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# Brent Spence Bridge Corridor Project Lewisburgh and Botany Hills Neighborhood Meeting Summary December 6, 2022

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## Introduction

The Lewisburg and Botany Hills neighborhood meeting was held on December 6, 2022 from 7:00 pm to 8:30 pm. The meeting was held at the Holiday Inn located at 200 Crescent Avenue in Covington, Kentucky. Flyers advertising the meeting were posted at locations throughout the Lewisburg and Botany Hills neighborhoods. In addition, the City of Covington provided contact information for some local schools, businesses, and residents in both neighborhoods. Flyers advertising the meeting were emailed to these individuals with a request to pass the information to others in their neighborhoods. Finally, a member of the Project Advisory Committee (PAC) who is also a resident of Lewisburg distributed English and Spanish flyers to contacts throughout the neighborhoods (see Attachment 1). Attendees at the meeting included the project team, residents, and business owners. In total, 23 people attended the meeting, including 6 individuals from the project team (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). During the presentation, the project team responded to questions and comments offered by those present. Exhibits showing the project corridor and renderings of what the project will look like once it is built were available for viewing before and after the meeting (see Attachment 5). Members of the project team were available to answer questions and receive feedback while attendees reviewed the exhibits. Written comment forms, a demographic survey, and a postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. One (1) comment form and one (1) demographic survey were returned at the meeting.

## Demographics

The demographic survey that was returned indicated the individual was an older adult (over 65 years of age) but not a member of any other underserved population groups (see Attachment 6).

A write in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. No comments were provided.

## Comments

Attendees posed a number of questions during the course of the presentation/meeting. Table 1 is provided as a summary of the questions and answers.



**Table 1: Q & A from Session**

Question	Project Team Response
Will the access road to the Clay Wade Bailey Bridge be from the express lanes?	Yes, the access to the Clay Wade Bailey Bridge will be provided from I-75.
Can you get on I-75 north from the existing bridge?	Yes. Travelers can follow the collector-distributor system north through downtown Cincinnati to access I-75 north.
How high is a typical noise wall?	Noise walls vary in height. A typical wall might be 18 to 20 feet high. The proposed noise walls in Lewisburg and Botany Hills range from 14 to 24 feet in height.
If a noise wall is not built, why is a fence needed along Crescent Avenue?	A fence is needed for safety to protect pedestrians from the drop off that will be present with the retaining wall along Crescent.
Has there been consideration for transit? Has KYTC coordinated with TANK and other transit agencies for routes on either the new companion or existing Brent Spence Bridge?	TANK is on the Project Advisory Committee (PAC) and has been providing input throughout the course of the project. TANK is ultimately responsible for determining their routing.
Will the West 4 <sup>th</sup> Street ramp be opened again when the project is built? Will the Texas Turnaround be removed? If so, why is it being built?	Yes, the West 4 <sup>th</sup> Street ramp will be opened back up to vehicular traffic once the project is built. The Texas Turnaround will be removed. It is being built as an interim safety project to address safety issues at the West 4 <sup>th</sup> Street ramp. It will also help with maintenance of traffic during construction of the Brent Spence Bridge Corridor Project.
If I'm going from West 4 <sup>th</sup> Street in Covington to Ohio for shopping, what roads would I take?	One option would include taking the local (existing) bridge to I-71 or onto the collector-distributor system to access I-75. Another option would include taking the Clay Wade Bailey Bridge to I-75.

The survey form returned at the meeting did not include any comments. This individual provided contact information and will be added to the project mailing list.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.



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## Attachment 1: Advertising Materials

## **Lewisburg/Botany Hills Neighborhood Outreach Meeting Advertising**

On November 16, 2022, personnel from HNTB visited the Lewisburg/Botany Hills neighborhood to distribute flyers advertising the outreach meeting scheduled for December 6, 2022 at 7:00pm.

The following businesses/organizations agreed to post a flyer:

- West Covington Baptist Church | 1003 Highway Avenue
- Yabbi's Corner Ethiopian Market | 1255 Parkway
- Liquor City Uncorked | 501 Crescent Avenue
- Save on Cigs | 670 West 3<sup>rd</sup> Street
- DEP's Fine Wine and Spirits | 670 West 3<sup>rd</sup> Street
- Family Grocery | 940 Western Avenue
- Be Concerned: The People's Pantry and Thrift Shop | 1100 West Pike Street

Flyers were left in mailboxes at the following locations:

- St. John Congregational Church | 1235 Highway Avenue
- Covington Fire Department Company 2 | 1252 Parkway Avenue
- St. Ann Roman Catholic Church and School | 1274 Parkway Avenue
- Church of the Living God | 1279 Parkway Avenue
- VFW Post 7099 | 1201 Kelly Furnish Street

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**From:** Sherry Kish  
**Sent:** Tuesday, November 15, 2022 2:59 PM  
**To:** Sherry Kish  
**Cc:** Seth Turner; Jodi Heflin  
**Subject:** RE: Botany Hills and Lewisburg Community Meeting  
**Attachments:** 11-08-2022\_Invitation Flyer\_Lewisburg and Botany Hills.pdf

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

Good Afternoon!

I wanted to follow back up and let you know that we have scheduled a meeting for the Botany Hills and Lewisburg neighborhoods on December 6<sup>th</sup> at the Holiday Inn Express. I've attached a flyer for the meeting in case you are able to share it with residents from these two neighborhoods. Suggested sharing is with neighborhood email lists, PTA/PTO members or posting it in popular businesses. Jodi Heflin on our project team is going to be asking a few businesses to post this flyer in their location tomorrow morning – just in case you see one 😊

Any help you can provide, especially attending, is greatly appreciated!

Thanks,  
Sherry

**Sherry R. Kish**

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**From:** Sherry Kish  
**Sent:** Monday, October 24, 2022 12:02 PM  
**To:** Sherry Kish  
**Subject:** Botany Hills and Lewisburg Community Meeting

Good Afternoon!

I received your contact information from the Center for Great Neighborhoods as residents of either the Lewisburg or Botany Hills neighborhoods. HNTB has been contracted by the Kentucky Transportation Cabinet and the Ohio Department of Transportation and to assist with public outreach for the Brent Spence Bridge project. We are looking to schedule a meeting with your neighborhood's residents the week of December 5<sup>th</sup> to help provide an overview of the project and host a community conversation to give residents an opportunity to ask questions or share comments. We are reaching out for help as we set these meetings. Can you help us with the following areas:

1. Share if is an existing neighborhood group meeting that we can join (even if it's not during the week of December 5<sup>th</sup>)
2. Suggest an appropriate location to host a neighborhood meeting (maybe 25-50 people)
3. Identify the best ways to advertise the meeting to the Lewisburg and Botany Hills residents

We appreciate any information you can share about your neighborhood so we can make sure this important information is available to your neighbors. Finally, please let me know which neighborhood you belong to when you respond.

Thanks,  
Sherry

**Sherry R. Kish**

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



**TEAM**  
**KENTUCKY**  
TRANSPORTATION  
CABINET



**INVESTING IN LOCAL COMMUNITIES.  
GROWING AMERICA'S ECONOMY.**

## You're Invited

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of **neighborhood outreach meetings** for the Brent Spence Bridge (BSB) Corridor Project

**Purpose:** To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team

**When:** December 6, 2022 | 7:00pm

**Where:** Holiday Inn Express  
200 Crescent Avenue, Covington, KY 41011

## Hope to see you there!

If you need assistance attending this meeting, please call Stacey Hans at [\(859\) 341-2700](tel:8593412700).

View the most recent project newsletter at the following link:  
<https://mailchi.mp/aae28ef3744d/brent-spence-bridge-e-newsletter-16606655>

BRENT SPENCE  
BRIDGE CORRIDOR



TEAM  
KENTUCKY  
TRANSPORTATION  
CABINET



INVIRTIENDO EN LAS COMUNIDADES LOCALES.  
HACIENDO CRECER LA ECONOMÍA ESTADOUNIDENSE.

## Invitación

El Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC) están llevando a cabo una serie de reuniones vecinales para el Proyecto del corredor del puente Brent Spence

**Objetivo:** El propósito de las reuniones será presentar los planes actuales para el corredor del puente Brent Spence y obtener comentarios sobre cómo el proyecto puede afectar a las personas que viven cerca del área del proyecto.

**Fecha y hora:** el 6 diciembre de 2022 | 7:00pm

**Ubicación:** Holiday Inn Express  
200 Crescent Avenue, Covington, KY 41011

## Esperamos verte ahí!

Si necesita ayuda para asistir a esta reunión, llame a Stacey Hans al [\(859\) 341-2700](tel:8593412700).

Boletín electrónico del corredor del puente Brent Spence:  
<https://mailchi.mp/aae28ef3744d/brent-spence-bridge-e-newsletter-16606655>



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Attachment 2: Sign-In Sheet





**BRENT SPENCE BRIDGE CORRIDOR PROJECT**

Neighborhood Outreach Meeting – December 6, 2022

Name	Representing	Address	Phone	Email
1. John Beatty & Tim Schuch	Lewisburg Properties			
2. Johnny Arnold	ODOT DB			
3. Scott Aeffler	AMTB			
4. Mark Becker	AMTB			
5. Mour Dior	FHWA-KY			
6. Nick Popper	Boats Charters			
7. Le Rubin	to			
8. Don Corkey Jr	CORKEN STEEL			
9. Esther Johnson	Pauline Boyd			
10. Deb + Greg Reder	DC Morrison			
11. Paul Corkey	Corkey Feed			
12. Steven Hall	Lewisburg			
13. Rums Kelly	Lewisburg			
14. Chai a Popper	Lewisburg			
15. DAVID Hoge	Lewisburg			
16. JAMES DILLON	Lewisburg			
17. Nick Wengren	Lewisburg			
18. JOSHUA MARTIN	POWERTON			
19. Julie Under	KYTC			
20. Stacey Hayes	KYTC			
21. Gary Valentine	KYTC			

Please print clearly. Contact information is optional but must be included if you wish to receive updates.



Attachment 3: Photographs





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[Attachment 4: Presentation](#)

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



# Lewisburg and Botany Hills Neighborhood Meeting (KY)

December 6, 2022



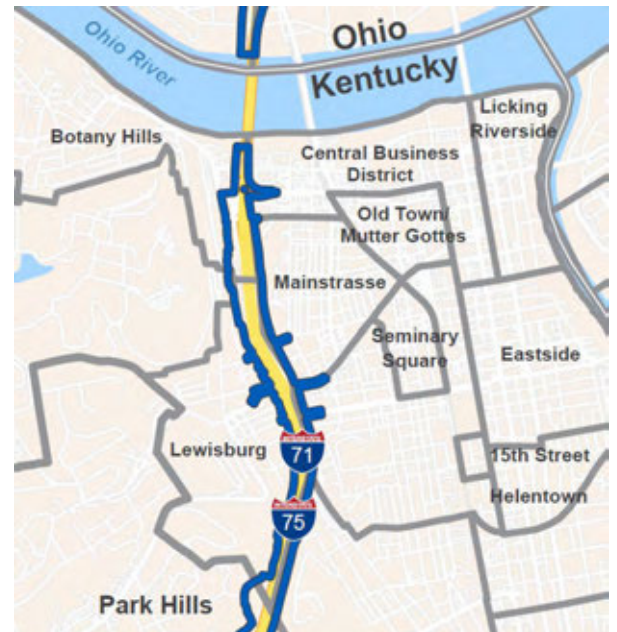
INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with remarks by the City of Covington (5 min) and introductions of the Project Team in attendance (2 min).

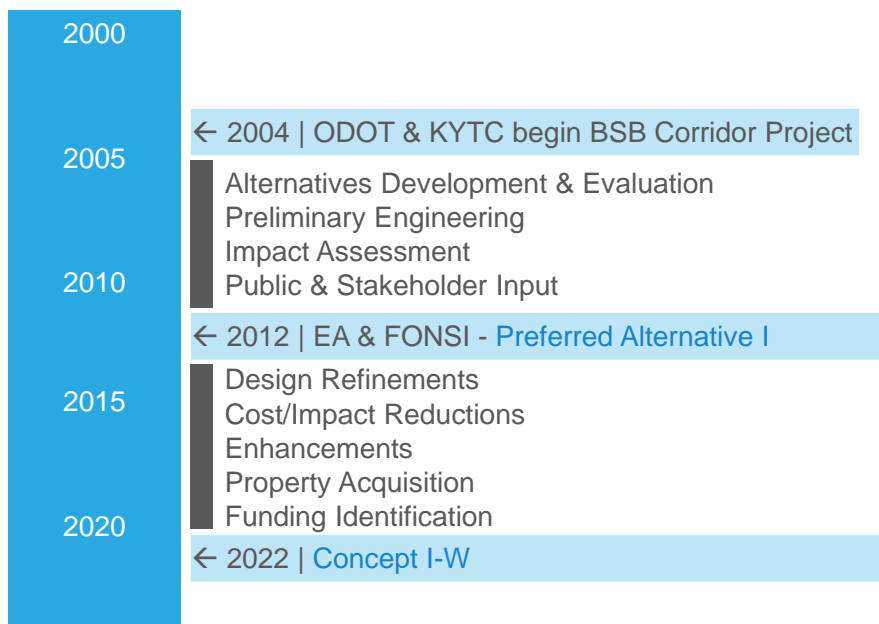
# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the Lewisburg and Botany Hills neighborhoods the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Lewisburg/Botany Hills area
  - Discussion/feedback from Lewisburg/Botany Hills residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Lewisburg and Botany Hills neighborhoods. Most importantly, tonight we're focusing on hearing from the residents of Lewisburg and Botany Hills. We're here to answer your questions and to listen to your feedback about the project.

# Project History



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For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

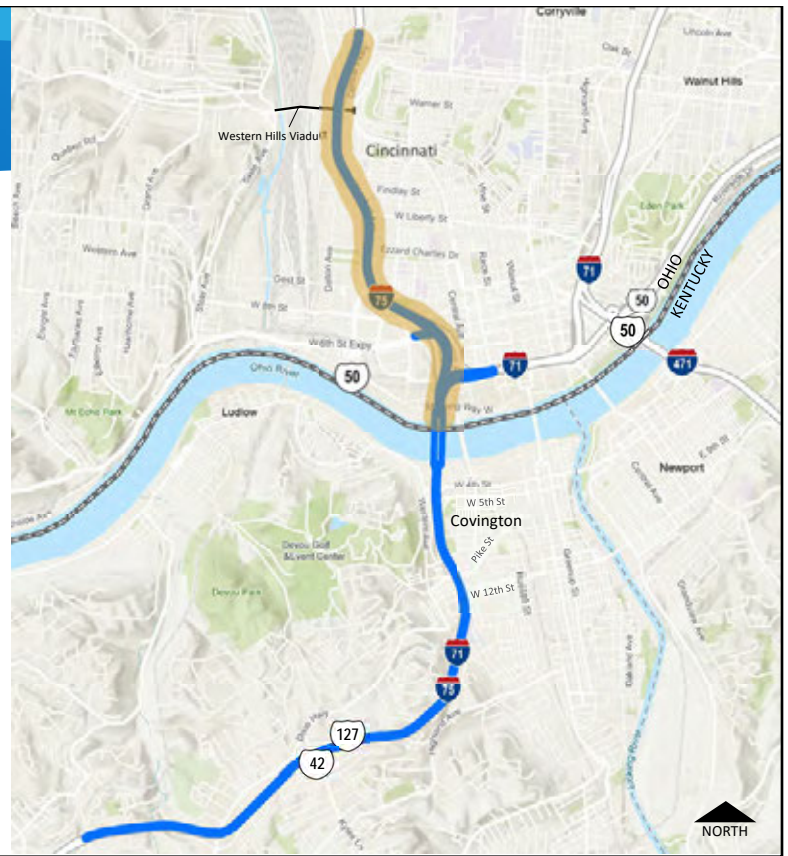
Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2022, KYTC has begun purchasing the land needed to build the project.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E

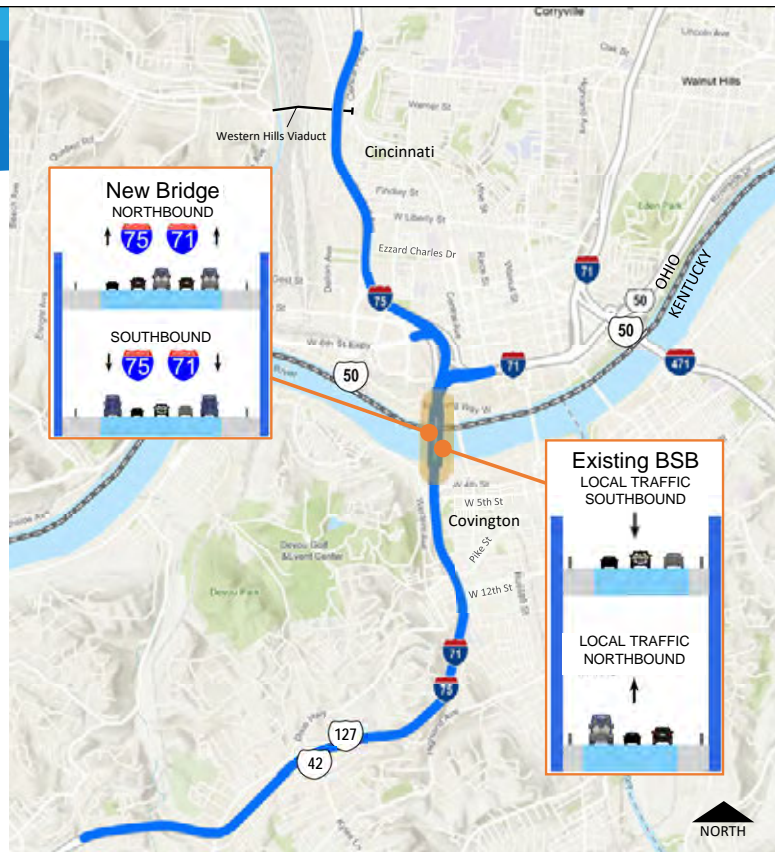


In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

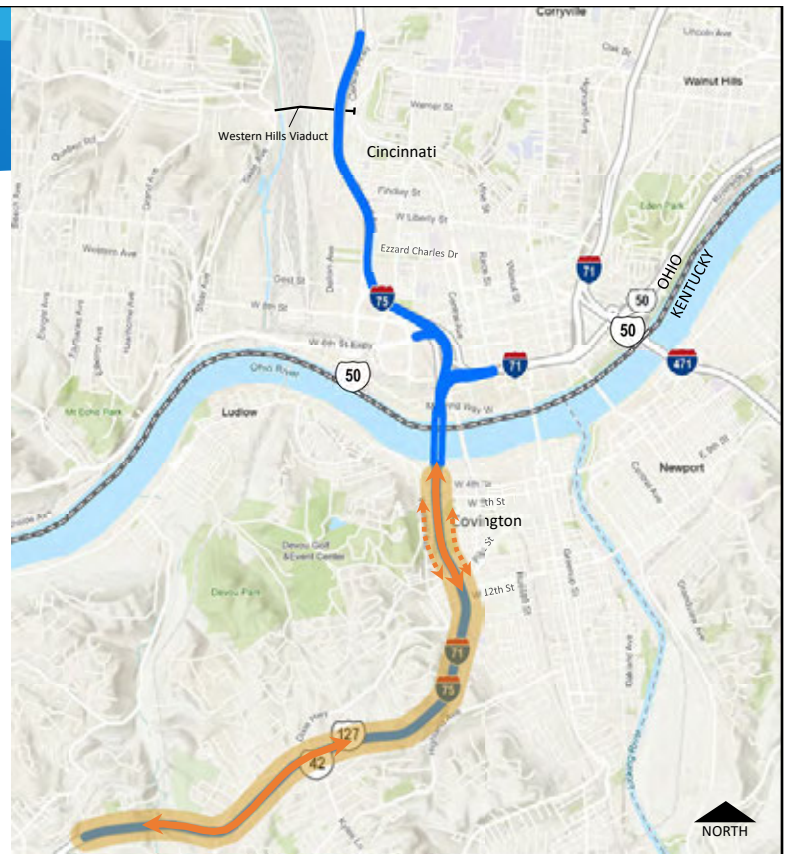
The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.



# Project Description

## Kentucky

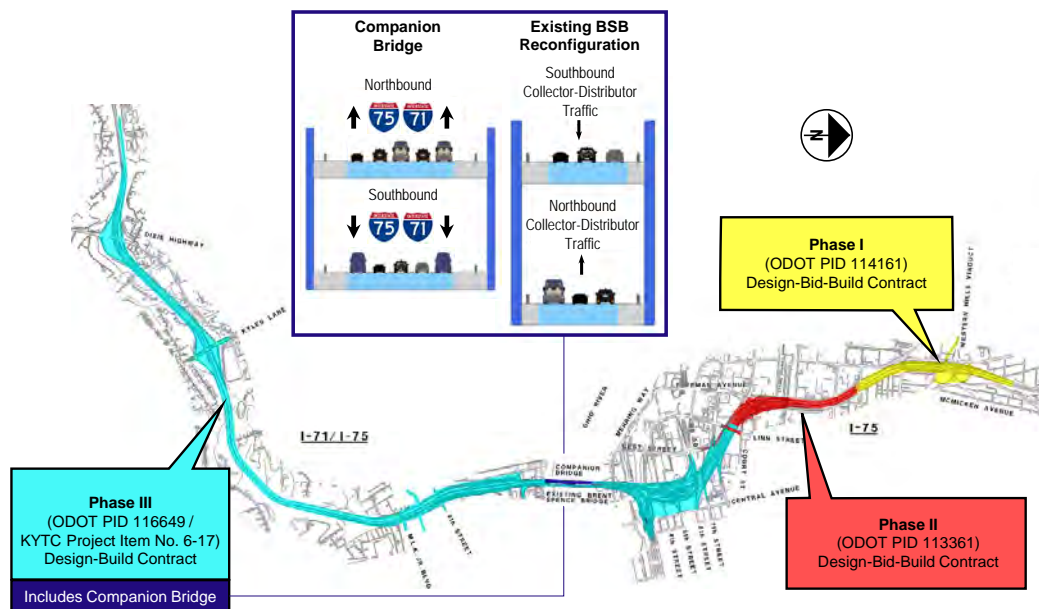
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles Lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

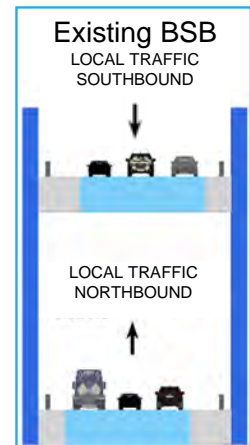
# Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

# What Has Changed?

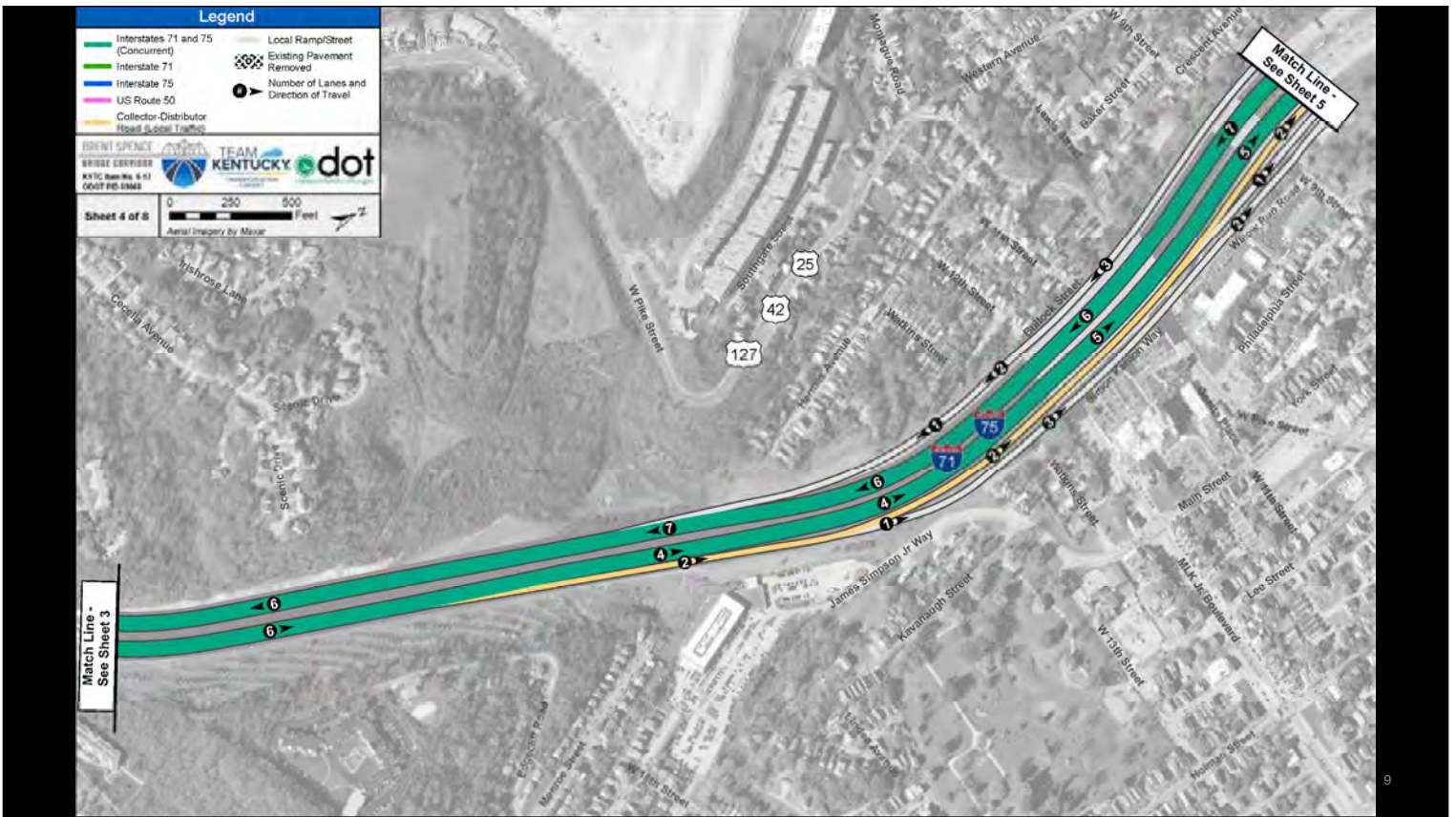
- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



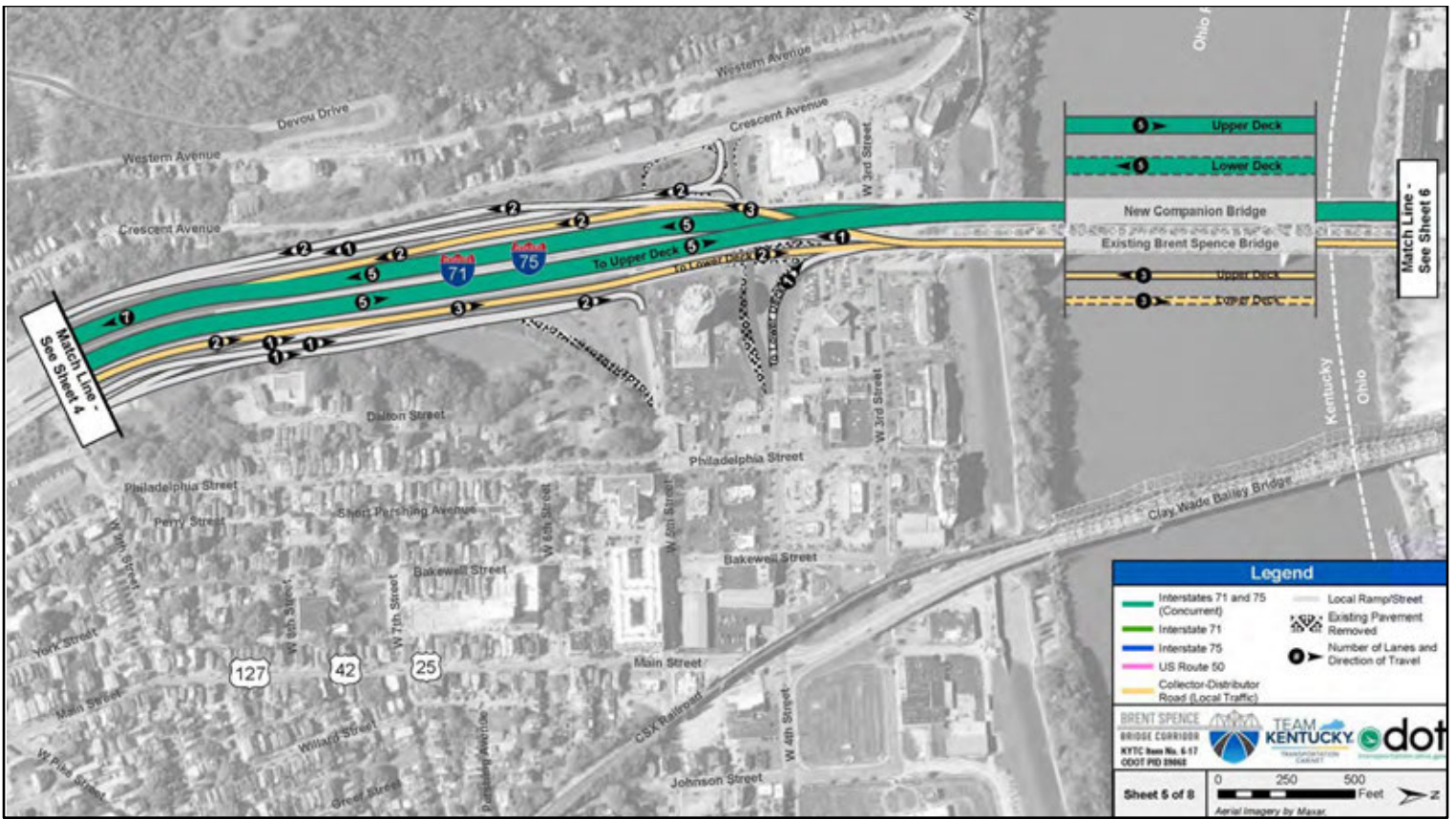
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As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

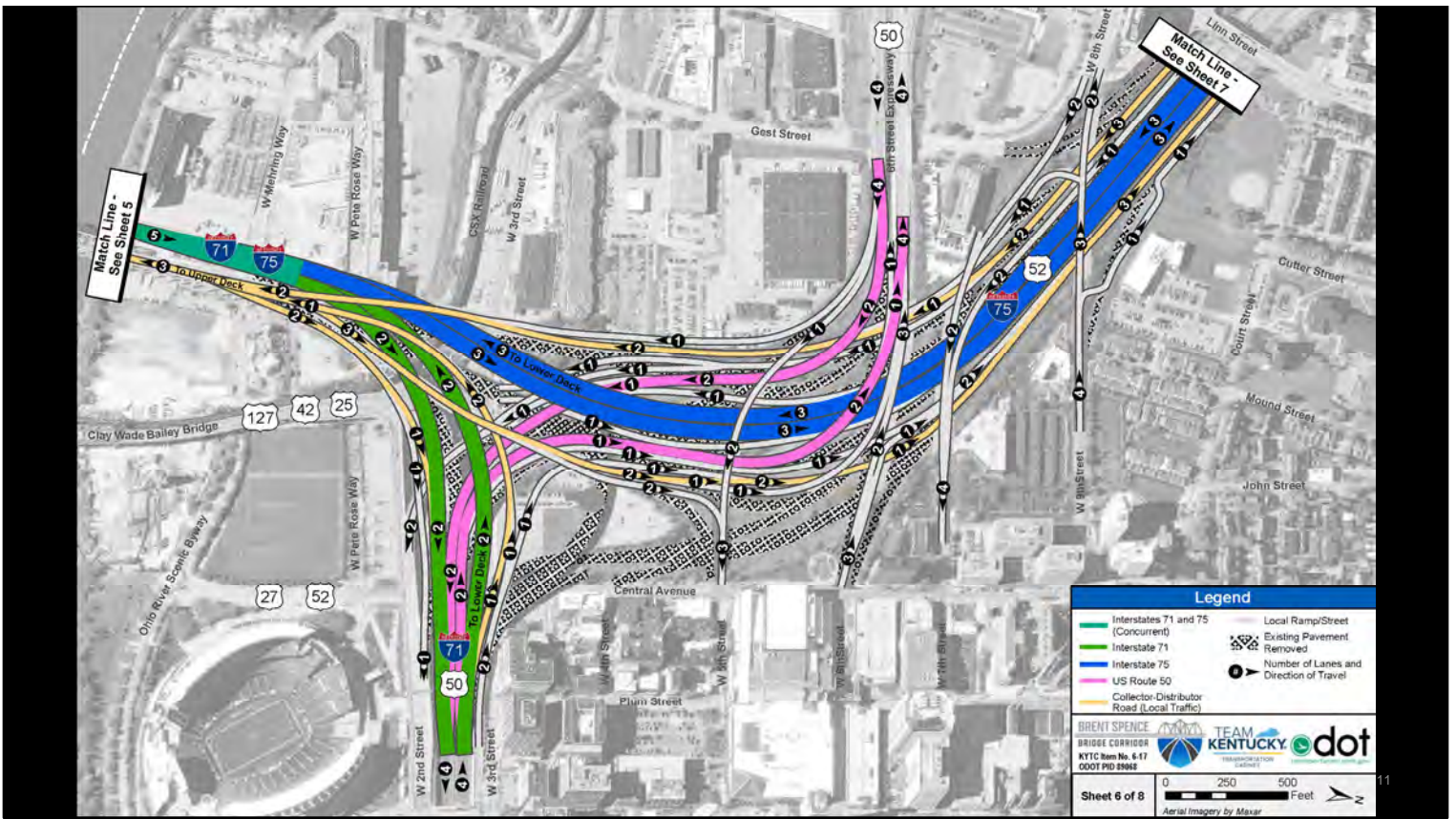
Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will move in downtown Cincinnati. The dark blue shows I-75. The dark green shows I-71. The pink shows US-50. The orange shows the collector-distributor roads. The grey shows the local access ramps.



This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

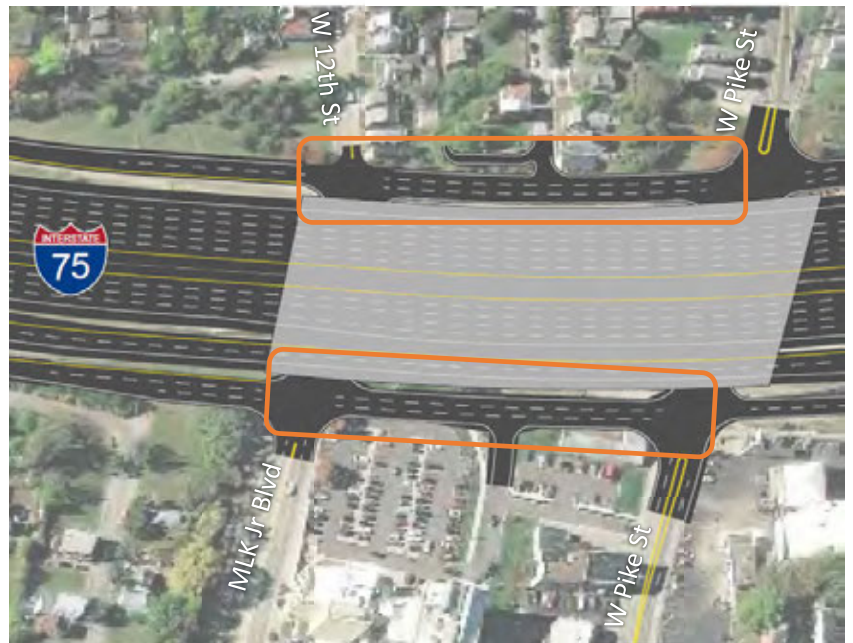


Here's another view from Covington near Lewisburg and Botany Hills.



# What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 40+ then vs. 4 now
  - Commercial | 6 then vs. 5 now

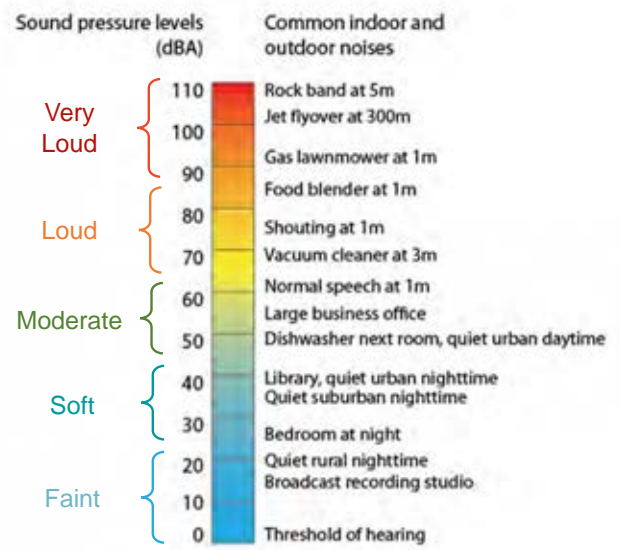


Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet. In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound



Source: Minnesota Pollution Control Agency

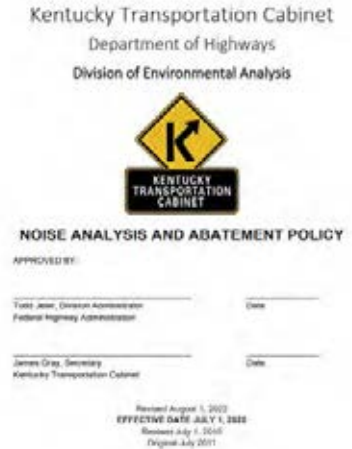
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Now let's dig into how the project will impact Lewisburg and Botany Hills, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

## KYTC Noise Policy

- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.



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KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dbA. We also determine if there is a substantial increase – specifically a 10 dbA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dbA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

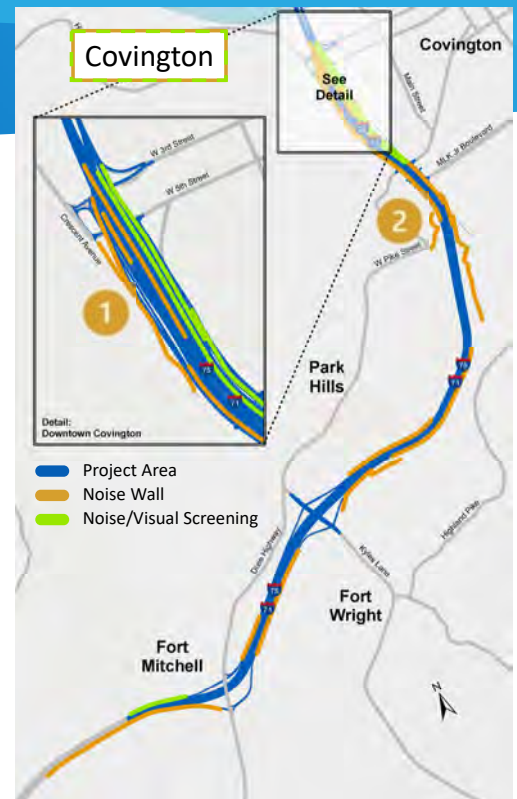
If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

# Noise

## Noise walls reasonable/feasible per KYTC Noise Policy

- 1 • Western Ave & Crescent Ave (Botany Hills/Lewisburg)
  - Without noise wall = 70 dBA
  - With noise wall = 67 dBA
- 2 • Lewisburg Historic District (Lewisburg)
  - Without noise wall = 70 dBA
  - With noise wall = 64 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office

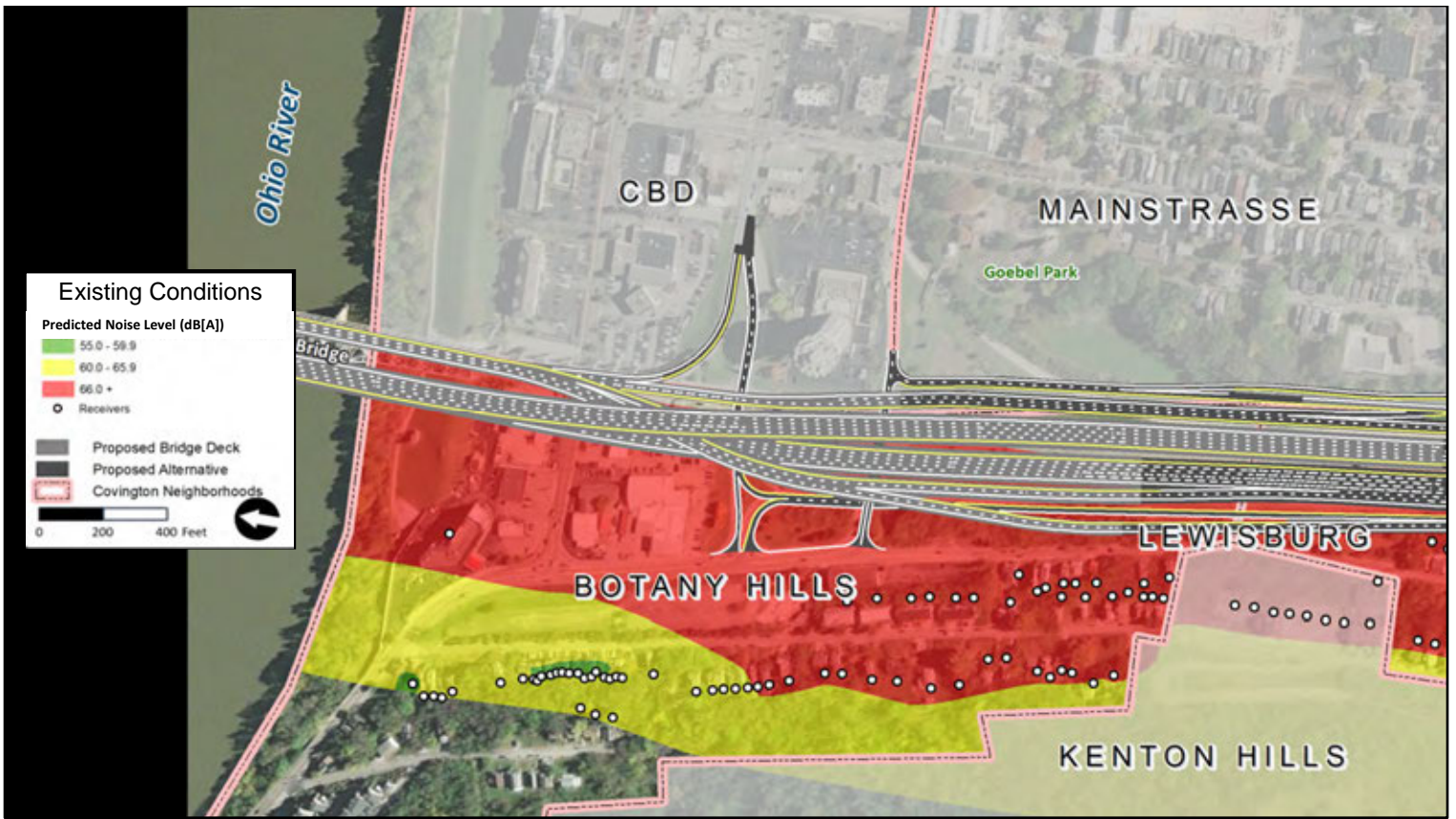


In the Botany Hills/Lewisburg neighborhoods, a noise wall system (shown by the number 1) was found to be reasonable and feasible. It includes several noise walls along Crescent Avenue, the Interstate, and a ramp to reduce noise for residencies along Western and Crescent avenues. The average exterior noise levels are predicted to be around 70 decibels without the noise walls and 67 decibels with the noise walls. These sound levels are averages for all the people who will benefit from the walls. People living closer to the highway will have higher noise levels but will also experience larger noise reductions if a noise wall is built.

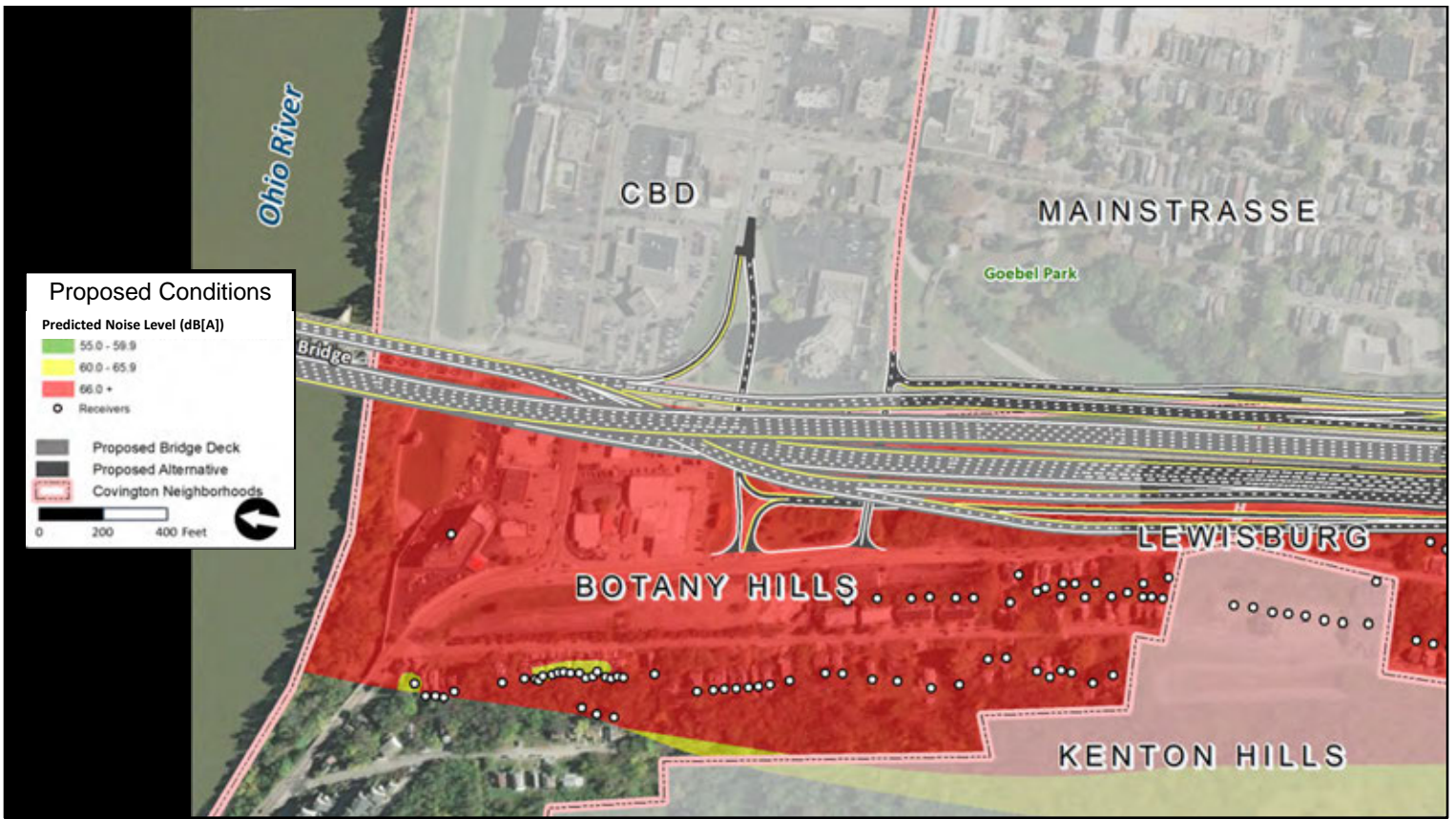
The noise wall system extends further south through the Lewisburg Historic District along the Interstate (shown by the number 2). In this area, the average exterior noise levels are predicted to be 70 decibels without noise walls and about 64 decibels with noise walls.

For reference, the average exterior sound levels with noise walls in west Covington are predicted to be near what you'd experience in normal conversation.

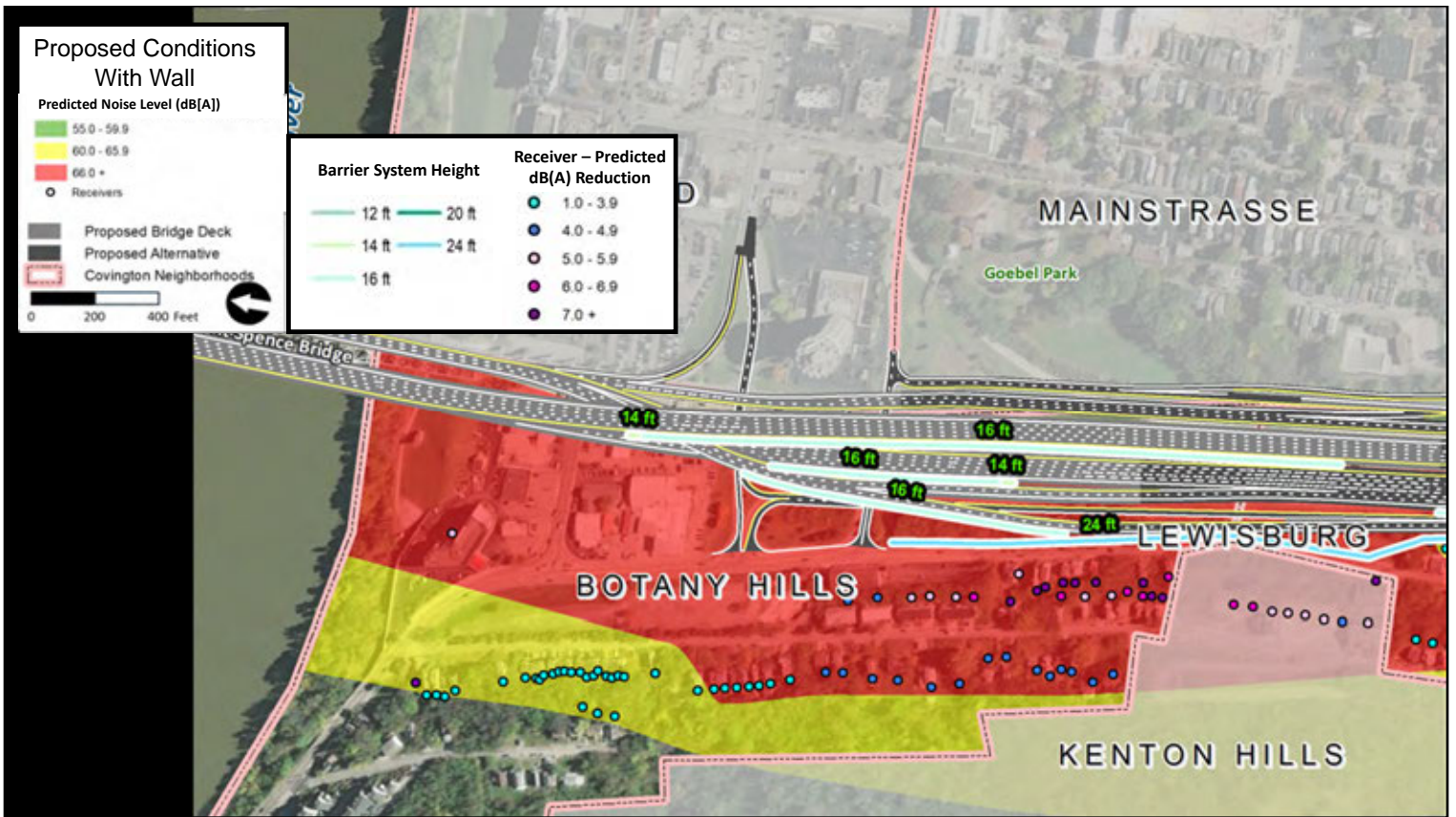




This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.

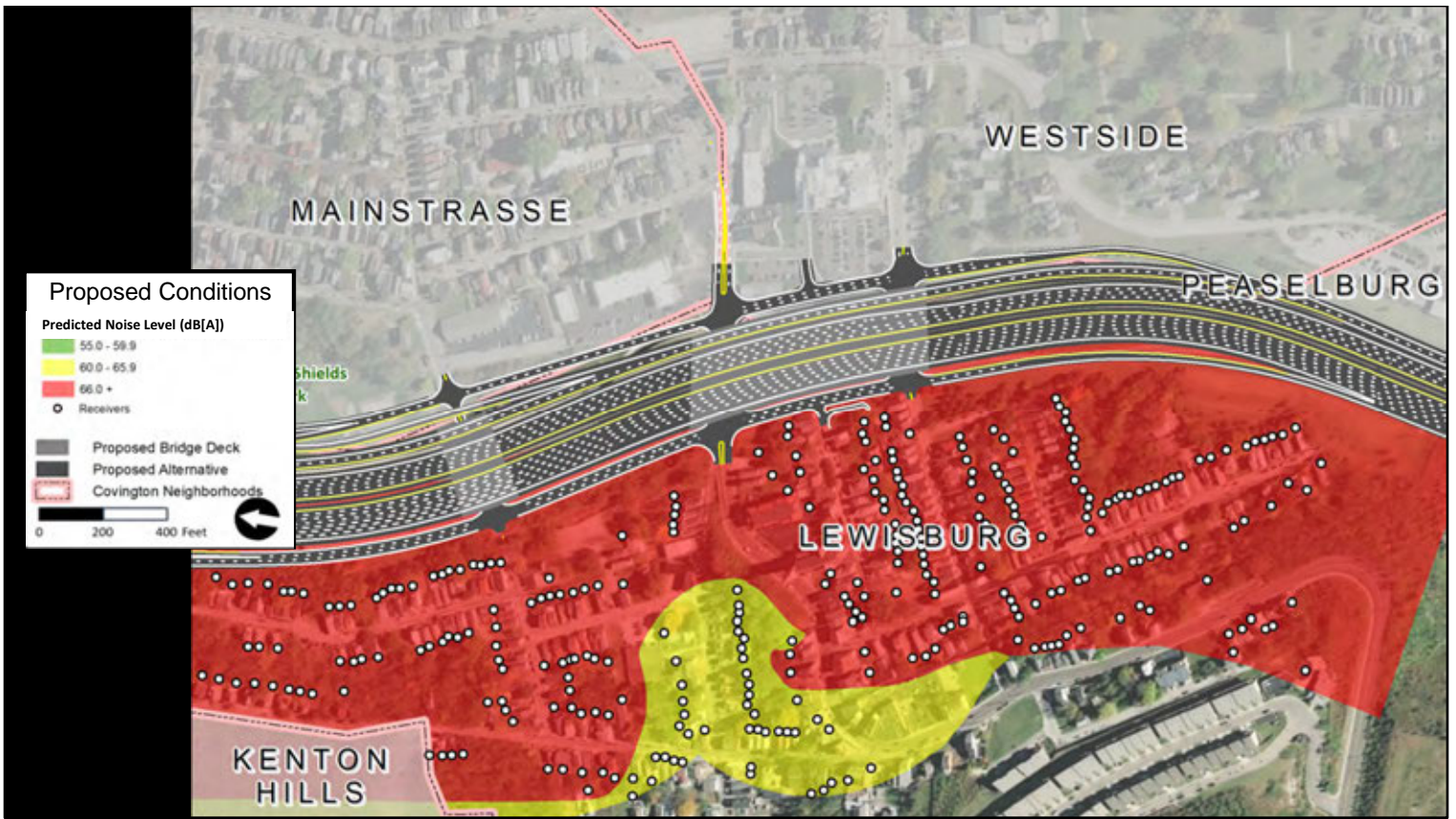


This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

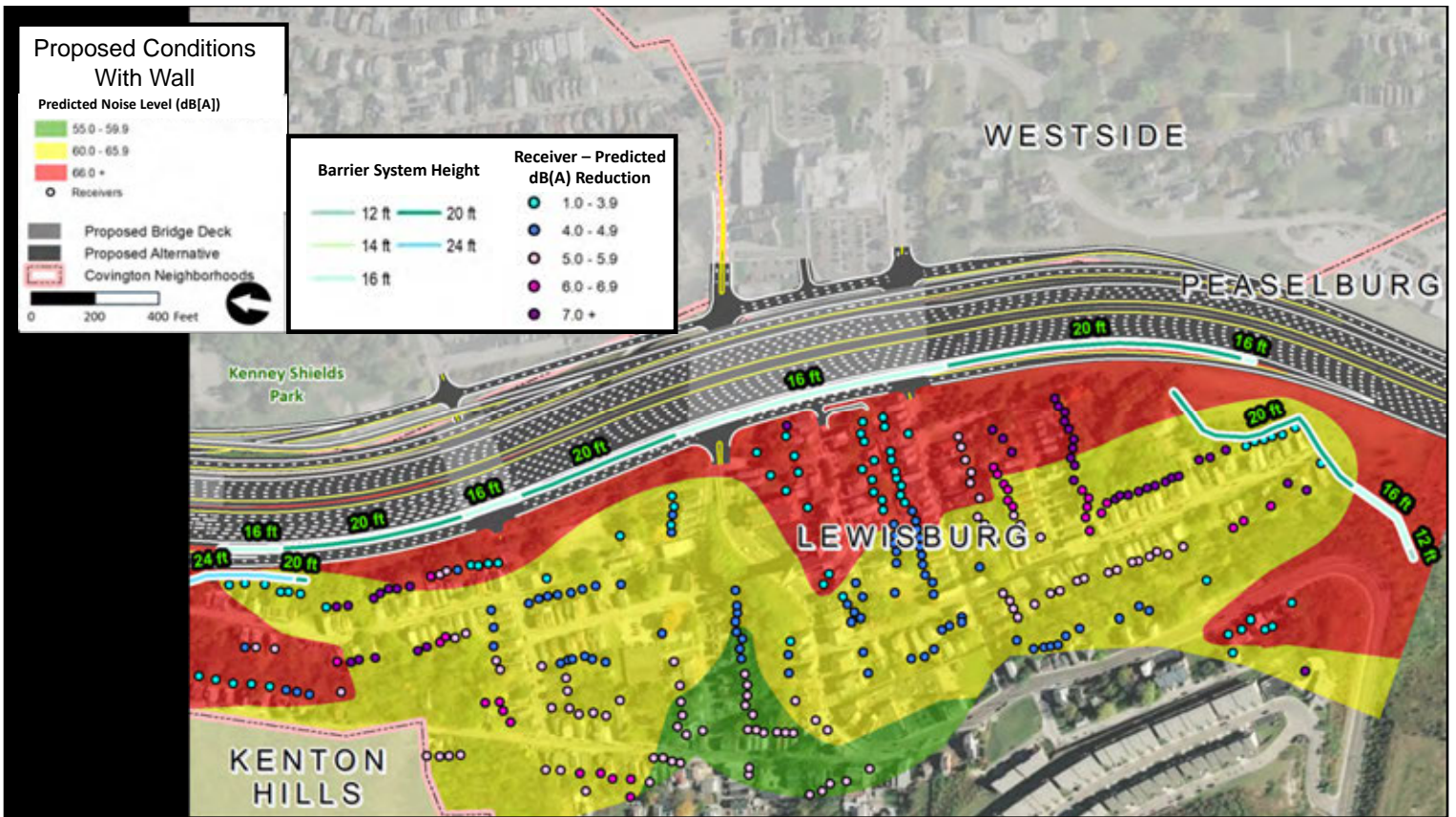




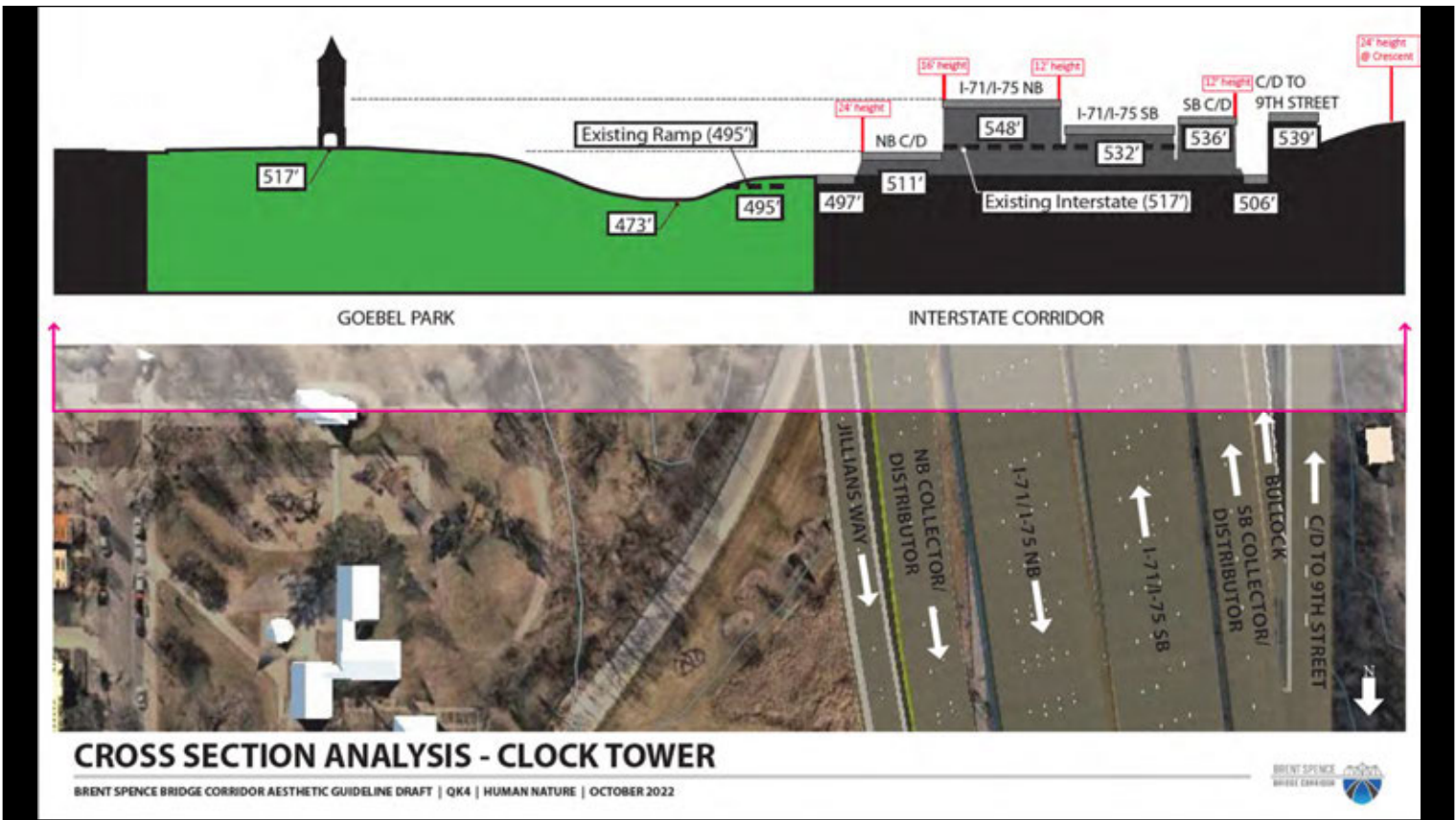
This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



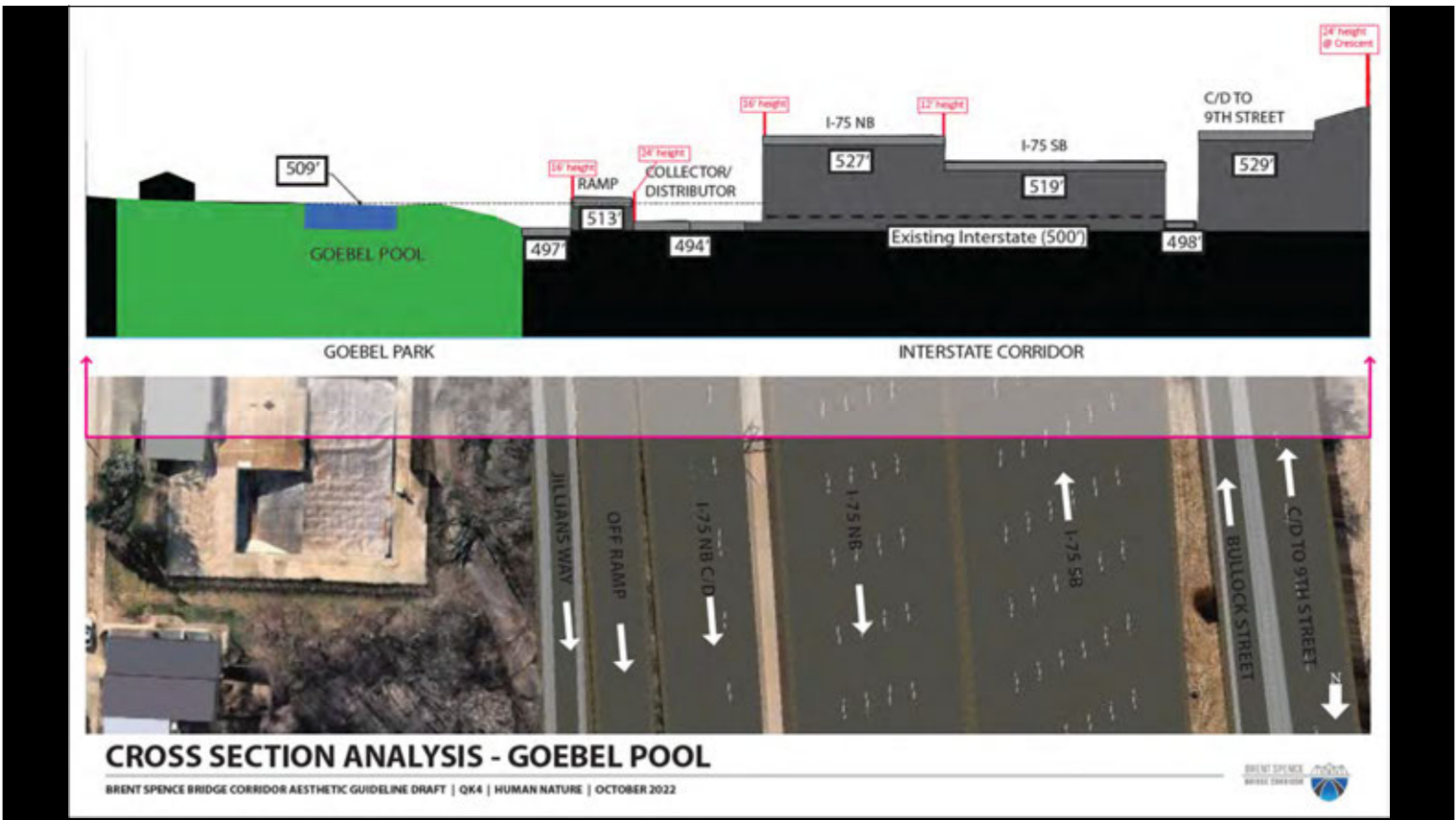
This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



This slide shows a cross-section of the roadway and noise barriers at the clock tower to demonstrate the overall schematic of the roadway and the height of potential noise walls.



This slide shows a cross-section of the roadway and noise barriers at the Goebel pool to demonstrate the overall schematic of the roadway and the height of potential noise walls.

# Noise



Pike Street Without Noise Wall (looking east)  
71 dBA (average)



Pike Street With Noise Wall (looking east)  
64 dBA (average)

To the left is a drawing showing what the finished project might look like at Pike Street if a noise wall is not built. To the right is a drawing showing what Pike Street might look like with a noise wall. These photos are looking east on Pike Street.

# Noise



Crescent Avenue Without Noise Wall  
70 dBA (average)



Crescent Avenue With Noise Wall  
67 dBA (average)

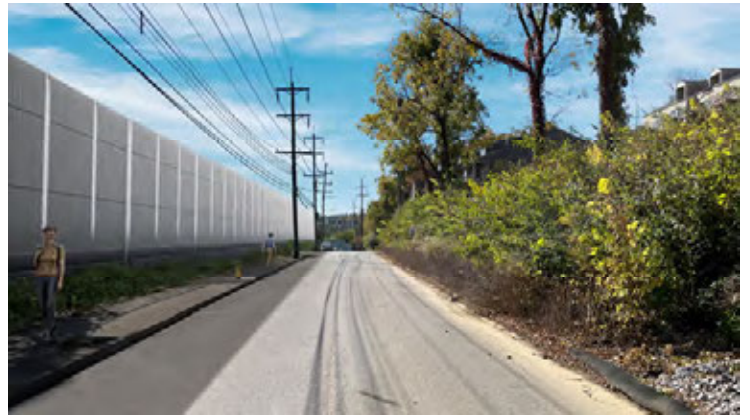
28

To the left is a drawing showing what the finished project will look like from Crescent Avenue if a noise wall is not built. To the right is a drawing showing what Crescent Avenue will look like with a noise wall. These views are looking north.

# Noise



Crescent Avenue Without Noise Wall  
70 dBA (average)



Crescent Avenue With Noise Wall  
67 dBA (average)

29

Here are more views showing what Crescent Avenue will look like with and without noise walls. These views are looking south.



# Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. As we discussed earlier, KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.

# Enhancement Measures

- Working with Covington to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, and pedestrian paths
- Pedestrians and Bicycles
  - Connections across I-71 and I-75
  - New shared use paths
  - New and rebuilt sidewalks

31

In addition to coordinating work in Goebel Park, KYTC is also working with Covington to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.





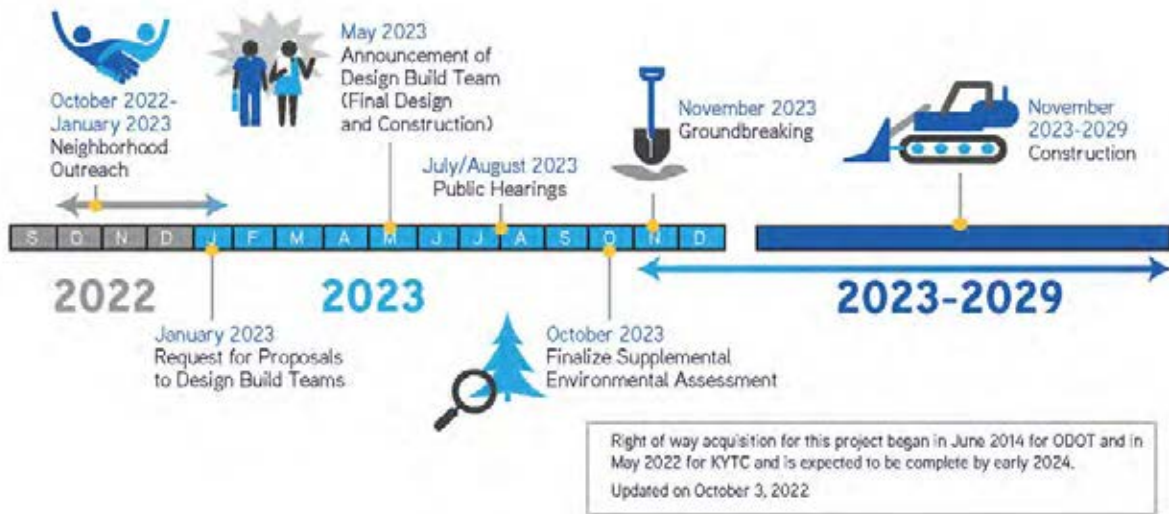


This is a view of what Goebel Park might look like once the highway improvements are constructed, the new shared use paths are built, and agreed upon landscaping is in place.



This view shows how the finished project might appear looking down along Botany Hills and Lewisburg.

# Schedule



This slide shows the project schedule from today to the completion of construction.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.





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Attachment 5: Exhibits

# Legend

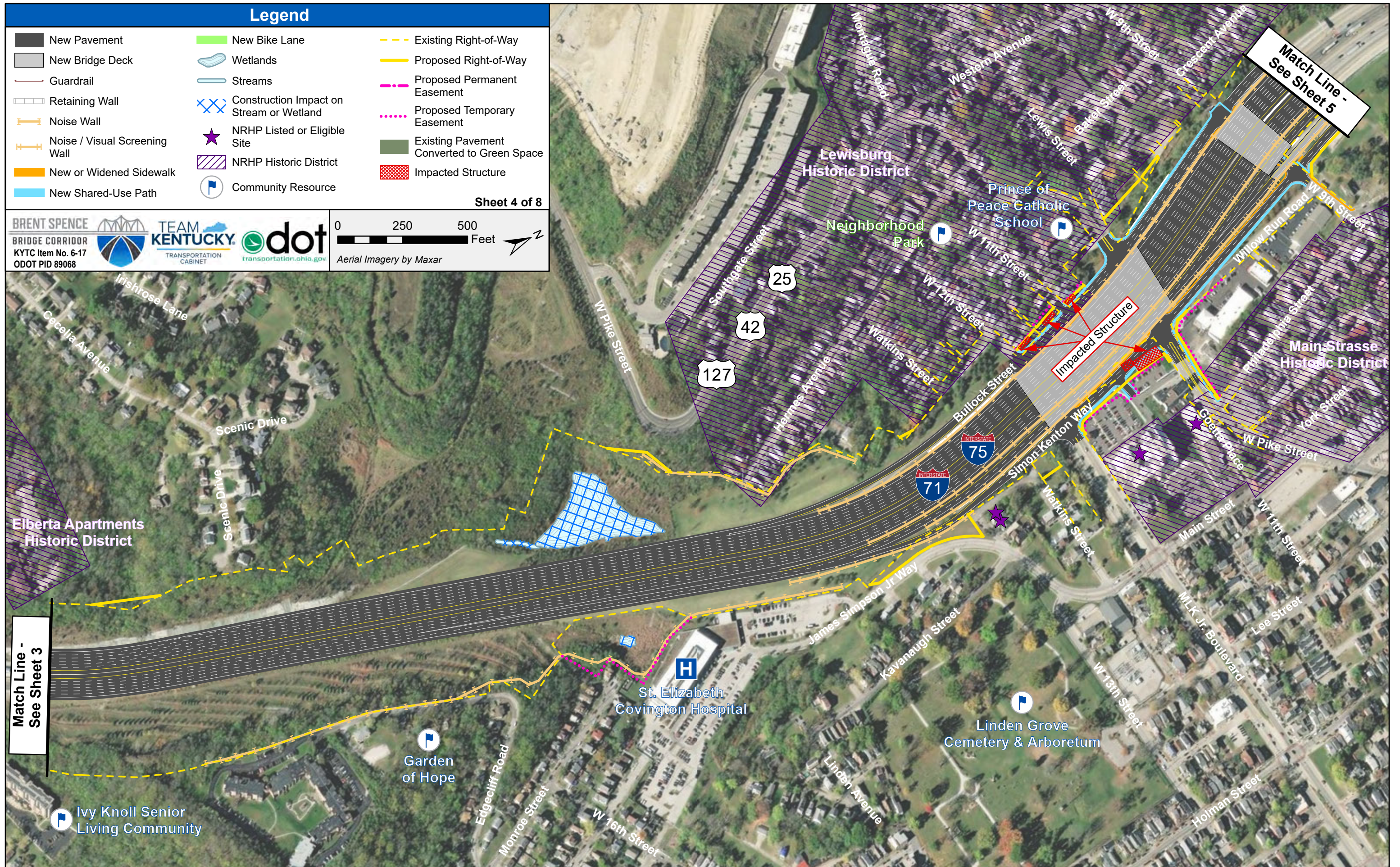
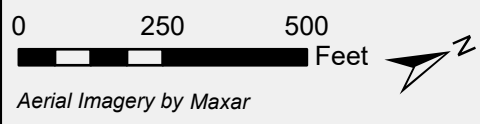
- |  |                               |  |  |  |  |
|--|-------------------------------|--|--|--|--|
|  | New Pavement                  |  | New Bike Lane                            |  | Existing Right-of-Way                      |
|  | New Bridge Deck               |  | Wetlands                                 |  | Proposed Right-of-Way                      |
|  | Guardrail                     |  | Streams                                  |  | Proposed Permanent Easement                |
|  | Retaining Wall                |  | Construction Impact on Stream or Wetland |  | Proposed Temporary Easement                |
|  | Noise Wall                    |  | NRHP Listed or Eligible Site             |  | Existing Pavement Converted to Green Space |
|  | Noise / Visual Screening Wall |  | NRHP Historic District                   |  | Impacted Structure                         |
|  | New or Widened Sidewalk       |  | Community Resource                       |  |  |
|  | New Shared-Use Path           |  |  |  |  |

Sheet 4 of 8

**BRENT SPENCE**  
BRIDGE CORRIDOR  
KYTC Item No. 6-17  
ODOT PID 89068

**TEAM KENTUCKY**  
TRANSPORTATION CABINET

**dot**  
transportation.ohio.gov





Match Line -  
See Sheet 4

Match Line -  
See Sheet 6

Area to be  
incorporated into  
Goebel Park

### Legend

	New Pavement		New Bike Lane		Existing Right-of-Way
	New Bridge Deck		Wetlands		Proposed Right-of-Way
	Guardrail		Streams		Proposed Permanent Easement
	Retaining Wall		Construction Impact on Stream or Wetland		Proposed Temporary Easement
	Noise Wall		NRHP Listed or Eligible Site		Existing Pavement Converted to Green Space
	Noise / Visual Screening Wall		NRHP Historic District		Impacted Structure
	New or Widened Sidewalk		Community Resource		
	New Shared-Use Path				

<p><b>BRENT SPENCE</b> BRIDGE CORRIDOR KYTC Item No. 6-17 ODOT PID 89068</p>		<p>0 250 500 Feet</p> <p>Aerial Imagery by Maxar</p>
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Match Line - See Sheet 5

Match Line - See Sheet 7



## Brent Spence Bridge Corridor - Kentucky (Looking Southeast)











## Pike Street With Noise Wall (Looking East) - Kentucky



## Crescent Avenue Without Noise Wall - Kentucky (Looking South)



## Crescent Avenue With Noise Wall - Kentucky (Looking South)



## Crescent Avenue Without Noise Wall - Kentucky (Looking North)



## Crescent Avenue With Noise Wall - Kentucky (Looking North)



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Attachment 6: Comment Forms



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

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Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

