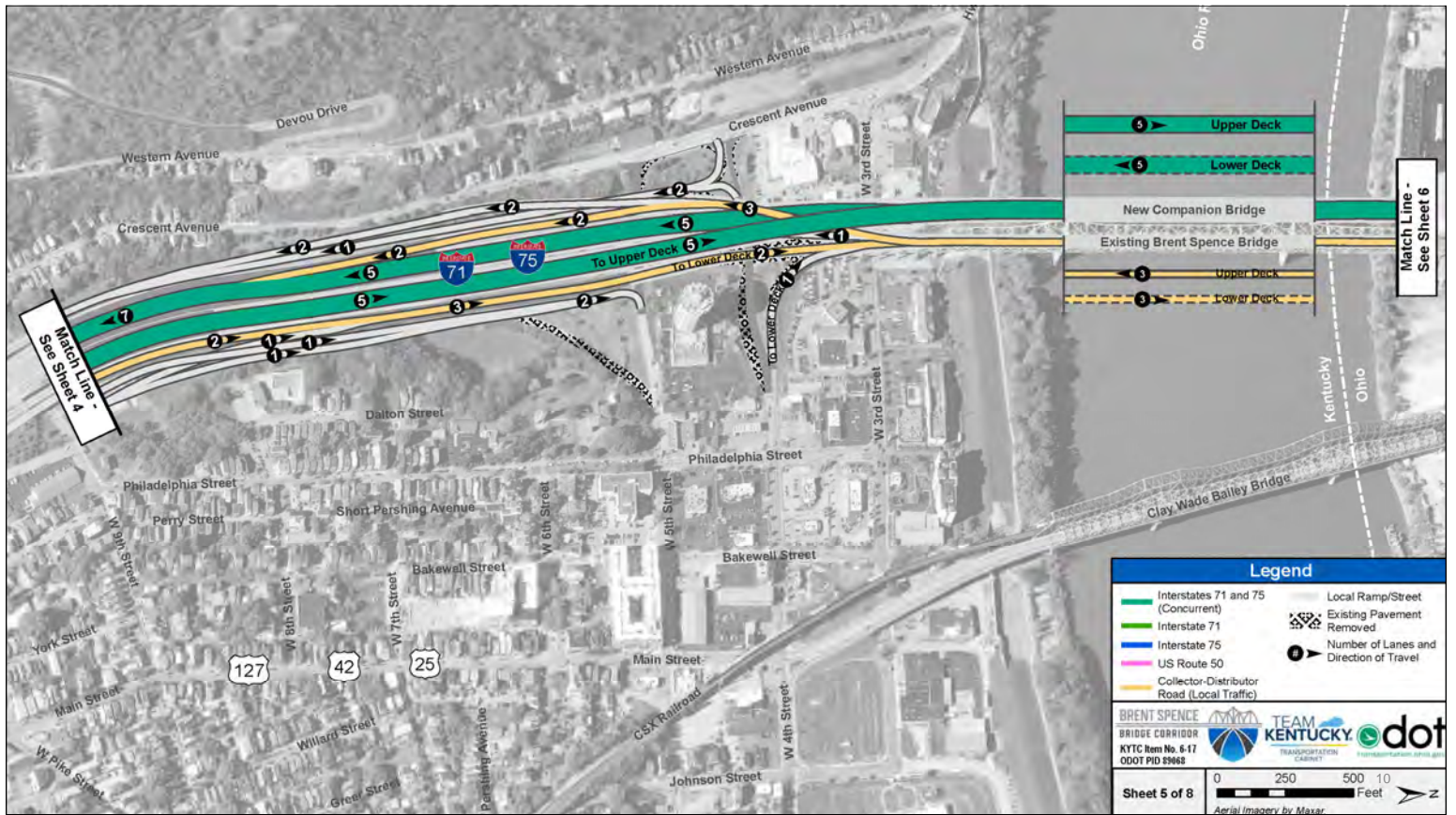
8

As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

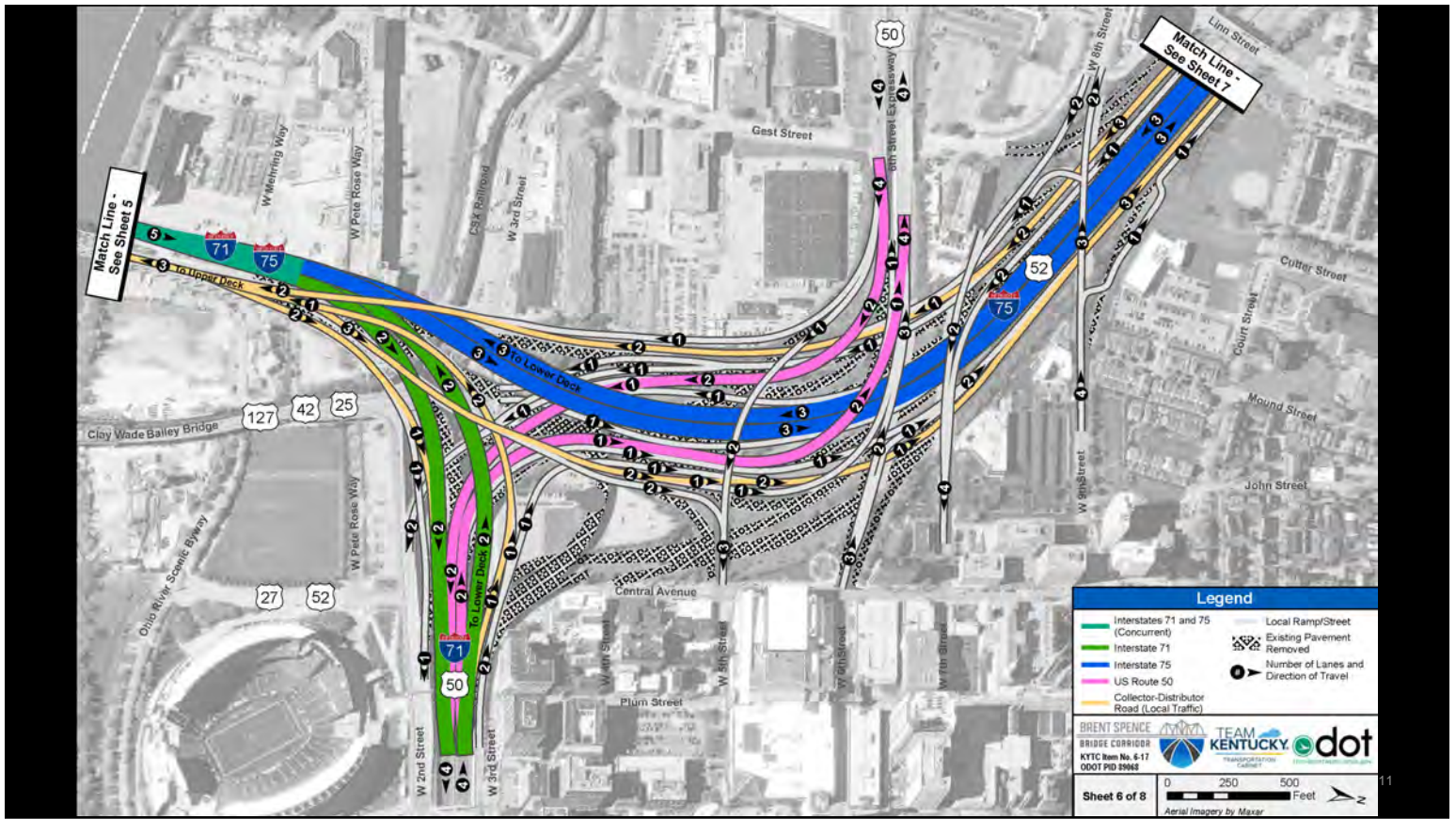
Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will move in downtown Cincinnati. The dark blue shows I-75. The dark green shows I-71. The pink shows US-50. The orange shows the collector-distributor roads. The grey shows the local access ramps. The project also provides the opportunity for a direct connection from the Clay Wade Bailey Bridge to I-75.



This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

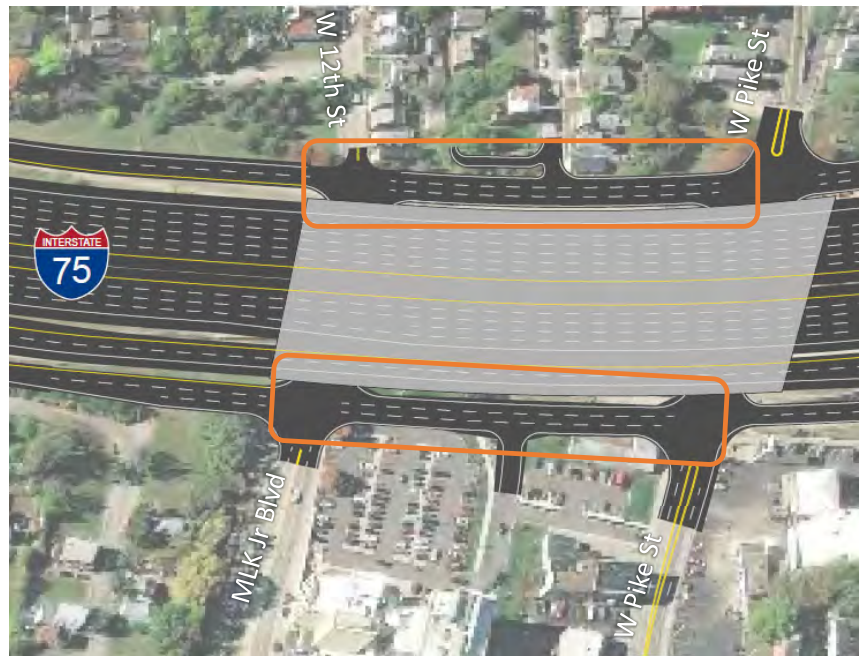




Here's another view from Covington near Mutter Gottes and the CBD.

# What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 40+ then vs. 4 now
  - Commercial | 6 then vs. 5 now



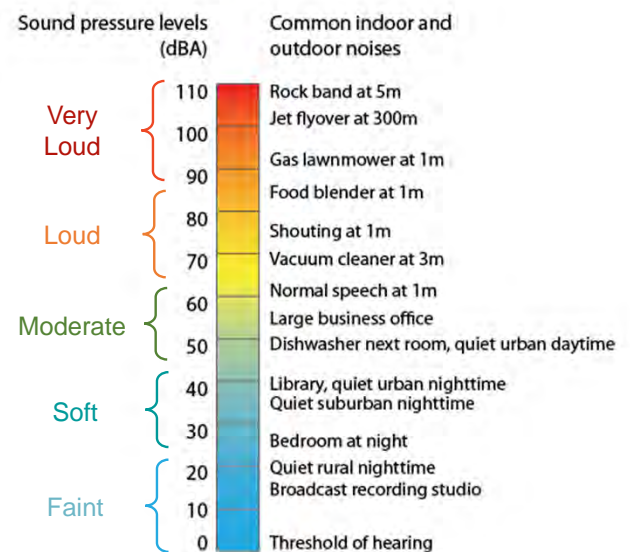
Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound



Source: Minnesota Pollution Control Agency

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Now let's dig into how the project will impact Mutter Gottes and the CBD, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

## KYTC Noise Policy

- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.

Kentucky Transportation Cabinet  
Department of Highways  
Division of Environmental Analysis



### NOISE ANALYSIS AND ABATEMENT POLICY

APPROVED BY:

\_\_\_\_\_  
Todd Jeter, Division Administrator  
Federal Highway Administration

\_\_\_\_\_  
Date

\_\_\_\_\_  
James Gray, Secretary  
Kentucky Transportation Cabinet

\_\_\_\_\_  
Date

Revised August 1, 2022  
EFFECTIVE DATE JULY 1, 2020  
Revised July 1, 2015  
Original July 2011

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KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dBA. We also determine if there is a substantial increase – specifically a 10 dBA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, “Can it effectively block noise?” In Kentucky, that means determining if a barrier can provide a minimum 5 dBA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

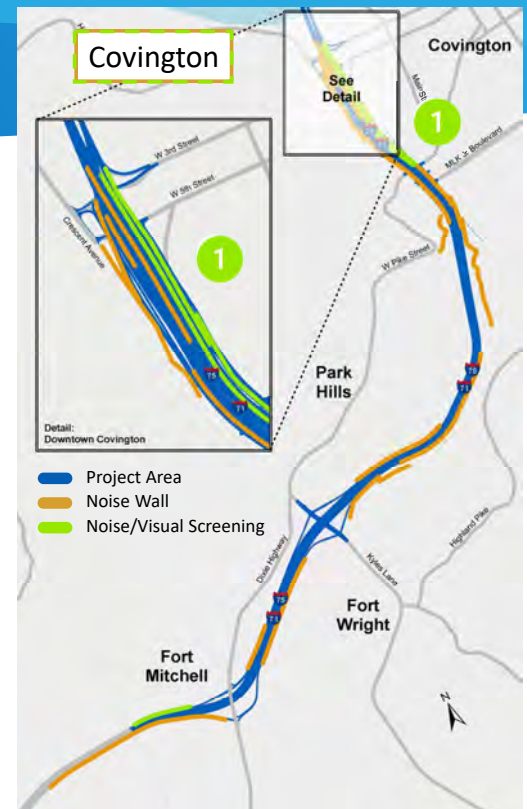
If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

# Noise

## Noise walls not reasonable/feasible per KYTC Noise Policy

- 1 • W. 5th St to Pike St (Mainstrasse)
  - Feasible
  - Not cost effective
  - Did not meet design goals
  - KYTC is evaluating further
  - Without noise wall = 67 dBA
  - With noise wall = 62 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office



Now that we've provided some noise background, let's review how KYTC is addressing traffic noise in the CBD and the areas just west of Mutter Gottes. KYTC evaluated several noise walls and noise wall systems in Covington. Some of them met the feasible and reasonable criteria we just discussed. However, in Mainstrasse and the CBD (just west of Mutter Gottes), a noise wall system benefitting the area labeled with the number 1 was found to be feasible but not reasonable because it was not cost effective and did not meet design goals.

Recognizing that traffic noise is a concern, KYTC is currently conducting a technical study to further evaluate a system of noise/visual screening walls to help provide some noise and to shield views of the highway. KYTC will document their decision about this noise wall system in a technical memo based on the technical feasibility and comments received during this meeting and other outreach activities.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

# Noise



Pike Street Without Noise Wall (looking west)  
71 dBA (average)



Pike Street With Noise Wall (looking west)  
64 dBA (average)

To the left is a drawing showing what the finished project might look like from Pike Street if a noise wall is not built. To the right is a drawing showing what Pike Street will look like with a noise wall. These photos are looking west on Pike Street.

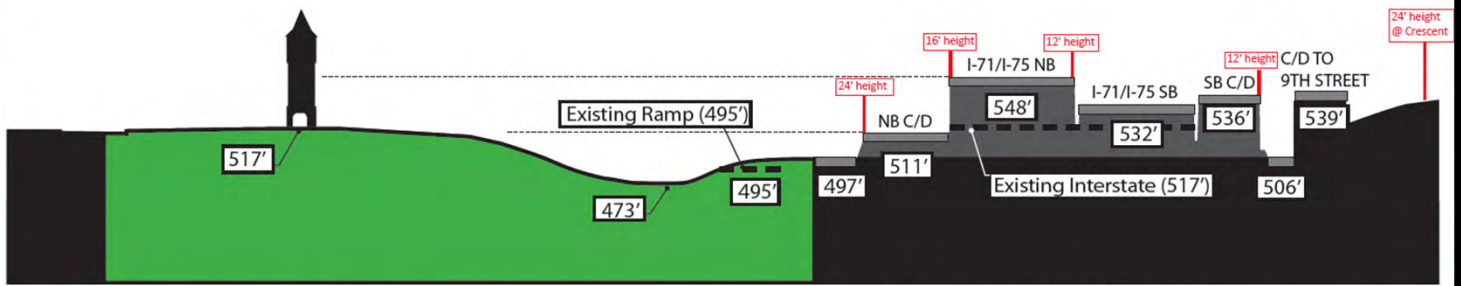
# Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. As we discussed earlier, KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.



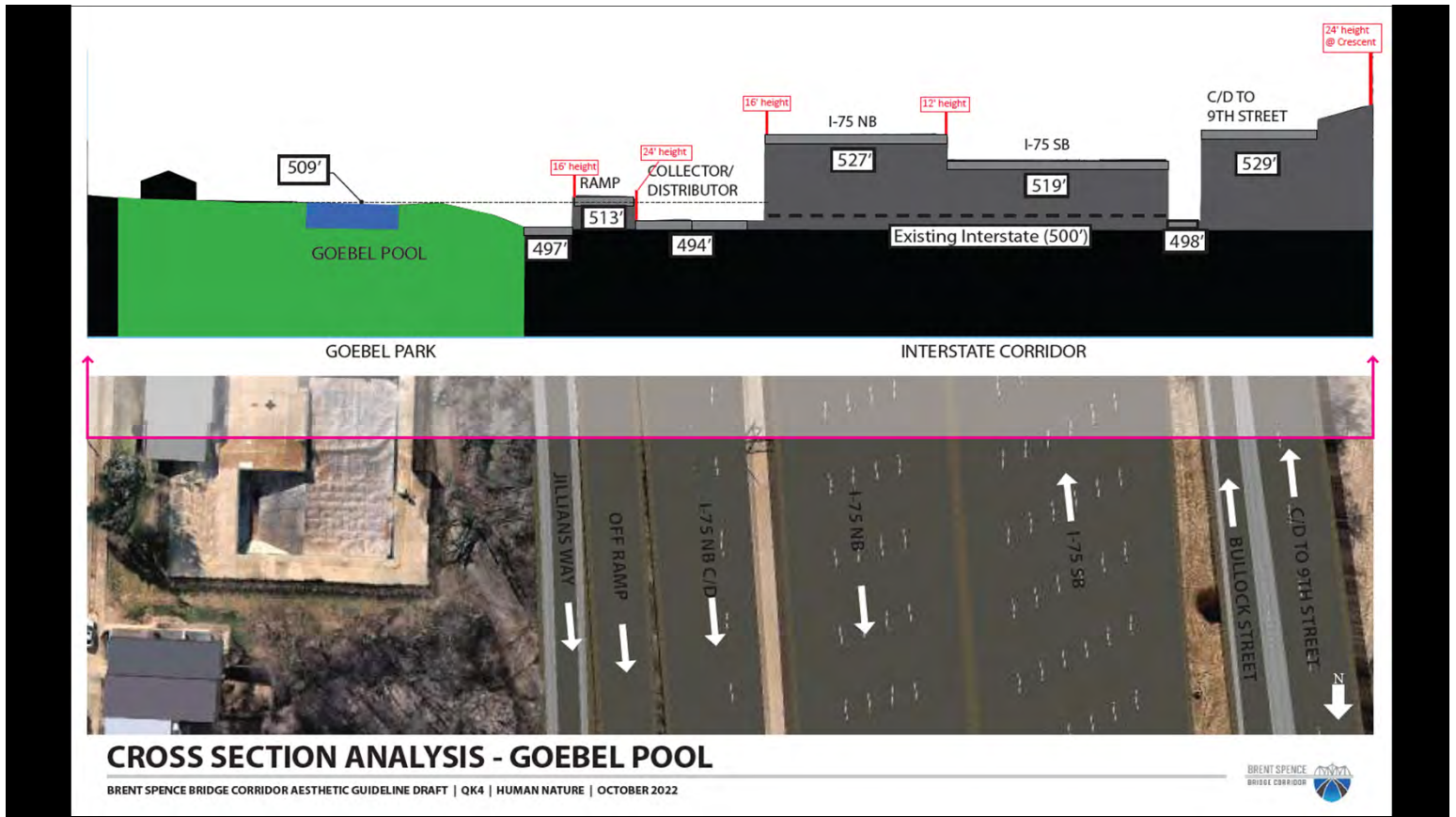


## CROSS SECTION ANALYSIS - CLOCK TOWER

BRENT SPENCE BRIDGE CORRIDOR AESTHETIC GUIDELINE DRAFT | QK4 | HUMAN NATURE | OCTOBER 2022



This slide shows a cross-section of the roadway and noise barriers at the clock tower to demonstrate the overall schematic of the roadway and the height of potential noise walls.



This slide shows a cross-section of the roadway and noise barriers at the Goebel pool to demonstrate the overall schematic of the roadway and the height of potential noise walls.

# GOEBEL PARK EXISTING AND PROPOSED PROPERTY MAP WITH 9th TO 5th ST. CONNECTOR



23

KYTC is evaluating two options for providing connectivity in the vicinity of Goebel Park. As currently designed, the project includes a local street connector from West 9<sup>th</sup> Street to West 5<sup>th</sup> Street in the northbound direction to provide an enhanced local street network. This roadway is indicated by the blue line.

Also on the diagram:

Red Line: West 5<sup>th</sup> Street off ramp from the collector-distributor road

Green Line: West 9<sup>th</sup> Street on ramp to the collector-distributor road

Yellow Line: Collector-distributor system

# GOEBEL PARK EXISTING AND PROPOSED PROPERTY MAP WITHOUT 9th TO 5th ST. CONNECTOR



24

This graphic shows what the project would look like without the connector between West 9th Street and West 5th Street.

Still shown on the diagram:

Red Line: West 5<sup>th</sup> Street off ramp from the collector-distributor road

Green Line: West 9<sup>th</sup> Street on ramp to the collector-distributor road

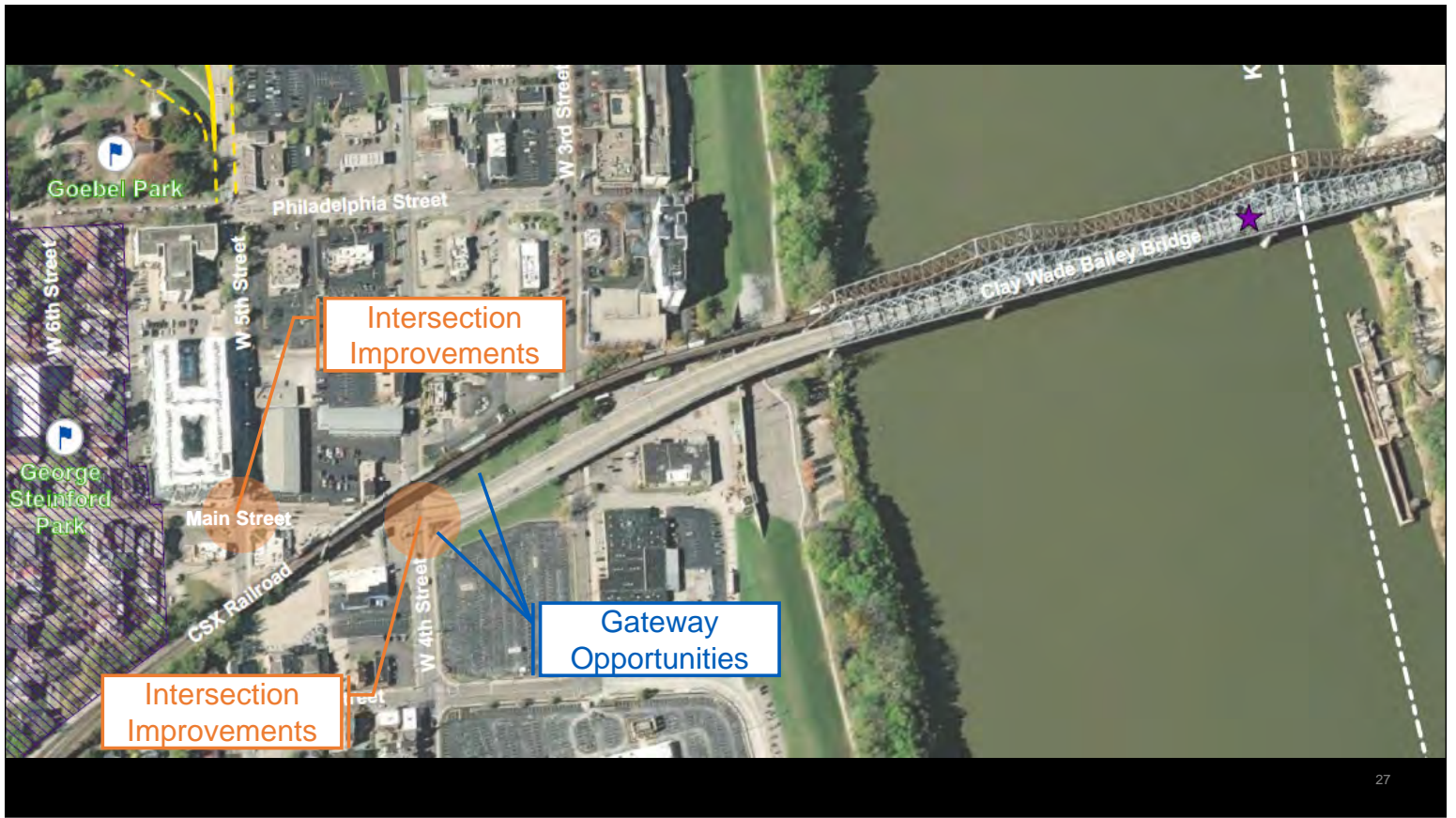
Yellow Line: Collector-distributor system

# Enhancement Measures

- Working with Covington to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, and pedestrian paths

25

In addition to coordinating work in Goebel Park, KYTC is also working with Covington to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.



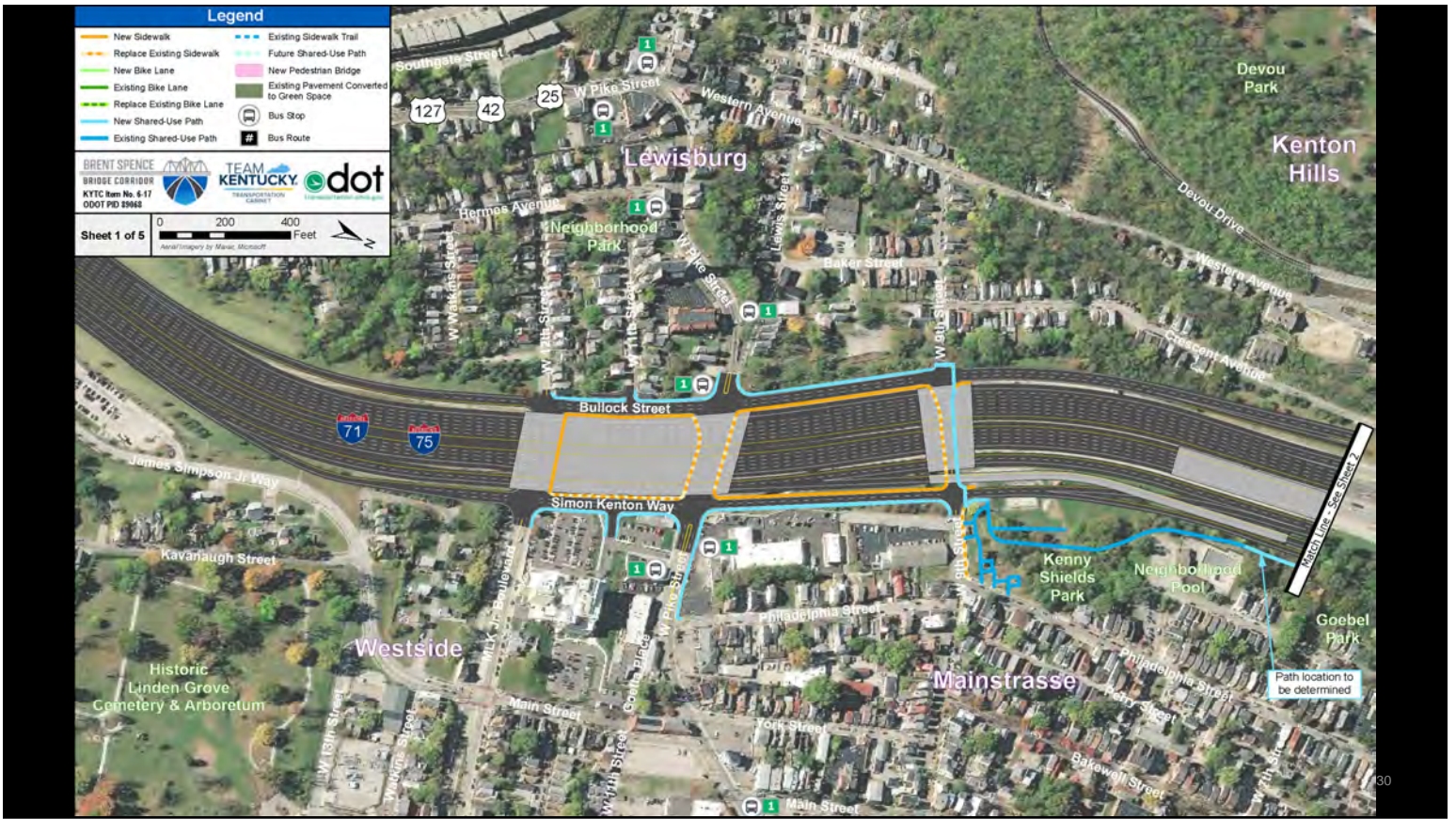
27

KYTC is working with the City of Covington to identify improvements to the Main Street intersections with West 5th Street and West 4th Street. KYTC and Covington are also exploring gateway opportunities at the end of the Clay Wade Bailey Bridge.

# Enhancement Measures

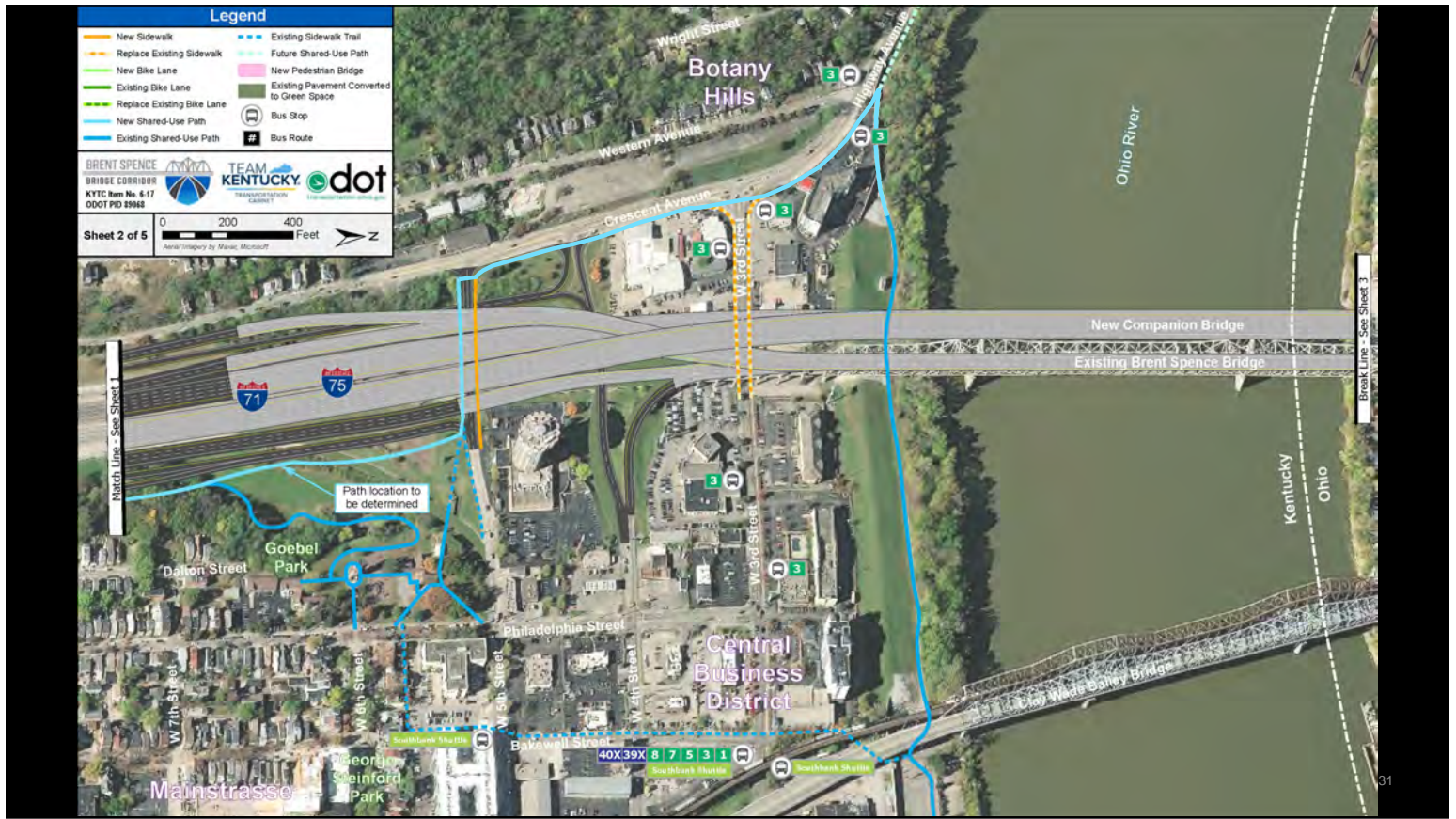
- Pedestrians and Bicycles
  - Connections across I-71 and I-75
  - New shared use paths
  - New and rebuilt sidewalks

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.



This exhibit shows some of the planned pedestrian and bicycle facilities near Mutter Gottes and the CBD. Note the new shared use paths and sidewalks along the frontage roads. Also notice the new and rebuilt sidewalks under the MLK/West 12th, Pike, and West 9th Street bridges. A new shared use path will be built under the West 9th Street bridge, which will tie into the shared use paths in Goebel Park.





Moving north, note the new shared use path in Goebel Park and under the West 5th Street bridge. The shared use path will be extended along Crescent Avenue to connect to the existing shared use path along the river. Also notice the new and rebuilt sidewalks along West 5th and West 3rd streets. Throughout Covington, the shared use paths and sidewalks will improve connections to existing transit stops.

# Enhancement Measures

- Willow Run Watershed Drainage Improvements
  - Construct a new interstate storm sewer system to remove stormwater runoff from the Willow Run combined sewer system
  - Reduce combined sewer overflows in Goebel Park



32

Existing KYTC is working with the City of Covington and Sanitation District No. 1 to address drainage issue in the project area. Existing highway runoff flows in the combined sewer system in Covington, and overflow events negatively affect residents, including in Goebel Park. KYTC is committed to separating the highway drainage from the combined sewer systems as part of the project. As part of the Brent Spence Bridge Corridor Project, KYTC will be a new separated storm sewer system along the reconstructed interstate. We are not just stiving to separate additional runoff from the combined sewer systems, but we are working to separate all runoff in in Covington, which will reduce overflow events in Goebel Park.

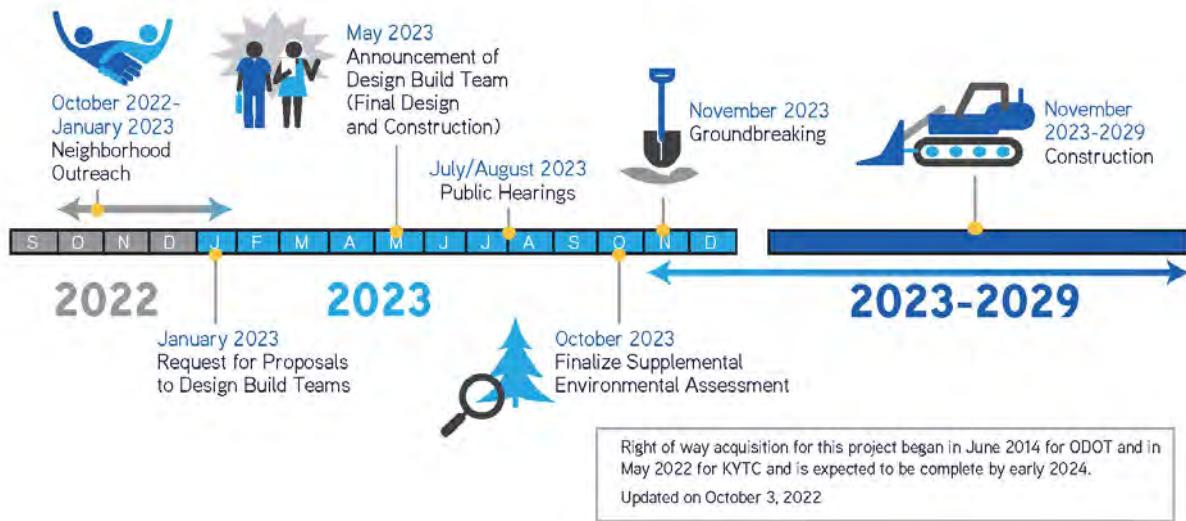


This is a view of what Goebel Park might look like once the highway improvements are constructed, the new shared use paths are built, and agreed upon landscaping is in place. The stormwater improvements should also reduce flooding from the overflows of the combined sewer system. The different roadways are also visible. Farthest left and highest is I-71/I-75. The collector-distributor road is immediately east of the highway and at a lower elevation with a retaining wall. The local street connection to West 5th Street is east of the collector-distributor road and closer to the park elevation.



This view shows how the finished project might appear when looking across I-71 and I-75 toward Mutter Gottes and the CBD.

# Schedule



This slide shows the project schedule from today to the completion of construction.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.



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Attachment 5: Exhibits

# Legend

- New Sidewalk
- - - Replace Existing Sidewalk
- New Bike Lane
- - - Existing Bike Lane
- - - Replace Existing Bike Lane
- New Shared-Use Path
- - - Existing Shared-Use Path
- - - Existing Sidewalk Trail
- - - Future Shared-Use Path
- New Pedestrian Bridge
- Existing Pavement Converted to Green Space
- Bus Stop
- # Bus Route

KYTC Item No. 6-17  
 ODOT PID 89068

Sheet 1 of 5

0      200      400  
 Feet

Aerial Imagery by Maxar, Microsoft



Path location to be determined



**Legend**

- New Sidewalk
- - - Replace Existing Sidewalk
- New Bike Lane
- - - Existing Bike Lane
- - - Replace Existing Bike Lane
- New Shared-Use Path
- - - Existing Shared-Use Path
- - - Existing Sidewalk Trail
- - - Future Shared-Use Path
- New Pedestrian Bridge
- Existing Pavement Converted to Green Space
- Bus Stop
- # Bus Route


**BRENT SPENCE BRIDGE CORRIDOR**  
 KYTC Item No. 6-17  
 ODOT PID 89068



Sheet 2 of 5

0 200 400 Feet

Aerial Imagery by Maxar, Microsoft




Break Line - See Sheet 3



## Brent Spence Bridge Corridor (Looking North from Kentucky)





# Brent Spence Bridge Corridor - Kentucky (Looking Southeast)





## Pike Street With Noise Walls (Looking Northwest) - Kentucky



## Goebel Park (Looking North) - Kentucky



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Attachment 6: Comment Forms

# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter

Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Interested in historic preservation

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



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Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: RETIRED \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

NICE PRESENTATION

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
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\_\_\_\_\_  
\_\_\_\_\_

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Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: NEIGHBORHOOD PRESIDENT

General Comments (Please attach additional pages, if needed):

Community engagement is critical & much as practical will be key to adoption. What are projected project cost and tax impact?  
If you wish to get in touch with Matt & Gabe Neighbors, please email MOGNA.COVK@gmail.com

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Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Please protect exits & entrances to & from  
North Caenloger, especially at  
4th & 5th streets

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Mailing address (or nearest cross streets): \_\_\_\_\_  
\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

*Definitely build the sound walls!!! The*

• Is it possible to increase local bridge traffic to 55 mph?  
45 mph seems too slow for the bridge

• 9<sup>TH</sup> Street already has a speed problem, especially at 9<sup>TH</sup> + Bakewell and 9<sup>TH</sup> + Willard. Can you add stop signs at each intersection to help w/ the added traffic?  
~~Can~~ I'm worried about pedestrian safety on 9<sup>TH</sup>.

• Will the combined Kyles + Dixie exit <sup>(south)</sup> have 2 lanes?  
Kyles exit is already very slow, but maybe that's because of the trucks?

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KYTC Item No. 6-17 | ODOT PID 89068



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### What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

### How many people live in your household?

- 1 - 2     3 - 5     6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

### What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

### What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

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Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



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- \$150,000 +

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- No
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- Middle school
- High school
- College/university
- Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

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All Cincinnati neighborhoods are affected. Maybe reach out to other neighborhood groups

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- Yes     No

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- Under 18    19 - 44    45 - 64    65 +

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 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: Trade School

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

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- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

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- \$150,000 +

### What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Post<sup>invites</sup> in neighborhood Facebook groups ~~and~~ to the public meetings

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

## How many people live in your household?

- 1 - 2     3 - 5     6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

## What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

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