

BRENT SPENCE
BRIDGE CORRIDOR



BRENT SPENCE BRIDGE CORRIDOR PROJECT

PUBLIC INVOLVEMENT SUMMARY

Part 1

KYTC PROJECT ITEM NO. 6-17 | ODOT PID 89068
JANUARY 11, 2024

This report consists of 4 parts. Click to access [Part 2](#), [Part 3](#), and [Part 4](#).



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1. INTRODUCTION

Recognizing the need to improve the Brent Spence Bridge (BSB) corridor, on October 14, 2004, the Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT) formally entered into an agreement to jointly develop and deliver a project to replace the existing BSB over the Ohio River. KYTC and ODOT developed a range of alternatives for improving the I-71/I-75 corridor in Kentucky and Ohio. Through a series of preliminary engineering and planning studies coupled with public and stakeholder involvement, KYTC and ODOT narrowed the range of alternatives to two feasible alternatives, which were evaluated in the 2012 Environmental Assessment (EA). In August 2012, FHWA issued a Finding of No Significant Impact (FONSI) identifying Alternative I as the selected alternative for the BSB Corridor Project. Reevaluations of the EA/FONSI subsequently completed in 2015 and 2018 concluded that the 2012 FONSI remained valid.

Shortly following the approval of the 2012 EA/FONSI, KYTC and ODOT began efforts to identify methods to deliver the project as efficiently as possible. These efforts resulted in several refinements to the project's design, which have been designated Refined Alternative I (Concept I-W), referred to hereinafter as Concept I-W. KYTC and ODOT are preparing a supplemental EA for Concept I-W. As part of that effort, KYTC and ODOT are updating studies to reflect any changes in regulatory or site conditions that have occurred since they were originally prepared.

KYTC and ODOT prepared a *Public Engagement Plan* to provide a framework for public involvement efforts associated with the BSB Corridor Project (see Appendix Q). This *Public Involvement Summary* discusses the ways in which stakeholder and public feedback have been collected and how they have shaped the project since the 2012 EA/FONSI.

2. TRAVEL SURVEY ANALYSIS

In December 2013, KYTC and ODOT conducted a travel survey to elicit qualitative and quantitative information from drivers who made private (car, van, truck, SUV) trips using I-71/I-75 or I-471 across the Ohio River between Covington, Kentucky and Cincinnati, Ohio.

The survey was structured as follows:

- Questions about trips made across the Ohio River using either the I-71/I-75 BSB or the I-471 Daniel Carter Beard Bridge in the last 6 months, including trip frequency.
- Questions about a typical recent trip across the Ohio River, including trip purpose, time and day, origin-destination, total travel time, perception of congestion, travel costs, the next best alternative route/bridge, and travel party and paying member(s).
- Questions about congestion and other bridge usage.
- Introduction to the project, including questions about respondents' attitudes toward the project and alternate routes they would be seeking.
- Route choice exercise consisting of nine trade-off questions.
- Questions about perception on tolling.
- Questions pertaining to income, occupational status, age, transit usage, etc.



The survey was conducted online (web-based) through a market research firm. A total of 1,001 questionnaires were completed from people across the region.

The data gathered from the travel survey was utilized to develop data and forecasting model inputs for various traffic and revenue studies. Additional details regarding the survey are included in the *Draft Travel Survey Analysis* (February 2014), which can be found in Appendix A.

3. LOCAL STAKEHOLDERS

3.1 Project Advisory Committee

A Project Advisory Committee (PAC) was formed early in the project development process (during the development of the 2012 EA/FONSI) to better align the project with regional and community needs. Current PAC member organizations are listed to the right.

Representatives from government agencies, community groups, and businesses with vested interests in the project area were invited to join the PAC, including:

- Cities and counties abutting the project;
- Groups that represented the communities abutting the project;
- Representatives from the downtown Covington and Cincinnati business communities;
- Major freight generators;
- Major impacted utilities; and
- Major traffic generators and attractions.

Providing balanced representation from both Kentucky and Ohio was a primary focus during the formation of the PAC. This included pairing organizations with similar responsibilities in both states as well as balancing the number of representatives from each state.

Project Advisory Committee

Agencies

- Boone County Engineer
- Boone County Fiscal Court
- Campbell County Fiscal Court
- Cincinnati Park Board
- City of Cincinnati
- City of Cincinnati Community and Economic Development
- City of Cincinnati Dept. of Planning and Engagement
- City of Cincinnati Dept. of Transportation & Engineering
- City of Covington, City Manager
- City of Covington, Economic Development Director
- City of Covington, Mayor
- City of Ft. Mitchell
- City of Ft. Wright
- City of Park Hills
- Governor's Southwest Ohio Regional Liaison
- Hamilton County Commissioners
- Hamilton County Engineer
- Hamilton County Regional Planning Commission
- Kenton County Engineer
- Kenton County Fiscal Court
- Ohio-Kentucky-Indiana Regional Council of Governments (OKI)
- Planning and Dev. Services of Kenton County
- Southwest Ohio Regional Transit Authority
- Transit Authority of Northern Kentucky (TANK)

Local Community Groups

- Center for Great Neighborhoods
- Cincinnati Center City Development Corporation
- Cincinnati Business Committee
- Cincinnati USA Regional Chamber
- Citizens for Civic Renewal
- Lewisburg Neighborhood Association
- Northern Kentucky Tri-Ed



Participants in the PAC have been updated throughout the course of the project to reflect the most current stakeholder groups present in the area and to reflect current staff positions. A comparison of the PAC membership during the development of the 2012 EA/FONSI to the current membership is included in Appendix B.

The role of the PAC is to review various components of the project and offer feedback to allow the views of the community to be addressed as the project is developed and implemented. The PAC members act as liaisons between their respective organizations and communities and the project team. The PAC also assists with distributing information provided by the project team to their respective community members.

Two PAC meetings have been held Since the 2012 EA/FONSI. Topics discussed during a PAC meeting held on June 29, 2022 included:

- Project history;
- Current project status, including design and project costs, traffic, funding and financing, grant applications, environmental, outreach, and pre-procurement;
- Cost savings and refinements to Selected Alternative I (from the 2012 EA/FONSI); and
- Next steps.

Topics discussed during a PAC meeting held on August 21, 2023 included:

- Project history;
- Purpose and need;
- Construction phases and schedule;
- Project overview;
- Progressive design-build process;
- Public outreach update; and
- Environmental process update.

Project Advisory Committee (continued)

- Northern Kentucky Chamber of Commerce
- Port of Greater Cincinnati Development Authority
- Queensgate Business Alliance
- Sierra Club
- SouthBank Partners
- West End Community Council

Local Businesses

- Castellini Management Co.
- Cincinnati Bengals
- Cincinnati Bulk Terminals, LLC
- Cincinnati Museum Center
- Cincinnati/Northern Kentucky International Airport
- Cincinnati Reds
- Duke Energy
- Government Strategies Group
- National Underground Railroad Freedom Center

Aesthetics Committee

- American Institute of Architects – Cincinnati Chapter
- American Institute of Architects Students – University of Cincinnati
- American Society of Civil Engineers Students – University of Cincinnati
- Cincinnati USA Regional Chamber of Commerce
- Cincinnati History Library and Archives
- Cincinnati Railroad Club
- City of Cincinnati
- City of Cincinnati – Park Planning
- City of Covington
- Hamilton County Regional Planning Commission
- Kenton County Historical Society
- Kenton County Planning and Development Services
- Kentucky Transportation Cabinet
- Michael Schuster Associates
- Northern Kentucky University
- Northern Kentucky Urban & Community Forestry Council



Detailed summaries of the PAC meetings are included in Appendix B. Additional PAC meetings are planned prior to the public hearings (see Section 8) and during the project's final design (see Section 9).

3.2 Aesthetics Committee

A sub-group of the PAC, referred to as the Aesthetics Committee, was formed to evaluate aesthetic elements throughout the corridor, including structure type for the companion bridge and corridor theme. The current Aesthetics Committee members are listed to the right.

The Aesthetics Committee membership was established based on feedback from the PAC and the project team's local knowledge. While there is some commonality with the PAC, the Aesthetics Committee members provide a mix of technical expertise in aesthetics (such as architecture and engineering professionals and local public agencies) and representation of community interests (such as historical societies and local universities). The roles and responsibilities of the Aesthetics Committee are described in the *Brent Spence Bridge Project Aesthetic Committee Charter* (2005).

Since the 2012 EA/FONSI, several Aesthetics Committee meetings were held to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in the design and construction of the project. Aesthetics Committee meetings focused on different portions of the BSB corridor at different times. As a result, the entire Aesthetics Committee was not in attendance at every meeting. Rather, several meetings were held with subcommittees that consisted of only those members with a vested interest in the portion of the corridor under discussion. Furthermore, Fort Wright and Fort Mitchell, although not included on the Aesthetics Committee, are providing input on the development of aesthetics guidelines for the portions of the corridor located in those cities and were therefore engaged at the subcommittee level.

Aesthetics Committee (continued)

- The Ohio River Foundation
- University of Kentucky Department of Engineering
- University of Cincinnati

Fort Wright / Fort Mitchell Aesthetics Subcommittee

- City of Ft. Wright
- City of Ft. Mitchell
- Kenton County Engineer
- Kenton County Fiscal Court
- Kenton County Planning and Development Services

Covington Aesthetics Subcommittee

- City of Covington Economic Development
- City of Covington, Mayor
- City of Covington Parks and Recreation
- Kenton County Fiscal Court
- Lewisburg Neighborhood Association
- RL Record, LLS

Ohio Aesthetics Subcommittee

- American Institute of Architects – Cincinnati Chapter
- American Institute of Architects Students – University of Cincinnati
- American Society of Civil Engineers Students – University of Cincinnati
- Cincinnati USA Regional Chamber of Commerce
- Cincinnati History Library and Archives
- Cincinnati Railroad Club
- Cincinnati Museum Center
- Cincinnati Park Board
- Cincinnati USA Regional Chamber of Commerce
- City of Cincinnati Dept. of Transportation & Engineering
- City of Cincinnati – Park Planning
- Hamilton County Planning & Development
- Hamilton County Regional Planning Commission
- The Ohio River Foundation
- University of Cincinnati



The Aesthetic Committee and Subcommittee meetings are summarized in Table 1. Detailed summaries of the Aesthetic Committee meetings are included in Appendix C.

KYTC and ODOT will continue to engage the project Aesthetics Committee as described in the *Brent Spence Bridge Project Aesthetic Committee Charter* for final confirmation of the aesthetic treatments included in Phase III of the project. The Aesthetics Committee will also be engaged to provide feedback on the aesthetic elements of the new companion bridge and the existing BSB. KYTC will continue to coordinate with the Covington and Fort Wright/Fort Mitchell Aesthetics Subcommittees during the project’s design phase to finalize aesthetic treatments in those cities. Aesthetic features will also be coordinated and confirmed with the City of Cincinnati and the Ohio Aesthetics Subcommittee at the completion of each design stage review for Phases I and II and during the Phase III progressive design-build contract.

Table 1: Aesthetic Committee and Subcommittee Meeting Summary

Meeting and Date	Location	Topics Discussed
Meeting #1 June 28, 2022 (Covington Subcommittee)	KYTC District 6 Office 421 Buttermilk Pike Covington, KY 41017	<ul style="list-style-type: none"> • Purpose of Aesthetics Committee • Project history and overview • Aesthetics opportunities • Covington values
Meeting #2 June 28, 2022 (Ohio Subcommittee)	Virtual Meeting	<ul style="list-style-type: none"> • Project overview • ODOT’s aesthetics process • Phases I and II aesthetics design checklist • Aesthetic treatments for bridges, lighting, and retaining walls • Project schedule
Meeting #3 August 10, 2022 (Ft. Wright/Ft. Mitchell Subcommittee)	Kenton County Court Conference Room 230 Madison Avenue Covington, KY 41011	<ul style="list-style-type: none"> • Project history • Design details and impacts • Aesthetic guidelines • Gateway opportunities • Sound barriers • Right-of-way acquisition • Drainage and traffic issues
Meeting #4 August 10, 2022 (Covington Subcommittee)	KYTC District 6 Office 421 Buttermilk Pike Covington, KY 41017	<ul style="list-style-type: none"> • Example aesthetic guidelines • City Master Plan • Design overview • Design parameters
Meeting #5 October 20, 2022 (Covington Subcommittee)	First Financial Bank Conference Room 601 Madison Avenue Covington, KY 41011	<ul style="list-style-type: none"> • Project overview • Streetscape details • Shared-use paths • Goebel Park • Areas under interstate bridges • Gateway opportunities



Meeting and Date	Location	Topics Discussed
<i>Table 1 (continued)</i>		
Meeting #6 November 16, 2022 (Ft. Wright/Ft. Mitchell Subcommittee)	Kenton County Court Conference Room 230 Madison Avenue Covington, KY 41011	<ul style="list-style-type: none"> • Project overview • Noise barriers • Landscaping and gateway concepts
Meeting #7 January 31, 2023	Virtual	<ul style="list-style-type: none"> • Project overview and history • Recent project activities • Ohio River bridge types • Project schedule
Meeting #8 January 31, 2023 (Ohio Subcommittee)	Artemis Conference Room (Included Virtual Option) 508 West 3 rd Street Cincinnati, OH	<ul style="list-style-type: none"> • Aesthetics process and history • Phase III aesthetics design checklist • Project schedule
Meeting #9 March 29, 2023 (Ft. Wright/Ft. Mitchell Subcommittee)	Kenton County Government Building, Fiscal Court Room 1840 Simon Kenton Way Covington, KY 41011	<ul style="list-style-type: none"> • Project overview • Noise barriers • Aesthetic options for Kyles Lane interchange • Aesthetic options for Dixie Highway interchange

3.3 Other Stakeholders

KYTC and ODOT are engaged in ongoing outreach efforts to inform stakeholders and the public about the project, gather feedback, and answer questions. Activities include delivering project update presentations to business groups, industry groups, and professional societies. These activities are informational in nature, and no substantial feedback about the project has been received. A summary of ongoing outreach efforts (including dates, times, locations, participants, discussion items, and key issues raised) is provided in Appendix D.

4. ELECTRONIC MEDIA

A variety of media are utilized to provide information to and collect feedback from the public, including websites and social media accounts. Some of these are general KYTC or ODOT accounts and are not project specific. Project-specific electronic media is discussed in the following sections.

4.1 BSB Corridor Website

The project website received a major update in 2014. The updated website included information about the need for the project, the current project status, tolling studies that were currently underway, frequently asked questions, and a library of project documents. The website also included an opportunity to provide project feedback using an embedded comment form.



The project was placed on hold in 2015. Although the project website remained active, it was not regularly updated until 2021.

The project website received another major update in 2022. The updates focused on providing the most recent project information, including:

- Updated project descriptions;
- Public involvement information;
- Funding facts;
- A library of documents and studies produced for the project;
- Information about the environmental process;
- Links to articles in local news media and project newsletters;
- Lists of PAC and Aesthetic Committee members;
- Project mapping;
- A comment form; and
- The opportunity to subscribe to project updates.

In 2023, the website was updated again in preparation for the project to move into design and construction. Information added to the website included a page providing information to individuals and businesses desiring to work on the project and pages dedicated to traffic and construction updates.

The current project website is jointly managed by KYTC and ODOT and addresses both the Kentucky and Ohio portions of the project. It is maintained and updated on a regular basis and can be accessed at <https://brentspencebridgecorridor.com/>.

4.2 Project Websites

In addition to the website for the overall BSB Corridor Project, ODOT maintains separate project-specific websites for the three separate construction phases and the new companion bridge. These project websites currently redirect individuals to the BSB Corridor Project website. However, they may be reestablished during the construction phase to provide up-to-date construction information.

4.3 Project Social Media Accounts

Project social media accounts are jointly managed by KYTC and ODOT and address both the Kentucky and Ohio portions of the project. A Facebook page (<https://www.facebook.com/BSBCorridor>) and an X channel (formerly Twitter) (<https://twitter.com/BSBCorridor>) established in December 2022 and a Threads feed



(<https://www.threads.net/@bsbcorridor>) established in July 2023 provide relevant project details, project updates, and announcements. Upcoming public involvement activities such as the neighborhood outreach meetings (see Section 5), the open-house project update meetings (see Section 6), and the public hearings (see Section 8) are also advertised on the social media accounts. The public will often “like,” “share,” or post generic comments on the project social media accounts. Only those public posts with a specific question or comment receive an individualized response on the social media platform. Copies of social media posts that have received an individual response from the project team are included in Appendix I.

4.4 Project Mailing List

A project mailing list was established to keep interested parties informed about the project. It is jointly managed by KYTC and ODOT and addresses both the Kentucky and Ohio portions of the project. The project mailing list includes members of the PAC, the Aesthetics Committee and Subcommittees, other local stakeholders, and the public. The opportunity to sign up for the project mailing list is prominently featured on the project website. The project mailing list is updated on a weekly basis and will be maintained until the project is constructed. The current mailing list includes over 1,500 individuals.

4.5 Electronic Project Newsletters (E-Newsletters)

Three project e-newsletters were distributed to the project mailing list in January, February, and March 2013. These newsletters provided project updates, information about tolling, and frequently asked questions. The newsletters were distributed to 300-325 individuals, depending on the current mailing list.

The project was placed on hold in 2015, and no newsletters were published while the project was on hold. Beginning in January 2022, KYTC and ODOT began issuing quarterly newsletters to provide updates on current activities for the entire project. In July 2022, KYTC and ODOT announced they would increase the frequency of the newsletters, which are currently released on a monthly basis. Copies of project e-newsletters are included in Appendix E.

4.6 Press Releases

KYTC and ODOT issue press releases to provide periodic project updates and announce key project milestones. Copies of press releases issued since 2012 are included in Appendix F.

5. TARGETED ENVIRONMENTAL JUSTICE/NEIGHBORHOOD OUTREACH

The following sections summarize outreach efforts conducted by KYTC and ODOT to engage communities at the neighborhood level, including environmental justice (EJ) (minority and low-income) communities and communities with older adults, individuals with limited English proficiency, individuals with disabilities, and zero-car households.



5.1 Small-Scale Environmental Justice/Neighborhood Outreach Meetings

KYTC and ODOT held 12 small-scale targeted EJ/neighborhood outreach meetings in areas directly adjacent to the project's construction limits to share project updates and to offer residents the opportunity to share feedback with the project team.

Phone interviews were conducted with neighborhood associations and community councils to determine contact information, constituencies/membership, advertising strategies, meeting schedules, and potential meeting locations that are accessible for individuals with disabilities and those who are transit dependent. If a neighborhood did not have an organized council, KYTC and ODOT coordinated with city officials and PAC members familiar with the specific area to determine the best ways to reach that neighborhood.

KYTC and ODOT scheduled opportunities to attend regularly scheduled neighborhood meetings. The Lewisburg and Botany Hills neighborhoods, which border each other, do not have organized neighborhood associations. As a result, meetings in those neighborhoods were combined and scheduled at a venue in Botany Hills that was accessible by transit and for persons with disabilities. Similarly, the Queensgate neighborhood does not have an organized community council; however, this area consists primarily of commercial and industrial facilities with minimal residential land use. The project team determined that the scattered residential areas in Queensgate would have opportunities to attend meetings in adjacent neighborhoods. As a result, a meeting was not scheduled in Queensgate. Finally, the cities of Fort Mitchell, Fort Wright, and Park Hills operate as independent cities without smaller, defined neighborhoods; therefore, the project team presented at meetings organized through city officials. City officials were also invited to attend every meeting.

KYTC and ODOT advertised the small-scale EJ/neighborhood outreach meetings via communication methods each neighborhood group already had in place. Depending on the neighborhood, advertisement methods included neighborhood web sites, Facebook pages, announcements at neighborhood meetings, email notifications, and printed flyers. In the Lewisburg and Botany Hills neighborhoods, the project team emailed meeting flyers to known community contacts and distributed printed flyers at community facilities and businesses in both neighborhoods. Responding to feedback received during the initial EJ/neighborhood outreach, the December meetings were also advertised in the project's e-newsletter.

Information from the U.S. Census Bureau indicated that seven census block groups in the area targeted for EJ/neighborhood outreach have greater than five percent of the population with limited English proficiency. In these census block groups, 86 percent of the individuals with limited English proficiency speak Spanish. Accordingly, advertising materials included information in Spanish offering translation and interpretation services. In addition, based on feedback from a PAC member, information about the meeting was printed in Spanish and distributed in the Lewisburg and Botany Hills neighborhoods. Comment forms were also available in both Spanish and English. No requests for translation or written comments in Spanish were received.

Information presented at the meetings included a general project overview, refinements incorporated into the project's design since the 2012 EA/FONSI, and proposed mitigation and enhancement measures. Exhibits on



display at the meetings showed the proposed design, including right-of-way, relocated structures, noise barriers, historic properties and districts, parks, wetlands, streams, and multimodal facilities. Renderings and a flyover video illustrating what the finished project might look like were also displayed.

Table 2 summarizes the small-scale EJ/neighborhood outreach meetings. Detailed summaries are included in Appendix G.

Table 2: Small-Scale EJ/Neighborhood Outreach Meeting Summary

Neighborhood and Date	Location	General Summary
CUF Neighborhood Association (OH) November 15, 2022 7:00 pm	Hughes STEM High School 2515 Clifton Avenue Cincinnati, OH	<ul style="list-style-type: none"> • 30 in attendance (excluding the project team) • Questions about local connections to the interstate, multimodal accommodations, construction, funding, aesthetics, and property impacts.
Mainstrasse (ROMA) (KY) November 21, 2022 6:00 pm	Center for Great Neighborhoods 321 West 12 th Street Covington, KY	<ul style="list-style-type: none"> • 12 in attendance (excluding the project team) • Questions about how traffic will move through the corridor and construction timeline. • Concerns about noise, drainage in low lying areas of Goebel Park, and traffic impacts during construction and when incidents occur on the highway.
Friends of Peaselburg Neighborhood Association (KY) November 28, 2022 6:00 pm	Covington Firefighters Hall 2232 Howell Street Covington, KY	<ul style="list-style-type: none"> • 16 in attendance (excluding the project team) • Questions about how traffic will move through the corridor, construction timeline, and property impacts. • Concerns about noise, flooding in neighborhoods, truck traffic, and traffic during construction. • Supportive of refinements incorporated into the project.
CBD Riverfront (OH) November 29, 2022 7:00 pm	First Financial Bank/ Community Center 1 East 4 th Street Cincinnati, OH	<ul style="list-style-type: none"> • 24 in attendance (excluding the project team) • Concerns about the project footprint, multimodal accommodations, connections across I-75, project costs, and traffic volumes/operations. • General project opposition.
Westside Covington (KY) November 29, 2022 6:30 pm	Center for Great Neighborhoods 321 West 12 th Street Covington, KY	<ul style="list-style-type: none"> • 13 in attendance (excluding the project team) • Questions about how traffic will move through the corridor, construction timeline, project funding, and property impacts. • Concerns about noise.
Fort Mitchell (KY) November 30, 2022 7:00 pm	Fort Mitchell Administrative Building 2355 Dixie Highway Fort Mitchell, KY	<ul style="list-style-type: none"> • 46 in attendance (excluding the project team) • Questions about how traffic will move through the corridor, construction timeline, and property impacts. • Concerns about noise and pedestrian safety in the Dixie Highway interchange area.



Neighborhood and Date	Location	General Summary
<i>Table 2 (continued)</i>		
Fort Wright (KY) December 1, 2022 7:00 pm	South Hills Civic Club 10 Bluegrass Avenue Fort Wright, KY	<ul style="list-style-type: none"> • 57 in attendance (excluding the project team) • Questions about how traffic will move through the corridor, construction timeline, project funding, and property impacts. • Concerns about noise, traffic during construction, and truck traffic.
Mutter Gottes/ Covington CBD (KY) December 5, 2022 7:00 pm	Kenton County Public Library 502 Scott Street Covington, KY	<ul style="list-style-type: none"> • 17 in attendance (excluding the project team) • Questions about how traffic will move through the corridor and construction timeline. • Concerns about noise, traffic during construction and incidents, and access for first responders.
Lewisburg/Botany Hills (KY) December 6, 2022 7:00 pm	Holiday Inn 200 Crescent Avenue Covington, KY	<ul style="list-style-type: none"> • 17 in attendance (excluding the project team) • Questions about how traffic will move through the corridor and multimodal accommodations. • Concerns about noise.
Camp Washington Community Council (OH) December 12, 2022 7:00 pm	Camp Washington Urban Revitalization Corporation 2951 Sidney Avenue Cincinnati, OH	<ul style="list-style-type: none"> • 9 in attendance (excluding the project team) • Questions about traffic volumes and operations, alternative design concepts, the design of the interchange at the Western Hills Viaduct, and property impacts. • Concerns about local access, damage to local roadways during construction, maintenance of ODOT-owned property,
Park Hills (KY) December 12, 2022 7:00 pm	Garden of Park Hills 1622 Dixie Highway Park Hills, KY	<ul style="list-style-type: none"> • 42 in attendance (excluding the project team) • Questions about how traffic will move through the corridor, right-of-way acquisition, project schedule, project costs, and construction timeline. • Concerns about noise and truck traffic.
West End Community Council (OH) December 20, 2022 6:00 pm	Lincoln Recreation Center 1027 Linn Street Cincinnati, OH	<ul style="list-style-type: none"> • 42 in attendance (excluding the project team) • Questions about alternative design concepts, project schedule, funding, and landscaping. • Concerns about noise and construction impacts.

5.2 Broad-Scale Environmental Justice/Neighborhood Outreach Meetings

KYTC and ODOT held one daytime and one evening broad-scale EJ/neighborhood outreach meeting in each state to engage neighborhoods that are near the BSB corridor but will not be directly impacted. These meetings were promoted via fliers emailed to neighborhood associations and community councils, the project website, the project's December e-newsletter, ODOT District 8's events page, Facebook, X (formerly Twitter), Nextdoor.com, an article in the Northern Kentucky Tribune, and local radio and TV media. Information was also provided to a Cincinnati City Council member, and the City of Cincinnati shared information regarding these meetings on social media platforms.



The same information was presented and on display at the small-scale and the broad-scale meetings. The broad-scale neighborhood meetings are summarized in Table 3. Detailed summaries of the broad-scale neighborhood meetings (including advertising, sign-in sheets, photographs, presentations, exhibits, and comments) are provided in Appendix G.

Table 3: Broad-Scale EJ/Neighborhood Outreach Meeting Summary

State and Date	Location	General Summary
Ohio December 13, 2022 12:00pm and 5:00pm	Lincoln Recreation Center 1027 Linn Street Cincinnati, OH	<ul style="list-style-type: none"> • 12 in attendance (excluding the project team) • Open house-style format with 30-minute formal presentation
Kentucky December 14, 2022 12:00pm and 5:00pm	Kenton County Fiscal Court 1840 Simon Kenton Way Covington, KY	<ul style="list-style-type: none"> • 81 in attendance (excluding the project team) • Open house-style format with 30-minute formal presentation

5.3 Environmental Justice/Neighborhood Outreach Website

KYTC and ODOT developed a “PublicInput.com” website specific to neighborhoods in and near the project area that was available for the duration of the EJ/neighborhood outreach effort. The site was made available when the first EJ/neighborhood outreach meeting was held (November 15, 2022), and the comment period ended 16 days after the final meeting (January 5, 2023). Information about the availability of project materials and the opportunity to comment online through PublicInput.com was provided at every outreach meeting and was distributed to each neighborhood group.

5.4 Environmental/Neighborhood Outreach Results

A total of 418 people signed in at the small-scale and broad-scale EJ/neighborhood outreach meetings, excluding the project team. Comments were accepted on the PublicInput.com site between November 15, 2022 and January 5, 2023. The site was viewed 2,559 times, and 218 individuals either submitted comments or responded to polling questions.

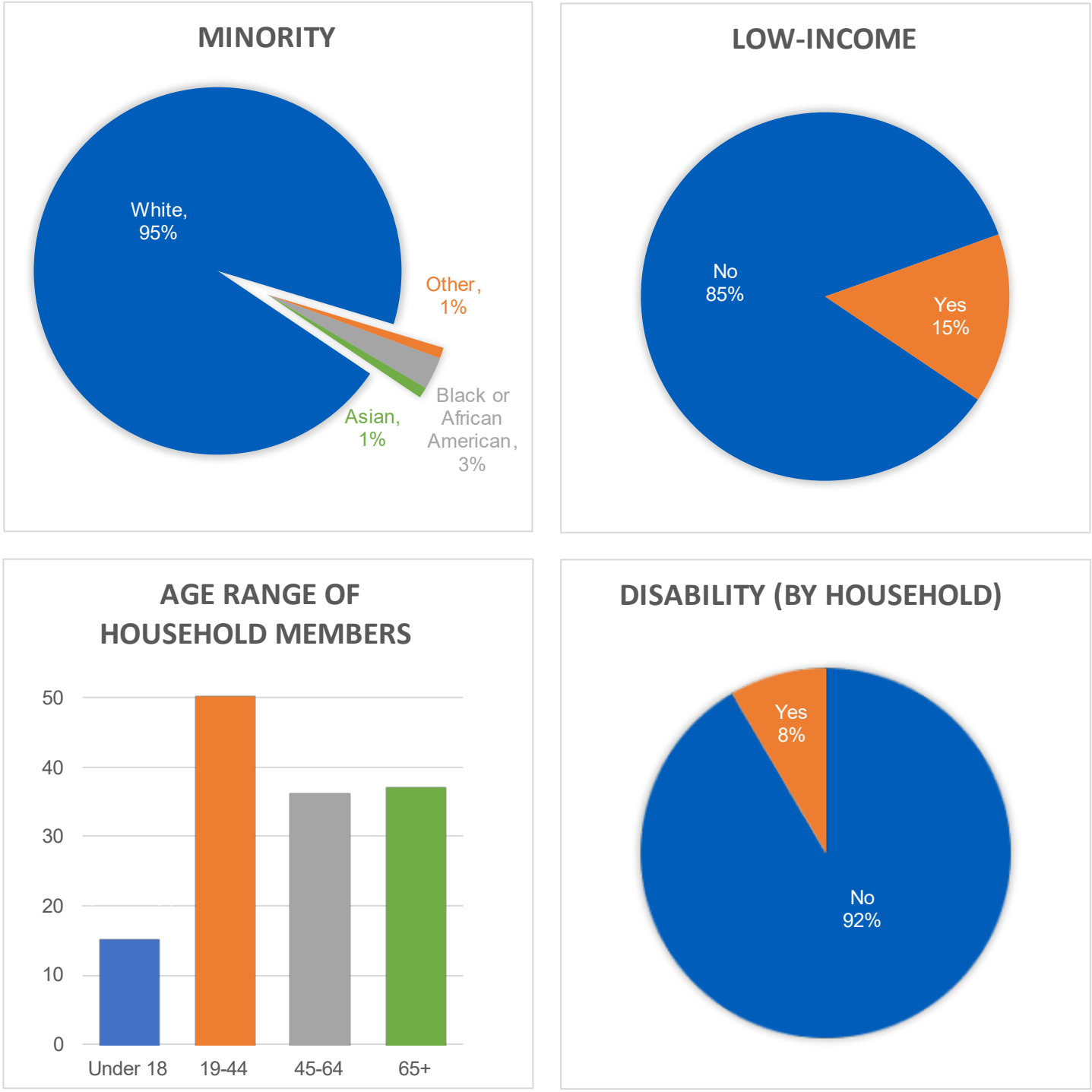
Demographic questionnaires were available at all in-person EJ/neighborhood outreach meetings, and polling questions on the PublicInput.com site sought demographic data of participants. A total of 111 individuals provided demographic information, although not every individual answered every question. Of the individuals who provided demographic information, 5 percent identified as minority, 15 percent were potentially low-income¹, 35 percent were from households with one or more older adult (over age 64), and 8 percent were from households with one or more individuals with a disability. All participants in the EJ/neighborhood outreach indicated English as their primary language. Only one response was received regarding the individual’s

¹ It was not possible to directly correlate all answers on demographic questions to U.S. Census Bureau poverty thresholds due to the ranges of responses offered. For example, household sizes were grouped into 1-2, 3-5, and 6+ persons. If a response fell within the range for low-income (defined as 1.99 times the U.S. Census Bureau poverty threshold), the individual was considered to be potentially low-income.



primary mode of transportation, and that response indicated a personal automobile. Figure 1 displays the level of participation in the neighborhood outreach activities by population group. The comments received during the targeted EJ/neighborhood outreach are discussed in Section 7.3.

Figure 1: EJ/Neighborhood Outreach Participation by Population Group



6. OPEN-HOUSE PROJECT UPDATE MEETINGS

In August 2023, KYTC and ODOT held two open-house style project update meetings to provide information about the project’s status, including Concept I-W, anticipated impacts, proposed mitigation and enhancement measures, and the progressive design-build process. The open-house project update meetings are summarized in Table 4. A detailed summary of the open-house project update meetings (including advertising, sign-in sheets, photographs, presentations, exhibits, comments, and responses) is provided in Appendix P.

Table 4: Open-House Project Update Meeting Summary

State and Date	Location	General Summary
Kentucky August 23, 2023 4:00pm to 7:00pm	Radisson Hotel 668 West 5 th Street Covington, KY	<ul style="list-style-type: none">• 181 in attendance (excluding the project team)• Open house-style format with pre-recorded, looping presentation
Ohio August 24, 2023 4:00pm to 7:00pm	Longworth Hall 700 West Pete Rose Way Cincinnati, OH	<ul style="list-style-type: none">• 137 in attendance (excluding the project team)• Open house-style format with pre-recorded, looping presentation

The meetings were advertised via the following methods:

- The project website (www.brentspencebridgecorridor.com);
- Press releases by KYTC and ODOT on August 9, 2023;
- Distribution of an advertising flyer to the PAC on August 9, 2023;
- Posts on the project social media pages on August 10, 2023;
- Distribution of an advertising flyer to the project Diversity & Inclusion Outreach Committee on August 11, 2023;
- A notification to members of the project mailing list on August 11, 2023;
- Facebook events for each meeting; and
- Coverage in local print and television media.

Attendees at the meeting included FHWA, KYTC, ODOT, the project team, local agencies, and members of the public. The meetings followed an open-house format. Attendees were invited to view a pre-recorded presentation and to browse exhibits providing details about the project. A looping presentation showing a flyover animation of the proposed project and multiple renderings of what the completed project might look like was also available for viewing. Members of the project team were present to answer questions and respond to feedback throughout the meetings. Two project fact sheets, a handout explaining collector-distributor system, and comment forms were also provided.

The project advertisements and project website provided information offering Spanish translation services upon request. Spanish written comment forms were also available at the public meetings. No requests for Spanish translation services or Spanish comment forms were received. Both locations were accessible to persons with disabilities and via local transit.



The comment period for the open-house project update meetings began on August 9, 2023 and concluded on September 8, 2023. Comments were collected via written forms returned at the meetings, email, phone, direct mail, and the project website. During the comment period, the following were received:

- 46 comments received via the project website;
- 26 comment forms returned at the public meetings; and
- 5 comments received via email.

No comments were received via phone or direct mail. The comments received during the open-house project update meetings are discussed in Section 7.4. Every comment received during the comment period for the open-house project update meetings was evaluated by the project team, and individual responses were documented in a comment and response summary, which was posted on the project website (www.brentspencebridgecorridor.com) in September 2023. Individuals on the project mailing list were notified about the availability of the comments and responses in the project's October 2023 e-newsletter.

7. PUBLIC COMMENTS

The project feedback received since 2012 is summarized below.

7.1 2012-2021

In 2014 and 2015, KYTC and ODOT received a number of constituent inquiries about the project. In addition, several comments were submitted via the project website. Individual responses were provided to these inquiries and comments (see Appendix H). The majority of the comments centered around opposition to tolling the BSB or the project in general. Studies related to tolling the BSB were stopped 2015, and tolling is not included in Concept I-W.

The project was placed on hold in 2015, with very few public comments received between 2015 and 2021. KYTC and ODOT responded to all inquiries and comments received during that time (see Appendix H).

7.2 2022-Present

Following a major website update and increased publicity for the project, public comments were regularly submitted to KYTC and ODOT beginning in 2022. Comments were also directly emailed to members of the project team or the project email address (info@brentspencebridgecorridor.com). Comments generally centered around:

- Questions about the project and information requests.
- Property and right-of-way impacts.
- Volume of truck traffic and associated traffic congestion and noise (particularly the use of engine brakes).



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- Future traffic volumes and traffic operations.
 - Traffic impacts during construction.
 - Increased traffic and associated noise and air quality concerns.
 - Multimodal accommodations, including fixed transit (such as light rail).
 - Aesthetics and gateway opportunities.
 - Project costs and funding.
 - Construction schedule and opportunities to work on the project.
 - Improving local street connections across I-75 in Ohio.
 - Reducing the project footprint.
 - Creating additional developable land.
 - Lowering (trenching) and/or constructing freeway caps on I-75 in Ohio.

The project team provided individual responses to all comments received via the project website or email on a weekly basis. A document listing all comments and responses is available on the project website and updated on a monthly basis. Copies of all comments received via the project website or email since 2022 and responses to each are included in Appendix I.

Alternative Concepts Letters

In June and July 2022, the Governor of Ohio and the Director of ODOT received several letters encouraging ODOT to reduce the project footprint and to explore alternative concepts to further community goals during the project's design-build process. ODOT responded to each letter stating its plans to deliver the project using a design-build approach that would allow design-build teams to submit alternative technical concepts (also called innovations). In addition, ODOT committed to exploring methods to further reduce the project footprint and to evaluating connectivity within central business districts. ODOT also committed to making community goals consistent with residential/commercial growth, aesthetics, and reduced overall environmental impacts a primary focus of the project. Copies of the comment letters and ODOT's responses are included in Appendix J.

Bridge Forward Coalition

In December 2021, KYTC and ODOT received a *Working Position Paper: Redesign of the Brent Spence Bridge Project* prepared by the Bridge Forward Coalition (Bridge Forward). The *Position Paper* included specific design strategies and three alternative design concepts for the BSB Corridor Project. The stated goals of the concepts presented in the *Position Paper* were to redesign the project to promote economic development in the region.

KYTC and ODOT reviewed the concepts presented in the *Position Paper*. Two concepts, Modified Alternative B and Modified Concept 85, were previously evaluated and eliminated from consideration during the project's preliminary development activities due to impacts, complexity, engineering concerns,



constructability, risk, costs, and/or local opposition. The final concept, the Boland Concept, was not found to be geometrically feasible and would result in a greater project footprint and more environmental impacts than Concept I-W. The Boland Concept would result in grades up to 7.7 percent, which would require design exceptions and would present traffic operational and safety concerns, particularly considering the high volumes of heavy truck traffic traveling through the corridor. In addition, the Boland Concept would not maintain continuity along US-50, would increase traffic on the local street network in the City of Covington, and would not provide additional options for maintaining cross-river traffic if an incident or future construction or maintenance activities occur on the BSB. The Boland Concept does not meet the project's purpose and need. It negatively affects traffic flow and safety, introduces substantial new geometric deficiencies, and does not maintain connections to US-50 (a key regional transportation corridor).

Given the above, none of the concepts presented in the *Position Paper* were recommended for further consideration. KYTC and ODOT prepared a detailed response to the *Position Paper*, which was emailed to the original commenter and made publicly available on the project website in October 2022. The *Position Paper* and the detailed response are provided in Appendix K.

Since October 2022, Bridge Forward has continued to share information and additional ideas for refining the project's design with KYTC and ODOT (see Appendix K). Responses to comments that were formally submitted via the project website or email are documented in the project comment and response summary in Appendix I.

In addition, KYTC and ODOT met with members of Bridge Forward eight times to discuss their ideas and concepts, answer questions, and provide information about the project (see Appendix D). After the third meeting in March 2023, Bridge Forward presented a concept for connecting US-50 to I-75, I-71, the C-D roadway system, and the local road system. In April 2023, Bridge Forward prepared a revised version of the same concept and presented it to the Cincinnati City Council on May 3, 2023. An engineering firm was hired by Bridge Forward to further refine the concepts, and Bridge Forward submitted a revised version to KYTC and ODOT on June 26, 2023 (the June 2023 Concept).

The June 2023 Concept alters the design of the I-75 ramps to and from downtown Cincinnati by stacking the movements and moving US-50 to the lowest level of the interchange with the goal of creating a concept similar to Fort Washington Way (the trenched portion of I-71 through downtown Cincinnati). KYTC and ODOT performed a high-level review of the June 2023 Concept. While the June 2023 Concept potentially provides an additional five acres of contiguous developable land when compared to Concept I-W, the construction costs would be at least \$100 million more than Concept I-W. In addition, due to a proposed tunnel, the operation and maintenance costs for the June 2023 Concept would be approximately \$1 million per year, while the operation and maintenance costs for Concept I-W are estimated at \$160,000 per year. The additional local streets and bridges included in the June 2023 Concept would also substantially increase the City of Cincinnati's local maintenance costs. A high-level traffic operational analysis showed that the June 2023 Concept would result in substantial queues on the local street network, which could result in gridlock during peak travel periods. Furthermore, the June 2023 Concept would result in grades up to 9 percent on local streets, and new arterial frontage roads would be about 30 to 40 feet higher than the surrounding land, which could present safety concerns and create a physical and/or visual barrier between downtown Cincinnati and Queensgate. The



added conflict points and lengthened pedestrian crossings could also result in potential safety concerns. The June 2023 Concept does not meet the project's purpose and need. It presents numerous technical challenges that negatively affect traffic flow and safety with substantially higher construction, operation, and maintenance costs. KYTC and ODOT prepared a response to the June 2023 Concept, which was provided to Bridge Forward, made publicly available on the project website in August 2023, and is included in Appendix K.

Features incorporated into Concept I-W address many of the goals articulated by Bridge Forward, including:

- Minimizing the footprint of the highway;
- Using the interstate primarily as an efficient processor of regional, through traffic;
- Providing a network of safe, multimodal streets for local traffic; and
- Using only modern, progressive engineering practices.

These features include reconfiguring the river crossing to use the existing BSB for local traffic as part of the C-D roadway system and a new double-decker companion bridge to the west for through (interstate) traffic. In addition, performance-based design principles have been incorporated into the design of Concept I-W, substantially reducing the project's footprint and associated impacts. Multimodal facilities have been incorporated into Concept I-W, and KYTC and ODOT are continuing to coordinate the project with the cities of Cincinnati and Covington to address local concerns while further reducing the highway's footprint and impacts to the communities in the project area. Finally, Concept I-W reconfigures ramps in downtown Cincinnati to open up approximately 10 acres of land for potential redevelopment and/or public use directly adjacent to the Cincinnati Central Business District.

During Phase III of the BSB Corridor Project, KYTC and ODOT will evaluate innovation concepts and will consider incorporating measures that improve project quality, reduce costs, shorten schedule, support the project goals and objectives, and have support at the local level (see the *Public Engagement Plan* in Appendix Q). Comments and concepts submitted by Bridge Forward will be further evaluated during this process.

Westway Emails

Beginning in August 2022, KYTC and ODOT received numerous emails suggesting potential changes to the Concept I-W. In most cases the subject line of the emails read: "Brent Spence Bridge Project – Reconnecting Cincinnati Westway Design Improvements," also known as the "Westway Emails." The Westway Emails contained several suggestions that have been incorporated into Concept I-W, including considering community priorities such as east-west connectivity, multimodal improvements, and economic development opportunities in the project's design; employing rigorous traffic forecasting methods; reducing the project footprint; maintaining local connectivity; following a flexible design-build process; and coordinating project details with local agencies. The Westway Emails also suggested depressing I-75 and extending local streets across the highway to form an urban street grid. Similar to the Boland Concept, these concepts would not be geometrically feasible and would not meet the project purpose and need. Steep roadway grades (up to 7.7 percent on I-75 and 9 percent on local streets) would require design exceptions and would present traffic



operational and safety concerns, particularly considering the high volumes of heavy truck traffic traveling through the corridor. The Westway Emails also suggested removing access points within downtown Cincinnati, which would substantially increase traffic on the local street network.

While depressing I-75 and creating an urban roadway grid are not feasible and do not meet the project purpose and need, several of the priorities mentioned in the Westway Emails have been incorporated into Concept I-W. These include:

- Minimizing the footprint of the highway;
- Maintaining and improving local access;
- Providing a network of safe, multimodal streets for local traffic;
- Providing transportation infrastructure that supports local development goals and initiatives; and
- Engaging in a design-build process that provides flexibility and opportunities to maximize benefits and minimize costs.

KYTC and ODOT prepared a detailed response to the Westway Emails, which was made publicly available on the project website in October 2022. The *Westway Emails* and the detailed response are provided in Appendix L.

City of Cincinnati and Regional Chamber of Commerce

ODOT received comments from the City of Cincinnati on September 2, 2022 and the Cincinnati USA Regional Chamber of Commerce (Chamber) on July 11, 2022. The Chamber is a member of the PAC and provided feedback that was similar to comments received from the City. The comments focused on furthering the goals of getting the project done; reclaiming land; improving green space, pedestrian safety, bike facilities, etc.; and keeping a “city feel” on or under bridges for I-75. In response to the comments, ODOT incorporated refinements to the 3rd Street, 4th Street, 5th Street, and 6th Street ramps into Concept I-W. Based on the ramp refinements, there will be approximately 10 acres between 3rd Street and 6th Street opened up for potential redevelopment and/or public use directly adjacent to the Cincinnati Central Business District. In addition, ODOT will continue to support the City of Cincinnati’s efforts to accommodate alternative modes and improve livability during the project’s development. ODOT prepared a detailed response to the City and Chamber comments, which was made publicly available on the project website in November 2022. The comments and the detailed response are provided in Appendix M.

During public involvement activities, ODOT received multiple comments suggesting the inclusion of retail areas on the Ezzard Charles Drive bridge over I-75. On August 29, 2023, the City of Cincinnati requested that ODOT investigate decking or an expanded bridge on Ezzard Charles Drive to support future civic space or retail development. Based on further coordination with the City, ODOT has committed to building a wider bridge on Ezzard Charles Drive over I-75. The widened bridge will provide an additional 50 feet of green space on each side that could support potential future civic space or retail development by the City of Cincinnati. ODOT will fund the cost of the bridge design and will share the construction cost with the City. ODOT and the City will develop cost sharing and maintenance agreements prior to construction.



Cincinnati Process Improvements Emails

Beginning in November 2022, FHWA received numerous emails with the subject line: “Brent Spence Bridge Corridor – Cincinnati Process Improvements.” The emails were also received at various levels in the City of Cincinnati government. These emails contained several suggestions that have been incorporated into Concept I-W, including returning developable land to the City of Cincinnati; improving pedestrian connections between the Cincinnati Central Business District, Queensgate, and the West End neighborhood; designing urban streets to promote pedestrian and bicycle safety; and minimizing the project footprint. The emails also advocated for specific actions by elected officials from the City of Cincinnati. In February 2023, FHWA also received a copy of a Sierra Club Ohio letter to the City of Cincinnati Mayor and Council advocating for elected officials to share their local expertise in the project’s development. KYTC and ODOT have closely coordinated the project with the City of Cincinnati, and that partnership will continue through the project’s design and construction. Elected officials in the City of Cincinnati will continue to be afforded opportunities to provide feedback on the project. Copies of the Cincinnati Process Improvement Emails and Sierra Club Ohio letter are included in Appendix N.

Expand Transit Not Highways

In January 2023, A representative of the Devou Good Foundation sent numerous form letters with the subject line: “Expand Transit not Highways” to the project email address. The letters expressed opposition to highway expansion projects and the BSB Corridor Project and advocated for shifting funding to expanding transit options and multimodal transportation projects while reducing non-local truck traffic. In 2004, OKI and the Miami Valley Regional Planning Commission completed a major planning study known as the *North South Transportation Initiative* (Initiative) that considered highway improvements in addition to transit improvements such as express bus, commuter rail, and others. The Initiative concluded that a highway improvement project was necessary to address capacity issues on I-75, including the BSB corridor. Concept I-W addresses the highway component of the Initiative and will reduce traffic congestion, substantially improve safety, and enhance travel for multiple modes of transportation. The transit component included in the Initiative must be developed and championed regionally, and KYTC and ODOT are ready to support this when it is advanced at a regional level. KYTC emailed a response to the Devou Good Foundation. The form letters and the response are provided in Appendix I.

Coalition for Transit and Sustainable Development

In January 2023, FHWA received a letter from the Coalition for Transit and Sustainable Development (Coalition) expressing concerns about the project’s compliance with civil rights and EJ requirements. The letter discussed concerns about impacts to minority and low-income (EJ) populations, air quality, the West End neighborhood in Cincinnati, and the alternatives evaluated for the project. In February 2023, the Tri-State Trails, one of the signatories to the Coalition for Transit and Sustainable Development letter, withdrew its support for the Coalition’s activities regarding the BSB Corridor Project. In February 2023, FHWA also received an email from a concerned citizen expressing the opinion that the issues presented in the Coalition’s letter have been carefully and sufficiently studied. In March 2023, FHWA replied that concerns discussed in the Coalition’s letter will be considered prior to making a NEPA determination for the project. The January 2023



letter recommended three steps to address Coalition concerns (see underlined text below). Information on how these concerns have been addressed during the project’s NEPA review is provided below:

- Formal technical consideration of design alternatives that take affirmative action towards remedying the ongoing disparate negative impacts of interstate highway construction through predominantly Black and low-income communities. Impacts to EJ populations are evaluated in an *Environmental Justice Analysis Report* (December 2023) and Section 4.1.7 of the supplemental EA. The evaluation considers relocations, community resources, access, mobility, safety, air quality, greenhouse gases and climate change, noise, stormwater, visual setting, workforce development, indirect and cumulative effects, and temporary construction impacts. Cumulative effects on EJ populations are also evaluated in Section 4.10.2 of the supplemental EA.

The Coalition’s letter specifically mentioned impacts on the West End neighborhood in Ohio. Concept I-W results in minimal impacts on the West End neighborhood and includes mitigation and enhancement measures to enhance quality of life in West End. These measures include mitigation and enhancement for impacts to the Queensgate Playground and Ball Field, new and rebuilt pedestrian and bicycle facilities, safety improvements, noise barriers, aesthetic improvements, measures to minimize and mitigate temporary construction impacts, an interpretive display to be installed in a location in proximity to I-75, and additional width on the Ezzard Charles Drive bridge for potential civic space or retail development. Impacts to the human and natural environment in and around the West End neighborhood are evaluated in Chapter 4 of the supplemental EA.

- Rigorous exploration and objective evaluation of the use of congestion pricing or tolling as a “reasonable alternative” to highway expansion for congestion relief. Tolling interstate crossings of the Ohio River between Kentucky and Ohio is not permitted in the Commonwealth of Kentucky and is therefore not a feasible alternative. In addition, the *North South Transportation Initiative* (Initiative) considered policy alternatives such as tolling and transportation system management alternatives such as high-occupancy vehicle toll lanes. The Initiative concluded that such measures must be implemented in combination with other improvements to be effective.
- Analysis and mitigation of the adverse environmental impact of expanding interstate highway capacity through the cities of Cincinnati and Covington, which include but are not limited to concerns about air quality. The supplemental EA provides an analysis of the environmental impact of Concept I-W in accordance with the National Environmental Policy Act (NEPA). Air quality is evaluated in Section 4.6 of the supplemental EA. Mitigation and enhancement measures are listed in ES-Table II of the supplemental EA.

Copies of the Coalition’s letter and related correspondence, including FHWA’s response, are provided in Appendix I.

7.3 Targeted Environmental Justice/Neighborhood Outreach

The small-scale EJ/neighborhood outreach meetings generally followed an informal format, with a KYTC or ODOT representative walking through a presentation while encouraging those present to ask questions and give feedback throughout. In Ohio, representatives from the City of Cincinnati briefly spoke about their



involvement on the project at the beginning of each meeting. Some small-scale meetings were not conducive to this format due the large number of people present. In these cases, as well as for the broad-scale EJ/neighborhood meetings, the project team made a formal presentation, and attendees were encouraged to review exhibits, ask questions, and offer feedback one-on-one before and after the presentation. During the meetings, questions were posed to the project team and answered in real time. Questions most commonly centered around:

- How traffic will flow through the corridor, including how and when local traffic will enter and exit the C-D roadway system.
- Drainage and flooding issues in the Goebel Park Complex and the Peaselburg neighborhood in Kentucky.
- Noise analysis methodology.
- The timeframe for the project, including sequence of construction.
- Property impacts and right-of-way acquisition.
- Project costs and funding.

Concerns expressed during the meetings, on written comment forms, and on PublicInput.com generally included:

- The desire for noise barriers, specifically in the West End neighborhood in Ohio, the Mainstrasse neighborhood in Kentucky, and southwest of Dixie Highway in Fort Mitchell, Kentucky.
- Volume of truck traffic and associated traffic congestion and noise (particularly the use of engine brakes).
- Traffic impacts during construction.
- Increased traffic and associated noise and air quality concerns.
- Multimodal accommodations, including connections on local streets that cross I-71/I-75.
- Improving local street connections across I-75 in Ohio.
- Reducing the project footprint.
- Creating additional developable land.
- Lowering (trenching) and/or constructing freeway caps on I-75 in Ohio.
- Adding fixed transit (such as light rail) to the project.

Every comment received during the targeted EJ/neighborhood outreach was evaluated by the project team, and individual responses were prepared and published on the project website (see Appendix O). No additional small pockets of EJ populations or other socioeconomic groups were identified during the EJ/neighborhood outreach activities. To the extent the project team was able to ascertain, questions, comments and feedback were consistent across all population groups. The project team did not identify any concerns unique to



minorities, low-income individuals, older adults, individuals with limited English proficiency, adults with disabilities, or zero-car households. Likewise, unanticipated additional community impacts were not identified during the neighborhood outreach. Copies of all comments from the targeted EJ/neighborhood outreach activities and responses to each are included in Appendix O.

7.4 Open-House Project Update Meetings

The project update meetings followed an open-house format. Attendees were invited to view a pre-recorded presentation and to browse exhibits providing details about the project. Members of the project team were present to answer questions and respond to feedback throughout the meetings, and comments were accepted via written comment forms returned at the meetings, the project website, email, phone, and direct mail.

Concerns expressed during the meetings and in submitted comments generally included:

- Construction schedule and opportunities to work on the project.
- Property and right-of-way impacts.
- Future traffic volumes and traffic operations.
- Traffic impacts during construction.
- Multimodal accommodations, including fixed transit (such as light rail).
- Improving local street connections across I-75 in Ohio.
- Reducing the project footprint.
- Creating additional developable land.
- Support for Bridge Forward concepts.

Every comment from the open-house project update meetings was evaluated by the project team, and individual responses were prepared. A comment and response summary was posted to the project website, and the public was notified of its availability in the October 2023 e-newsletter. Copies of all comments from the open-house project update meetings and responses to each are included in Appendix P.

7.5 Public Comment Outcomes

Community members generally supported the refinements, mitigation, and enhancements incorporated into Concept I-W, including the reduction of the project footprint, additional developable land, additional noise and noise/visual screening barriers¹, measures to reduce flooding and combined sewer overflows, new and improved multimodal facilities, and aesthetic features. Throughout the project's development, the public offered

¹ Noise barriers have been determined to be reasonable and feasible per Title 23 of the Code of Federal Regulations part 772 and the applicable state noise policy and are proposed mitigation for noise impacts. Noise/visual screening barriers do not meet one or more of the reasonability criteria but are proposed enhancements to provide noise reduction above and beyond the requirements of 23 CFR part 772 and the applicable state noise policy.



additional feedback and suggestions. KYTC and ODOT have incorporated several refinements into Concept I-W in direct response to the additional comments and feedback that were gathered, which will be included in the environmental commitments in the supplemental EA. These include:

- KYTC will implement measures to improve safety for pedestrians and school-age children who cross the northbound entrance ramp from Dixie Highway to I-71/I-75. Measures will include reducing length of the crosswalk, installing warning signs, and enhancing the pavement markings to better define the crosswalk for pedestrians and vehicles.
- KYTC is proposing a noise/visual screening barrier in the vicinity of Maple Avenue, south and west of Dixie Highway in Fort Mitchell.
- KYTC is proposing a noise/visual screening barrier in the Mainstrasse neighborhood, including in the vicinity of the Goebel Park Complex.
- During final design, KYTC will coordinate with the City of Covington to evaluate the use of transparent noise barriers in some locations to preserve views of the Goebel Park Complex from the highway and to preserve views of the skyline and across I-71/I-75 from surrounding neighborhoods.
- In accordance with current policies, ODOT will transfer approximately ten acres of excess land opened up by refinements to the 3rd Street, 4th Street, 5th Street, and 6th Street ramps to the City of Cincinnati for potential redevelopment and/or public use.
- ODOT has committed to work with the City of Cincinnati to conduct before/after surveys of other roadways impacted by increased traffic during construction. ODOT will restore those roadways to pre-construction conditions once the project is complete.
- ODOT has committed to building a wider bridge on Ezzard Charles Drive over I-75 to provide an additional 50 feet of green space on each side that could support potential future civic space or retail development by the City of Cincinnati.

Refinements Considered and Dismissed

Based on public feedback, KYTC and ODOT considered several refinements that were ultimately dismissed, as summarized below:

- Depress I-75 and extend local streets across the highway to form an urban street grid in downtown Cincinnati. These concepts would not be geometrically feasible and would result in a greater project footprint than Concept I-W. Furthermore, these concepts would not maintain continuity along US-50, would increase traffic on the local street network in the City of Covington, and would not provide additional options for maintaining cross-river traffic if an incident or future construction or maintenance activities occur on the BSB.
- Extend the noise analysis area further west and include a noise barrier for residences in the vicinity of Summit Lane in Fort Mitchell. KYTC prepared a [*Technical Memorandum: Additional Traffic Noise Assessment Kentucky Southern Section*](#) (February 2023) which concluded that constructing a noise barrier along I-71/I-75 would not substantially reduce noise in the vicinity of Summit Lane.



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- Extend the pedestrian bridge across Winchell Avenue directly to Freeman Avenue in Cincinnati. Realigning the pedestrian bridge in this location would require constructing a pedestrian connection over I-75 and connecting it in the middle of the Freeman Avenue bridge. The current alignment for Concept I-W is more prudent from a design, constructability, and lifecycle cost perspective. The design of Concept I-W complies with the requirements of the Americans with Disabilities Act and provides an adequate crossing.
 - Construct a sidewalk or shared-use path along 5th Street in Cincinnati. A prior sidewalk connection on 5th Street was closed when the Fort Washington Way project was constructed due to safety issues associated with at-grade crossings of this high-speed connection. Those safety concerns will continue to exist for Concept I-W. KYTC and ODOT considered building a new pedestrian bridge or tunnel to provide grade-separated access along 5th Street. However, the cost was not found to be justified because alternate pedestrian and bicycle paths are available 500 feet north at 6th Street and another 400 feet north at 7th Street.
 - Reconnect Colerain Avenue across I-75 near the northern project limits. The abutments for the new Western Hills Viaduct bridge present a large obstruction that would preclude reconnecting Colerain Avenue across I-75.
 - Cap I-75 through downtown Cincinnati and the West End neighborhood. ODOT and KYTC considered options for capping I-75 in Ohio. Once the interstate passes over the Ohio River, it cannot descend directly into downtown Cincinnati. South of 5th Street, I-75 must stay elevated to cross active CSX rail lines between Pete Rose Way and 3rd Street. In addition, any design requires accommodating a complicated system of mainline and ramp movements to provide local access and continuity along I-71, I-75, and US-50. Depressing the roadway to support a freeway cap while meeting these geometric constraints would require steep roadway grades that would not meet design standards. Such steep grades would present traffic operational and safety concerns, particularly considering the high volumes of heavy truck traffic traveling through the corridor.

Between 5th Street and Ezzard Charles Drive, there are several areas where I-75 is relatively level with the surrounding land uses. A freeway cap could be constructed either by leaving I-75 at the current elevation or by lowering the interstate. If the existing I-75 elevation is maintained, a freeway cap would need to be constructed 20 to 30 feet over the highway to provide adequate clearance for the freeway lanes. Given the proximity of Western Avenue and Winchell Avenue, the freeway cap would either need to extend over these roads, or Western and Winchell avenues would need to be raised up to be level with the top of the cap. Transitioning from the top of the highway cap back to the elevations of the surrounding land uses in a way that provides accessible and open connections east and west of I-75 would substantially increase the project's footprint beyond what is considered reasonable and would impact low-income housing, schools, parks, historic structures, commercial and industrial businesses, and local streets. These impacts could be reduced through the extensive use of retaining walls along either I-75 or Western and Winchell avenues. However, the retaining walls would render the cap inaccessible from surrounding land uses and would only serve to create an even greater barrier through downtown Cincinnati and the West End neighborhood. Building a freeway cap by lowering I-75 would avoid the need for retaining walls; however, the interstate would need to be lowered by 20 to 30 feet,



which would require prohibitively steep grades to meet the geometric constraints of the CSX rail lines. Furthermore, capping the highway would likely require the removal of I-75 connections with 5th Street, 6th Street, 7th Street, and 8th Street and would not be able to accommodate US-50, which is an important regional connection.

I-75 is elevated above the surrounding land uses north of Ezzard Charles Drive. Capping the highway in this area would further exacerbate the concerns with geometric feasibility, impacts to surrounding land uses, and local accessibility discussed for portions of I-75 to the south.

Refinements to Be Evaluated During Design

In addition to the refinements already incorporated and based on preliminary investigations, several refinements suggested during public involvement activities appear to be feasible. During Phase III of the BSB Corridor Project, KYTC and ODOT will evaluate innovation concepts and will consider incorporating concepts that improve project quality, reduce costs, shorten schedule, support the project goals and objectives, and have support at the local level (see the *Public Engagement Plan* in Appendix Q). The following refinements suggested during public involvement activities will be further evaluated during the innovation process for the Phase III progressive design-build contract:

- Eliminate the 3rd Street ramp to the northbound collector-distributor system in Cincinnati and redirect traffic to the proposed connection at the end of the Clay Wade Bailey Bridge.
- Reconfigure the lanes on the Clay Wade Bailey Bridge to add bicycle lanes.
- Reconfigure 6th Street in Cincinnati to accommodate two-way traffic.
- Design concepts submitted by the Bridge Forward Coalition.

8. PUBLIC HEARINGS

Agencies and the public will have the opportunity to review the supplemental EA and other project information and provide comments to KYTC and ODOT for 30 days after it is made publicly available. During that time, in-person public hearings will be scheduled in Kentucky and Ohio. In addition, there will be a virtual public hearing. The public availability of the supplemental EA and the public hearings will be advertised through direct mailings, social media, press releases, print media, the project website, the project e-newsletter, and advertisements disseminated to the same neighborhoods that were engaged during the targeted EJ/neighborhood outreach. Direct mailings and flyers advertising the public hearings will include information in Spanish offering translation and interpretation services upon request. Comment forms will be available in both English and Spanish. The public hearings will provide opportunities for attendees to review exhibits and other project information. In addition, members of the project team will be available to answer questions. Verbal and written comments will be accepted at the hearing, as well. The comment period for the supplemental EA will last for 15 days after the public hearings.



9. ONGOING PUBLIC AND STAKEHOLDER INVOLVEMENT

KYTC and ODOT are committed to a robust public and stakeholder involvement process during the design and construction of the BSB Corridor Project. To facilitate public involvement and outreach, the project *Public Engagement Plan* (see Appendix Q) will be updated to guide public and stakeholder engagement (including environmental justice populations, identified socioeconomic groups, and disadvantaged communities) during detailed design and construction. As detailed in the *Public Engagement Plan* (see Appendix Q), the following opportunities for public and stakeholder involvement will occur:

- When innovations are proposed, KYTC and ODOT will share recommendations with local cities and counties and will gather feedback from local agencies that may be affected by any changes. Each local entity will be responsible for soliciting public feedback on innovations as part of their review and comment process. When KYTC, ODOT, and FHWA determine that an innovation will be incorporated into the project, the public will be informed of the decision.
- KYTC will coordinate with the Northern Kentucky cities along the corridor and Kentucky first responders to ensure the completed project accommodates emergency response access to the C-D and mainline roadways.
- KYTC, ODOT, and the design-build team will regularly engage with the BSB Corridor Diversity & Inclusion Outreach Committee during the Phase III progressive design-build contract.
- KYTC will continue to coordinate with the City of Covington during the implementation of measures to mitigate adverse effects to the Lewisburg Historic District.
- ODOT will continue to coordinate with the Cincinnati Preservation Association during the implementation of measures to mitigate adverse effects to Longworth Hall.
- KYTC will provide information about the demolition contractor to the Kenton County Historical Society and the City of Covington Historic Preservation Office to allow the interested parties to discuss the possibility of material recovery and salvage.
- ODOT will provide the Ohio Section 106 consulting parties an opportunity to review and comment on final design plans in Ohio.
- KYTC will conduct a noise abatement public meeting and surveys with benefited receptors at each location where noise barriers are proposed and will coordinate with the City of Covington to evaluate the use of transparent noise barriers to preserve views of Goebel Park from the highway and to preserve views of the skyline and across I-71/I-75 from surrounding neighborhoods.
- ODOT will conduct noise abatement public involvement with benefited receptors where noise abatement has been determined to be feasible and reasonable.
- KYTC and ODOT will engage the Aesthetics Committee and the Aesthetics Subcommittees to finalize and confirm aesthetic treatments in the corridor.



-
- Local cities and counties will be given the opportunity to review and comment on the traffic management plan, the maintenance of traffic plan, and the incident management plan for all project phases. These plans will also be coordinated with the regional incident management task force. KYTC and ODOT will also continue to communicate and coordinate construction activities with local cities and counties.
 - KYTC and ODOT will continue utility coordination throughout design and construction for each project phase.
 - KYTC will continue to coordinate with the City of Covington regarding its new master planning efforts for the Goebel Park Complex, the schedule for construction activities affecting the complex, and the transfer of replacement land within the complex.
 - ODOT will continue to coordinate construction activities affecting the Firefighters Memorial and Ezzard Charles Park with the City of Cincinnati.
 - Prior to construction, ODOT will coordinate with the City of Cincinnati develop cost sharing and maintenance agreements for the widened portions of the Ezzard Charles Drive bridge over I-75.
 - KYTC and ODOT will continue to coordinate with the Project Advisory Committee to provide project updates and gather feedback during the design and construction of the project.
 - For all project phases, information about design decisions, construction sequencing, project highlights, and construction schedules will be shared with the public through project website updates, social media, e-newsletters, local media, presentations to local groups, and virtual project updates. Information about ongoing project activities will be shared on a regular basis, and information about milestones (such as the start of a construction phase) will be shared as appropriate. Specific to the Phase III progressive design-build contract, the public will be informed of major decisions, as appropriate.
 - KYTC and ODOT will establish multiple methods for the public to make inquiries about the project during detailed design and construction (including via the project website, email, direct mailings, and phone) and will provide timely responses to inquiries that are received.



Appendix A:
Draft Travel Survey Analysis





BRENT SPENCE
BRIDGE CORRIDOR

Moving the Economy,
Creating Jobs

BRENT SPENCE BRIDGE PROJECT

DRAFT TRAVEL SURVEY ANALYSIS

FEBRUARY 2014



Prepared by Steer Davies Gleave

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1. Introduction

In order to better understand the Brent Spence Bridge market, and to estimate behavioral parameters specific to this market, Steer Davies Gleave conducted behavioral and stated preference (SP) surveys of current Brent Spence bridge users and neighboring competing bridge travelers. The behavioral survey is used to develop data and forecasting model inputs needed for the traffic and revenue study.

This section covers the following:

- The first part describes the survey goals, administration and sample profile results. This includes socio-economic profiles, current Brent Spence Bridge users behavioral characteristics and trip frequencies and patterns.
- The second part focuses on the quantitative behavioral analysis of Brent Spence Bridge users to derive values of time to be used in the modeling, based on stated preferences from the survey and on observed (revealed) travel behavior.

2. Survey Design

2.1 Survey Goals

In order to provide additional behavioral information on the markets served by the Brent Spence Bridge and neighboring competing roads and to establish key forecasting parameters - such as trip frequency and willingness to pay - a behavioral and trip pattern survey was conducted by Steer Davies Gleave.

The Brent Spence Bridge behavioral survey was used to elicit qualitative and quantitative information from drivers who currently make private vehicle (car, van, truck, SUV) trips using Interstates I-71/I-75 or I-471 across the Ohio River between Covington, Kentucky and Cincinnati, Ohio (see **Exhibit 2-1**).

Exhibit 2-1: SP Survey Qualifying Trip Study Area



The survey was designed to collect a wide range of contextual, attitudinal and choice data, as well as, socioeconomic and demographic characteristics of current Brent Spence Bridge users. The socioeconomic and demographic data was important to identify the current Brent Spence Bridge users. Contextual data was gathered to get as much information as possible on the factors influencing people's current trip making characteristic.

Choice data refers to the likelihood that a person might elect to keep using the proposed tolled Brent Spence Bridge in the future, over switching to using one of the free bridges for a similar trip. The choice information was collected using Stated Preference (SP) techniques in order to understand people's preferences, and how they use those preferences to make choices. Finally, attitudinal data was collected to evaluate people's inherent biases and opinions to the proposed project.

The web survey was designed by SDG and administered by Crescent, a market research firm. The main survey was completed between December 3 and December 12 2013, and a total of 1,001 completed surveys were received from a sample of corridor residents.

The goals of the behavioral and trip pattern survey included:

- Developing a qualitative and quantitative understanding of how people make choices between using a toll bridge and alternative un-tolled routes based on attitudinal questions
- Collecting trip pattern information in the markets served by the Brent Spence Bridge and gain insight on frequency profiles of road users
- Collecting willingness to pay for travel time savings information based on stated preference scenarios

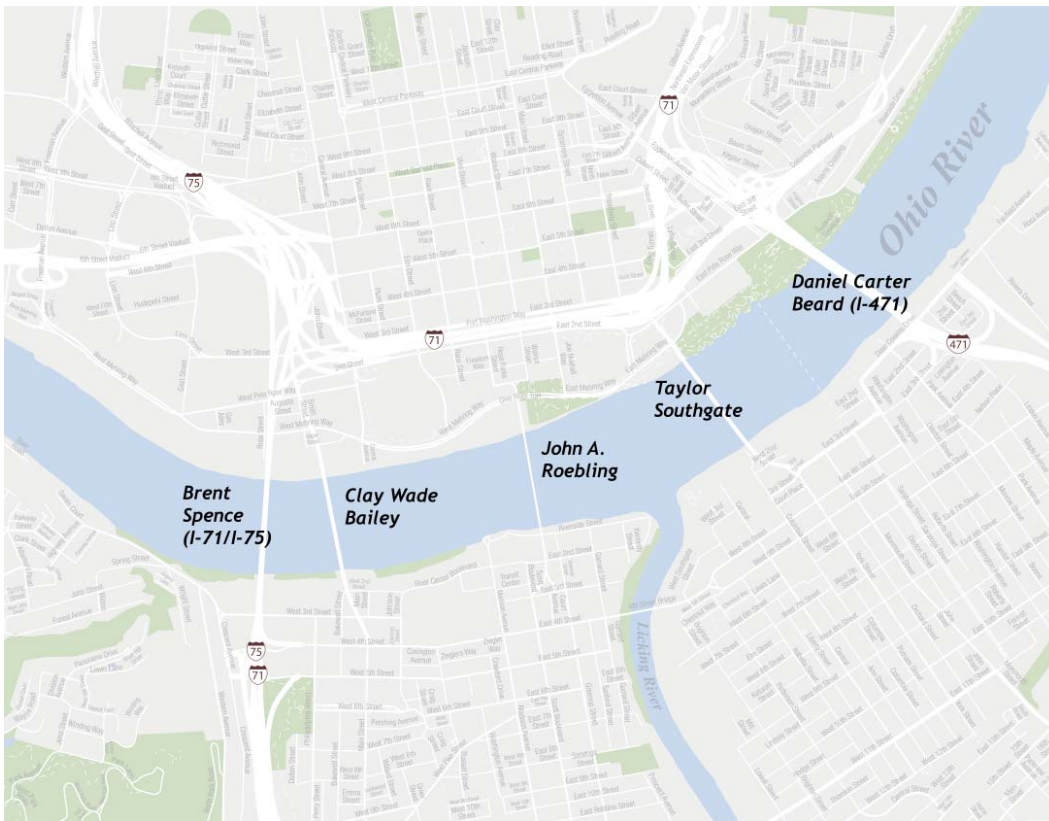
2.2 Survey Design

The survey questionnaire collected revealed preference and stated preference (SP) data.

The revealed preference data gathered information on actual driving behavior, based on respondents most recent trip using Interstates I-71/I-75 or I-471 across the Ohio River between Covington, Kentucky and Cincinnati, Ohio. Travel time, costs and other key information were collected, as well as attitudinal information such as perception of congestion and attitude toward tolling. Other information was also collected, including more detailed data on trip purpose, income categories and behavioral statements to allow further segmentation.

The SP data was collected through a route choice exercise, asking respondent to make hypothetical choices between using the tolled Brent Spence Bridge or the alternate free routes. Exhibit 2-2 shows the Brent Spence bridge and neighboring competing bridges.

Exhibit 2-2: Brent Spence Bridge and Neighboring Competing Bridges



The survey instrument was also designed to collect data on frequency of trips. The survey also collected travelers' knowledge of alternatives, price sensitivity, perceptions of congestion, attitudes about transportation issues such as congestion and tolling, and relevant socioeconomic data.

Various choice exercises were included to assess the willingness to pay of each market. The questionnaire took between 15-20 minutes to complete on average, with about 60 questions for any given respondent - including screening questions and the choice exercise.

The survey was structured as follows:

- Screening questions about trips made across the Ohio River using either the I-71/I-75 Brent Spence bridge or the I-471 Daniel Carter Beard bridge in the last 6 months including trip frequency
- Detailed questions about a typical recent trip across the Ohio River, including:
 - Trip purpose, time and day, and trip origin-destinations
 - Total travel time, perception of congestion
 - Travel costs
 - Next best alternative route / bridge

- Travel party and paying member(s)
- Questions about congestion and other bridges usage
- Introduction to the new bridge project, including questions about respondents attitudes toward the project and alternate route they would be seeking
- Route choice SP exercise consisting of 9 trade-off questions
- Attitudinal questions about perception on tolling
- Socio-economic questions pertaining to respondent income, occupational status, age, transit usage, etc.

2.3 Survey Implementation

Steer Davies Gleave prepared a survey questionnaire that was administered through an online survey to a selected panel of respondents. Steer Davies Gleave developed and analyzed the survey, while the Internet questionnaire was hosted by our partner Crescent Research, a market research firm with a large online panel of respondents across the region. The data were collected via the Internet in December 2013.

Respondents were recruited using the following sampling plan: A target of 1,000 returns or complete questionnaires was selected, a large sample that enables significant market segmentation of road users. A total of 1,001 completed questionnaires were received.

Steer Davies Gleave carefully designed the main questionnaire which was customized interactively for each respondent, based on their answers. For example, respondents are likely to have different behavioral patterns based on their attitude towards using freeways as against local roads. When respondents elected for the Daniel Carter Beard Bridge as their preferred alternate route, the detour presented in the SP exercise was via a freeway with access ramps; while when the respondents chose the Clay Wade Bailey Bridge, the John A. Roebling Bridge or the Taylor Southgate Bridge as their preferred alternate route to using Brent Spence, the detour was via a free route with local roads including traffic signals.

When respondents displayed lower than expected values of travel time savings, additional questions were asked to understand better their choices.

The advantage of using a pre-recruited panel of residents is that no additional incentives are required since the respondents are selected and remunerated by the market research firm. It therefore provides a random sample of residents with lower bias than self-selected respondents.

3. Brent Spence Bridge Travelers User Profiles

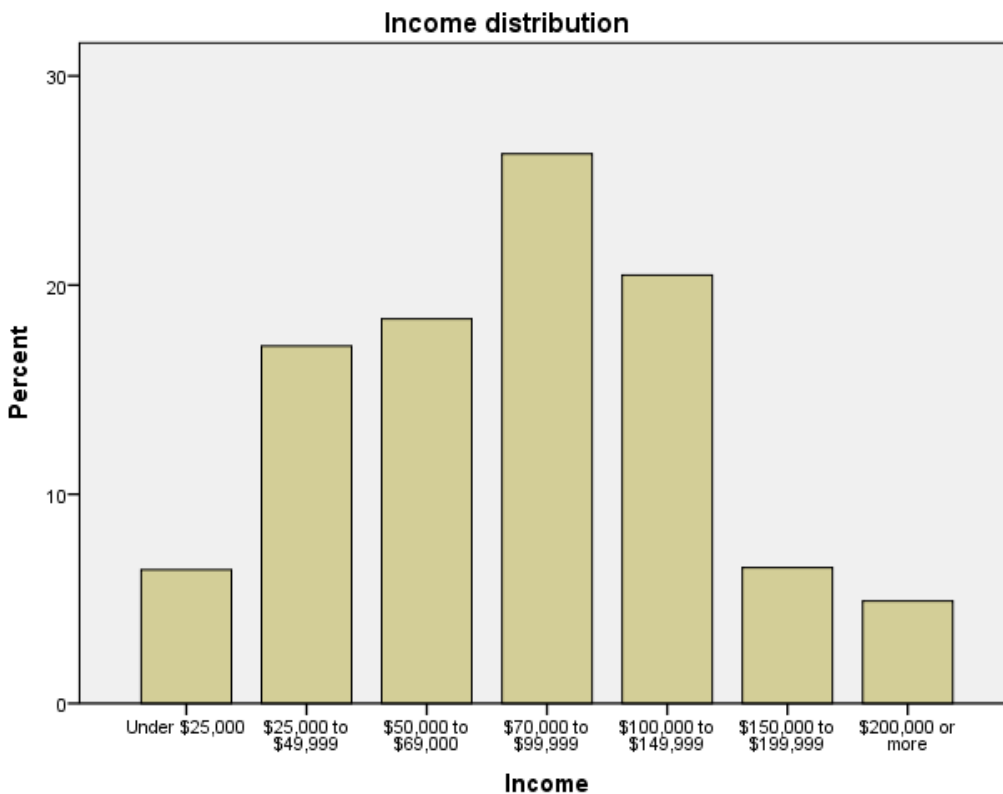
3.1 Sample Size

A summary of the socio-economic and demographic characteristics of the respondents, their current travel conditions, and their attitudinal and travel preferences are presented in this section. Unless indicated otherwise, results below are based on 1,001 respondents.

3.2 Socio-Economic Profile of Bridge Users

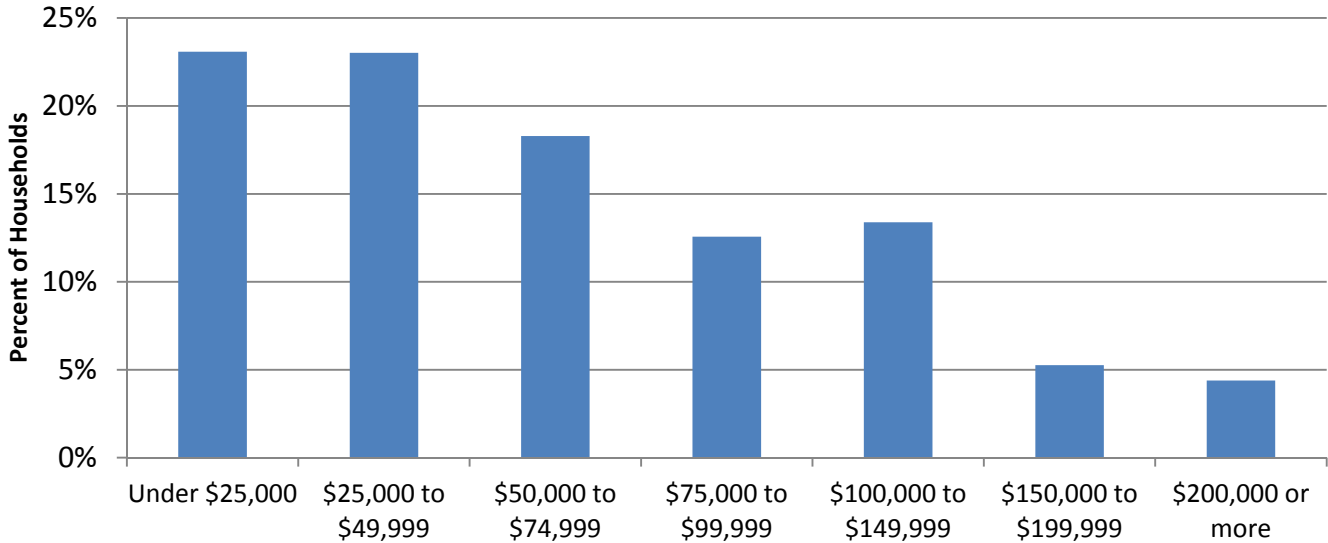
The household income distribution of the bridge users collected in our survey is higher to that of the Cincinnati MSA region¹, as indicated by the exhibits below. **Exhibit 3-1** shows the profile income distribution while **Exhibit 3-2** shows the MSA household income distribution from the US Census bureau.

Exhibit 3-1: Bridge Users Sample Income Distribution



¹ Data from US Census for Cincinnati-Middletown, OH-KY-IN Metro Area

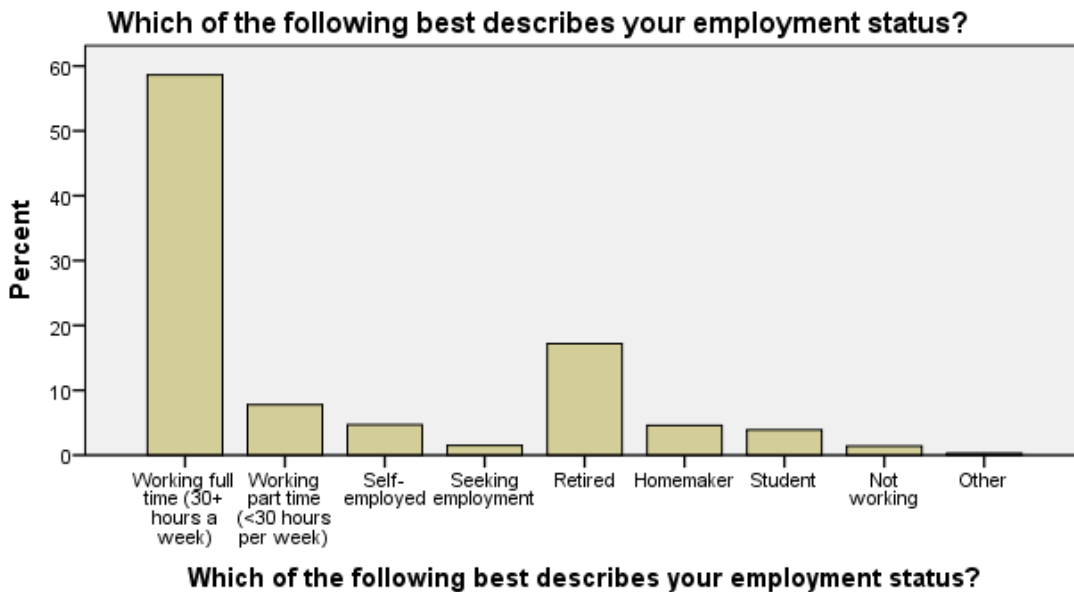
Exhibit 3-2: Cincinnati-Middletown, OH-KY-IN Metro Area Household income In the Past 12 Months



Source: U.S. Census Bureau, 2008-2012 American Community Survey 5-Year Estimates – Household income in the past 12 months (in 2012 inflation-adjusted dollars), Table B19001, Geography: Cincinnati-Middletown, OH-KY-IN Metro Area

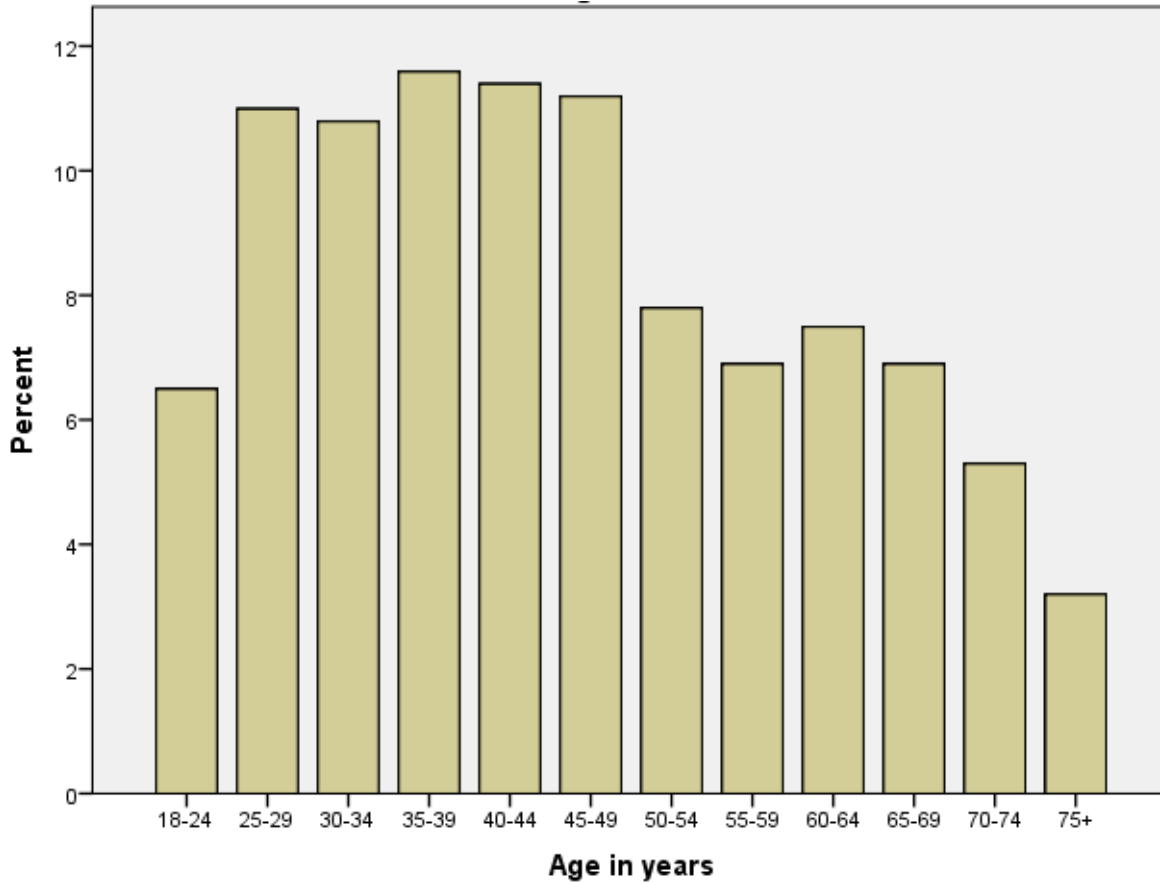
Almost 60% of the bridge users sample collected indicated working full time (Exhibit 3-3), which may explain the higher income distribution.

Exhibit 3-3: Bridge Users Sample Employment Status



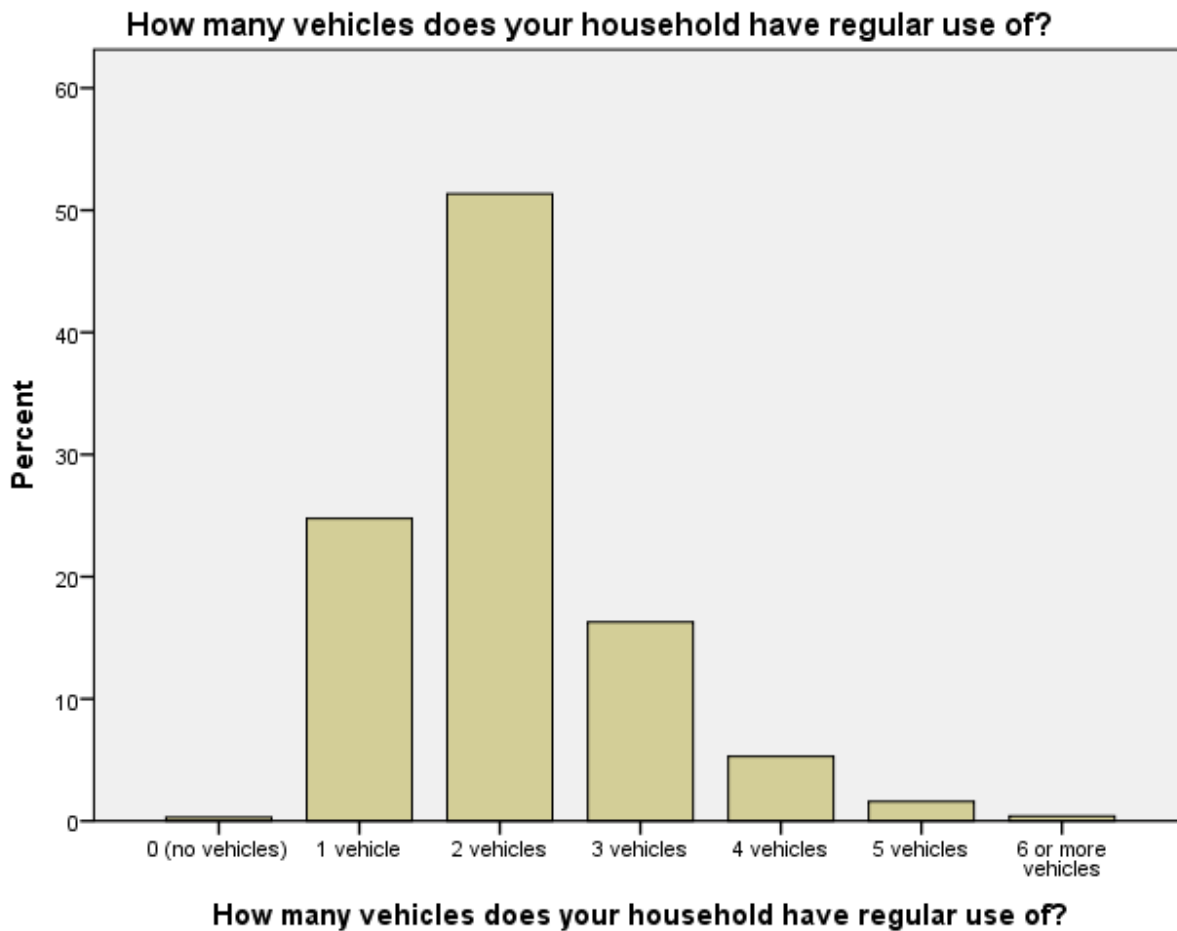
The 25 to 65 years old age group made up most of the responses (**Exhibit 3-4**).

Exhibit 3-4: Bridge Users Sample Age Distribution



75% of the respondents indicated having more than 1 vehicle (**Exhibit 3-5**), also indicating usage of the bridge by higher income households.

Exhibit 3-5: Bridge Users Sample Car Ownership



3.3 User Travel Patterns

The survey collected data on trip patterns for both frequent and non-frequent travelers, as shown in **Exhibit 3-6**.

The survey also collected public transit (PT) usage across the river (**Exhibit 3-7**). Almost 95% of the respondents indicated never or less than once a month using PT to cross the river, indicating that tolling the bridge should not significantly affect PT ridership across the bridges.

Exhibit 3-6: Bridges' Recent Usage

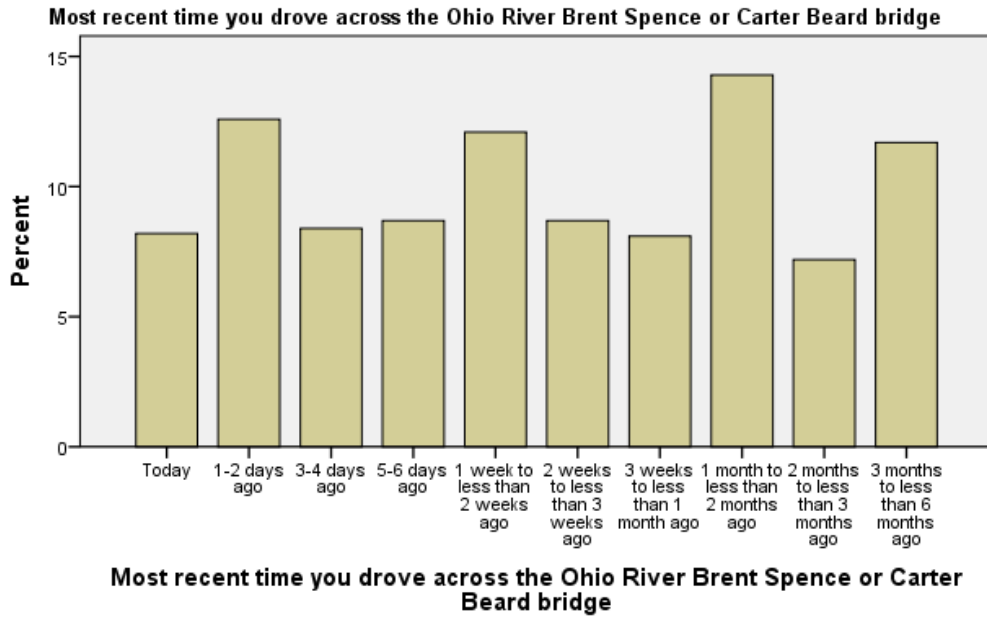
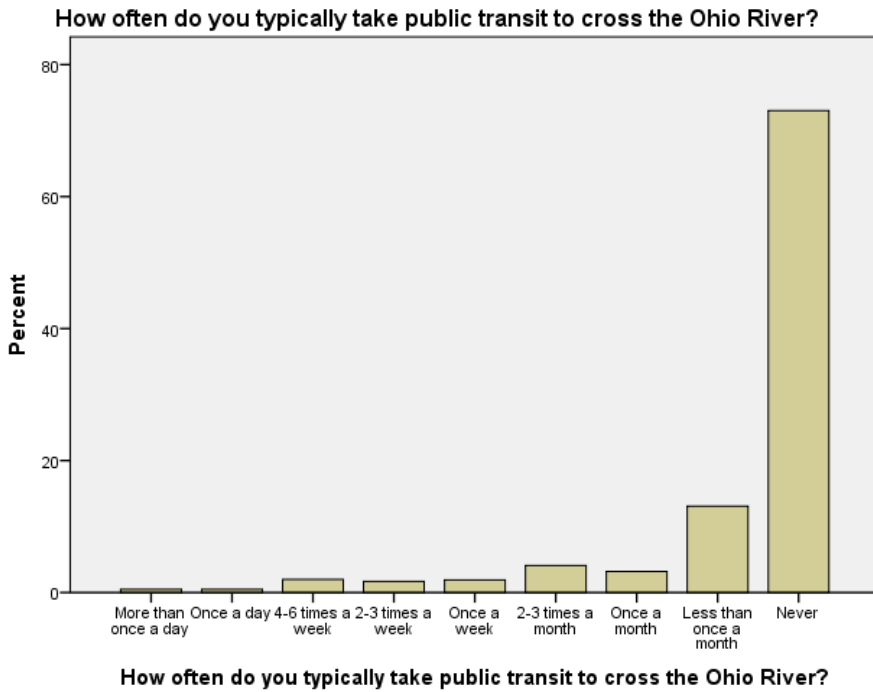


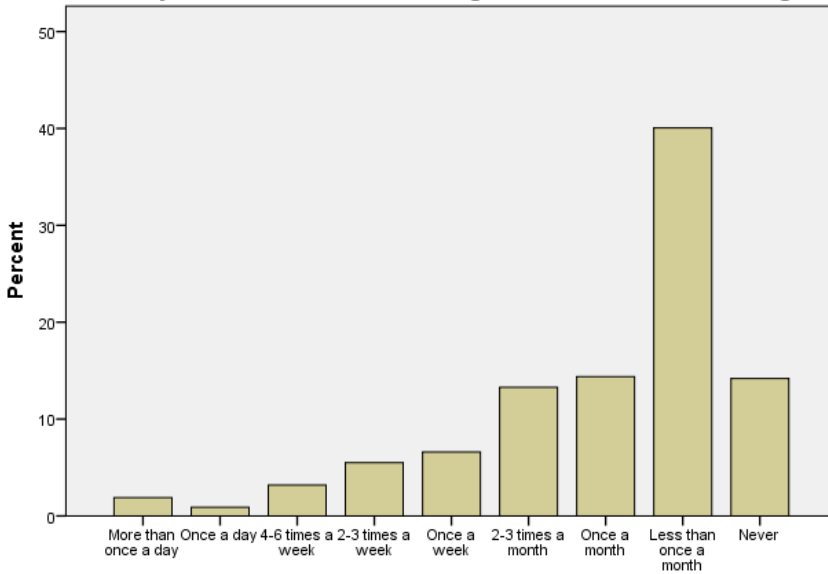
Exhibit 3-7: Public Transit Usage



A large proportion of the respondents indicated using the bridge less than once a month (Exhibit 3-8).

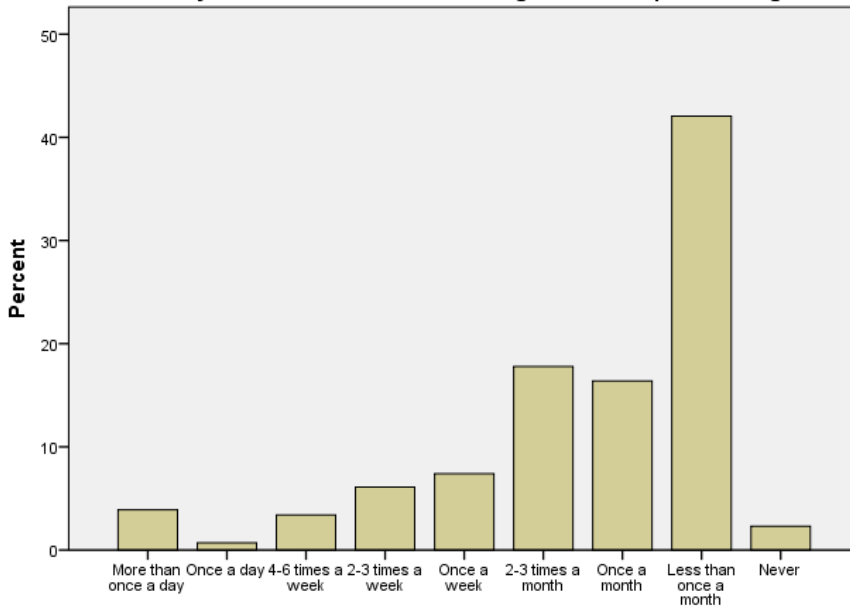
Exhibit 3-8: Bridges Usage Frequency

How often do you cross the Ohio River using the Daniel Carter Beard bridge?



How often do you cross the Ohio River using the Daniel Carter Beard bridge?

How often do you cross the Ohio River using the Brent Spence bridge?

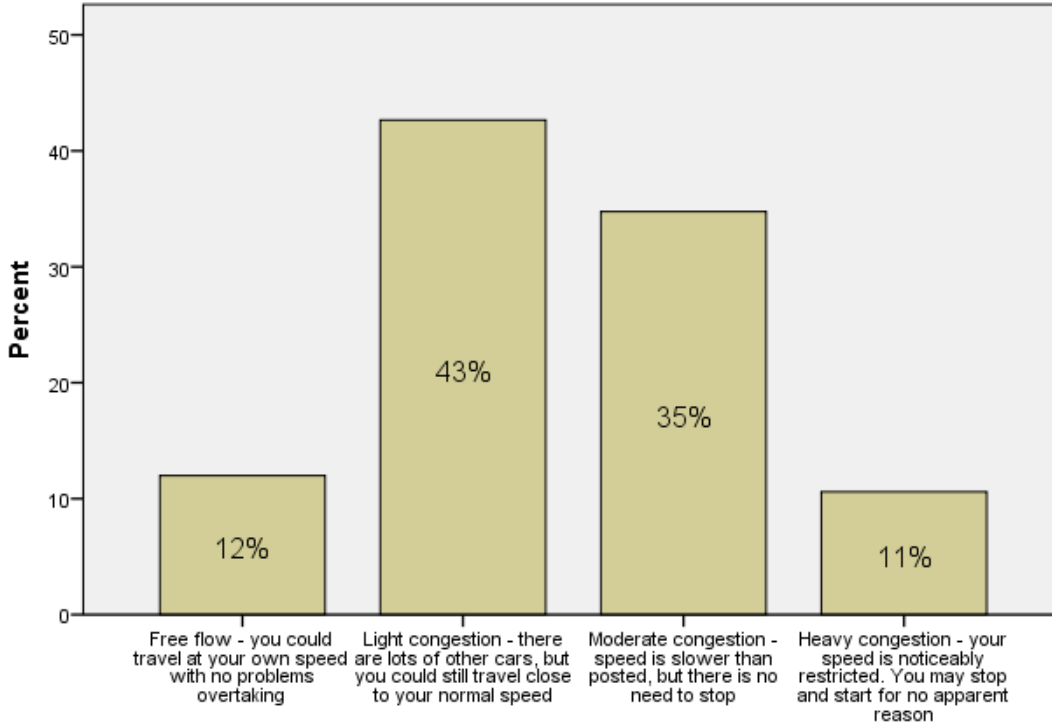


How often do you cross the Ohio River using the Brent Spence bridge?

Respondents were asked to qualify the driving condition during their most recent trip across the river. 88% reported some levels of congestion (Exhibit 3-9).

Exhibit 3-9: Perception of Congestions

In general how would you describe the driving conditions during a typical drive



In general how would you describe the driving conditions during a typical drive

A series of questions pertaining to the most recent trip were asked, and summarized below (Exhibit 3-10: Day of the Week, Exhibit 3-11: Trip Purpose, Exhibit 3-12: Origin Location, Exhibit 3-13: Departure Time, Exhibit 3-14: Destination Location).

Exhibit 3-10: Day of the Week

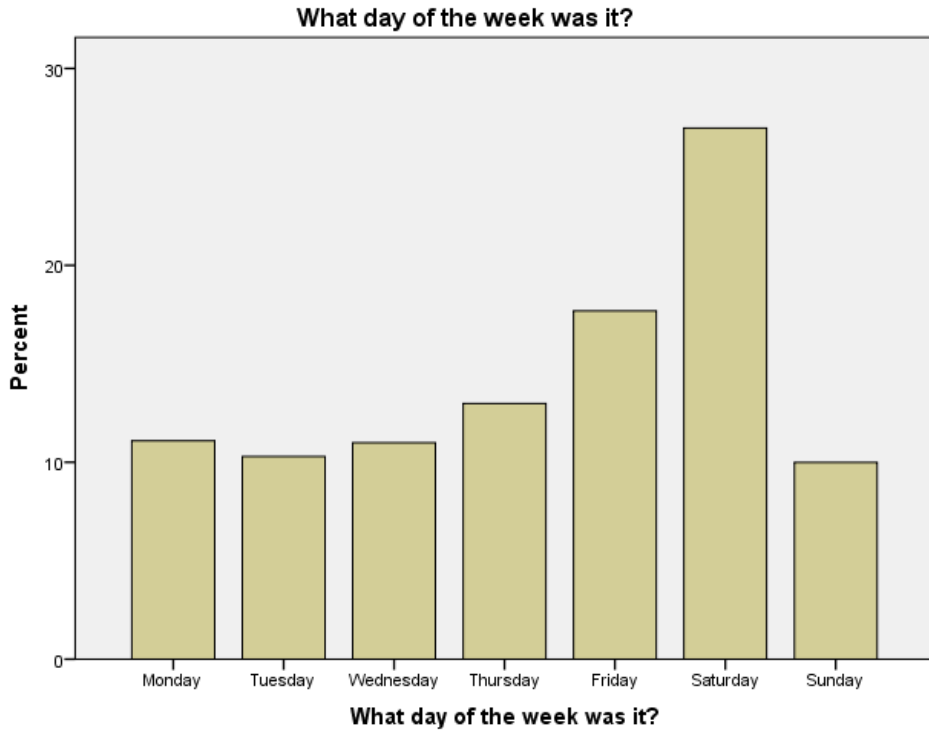


Exhibit 3-11: Trip Purpose

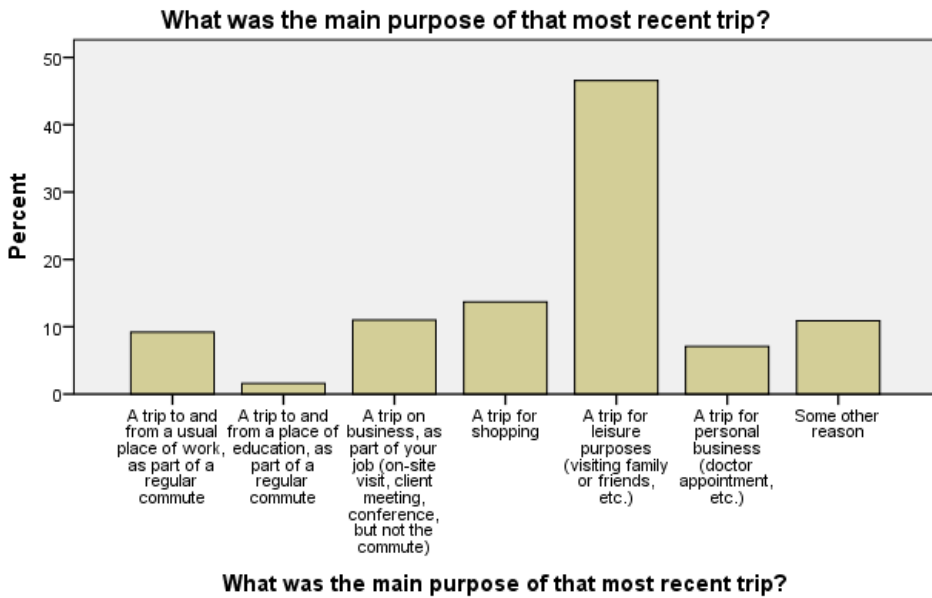
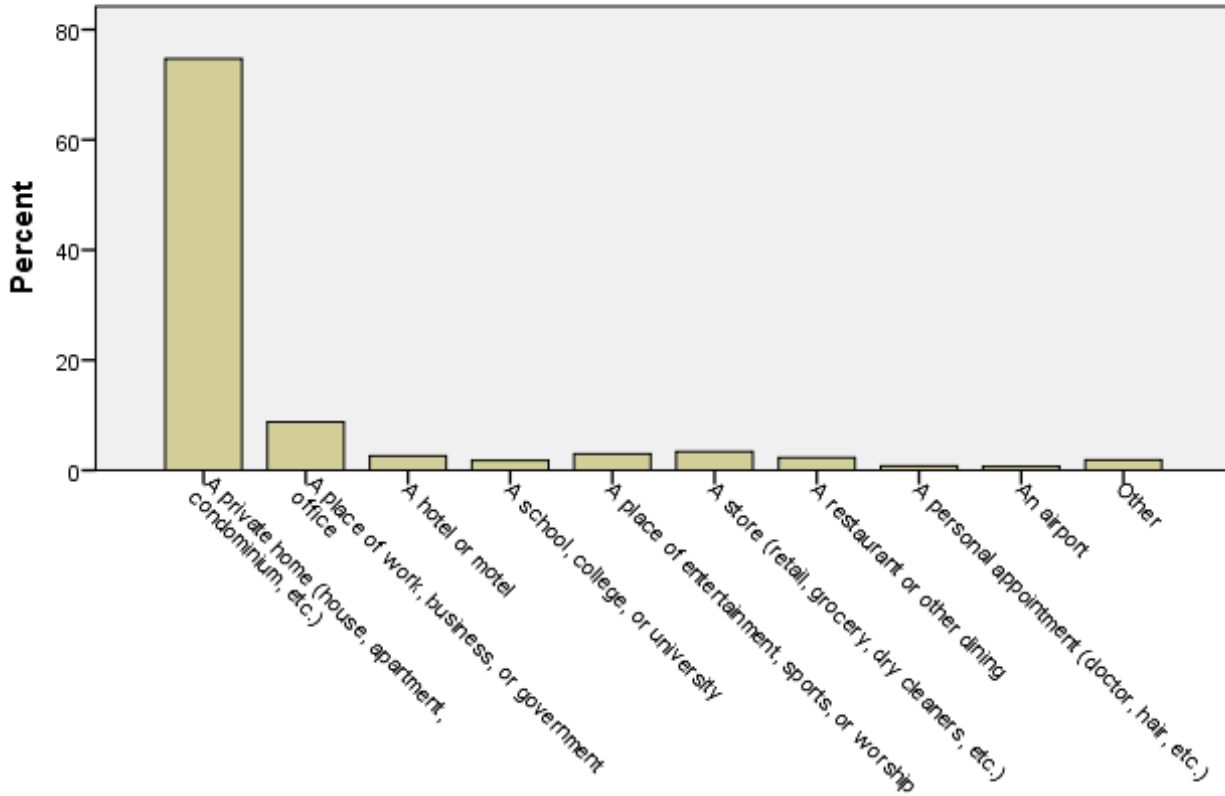


Exhibit 3-12: Origin Location

Which of the following best describes the point where your recent trip started?



Which of the following best describes the point where your recent trip started?

Exhibit 3-13: Departure Time

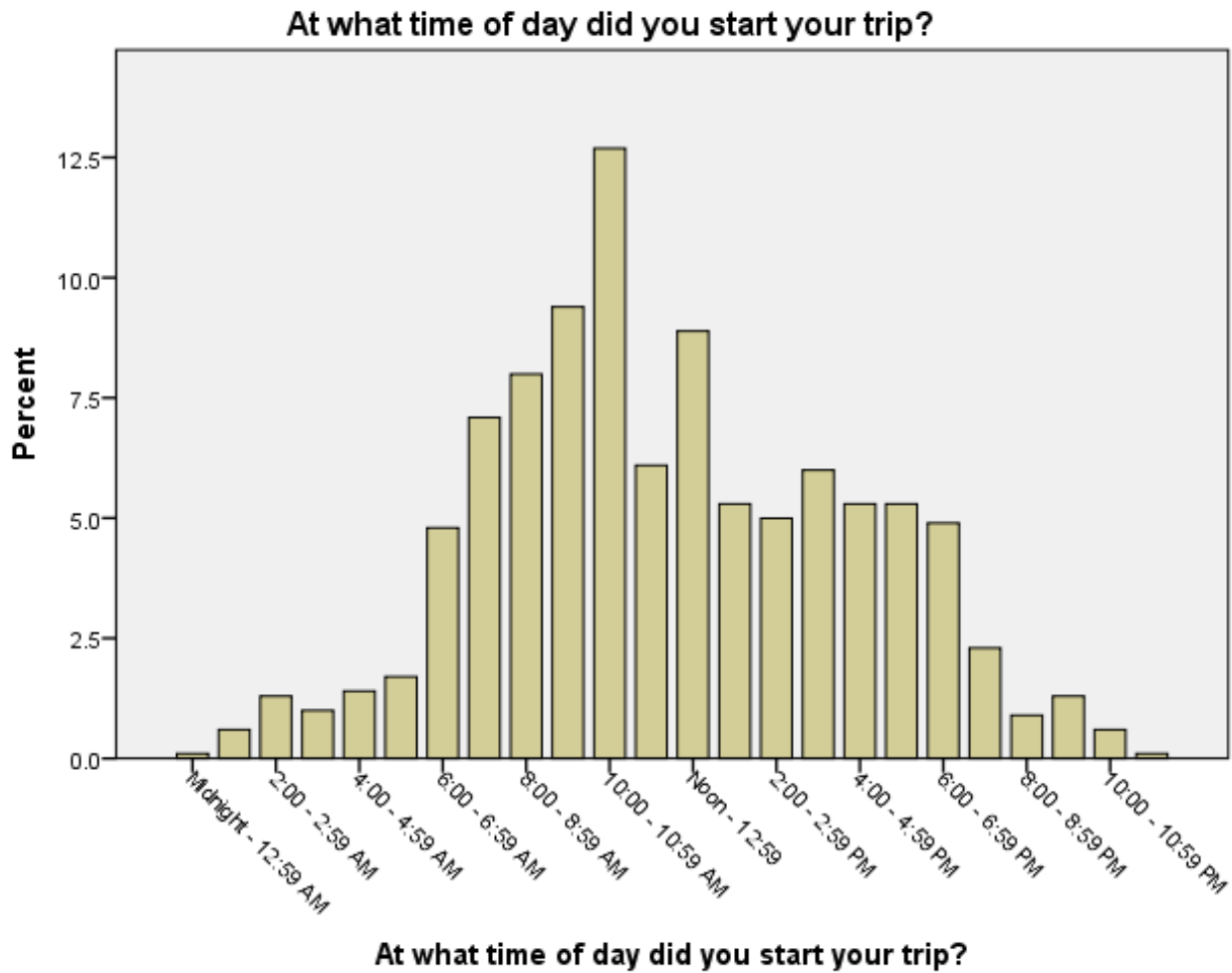
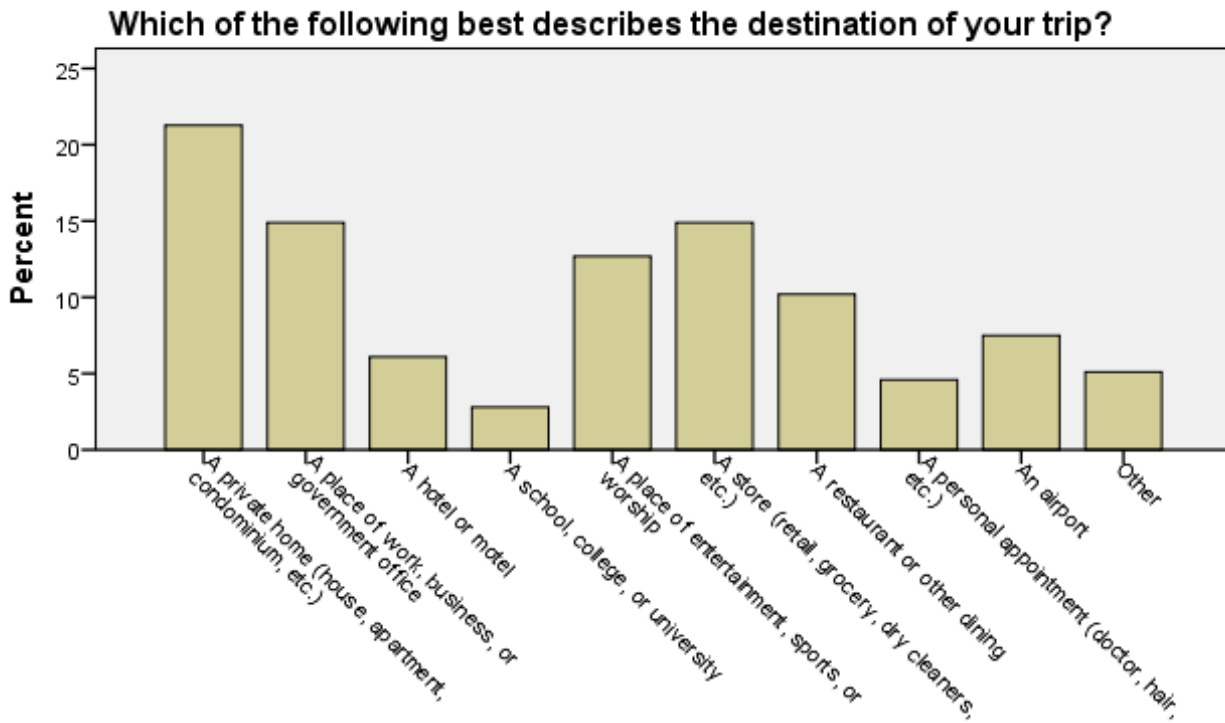


Exhibit 3-14: Destination Location

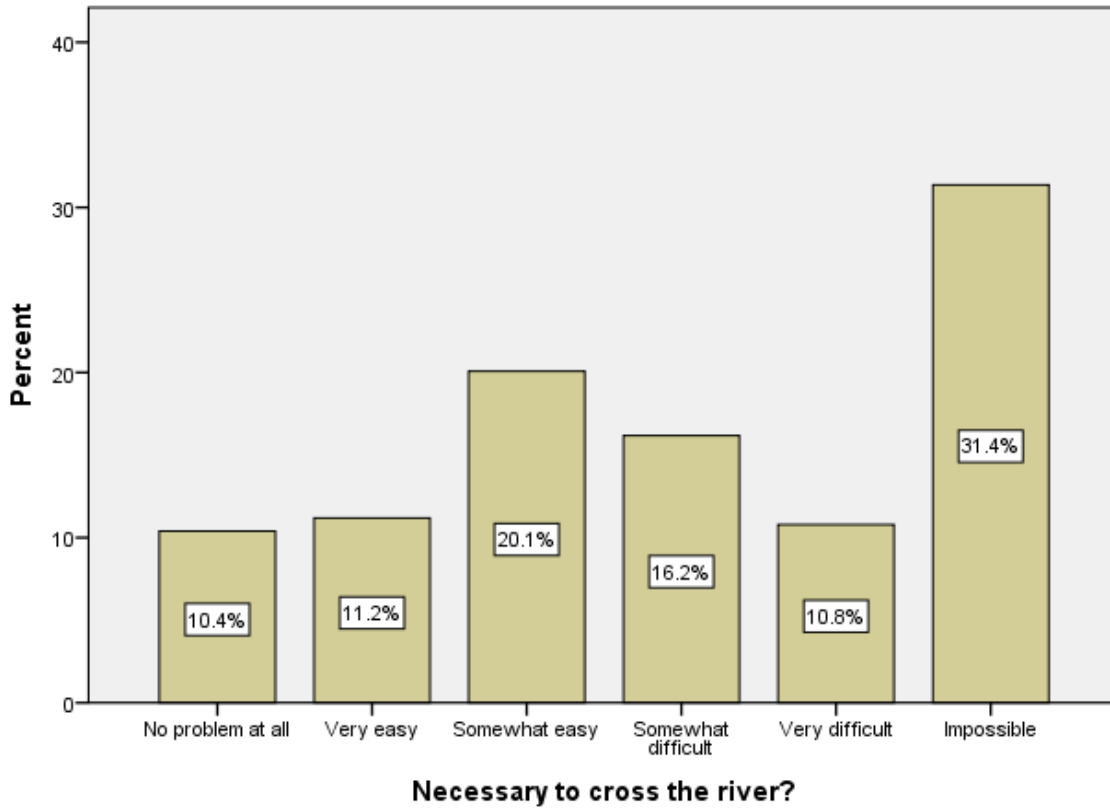


Which of the following best describes the destination of your trip?

More than 55% indicated that it would have been difficult or impossible to conduct the same activity without crossing the bridge (Exhibit 3-15).

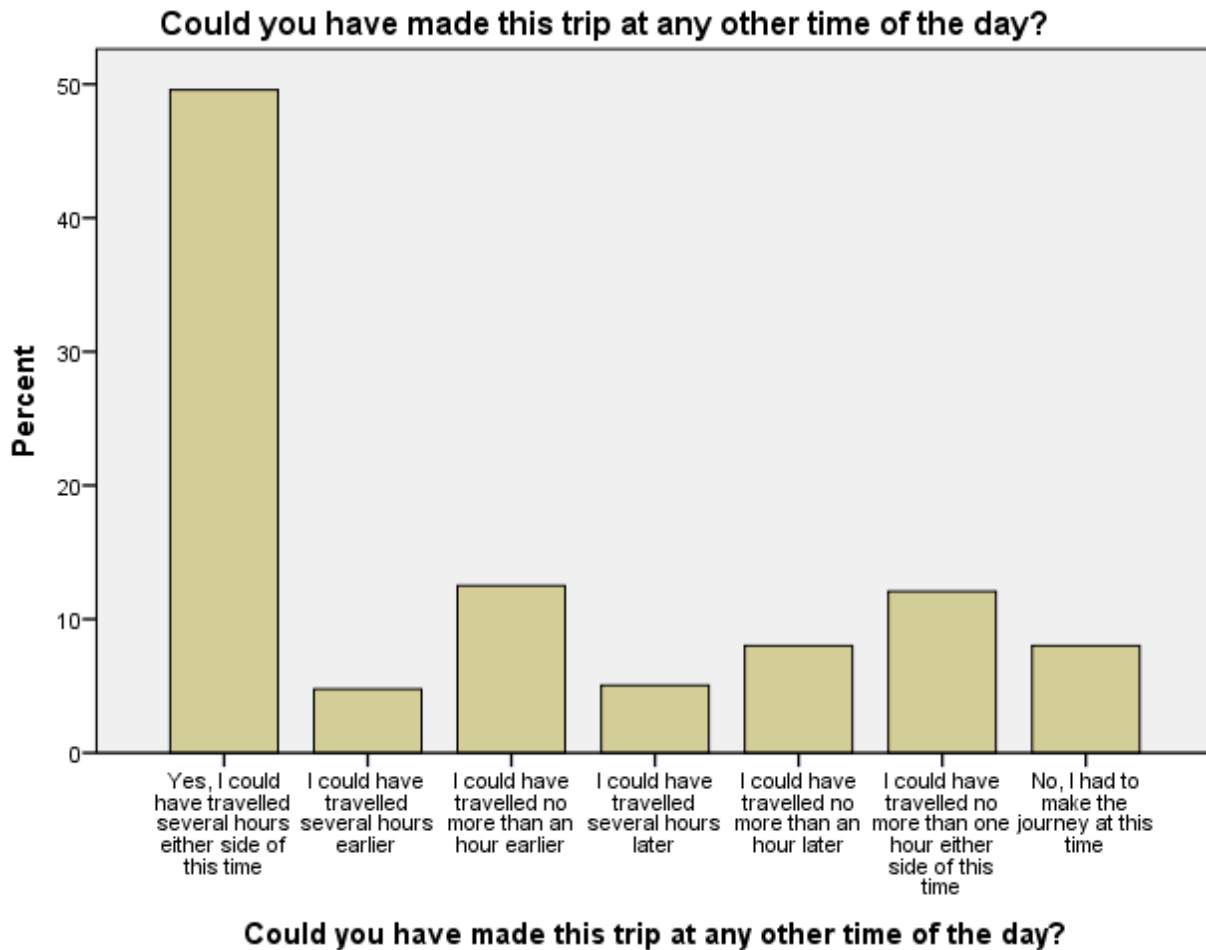
Exhibit 3-15: Necessity to Cross the Bridge

Could you complete the same activity so that it would not be necessary to cross the river?



71% indicated having flexibility on when to make the trip. 50% could have traveled several hours before or after their departure time (Exhibit 3-16).

Exhibit 3-16: Flexibility of Departure Time



Business trips and trips for work showed less flexibility, where most travel could have started no more than an hour earlier or later (**Exhibit 3-17**)

Exhibit 3-17: Flexibility of Departure Time by Trip Purpose

Trip Purpose	I could have travelled several hours either side of this time	I could have travelled several hours earlier	I could have travelled several hours later	I could have travelled no more than an hour earlier	I could have travelled no more than an hour later	I could have travelled no more than one hour either side of this time	I had to make the journey at this time
to and from a usual place of work	19.0%	9.5%	2.4%	16.7%	14.3%	16.7%	21.4%
to and from a place of education	30.0%	10.0%	0.0%	30.0%	10.0%	0.0%	20.0%
on business, as part of your job	39.4%	0.0%	9.1%	21.2%	6.1%	16.7%	7.6%
for shopping	68.8%	4.0%	5.6%	6.4%	7.2%	5.6%	2.4%
for leisure purposes (visiting family or friends, etc.)	52.5%	5.5%	4.7%	10.8%	7.4%	13.2%	5.8%
for personal business (doctor appointment, etc.)	34.5%	0.0%	3.4%	24.1%	13.8%	6.9%	17.2%
For some other reason	34.4%	4.9%	4.9%	14.8%	8.2%	14.8%	18.0%

25% of respondents made stops along the way (**Exhibit 3-18**), and 63% of these indicated choosing the route they did because they had to make these stops.

Exhibit 3-18: Stops Along The Way

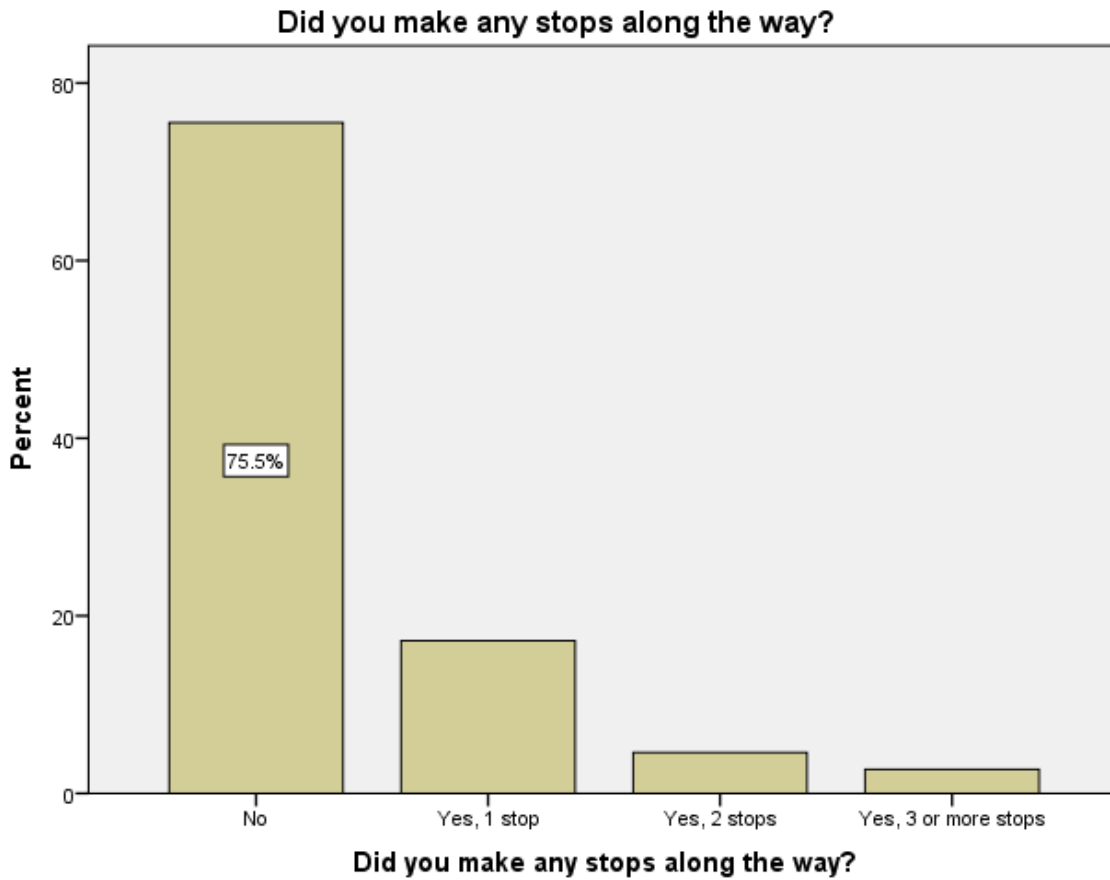


Exhibit 3-19 shows the outbound trip travel time distribution, with a mean travel time of 52 minutes and a median of 35 minutes. The return trip displayed similar (if somewhat slightly higher) statistics with a mean travel time of 54 minutes and a median of 40 minutes (Exhibit 3-20).

Exhibit 3-19: Outbound Travel Time Distribution

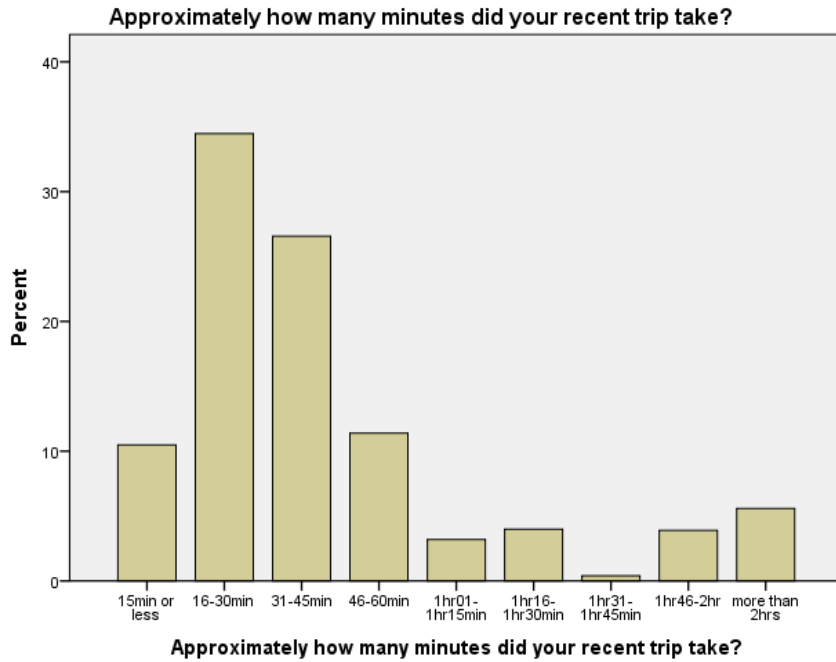
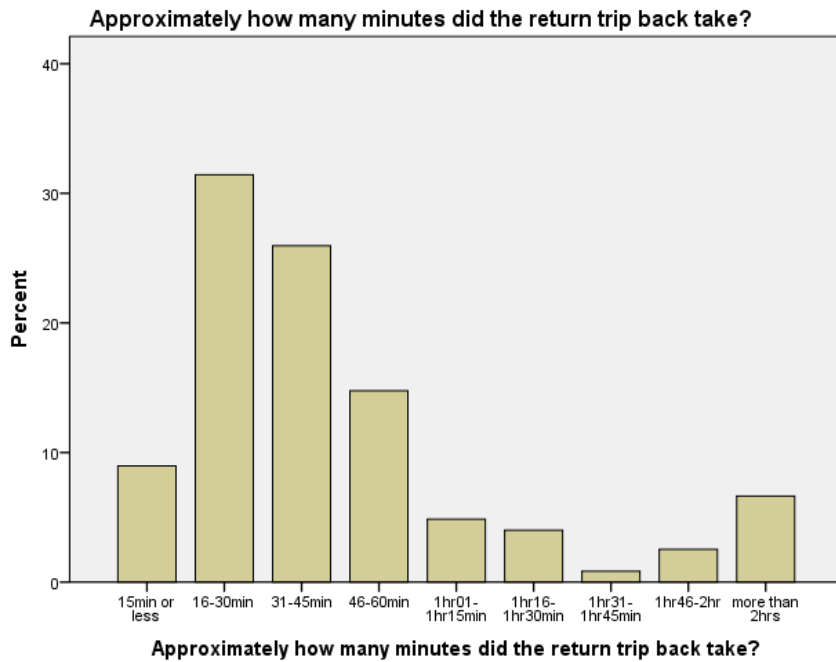


Exhibit 3-20: Return Trip Travel Time



Vehicle occupancy was on average 2 persons per car, with a mean of 2.02 and median vehicle occupancy of 2.00 (**Exhibit 3-21**). Commuting trips have the lowest vehicle occupancy (mean 1.21) as well as business trips (mean 1.41) as shown in **Exhibit 3-21**.

Exhibit 3-21: Vehicle Occupancy

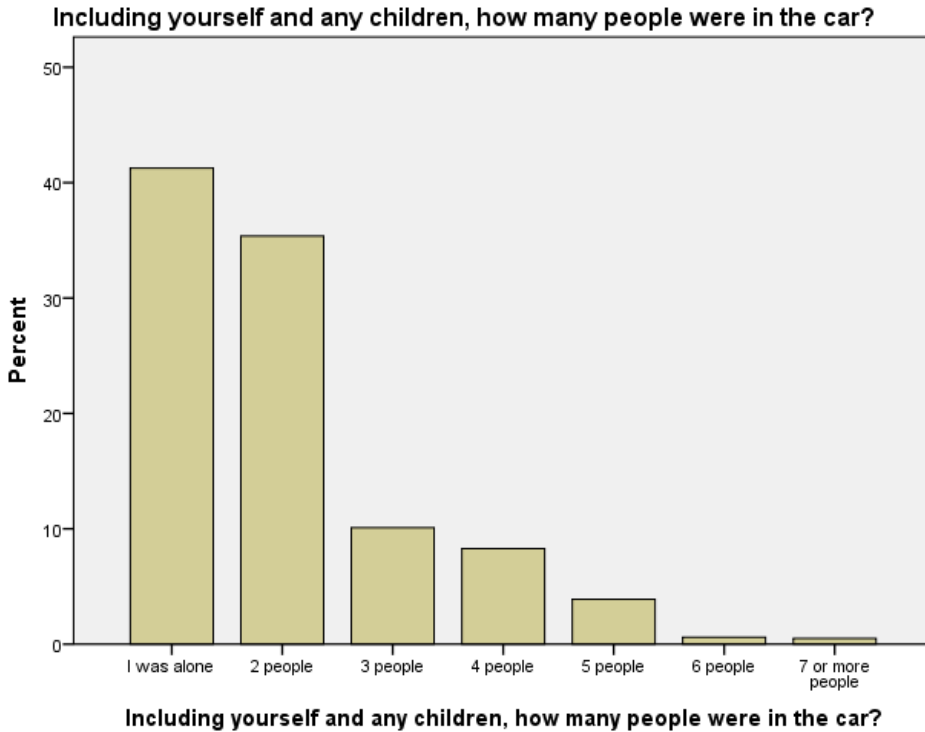
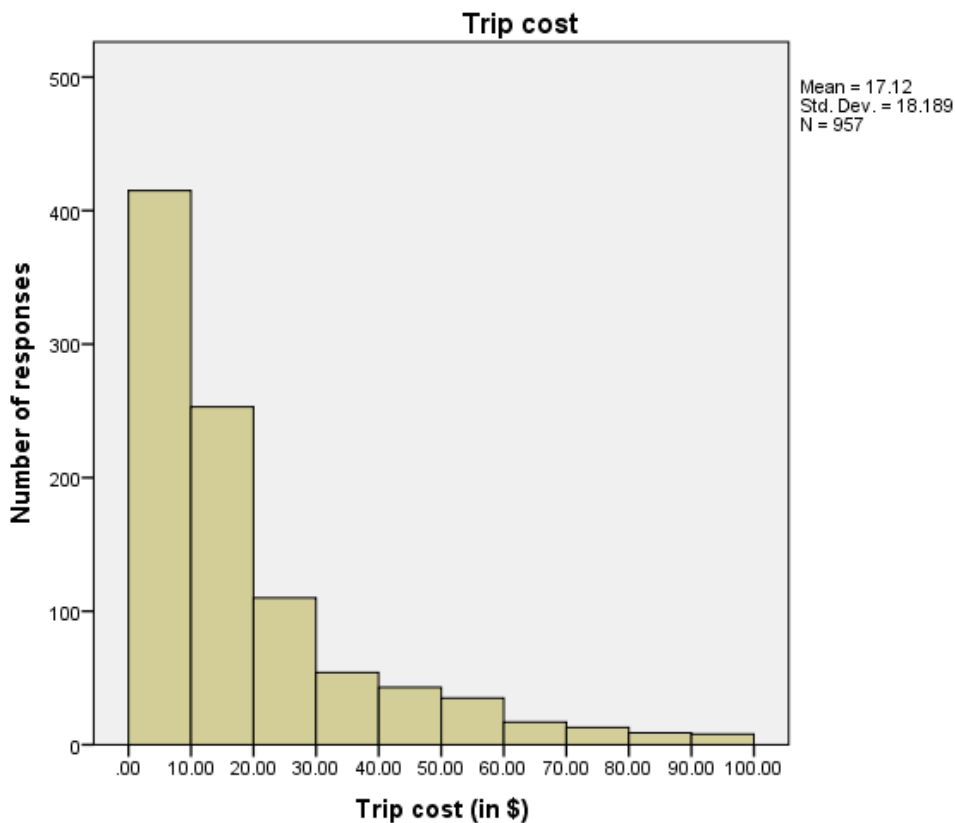


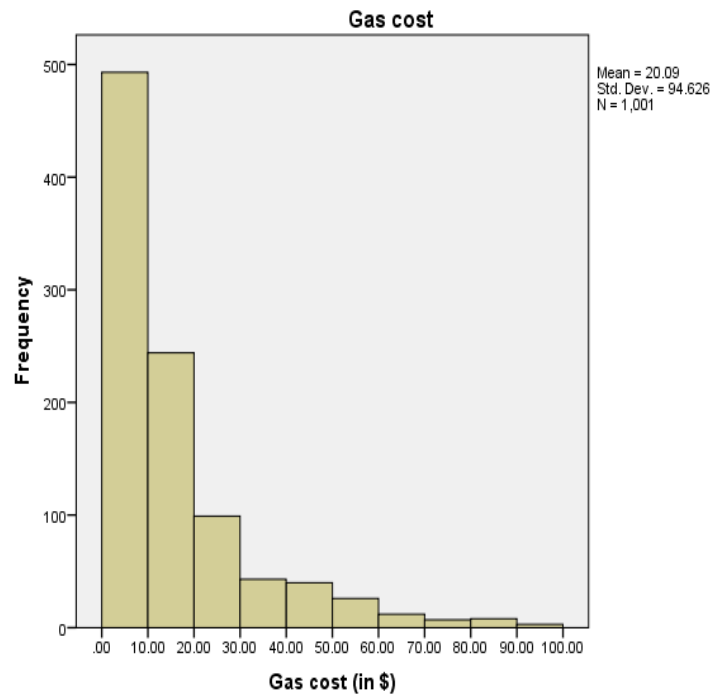
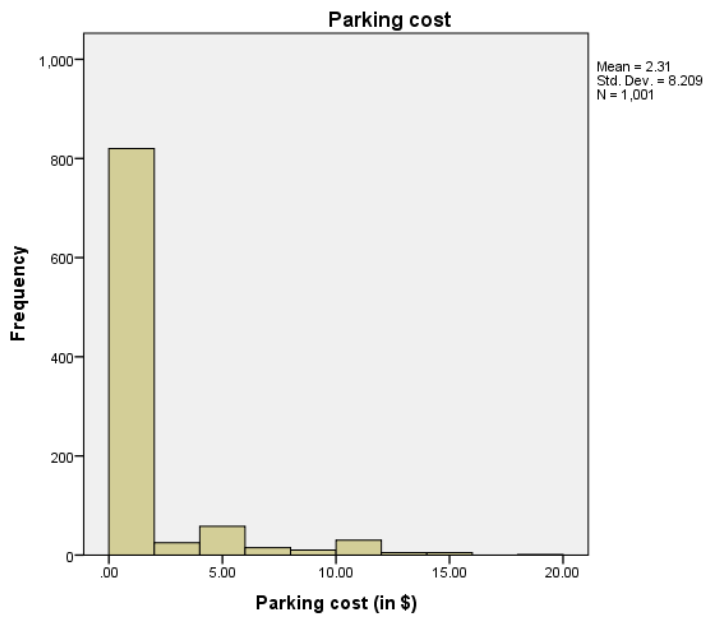
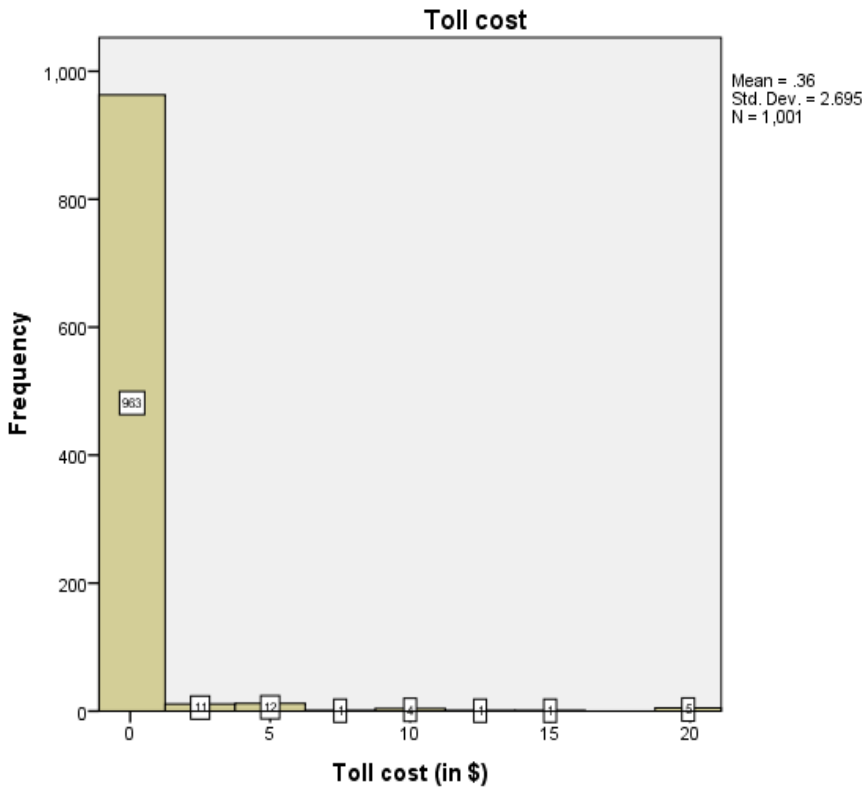
Exhibit 3-22: Vehicle Occupancy by Purpose

Trip Purpose	Mean	Median
to and from a usual place of work	1.21	1.00
to and from a place of education	1.94	1.50
on business, as part of your job	1.41	1.00
for shopping	2.04	2.00
for leisure purposes (visiting family or friends, etc.)	2.40	2.00
for personal business (doctor appointment, etc.)	1.65	1.00
For some other reason	1.92	2.00

The average trip cost (including gas, parking and toll) was \$17.0 (round trip) with a much lower median of \$10 (Exhibit 3-23).

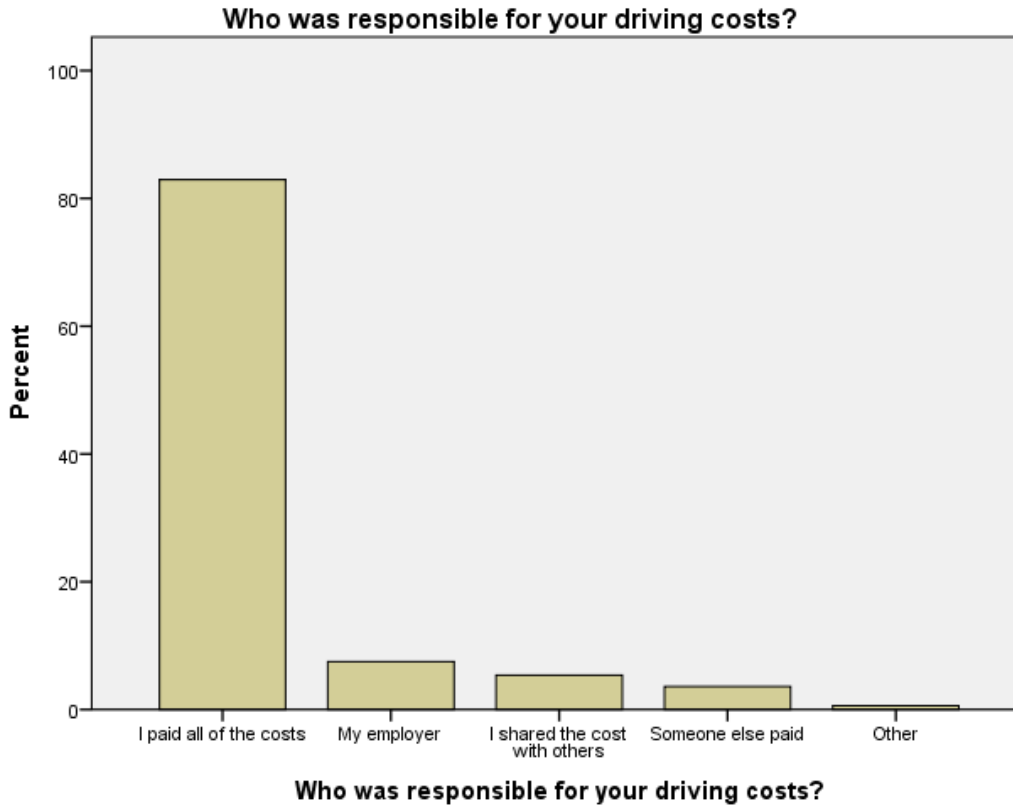
Exhibit 3-23: Round Trip Cost





Finally, more than 80% reported being solely responsible for the driving costs (Exhibit 3-24).

Exhibit 3-24: Driving Cost Responsibility

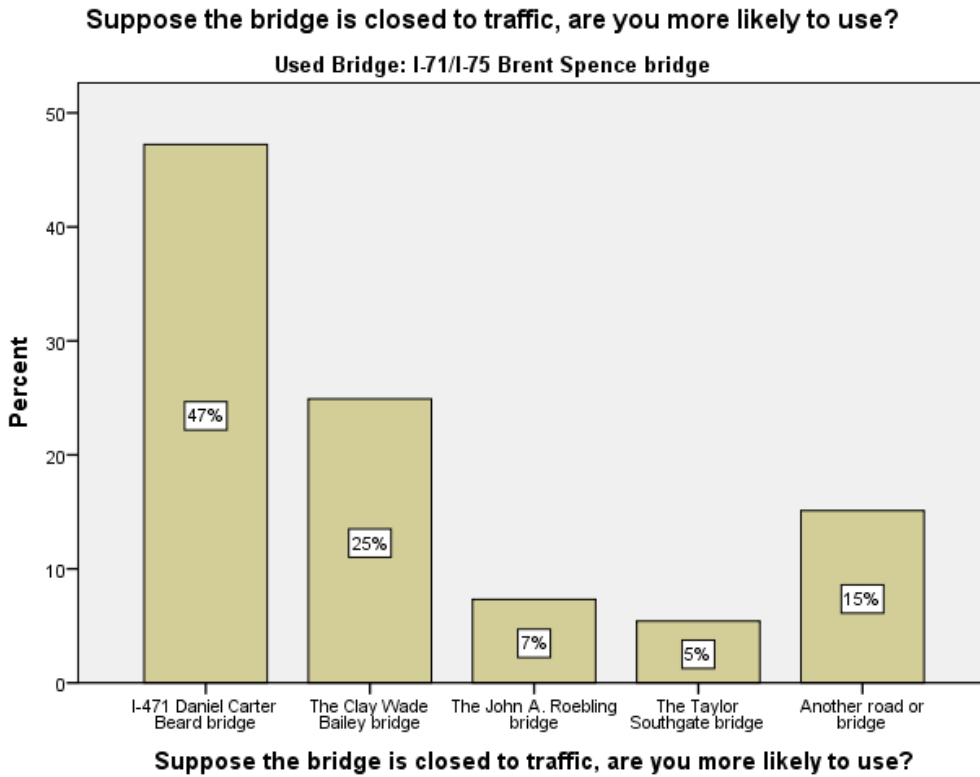


3.4 Attitude Toward Tolling and Alternative Routes

A large section of the survey was dedicated to gaining insight on travelers attitude toward tolling and what their attitude may be once the bridge is tolled.

Respondents were asked which bridge they would use if the Brent Spence Bridge was closed to traffic. Almost half (47%) indicated they would use the Daniel Carter Beard Bridge and 25% the Clay Wade Bailey Bridge (Exhibit 3-25).

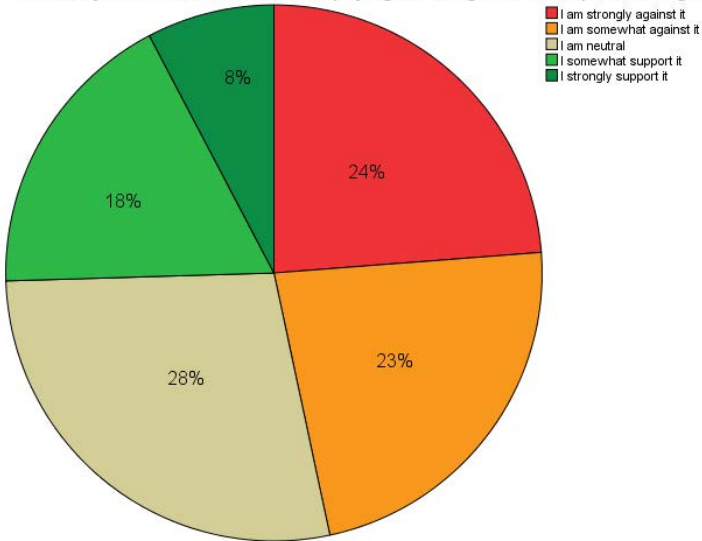
Exhibit 3-25: Alternate Bridge Used in case of Brent Spence Closure



More than half (54%) indicated being neutral or supporting the idea of paying to use the bridge (**Exhibit 3-26**).

Exhibit 3-26: Attitude Toward Paying to Use The Bridge

What do you think about the idea of paying for using the Brent Spence bridge?



While 40% indicated they will probably use the bridge even if it is tolled, 60% of the respondents indicated they may not use the bridge if it is tolled: of the 60%, 30% were not sure, 20% doubt they will use it while 10% indicated they will most certainly not use it (Exhibit 3-27).

Exhibit 3-27: Attitude Toward Using the Bridge If it is Tolled

Do you think you will use the Brent Spence bridge if it is tolled?

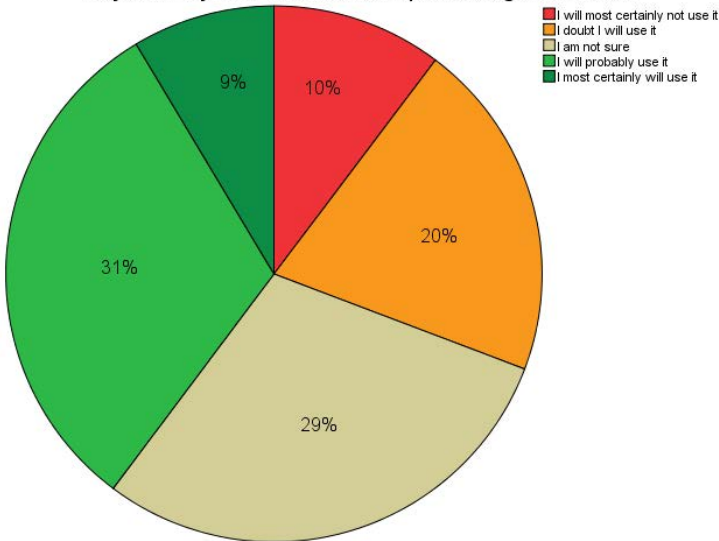
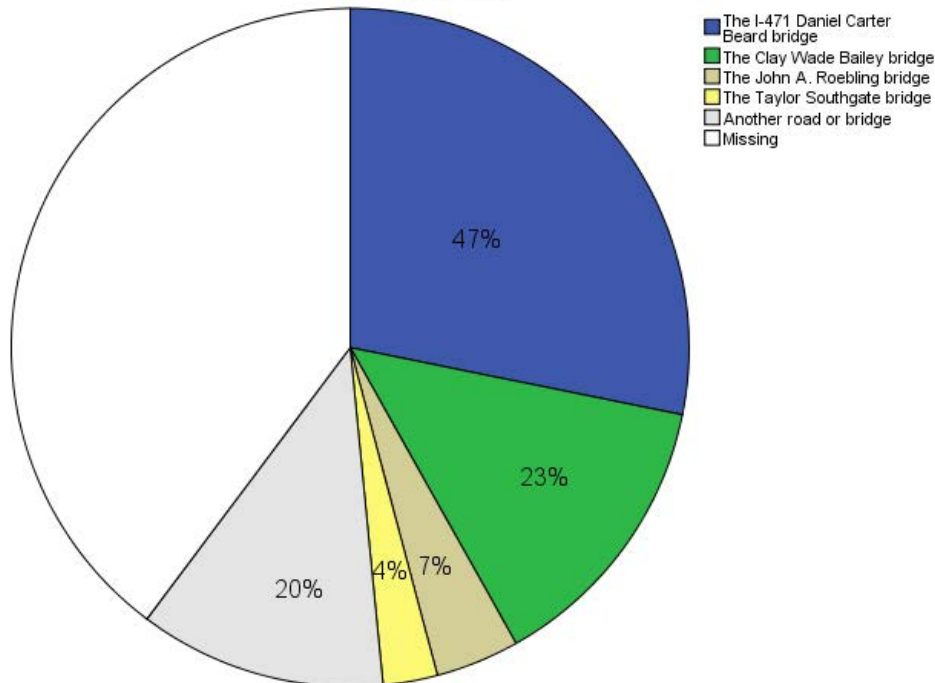


Exhibit 3-28 shows that 47% of those who may not use the tolled bridge elected for the Daniel Carter Beard Bridge as the alternate route option while 23% elected for the Clay Wade Bailey Bridge. These results are very consistent with the results displayed under **Exhibit 3-25** asking which bridge they would use if the Brent Spence Bridge was closed to traffic. In both cases, 47% indicated the Daniel Carter Beard Bridge as the preferred alternative, and 23-25% elected for the Clay Wade Bailey Bridge.

Exhibit 3-28: Alternate Bridge Used If Brent Spence is Tolled

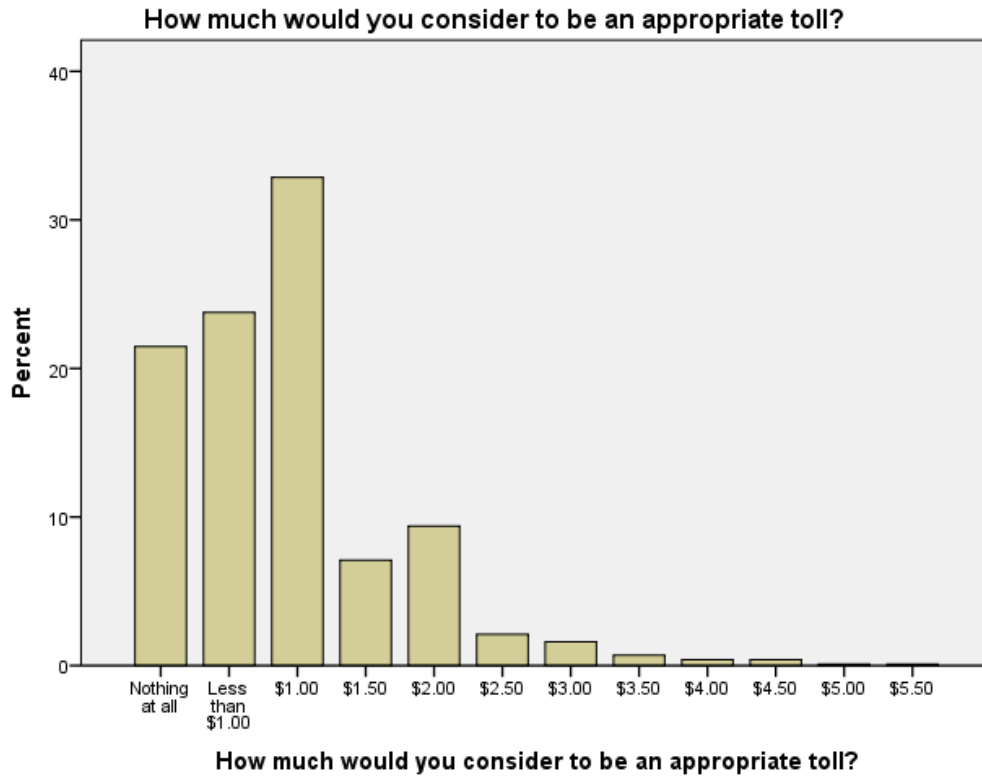
Which of the following bridges are you most likely to use if Brent Spence Bridge is tolled?



Note: The 40% "Missing" category represents the respondents who indicated they would keep using the Brent Spence Bridge (see Exhibit 3-27).

Respondents were also asked how much would they consider an appropriate toll to cross the bridge (**Exhibit 3-29**). The mean toll was \$0.91 while the median was \$1.00 (Exhibit 3-). Nevertheless more than 20% selected "nothing at all" when asked about an appropriate toll amount.

Exhibit 3-29: How Much Would You Consider an Appropriate Toll?



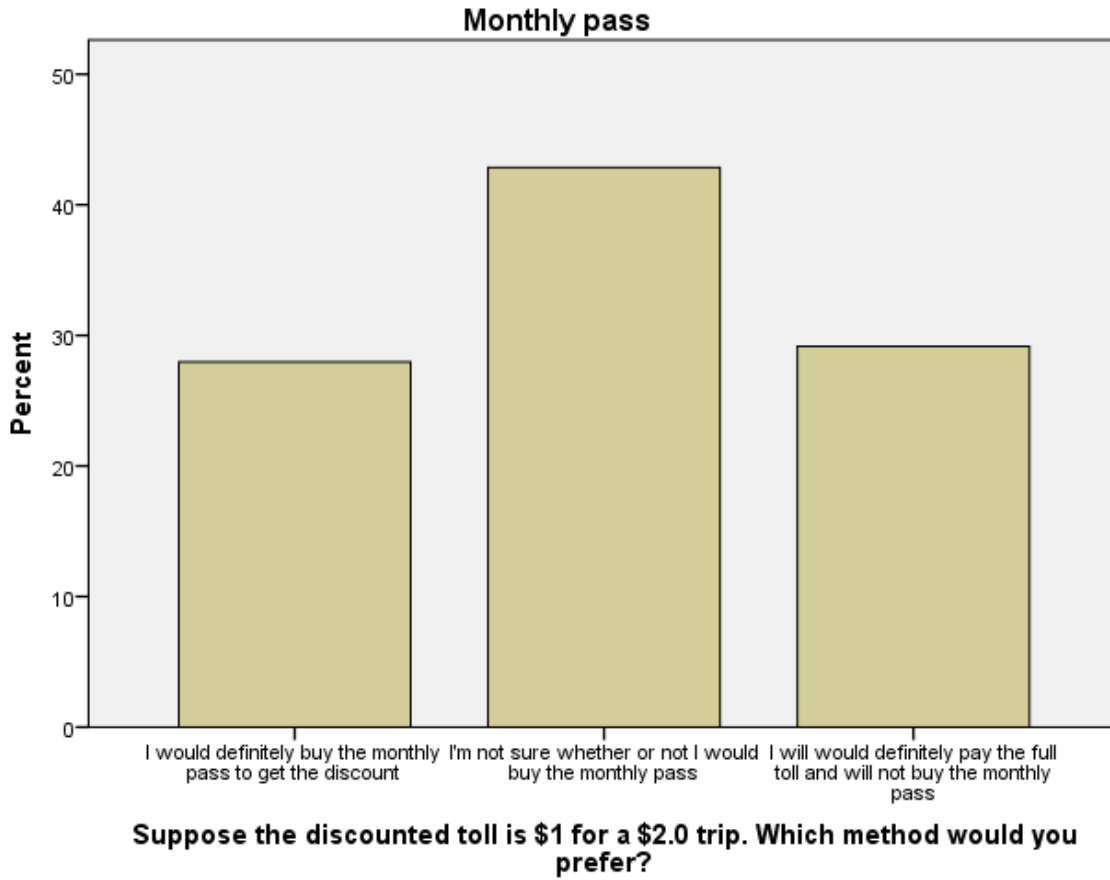
Respondents were also asked to tell us how much would the toll need to be for them to deem it 1) Too expensive, 2) Expensive, but would consider it and 3) A good value, they will use it. Results are shown in Exhibit 3-. \$3.00 was deemed too expensive by 50% of the respondent (median); \$1.75 was deemed expensive but would still consider using it and \$1.00 was seen as a good value. It is interesting to note that the modes (most frequent values) reported for too expensive, expensive but would consider it and a good value I will use it are \$5.0, \$2.0 and \$1.0 respectively; which are in cases higher than the median values reported above.

Exhibit 3-29: How Much Would The Toll Need to be for You to Deem it Too Expensive, Expensive or Good Value?

Toll amount stated preferences		How much would the toll need to be for you to deem it:			How much would you consider to be an appropriate toll?
		Too expensive	Expensive, but would consider it	A good value, I will use it	
Mean		\$3.72	\$2.34	\$1.65	\$0.91
Median (50 percentile)		\$3.00	\$1.75	\$1.00	\$1.00
Mode (most frequent value)		\$5.00	\$2.00	\$1.00	\$1.00
Std. Deviation		10.82	6.17	12.40	0.81
Percentiles	25	\$1.00	\$1.00	\$0.25	\$0.50
	50	\$3.00	\$1.75	\$1.00	\$1.00
	75	\$5.00	\$3.00	\$1.00	\$1.00
	80	\$5.00	\$3.00	\$1.50	\$1.50
	90	\$5.00	\$3.50	\$2.00	\$2.00

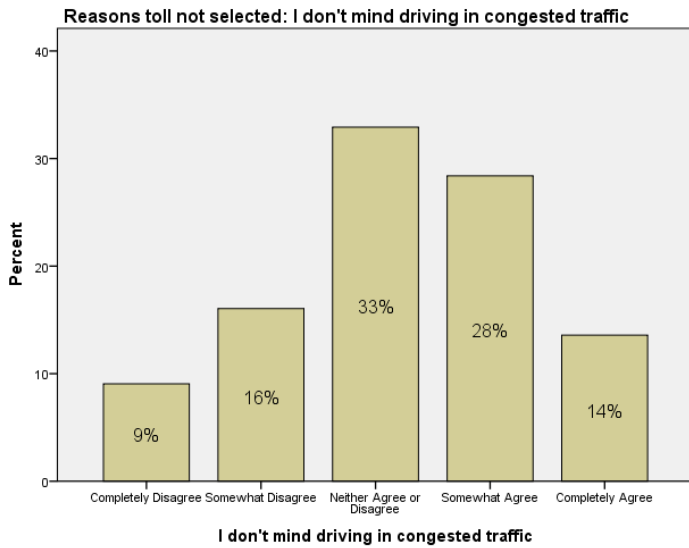
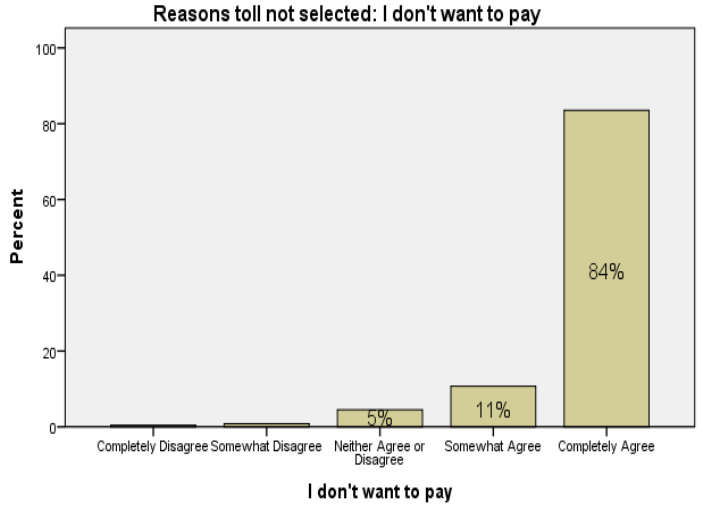
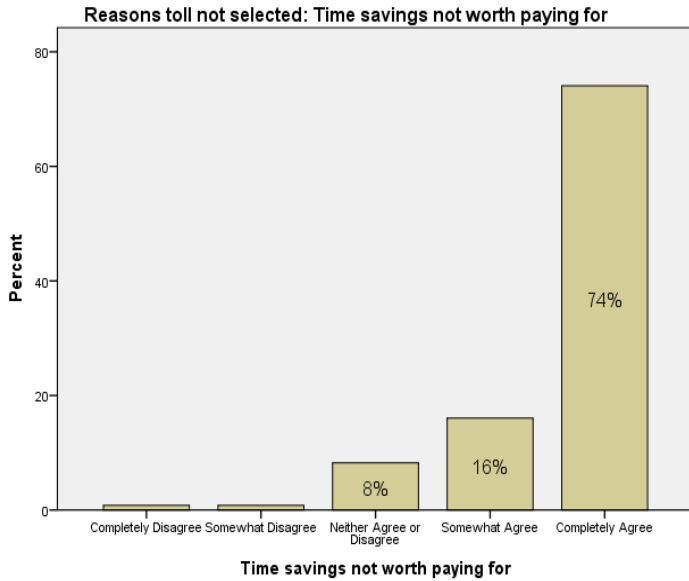
The preferred toll collection method was Electronic Toll Collection (ETC) for 85% of the respondents, while 15% elected for the video toll collection; when video collection was \$1 more expensive per trip. About 30% of the respondents would get a monthly pass if it offers a \$1 discount on a \$2 trip (**Exhibit 3-30**).

Exhibit 3-30: Monthly Pass Usage



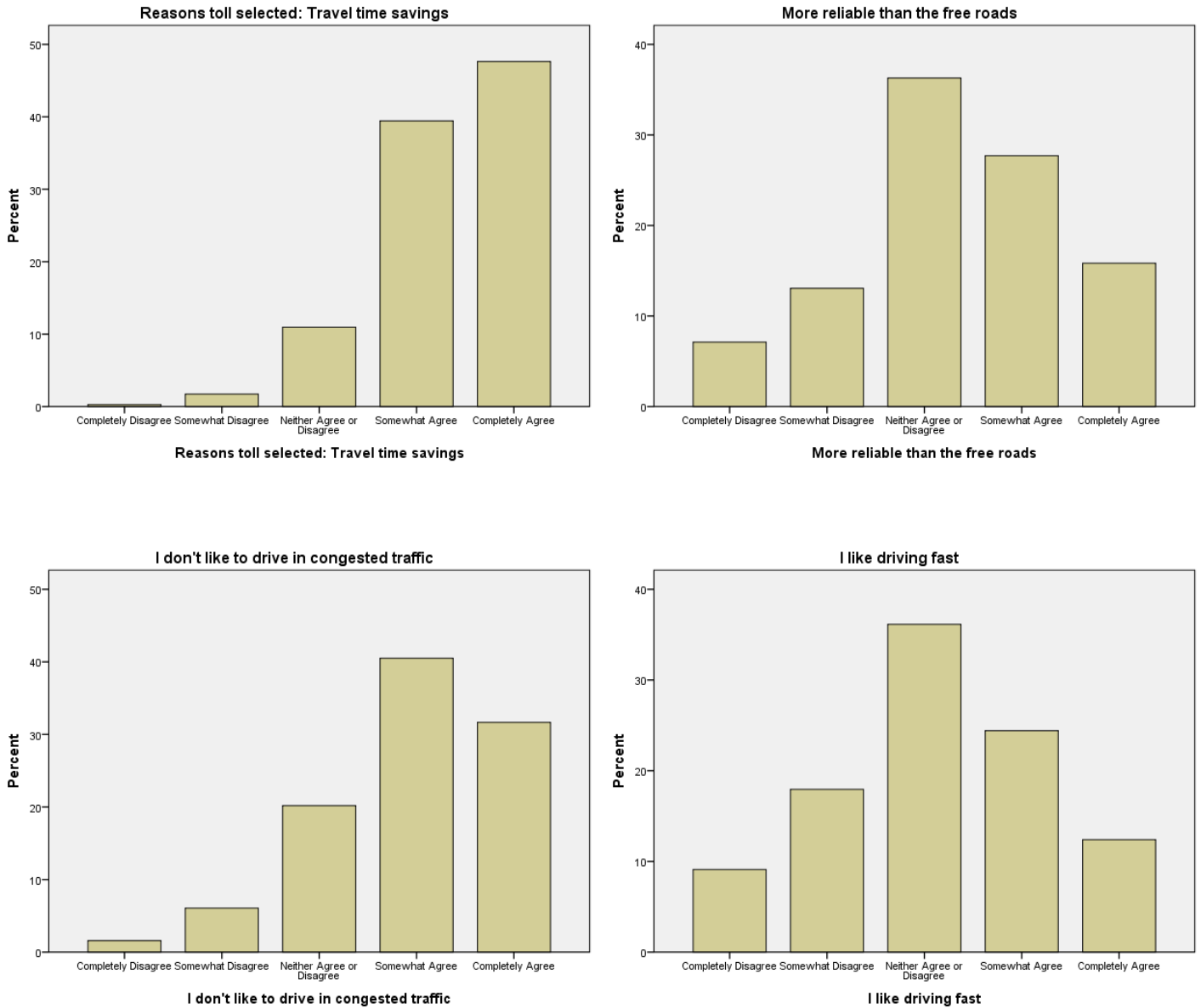
During the 9 SP choice exercise, presenting 9 trade-offs of cost and travel time savings ranging from \$2.0/hr to \$36/hr, 25% of the respondents always selected the free route alternative, indicating either a value of time of less than \$2/hr or, more likely, a personal bias against tolling. The reasons for never selecting the tolled bridge during the 9 choice experiments by these 25% respondents are reported in **Exhibit 3-31**. Most indicated that they simply didn't want to pay (95% agreement), while 90% of them indicated that time savings was not worth paying for. They also indicated that they don't like driving in congested traffic.

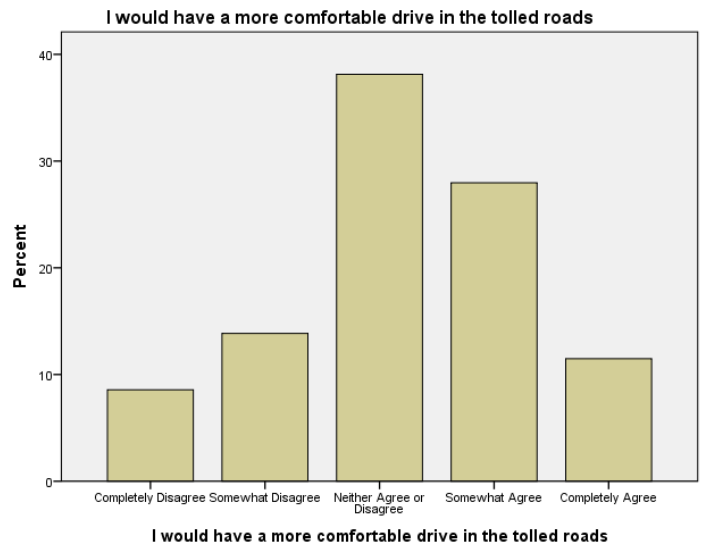
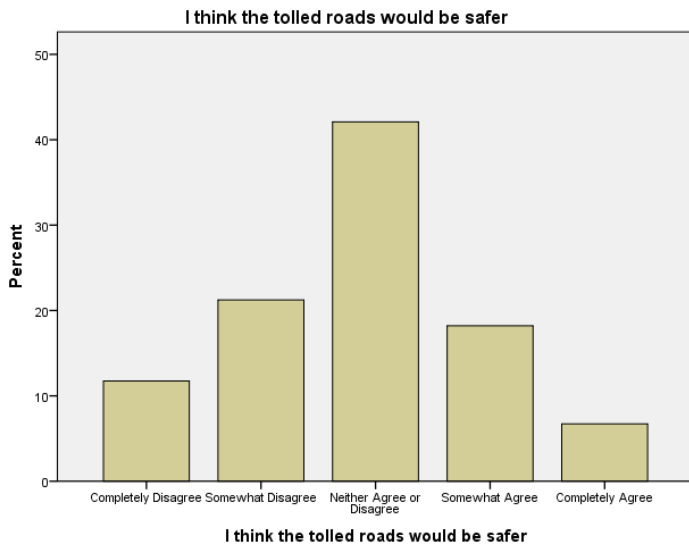
Exhibit 3-31: Reasons The Toll Bridge Was Not Selected



On the other hand, 75% of the respondents selected the tolled bridge alternative at least once. The reasons for selecting the tolled bridge are reported in **Exhibit 3-32**. Travel time savings, congestions and reliability were among the top reasons for selecting the tolled bridge.

Exhibit 3-32: Reason The Tolled Bridge Was Selected





A summary of the socio-economic and demographic characteristics of the respondents, their current travel conditions, their travel preferences and their attitude toward tolling was presented. In the next section, the results of the stated preference exercise to derive the value of travel time savings will be presented.

4. Preliminary Value of Time Results

Values of time are generally estimated at the market segment level using econometric models. In particular, binary logit-form choice models are being estimated based on a set of binary choices trading time and costs between using the new tolled bridge (faster but more expensive) and the alternate free routes (free but longer).

The survey data is currently being analyzed to estimate choice models that reflect the key variables influencing the choice of route. The key variables that determine the choice of route are:

- Travel time (Time);
- Monetary cost of the trip (Cost);
- The alternative specific constant that represents the net effect of other, unobserved attributes and capture a respondent's inherent preference that is not captured by travel time or monetary cost (Toll road constant).

While the time and cost coefficients are expected to be negative (as time or cost increase, the attractiveness of the route decreases), the toll road constant can be either positive (in favor of the toll bridge) or negative (against the toll bridge). Negative values for the constant may indicate a strong political bias against tolling in the region while a positive value may indicate advantages of the toll road that are not captured by travel time alone (such as increased reliability, comfort).

Preliminary value of time results, from the stated preference exercise, indicate value of time (VOT) averages in the \$12 to \$13/hr range. **Exhibit 4-1** shows preliminary values of time model results for the entire sample.

Exhibit 4-1: Preliminary Value Of Time Results - All Purposes All Income

Logit Models	Model without bonus		Model with bonus	
	Coefficient	T-statistic	Coefficient	T-statistic
Time	-0.326	-31.350	-0.320	-23.770
Cost	-1.540	-32.080	-1.549	-30.760
Toll bridge constant ¹	na	na	0.066	0.530
VOT (\$/hr)	\$12.69/hr		\$12.42/hr	
Toll bridge constant (min)			0.21min (~12 seconds)	

Interestingly, the toll bridge constant or toll bridge 'bias' is slightly positive (equivalent to a 12 seconds advantage to an alternate route with equal time and cost) but not significant (t-statistic less than 1.96). This may indicate that there is no particular bias, either against or in favor of tolling the bridge beyond purely time and cost considerations.

SDG is currently analyzing the data to present a more in-depth analysis of the value of time by market segments (purpose, income, etc.) and of its distribution across the population. The results will be summarized in a later memo.

Appendix B:
Project Advisory Committee



Brent Spence Bridge Corridor Project Advisory Committee Members

2012 EA/FONSI Organization	Supplemental Environmental Assessment Organization	Contact
Government Agencies		
--	Boone County Engineer	Alternate: Rob Franxman
Boone County Fiscal Court	Boone County Fiscal Court	Gary Moore, Judge Exec.
Campbell County Fiscal Court	Campbell County Fiscal Court	Steve Pendrey, Judge Exec.
Cincinnati Park Board	Cincinnati Park Board	Joel Gross
City of Cincinnati	City of Cincinnati	Aftab Pureval, Mayor
City of Cincinnati Economic Development Division	City of Cincinnati Community and Economic Development	Markiea Carter
--	City of Cincinnati, Dept. of Planning and Engagement	Katherine Keough-Jurs
City of Cincinnati Dept. of Transportation & Engineering	City of Cincinnati Dept. of Transportation & Engineering	John Brazina, Bryan Williams
City of Covington Department of Engineering	City of Covington, City Manager	Ken Smith
--	City of Covington, Economic Development Director	Tom West
--	City of Covington, Mayor	Joseph U. Meyer
--	City of Ft. Mitchell	Jude Hehman, Mayor
City of Ft. Wright	City of Ft. Wright	Dave Hatter, Mayor
City of Park Hills	City of Park Hills	Kathy Zembrodt, Mayor
--	Governor's Southwest Ohio Regional Liaison	Jason Gloyd
Hamilton County Commissioners	Hamilton County Commissioners	Alicia Reece, President of the Board
Hamilton County Engineer	Hamilton County Engineer	Eric Beck
Hamilton County Regional Planning Commission	Hamilton County Regional Planning Commission	David Okum
Kenton County Engineer	Kenton County Engineer	Alternate: Nick Hendrix
Kenton County Administration	Kenton County Fiscal Court	Kris Knochelman, Judge Exec.
Ohio-Kentucky-Indiana Regional Council of Governments (OKI)	Ohio-Kentucky-Indiana Regional Council of Governments (OKI)	Mark Policinski
Northern Kentucky Area Planning Commission	Planning and Development Services of Kenton County	Sharmili Reddy, AICP
Southwest Ohio Regional Transit Authority (SORTA)	Southwest Ohio Regional Transit Authority (SORTA)	Andrew Aiello, Chief of Staff
Transit Authority of Northern Kentucky (TANK)	Transit Authority of Northern Kentucky (TANK)	Gina Douthat, Interim General Manager
Local Community Groups		
3CDC	3CDC	Steve Leeper, President & CEO
Cincinnati Business Committee	Cincinnati Business Committee	Matt Jones
Cincinnati USA Regional Chamber	Cincinnati USA Regional Chamber	Brendon Cull, President & CEO
Citizens for Civic Renewal	Citizens for Civic Renewal	Jeffrey L. Stec, J.D., Director
Lewisburg Neighborhood Association	Lewisburg Neighborhood Association	Steve Hill
Port of Greater Cincinnati Development Authority	Port of Greater Cincinnati Development Authority	Laura N. Brunner, President & CEO
Queensgate Community Council	Queensgate Business Alliance	Melissa Wegman
Sierra Club	Sierra Club	Nathan Alley
SouthBank Partners	SouthBank Partners	Will Weber
West End Community Council	West End Community Council	Chris Griffin
Local Businesses		
Cincinnati Bengals	Cincinnati Bengals	Katie Blackburn
--	Castellini Management Co.	TBD
Cincinnati Bulk Terminals, LLC	Cincinnati Bulk Terminals, LLC	Jack Weiss
--	Cincinnati Museum Center	Elizabeth Pierce, President & CEP
Cincinnati Reds	Cincinnati Reds	Phil Castellini
Cincinnati/Northern KY International Airport	Cincinnati/Northern KY International Airport	Melissa Wideman
Cinergy Electric	Duke Energy, OH-KY	Amy Spiller, President
--	Government Strategies Group	Charles "Chip" Gerhardt
Nat'l. Underground Railroad Freedom Center	Nat'l. Underground Railroad Freedom Center	Woody Keown, CEO
--	Northern Kentucky Tri-Ed	Lee Crume
UPS, Kentucky District	--	--
Northern Kentucky Chamber of Commerce	Northern Kentucky Chamber of Commerce	Brent Cooper

Notes:

1. Organizations/positions that serve the same or similar purposes but have changed names since 2012 are listed in the same row.
2. Contact information for PAC members during the development of the 2012 EA/FONSI is not available.

**Brent Spence Bridge Corridor Project
Project Advisory Committee Meeting Summary
June 29, 2022**

Brent Spence Bridge Corridor Project Advisory Committee Meeting Summary

Brent Spence Bridge (BSB) Corridor Project Advisory Committee (PAC) meeting was held on June 29, 2022 from 10:00 am to 12:00 pm. The meeting was held at the Ohio-Kentucky-Indiana Regional Council of Governments (OKI) located at 720 East Pete Rose Way, Suite 420 in Cincinnati, Ohio. Invitations to the meeting were sent to PAC members via email on June 2, 2022. Attendees at the meeting included PAC members, the project team, and members of the public.

The meeting format included a formal presentation by Stefan Spinosa (ODOT District 8), Stacey Hans (KYTC District 6), Gary Valentine (KYTC State Highway Engineer's Office), and Mark Becherer (HNTB). The project team responded to questions posed by PAC members throughout the presentation. In addition, two members of the public offered comments at the end of the presentation.

Exhibits showing through and local traffic movements for Preferred Alternative I (Concept I-W), renderings of the project in the vicinity of the Ohio River, the project phasing, and a project schedule were also available for viewing before and after the meeting.

A summary of the presentation, questions, responses, and public comments is provided below.

1. Welcome and Introductions

- a. Mark Policinski (OKI) and Mark Becherer (HNTB) welcomed the members and other attendees to the meeting.
- b. All committee members shared their name and organization they were representing.

2. Project History

- a. 25 alternatives considered beginning in 2005, 1.5-2 years
- b. Alternative E and Alternative I identified as two feasible alternatives in 2009
- c. 2012 Environmental Finding of No Significant Impact (FONSI) based on Alternative I
- d. Value Engineering concepts developed in 2012
- e. Ohio begins ROW acquisition in 2013
- f. Initial cost estimate of \$2B (now \$2.96B)
- g. FHWA re-evaluations 2015 and 2018

3. Current Project Status

- a. Design and project cost
 - i. Value engineering concepts (I-M and I-W) developed to reduce impacts and costs; I-M discarded

- ii. Project estimate being updated in coming weeks
- b. Traffic
 - i. Updating modeling as appropriate based on latest data
 - ii. 2050 design year
- c. Funding and financing
 - i. 60% federal grants, 13% ODOT/KYTC federal funding, 27% ODOT/KYTC state funding
- d. Grant applications
 - i. Discussed Multimodal Project Discretionary Grant (MPDG) application that was submitted on May 23, 2022
 - ii. **Question: Is there another grant application? (Nan Cahall)** Yes. Additional grant (Large Bridge Project) application due August 9, 2022
 - iii. Decision on initial grant (MPDG) application expected in fall (September)
- e. Environmental re-evaluation
 - i. New field surveys/investigations will be complete by the end of 2022
 - ii. Additional hearings planned for early 2023 with re-evaluation complete in Q1 2023
- f. Outreach
 - i. **Question: What are the expectations of advisory committee? (Sharmili Reddy)** Expectations include two-way communication with the intent of soliciting feedback, providing information, and collaborating to reach consensus on how to best meet project goals; members can help the project by educating, communicating with, and advocating for the project with their groups, organizations, and/or constituencies
 - ii. **Question: Is the cost estimate for Phase 1 or the entire corridor? Adam Johnson** The entire corridor.
 - iii. **Question: Please discuss the NEPA process, including importance of timing, potential issues with approval, and time constraints for funding. (Nan Cahall)** To be competitive in the grant process, it is critical for an applicant to demonstrate project readiness. While the original NEPA decision is a decade old, it was re-evaluated in 2015 and 2018. The project team has outlined a plan to evaluate project impacts based on updated regulations and minimal design refinements to confirm previous NEPA decisions. This process is scheduled for completion in the first quarter of 2023. Significant changes to the project could dictate a new decision, jeopardizing our ability to successfully receive the grant funding imperative to moving this project forward.
 - 1. Grants do have requirements for completion of the work in specified time periods.
- g. Pre-procurement

-
- i. **Question: What is the timeline and procurement process for Phases I and II?** Phases I and II will be procured via traditional design-bid-build; Phase II construction is scheduled to begin in 2025, Phase I construction is scheduled to begin 2028
 - ii. ODOT and KYTC acknowledge that workforce labor is a project risk; agencies are planning workforce development outreach; ODOT and KYTC have signed a DBE reciprocity agreement
 - iii. **Question: Is there risk to the cost estimate and will an independent cost estimator be involved?** Yes, cost risk is acknowledged and included in risk assessments being developed or updated. An independent cost estimator (ICE) will be used, if necessary, based on the type of procurement. Using traditional design-bid-build delivery or a more traditional design-build delivery won't necessarily require an ICE.
 - iv. For a sense of project scale: In the past year, ODOT has let 950 projects for \$2.1B; KYTC has let projects totaling \$1B in the last year.
4. Cost Savings / Concept Overview
 - a. Alternative I was developed to separate interstate thru traffic from local traffic
 - b. Concept I-W refined Concept I by separating local and interstate traffic between bridges, with interstate thru traffic only on companion bridge; this resulted in a narrower companion bridge and reduced costs
 - c. **Question: How is traffic impacted from Concept I to Concept I-W?** Traffic flow is similar and slightly improved by Concept I-W
5. Upcoming Tasks / Timeline Review
 - a. Value-based scoring in procurement process will seek to reward footprint minimization
 - b. Best value to be announced October 2023
 - c. Construction to begin in earnest in 2024 following November 2023 groundbreaking
 - d. **Question: How will construction be initiated and phased following award?** Construction will proceed incrementally based on buildable units; construction will initiate with tasks generally requiring lower design or review effort, such as clearing; at the peak of construction, estimates include 500 workers and \$60M monthly payments for work completed
 - e. For the knowledge of the group: Of the \$3B project cost estimate, Kentucky highway/approach work, Ohio highway/approach work, and the main river crossing will each account for approximately \$1B
6. Committee Next Steps
 - a. **Question: When will the website be updated and how will traffic impacts (and other information) be communicated – using the website or advisory committee? (Phil Castellini)** Project information and graphics are currently being updated and

finalized; the website will grow with more content as procurement and the project progress; e-newsletters will serve as the primary communication method with pertinent information being uploaded to the website as well

7. Committee General Questions/Comments

- a. **Question: What is the status of right-of-way acquisition and how will residents be impacted? (Mayor Kathy Zembrodt)** In Ohio, 70 of 79 parcels have been acquired; KY is initiating its acquisition process and will begin in earnest following NEPA approval; in KY, no residential relocations will occur in Park Hills; one residential relocation will be required on the southeast side of the Kyles Lane interchange; the original NEPA document proposed 120 relocations, but refinement of concepts and the overall footprint has reduced the number of required relocations to four (with three in Covington and one south of 12th Street)
- b. **Question: There is only one public hearing in January of 2023 on the timeline. Why have so few public hearings been held? (Sharmili Reddy)** Hearings have occurred at each step of the process throughout the project development dating back to 2012 and including the next public hearing scheduled for January; communication via e-newsletters and the website will increase as the procurement and project progress; demonstrated project readiness – which includes public outreach and refinement of project details – has been a significant component of the grant application process
- c. **Question: Does the workforce analysis account for city, county, and smaller bridge projects? (Eric Beck)** The project team acknowledges the existence of the Western Hills Viaduct project and other concurrent projects; available workforce has been identified as a project risk and attempts to address or mitigate this through outreach are ongoing

8. Public Comments

a. Craig Fischer (Fischer Homes)

- i. Being a member of the advisory committee is a privilege
- ii. Engagement in the project development process by advisory committee members and meeting attendees is highly encouraged and members should be very involved owners and should give feedback on the project for today's needs.
- iii. Interested in Route 50 on-ramp at 75 south – suggested the members should get really engaged in the details
- iv. Post Meeting Discussion: Was very interested in traffic numbers and analysis. Specifically requested numbers on US 50

b. Brian Boland (Bridge Forward)

- i. This is a once in a lifetime project
- ii. Is the original design still relevant or applicable to the changed Cincinnati/Covington areas (rhetorical)?

- iii. Involvement by the advisory committee members is encouraged to update the bridge and overall project based on changes to the area in the past decade since the original project development
- iv. Asked Advisory committee to be thought leaders and to tell the project team what is planned is no longer suitable
- v. Redevelopment; reconnecting of land
- vi. Build back better
- vii. Not against this project but pushing for an update on what is needed now

Attachments:

- Email invitation
- Agenda
- Sign-in sheets
- Presentation
- Exhibits (low resolution)



Dear Advisory Committee Member:

I am excited to announce that the Brent Spence Bridge Advisory Committee is being reconvened. The next meeting of the Advisory Committee will be held from 10:00 AM – Noon on June 29, 2022, at:

OKI Regional Council of Governments
720 East Pete Rose Way
Suite 420
Cincinnati, Ohio 45202

The purpose of the Advisory Committee Meeting is to provide an overall project update. The Project Team will present a project history and update, the current status of the grant efforts, a discussion on upcoming focus and activities, a review of the schedule, and provide an opportunity for questions and comments. We ask that you act as liaisons between your interested groups and the Project Team.

We look forward to seeing you; in person on Wednesday, June 29, 2022, at OKI Regional Council of Governments. Please email Sherry Kish at skish@hntb.com to confirm your attendance. If you are unable to come, we invite you to send another representative from your organization in your place.

If there is a new contact or representative for your organization, please provide the update with the response. You can also call Sherry at 614.493.5510, or me at 414.559.8900 with any questions. Thank you for your involvement on the Advisory Committee.

Sincerely,

A handwritten signature in blue ink that reads "Mark Becherer".

Mark Becherer, PE
Vice President
HNTB Corporation

Advisory Committee Meeting Agenda June 29, 2022

1. Welcome and Introductions
Wifi: OKI-Guest
Password: #R1verBØat
2. Project History
3. Current Project Status
4. Cost Savings Concept Overview
5. Upcoming Tasks / Timeline Review
6. Committee Next Steps
7. Public Comments

This is a working meeting of the Project Team and the Advisory Committee. As such, participation from the general public during the meeting will be limited to the portion of the agenda designated for public comment.

Committee

Name	Organization	Confirmed?	Attended Meeting
Adam Johnson	FHWA Ohio	C	X
Alexis Kidd	West End Community Council	C	
Amy Spiller	Duke Energy, OH-KY	C	
Andrew Aiello	TANK	C	
Brent Cooper	Northern Kentucky Chamber of Commerce	C	
Bryan Williams	City of Cincinnati		
Caroline Blackburn	Cincinnati Bengals	C	X
Chip Gerhardt	Government Strategies Group	C	X
Cindy France	Cincinnati Park Board	C	X
David Okum	Hamilton County Regional Planning Commission	C	X
Elizabeth Pierce	Cincinnati Museum Center	C	X
Eric Beck	Hamilton County Engineer	C	X
Gary Moore, Judge Exec.	Boone County Fiscal Court	C	X
Gary Valentine	KYTC State Highway Engineer's Office	C	X
Jack Weiss	Cincinnati Bulk Terminals, LLC	C	X
Jason Gloyd	Governor's SW Ohio Regional Liaison	C	X
Jeffrey L. Stec	Citizens for Civic Renewal	C	
Jill P. Meyer	Cincinnati USA Regional Chamber	C	X
John Brazina	City of Cincinnati	C	X
Keith Smith	ODOT, District 8	C	X
Ken Smith	City of Covington	C	X
Kevin Randall	HNTB	C	X
Kris Knochelmann, Judge Exec.	Kenton County Fiscal Court		
Laura N. Brunner	The Port <i>Todd Castellini, VP</i>	C	X
Lee Crume	Northern Kentucky Tri-Ed	C	X
Leland Bennett	SouthBank Partners	C	
Lloyd MacAdam	ODOT Central Office	C	X
Lorrie Platt	OKI	C	X
Mark Becherer	HNTB	C	X
Mark Policinski	OKI	C	X
Markiea Carter	City of Cincinnati	C	X
Matt Jones	Cincinnati Business Committee	C	X
Mayor Aftab Pureval	City of Cincinnati		
Mayor Dave Hatter	City of Ft. Wright, KY		
Mayor Joseph U. Meyer	City of Covington	C	X
Mayor Jude Hehman	City of Ft. Mitchell, KY		
Mayor Kathy Zembrodt	City of Park Hills, KY	C	X
Melissa Wideman	Cincinnati/Northern KY International Airport	C	X
Michael Loyselle	FHWA Kentucky	C	X

Nan Cahall	Senator Portman's Office	C	x
Nathan Alley	Sierra Club	C	X
Nick Hendrix	Kenton County Engineer		
Phil Castellini	Cincinnati Reds	C	x
Rob Franxman	Boone County Engineer	C	X
Robert Yeager	KYTC, District 6	C	
Scott Stone	FHWA Ohio	C	x
Shannon Ratterman	Center for Great Neighborhoods	C	X
Sharmili Reddy	Planning & Devt Services of Kenton County	C	X
Sherry Kish	HNTB	C	x
Stacey Hans	KYTC, District 6	C	X
Stefan Spinosa	ODOT, District 8	C	x
Stephanie Dumas, President	Hamilton County Commissioners	C	
Steve Hill	Lewisburg Neighborhood Association	C	x
Steve Leeper	3CDC		
Steve Pendery, Judge Exec.	Campbell County Fiscal Court	C	X
Tammy Campbell	ODOT, District 8	C	X
Tom Arnold	ODOT, District 8	C	X
Tom West	City of Covington	C	X
Woody Keown	Nat'l. Underground Railroad Freedom Center	C	X

Keith Blake	West End Community Council (next time Chris Griffin)		X
Mour Dior	FHWA - Ky		x
Tim Long	FHWA - OH		x
Charlie Rowe	ODOT - OH		x

~~Kathy Zembrodt~~ name on 1st page

Public Sign-in

Print Name	Organization Represented	Email	Add me to Newsletter
Brian Poland	Bridge Forward	poland@fuse.net	✓
Betty Hull	Razor		
Gres Fischer	Fischer Homes	gfischer@fischerhomes.com	
Dana Martin	Fiske Plan	dmartrin@r1records.com	✓
Laura Tenfelde	PDS Kenton Co	ltenfelde@pdskc.org	✓
Giuseppe Rocco Giuseppe Rocco	City	Giuseppe.Rocco@Cincinnati-oh.gov	✓



**BRENT SPENCE
BRIDGE CORRIDOR**



PROJECT ADVISORY COMMITTEE
June 29, 2022

WiFi: OKI-Guest
Password: #R1verBøat





INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.
brentspencebridgecorridor.com



1

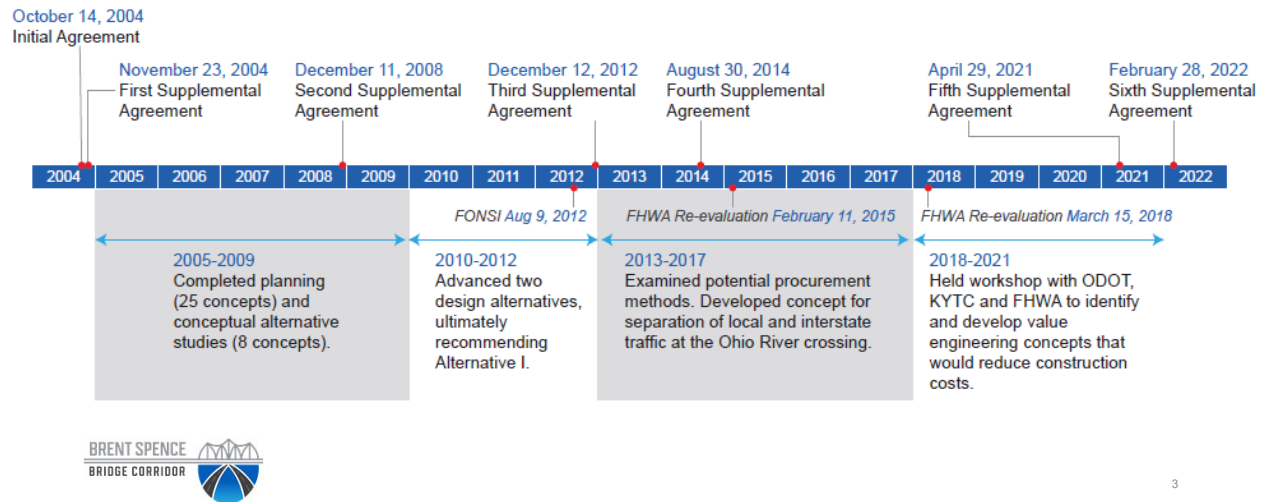
AGENDA

- OKI Welcome/Housekeeping
- Introductions
- Project History
- Current Project Status
- Cost Savings Concept I-W Overview
- Upcoming Tasks/Timeline Review
- Committee Next Steps
- Public Comments

2

PROJECT HISTORY



3

CURRENT PROJECT STATUS

- Design and Project Cost
- Traffic
- Funding/Financing
- Grant Applications
- Environmental Re-Evaluation
- Outreach
- Pre-Procurement



4

DESIGN and PROJECT COST

- Developed conceptual design for Value Engineering (VE) Concepts (I-W and I-M) to reduce project impacts and costs
- Coordinated with the traffic modeling process to accommodate changes and revisions to support traffic operations
- Reviewed Conceptual Signing Plan prepared for preferred FONSI Alternative to verify it works for the VE concepts
- Reviewed all VE recommendations for approval, rejection, or pending status
- Right of Way acquisition occurring in Kentucky
- Developed project costs for the VE Concepts (I-W and I-M)
 - Each within \$30M of one another
 - Overall project cost estimate for Value Engineering Concept I-W is \$2.9B
- Preparing Design Summary Report

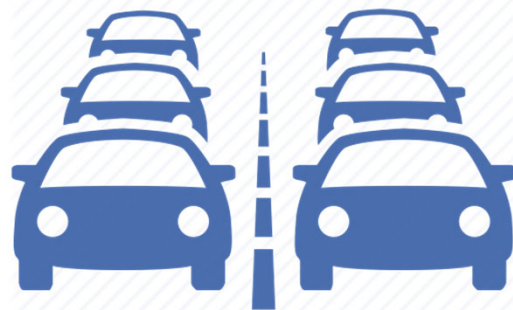


5

5

TRAFFIC

- Worked with ODOT/KYTC/OKI to validate the OKI regional model for the project area
- Prepared design year 2050 AM and PM peak period planning level traffic forecasts for the study network
- Utilized traffic simulation to analyze and inform design updates for the two Value Engineering Concepts: I-W and I-M
- Prepared Traffic Operations Report that summarizes the traffic modeling and analysis process and results



6

6

FUNDING/FINANCIAL

- Determined the capacity and potential use of state funding for both Ohio and Kentucky
- Evaluated the capacity and potential use of federal funding, including grant programs
- Determined the potential yearly funding needs and the potential sources of funding in each of those years (including grants)
- Preparing final funding/financing technical memorandum

	FY 23	FY 24	FY 25	FY 26	FY 27	FY 28	Total	%
Federal Grants	\$40	\$444	\$600	\$575	\$0	\$0	\$1,660	60%
KYTC Federal	\$33	\$36	\$40	\$15	\$7	\$0	\$131	13%
ODOT Federal	\$13	\$0	\$94	\$0	\$0	\$129	\$236	
KYTC State	\$137	\$176	\$67	\$58	\$2	\$0	\$441	27%
ODOT State	\$3	\$0	\$24	\$0	\$244	\$32	\$303	
TOTAL	\$226	\$657	\$825	\$648	\$253	\$161	\$2,771	
% Federal								73%

**Sources of Project Funds
(Funding by Fiscal Year, in
millions)**

Note: Totals may not add due to rounding.



7

7

GRANT APPLICATIONS

- Determined to pursue two grant programs:
 - Multimodal Project Discretionary Grant - MPDG (MEGA and INFRA)
 - Large Bridge Project Grant (part of the Bridge Investment Program)
- Prepared and submitted MPDG grant on May 23, 2022
- Currently preparing Large Bridge Project grant – Due August 9, 2022



8

8

ENVIRONMENTAL RE-EVALUATION

- Assessing changes in the corridor that have occurred since the last Environmental Re-Evaluation
- Performing field surveys
 - Ecological
 - Endangered Species
 - Noise
- Submitting a Supplemental Environmental Assessment for review and concurrence by FHWA
- Will hold public hearing in early 2023



9

9

OUTREACH

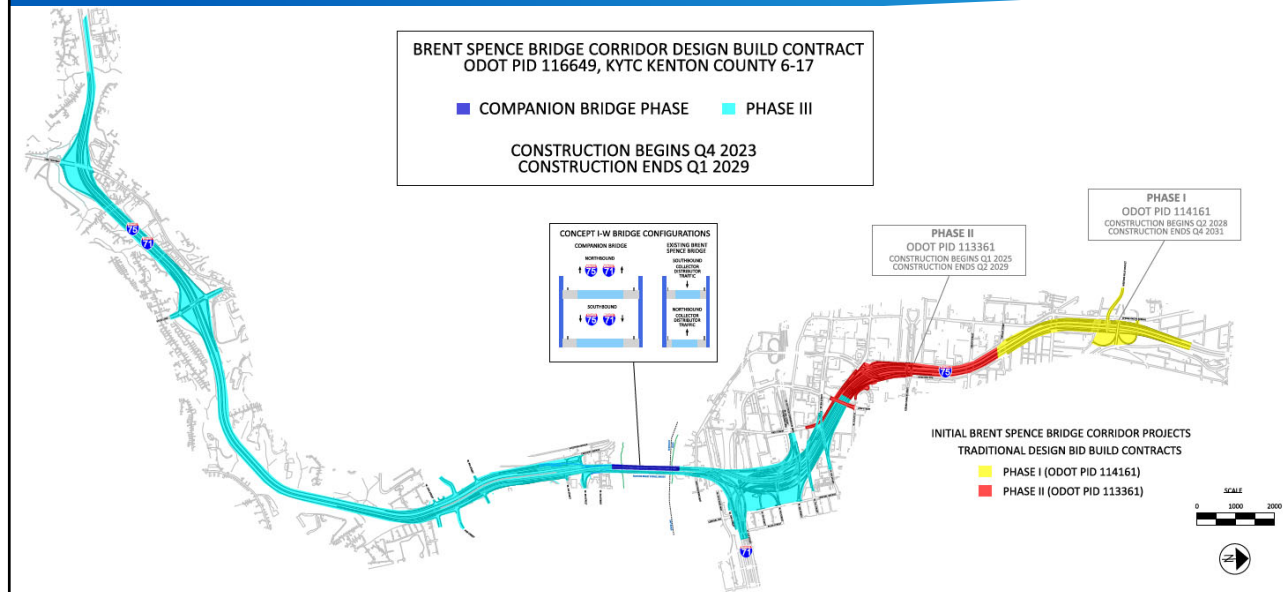
- Updated the Corridor Website – www.brentspencebridgecorridor.com
- Held individual meetings with Covington and Cincinnati
- Distributed two project newsletters and plan to distribute monthly moving forward
- Reconvening this committee



10

10

CORRIDOR PROJECT PHASING



11

PRE-PROCUREMENT

- Beginning pre-procurement process for a design build project delivery
- Held outreach meeting with the Construction and Engineering community in Covington on June 7, 2022
 - Over 250 attendees
 - Held one on one meetings with seven construction firms/teams
- Preparing risk assessment and additional information in preparation of a fall RFQ



12

12

CONCEPT I-W OVERVIEW

- Developed value engineering concepts
 - Based on performance-based design concepts
 - Local traffic (CD) on existing bridge
 - Each deck has all traffic in the same direction
- Same access Point as Alternative I – Major difference is at the River Crossing
- Current Estimated Project Cost is \$2.96B

ALTERNATIVE I
(Current Preferred Alternative)

CONCEPT W

13

UPCOMING TASKS / TIMELINE

14







COMMITTEE NEXT STEPS

15

COMMITTEE NEXT STEPS

- Send feedback to ODOT/KYTC
- Share this information within your organization(s) and others
- Sign up for the e-newsletter
- Write a support letter for future grants
- Attend future meetings





16

16

BRENT SPENCE
BRIDGE CORRIDOR 



PUBLIC COMMENTS

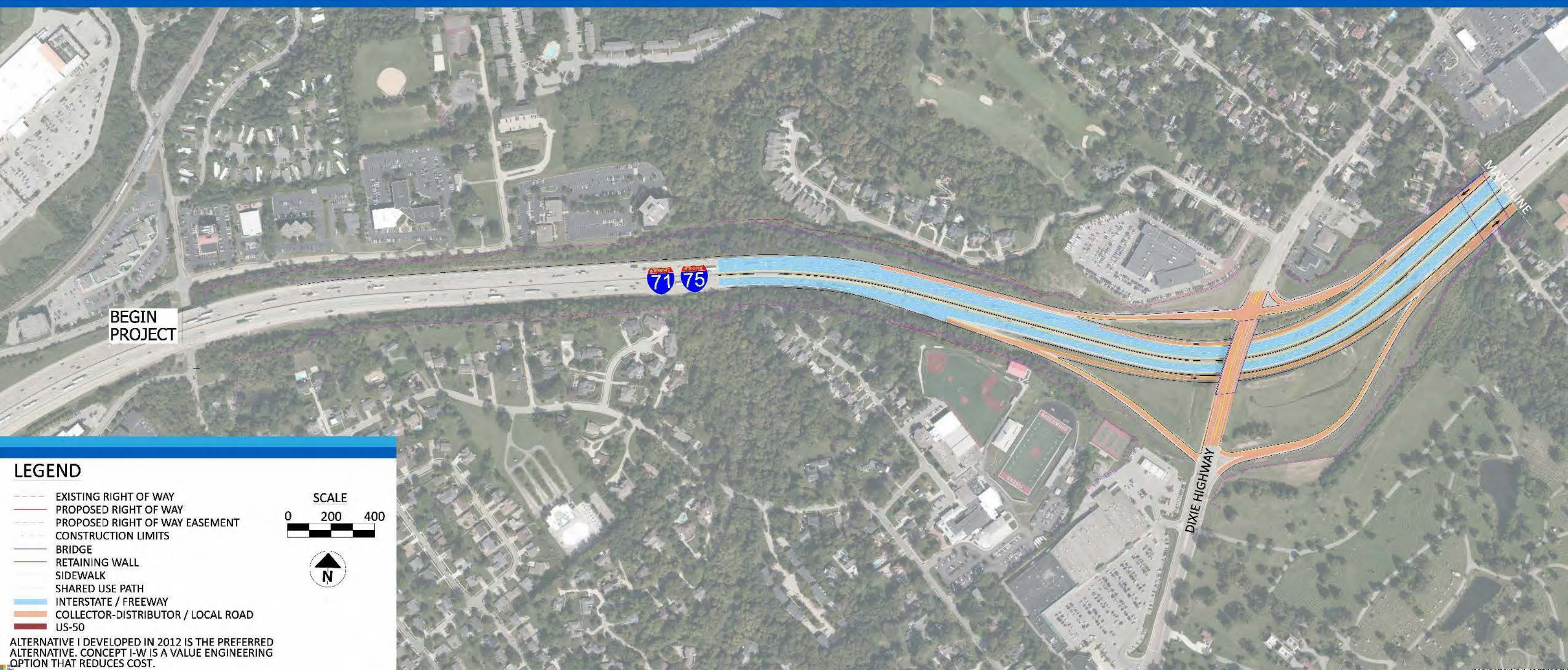
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BRENT SPENCE
BRIDGE CORRIDOR 

PROJECT TEAM CONTACTS

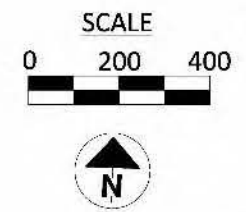
Stefan Spinosa ODOT - D8 stefan.spinosa@dot.ohio.gov	Stacee Hans KYTC - D06 stacee.hans@ky.gov
Mark Becherer HNTB mbecherer@HNTB.com	

18

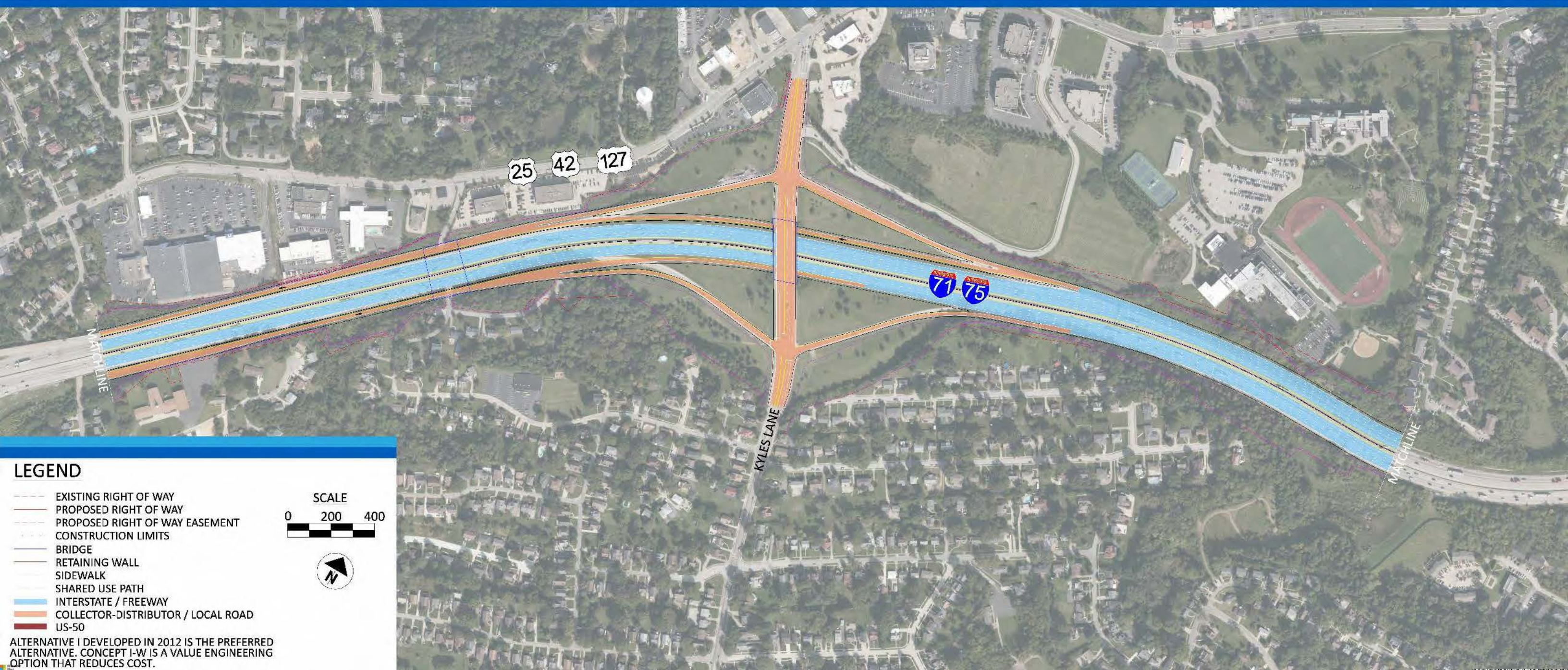


LEGEND

- EXISTING RIGHT OF WAY
- PROPOSED RIGHT OF WAY
- PROPOSED RIGHT OF WAY EASEMENT
- CONSTRUCTION LIMITS
- BRIDGE
- RETAINING WALL
- SIDEWALK
- SHARED USE PATH
- INTERSTATE / FREEWAY
- COLLECTOR-DISTRIBUTOR / LOCAL ROAD
- US-50

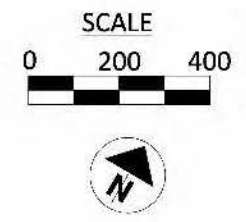


ALTERNATIVE I DEVELOPED IN 2012 IS THE PREFERRED ALTERNATIVE. CONCEPT I-W IS A VALUE ENGINEERING OPTION THAT REDUCES COST.

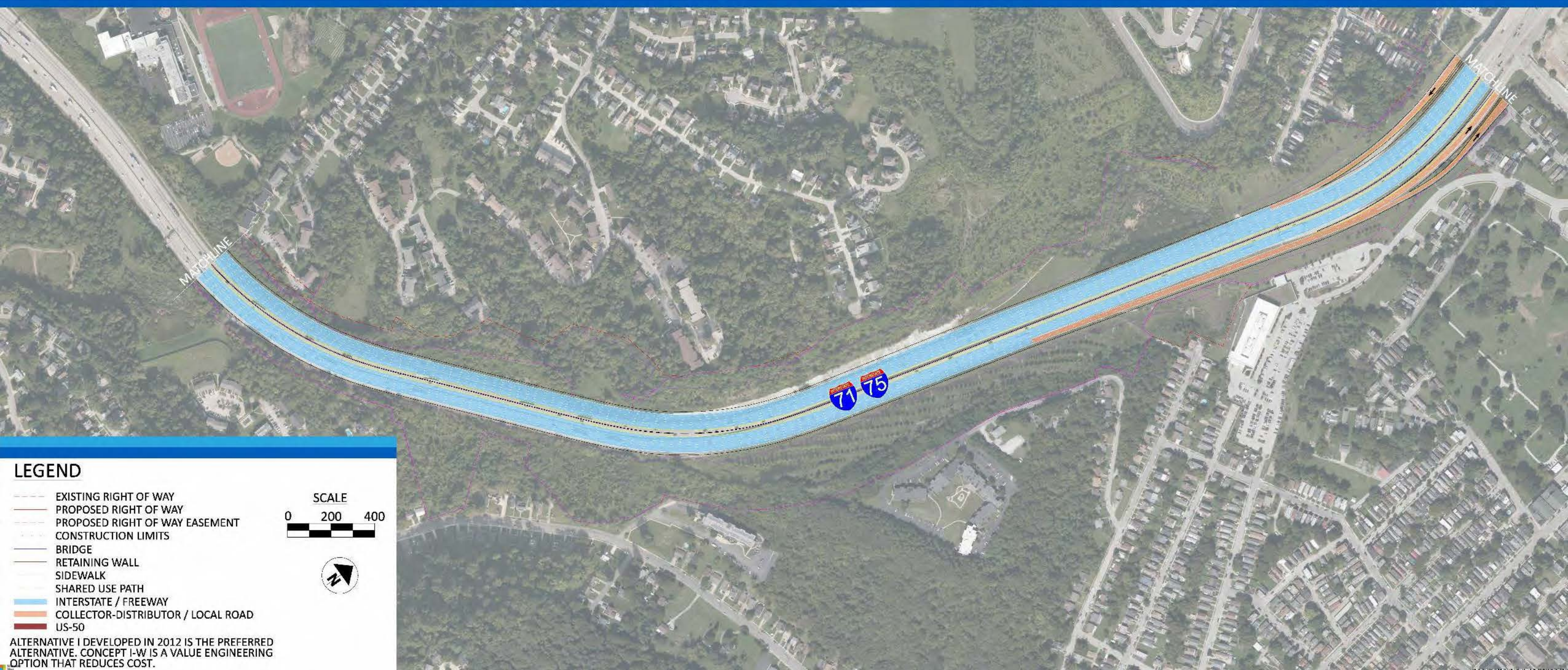


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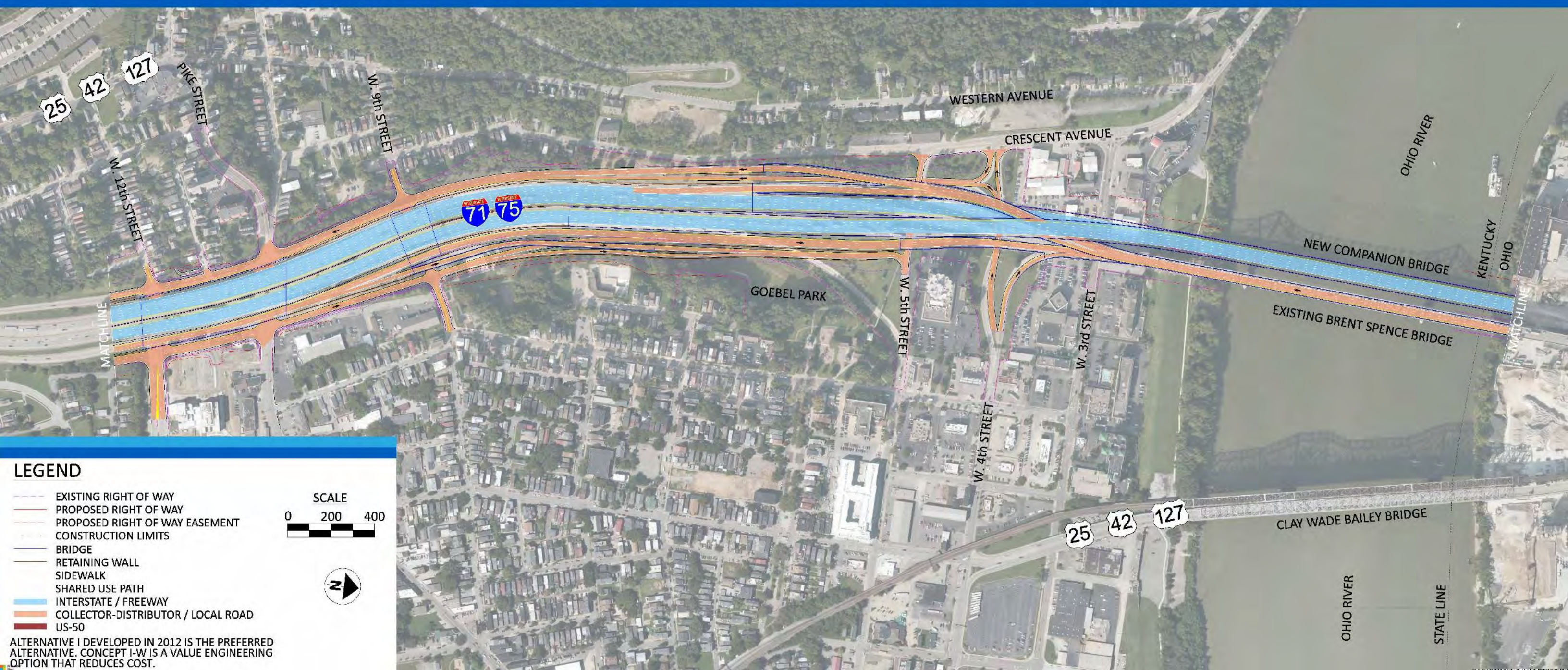


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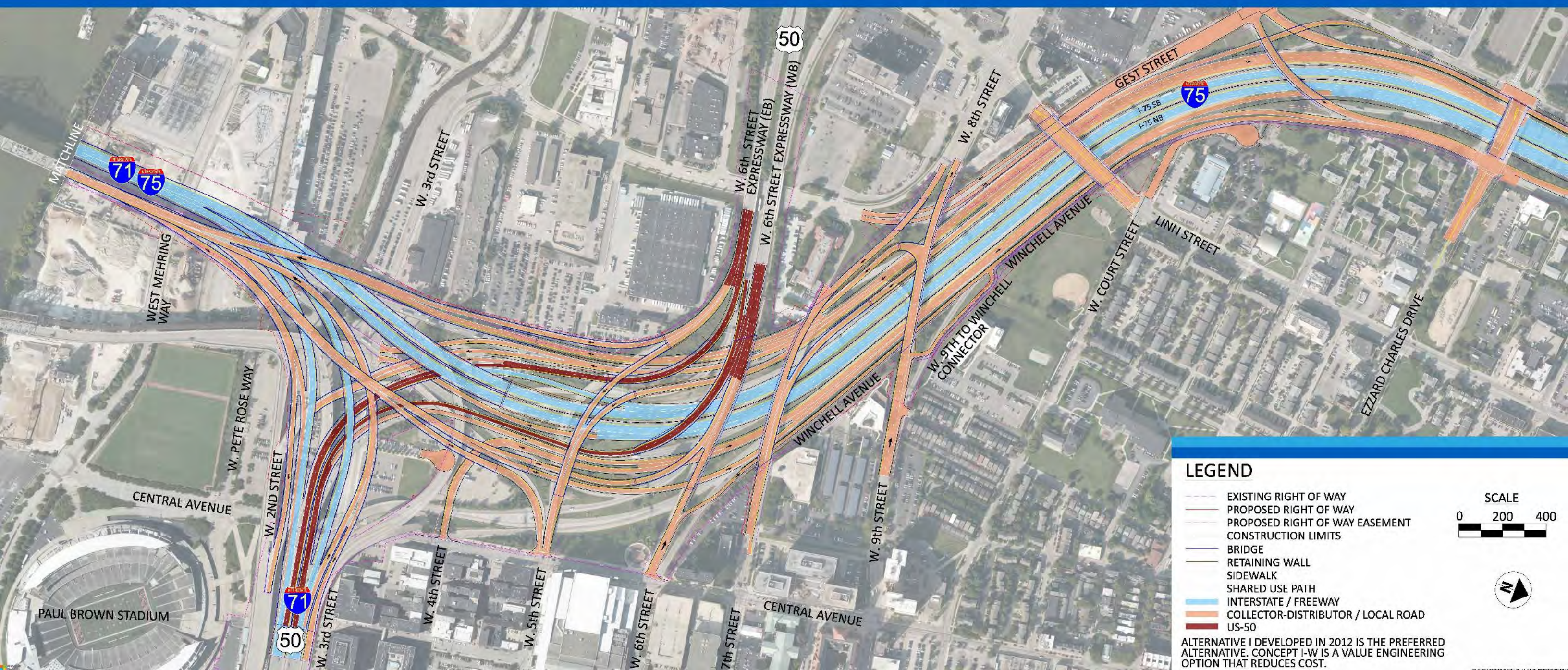
LEGEND

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

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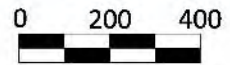



LEGEND

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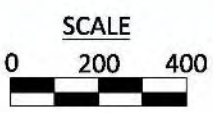
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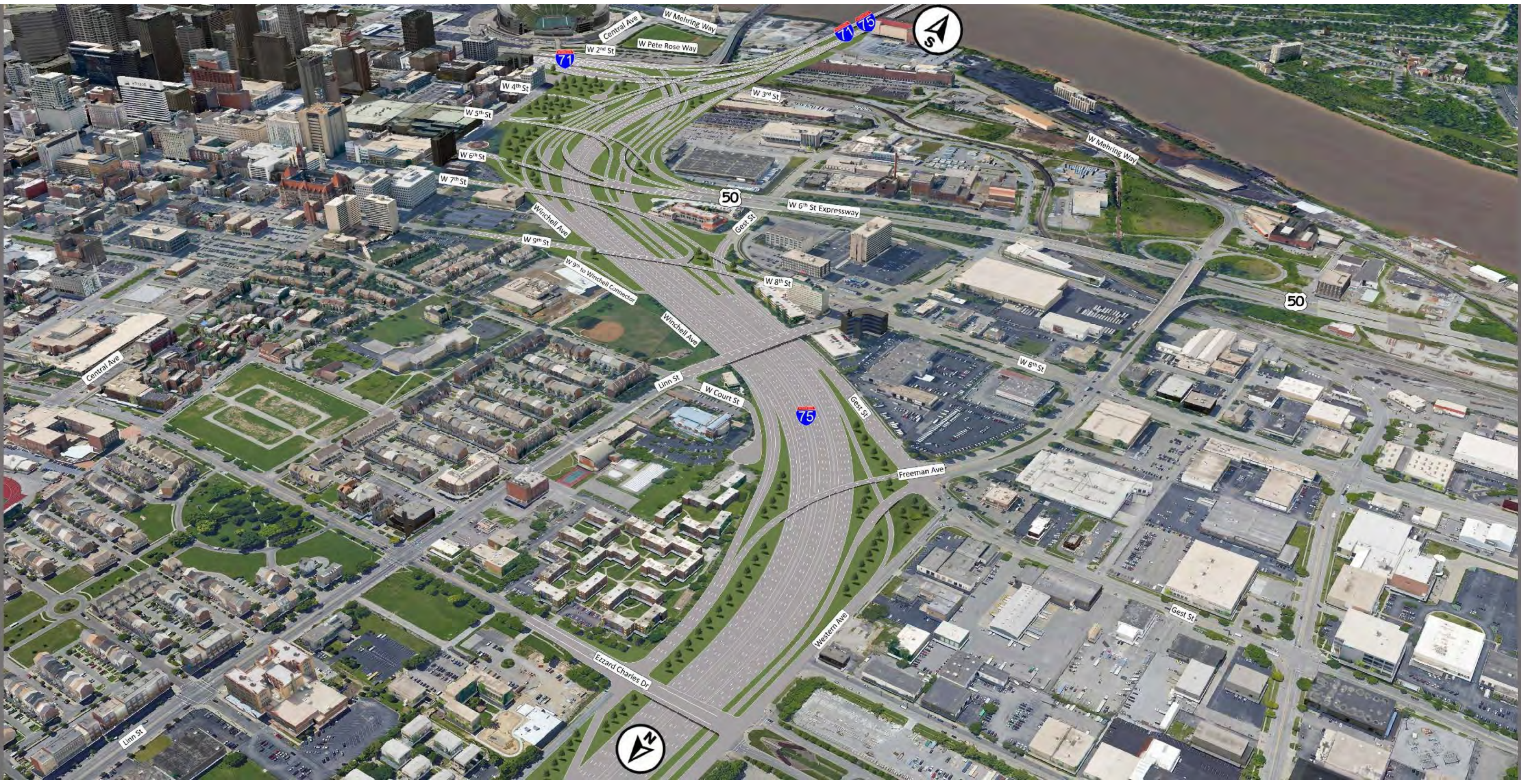


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ALTERNATIVE I DEVELOPED IN 2012 IS THE PREFERRED ALTERNATIVE. CONCEPT I-W IS A VALUE ENGINEERING OPTION THAT REDUCES COST.





BRENT SPENCE BRIDGE CORRIDOR PROJECT PHASING

BRENT SPENCE BRIDGE CORRIDOR DESIGN BUILD CONTRACT
ODOT PID 116649, KYTC KENTON COUNTY 6-17

■ COMPANION BRIDGE PHASE ■ PHASE III

CONSTRUCTION BEGINS Q4 2023
CONSTRUCTION ENDS Q1 2029

CONCEPT I-W BRIDGE CONFIGURATIONS

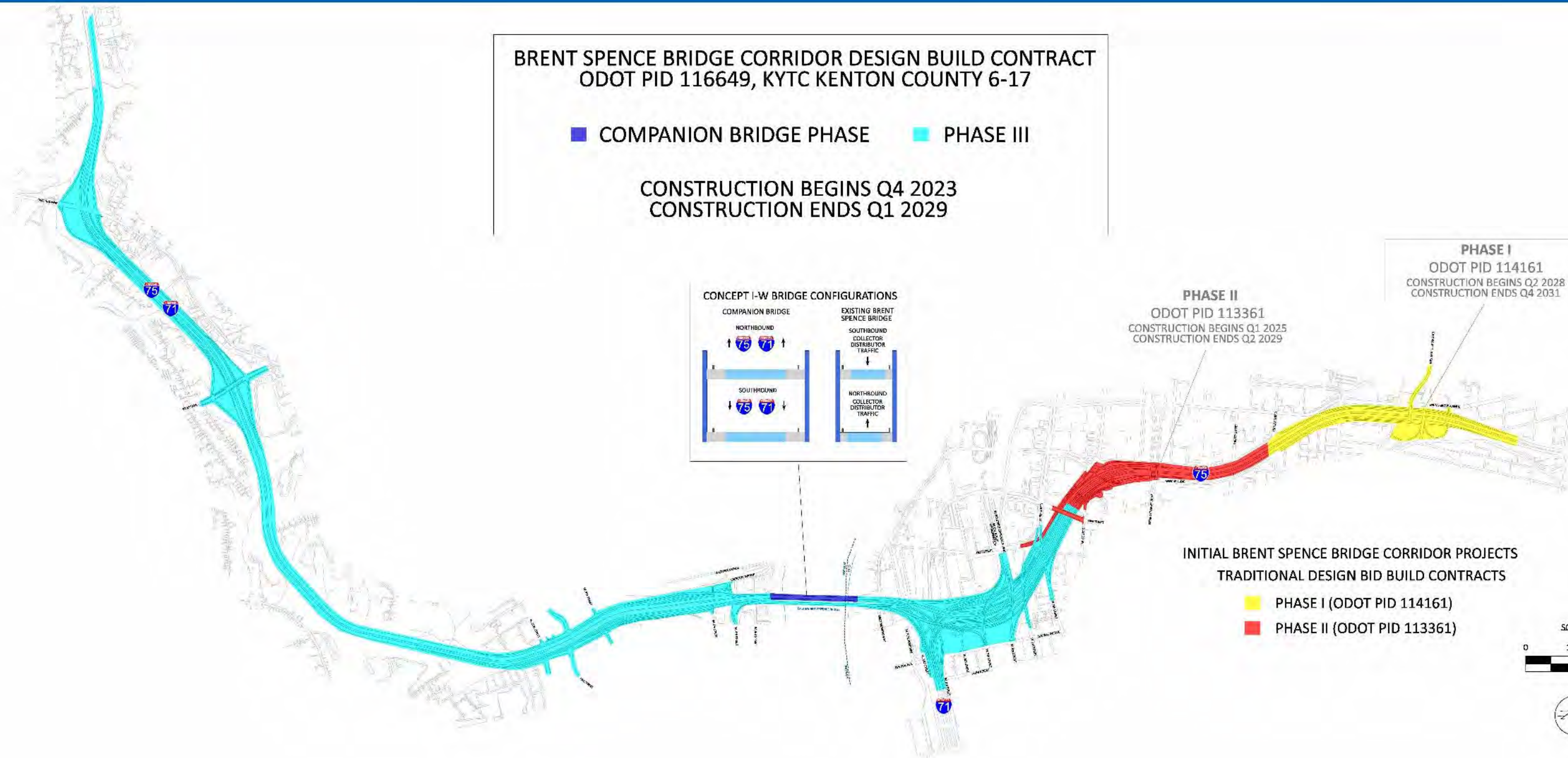


PHASE II
ODOT PID 113361
CONSTRUCTION BEGINS Q1 2025
CONSTRUCTION ENDS Q2 2029

PHASE I
ODOT PID 114161
CONSTRUCTION BEGINS Q2 2028
CONSTRUCTION ENDS Q4 2031

INITIAL BRENT SPENCE BRIDGE CORRIDOR PROJECTS
TRADITIONAL DESIGN BID BUILD CONTRACTS

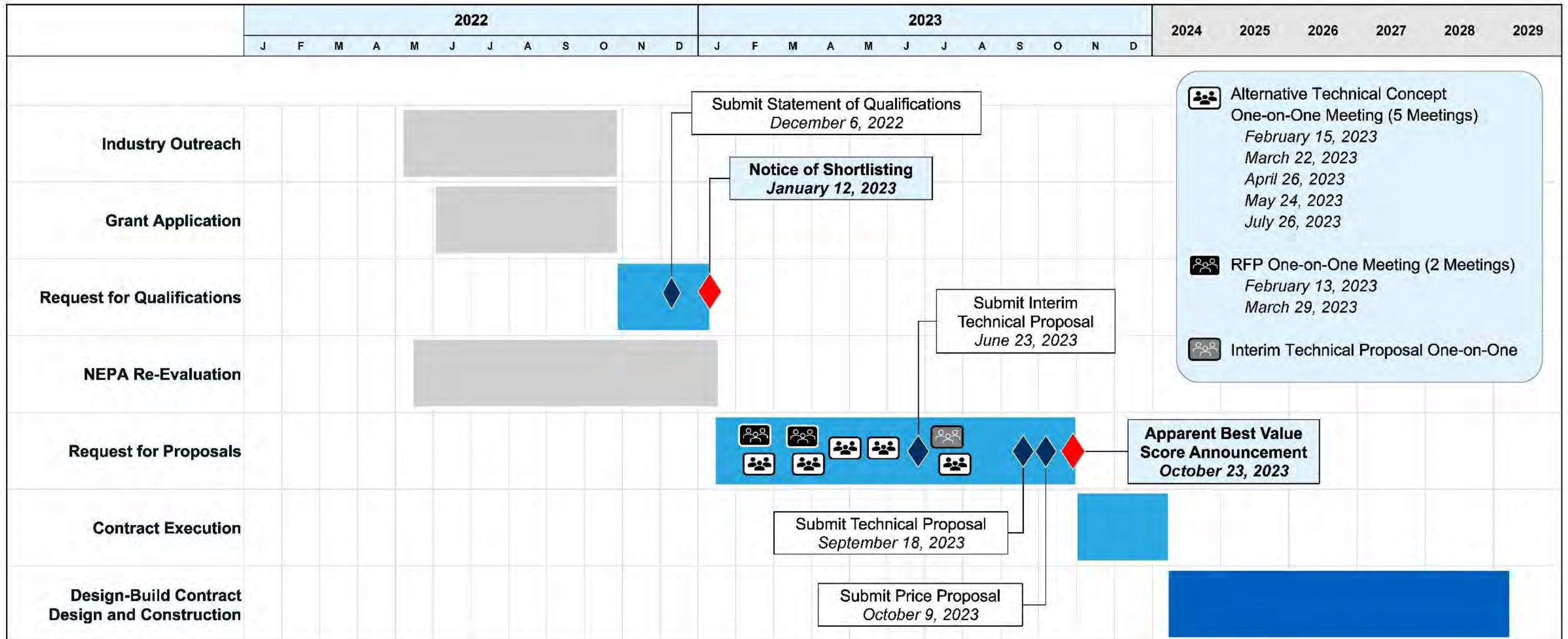
- PHASE I (ODOT PID 114161)
- PHASE II (ODOT PID 113361)





DRAFT DELIVERY TIMELINE

**BRENT SPENCE BRIDGE CORRIDOR DESIGN BUILD PROJECT
ODOT PID 116649 & KYTC KENTON COUNTY 6-17**



Brent Spence Bridge Corridor Project
Email Update
August 19, 2022

Advisory Committee E-mail Update

Aug. 19, 2022

Subject Line:

Brent Spence Bridge Corridor Project – Procurement Update

E-mail Copy:

Dear Advisory Committee Members –

By now, you've likely heard that Ohio and Kentucky submitted our second funding request to the United States Department of Transportation earlier this month.

The current funding request is through the Bridge Investment Program, which follows the May funding request through the Multimodal Projects Discretionary (MEGA) Grant Program. Ohio and Kentucky articulated in both applications that a total of \$1.66 billion in federal grant funding is needed regardless of which discretionary grant program awards funds to the project. We applied to multiple grant programs to give us the best chance of receiving maximum funding, in keeping with the pledges of Governors DeWine and Beshear to pursue every available federal dollar.

We are optimistic about our applications and expect to begin receiving feedback as soon as this fall.

As the project moves closer to full funding, the pace of activity has picked up to ensure construction readiness. The bi-state project team is prioritizing the following activities while the federal funding requests are under review:

- Updating the financial plan to align with existing funding opportunities and anticipated project needs, including evaluation of the potential impacts on inflation on the funding plans for each state;
- Continuing development of the revised concept for the new companion bridge, which significantly improves safety by separating through and local traffic;
- Analyzing potential options for construction using the design/build process; and
- Performing environmental field work, including updating air and noise evaluations.

In addition, we have crossed a significant threshold in terms of our procurement process.

Following a successful Industry Forum that we hosted back in June, we have continued to discuss various items that were raised during meetings held with members of the design build community. To offset the uncertainties in today's construction market, including inflation and supply chain pricing and availability, the project team has made the decision to move forward with Progressive Design Build

Procurement, which will allow the Owners (ODOT and KYTC) and the Design Build Team (Design Firm and Contractors) to price materials closer to when they will actually be used for construction.

The Design Build Team will be selected based on qualifications while considering their pricing approach rather than qualifications and just a fixed or lowest bidder price. This approach also allows the project team to work collaboratively on the preferred alternative with the Design Build Team to identify cost-effective solutions that meet the goals of the project and community. It also brings more certainty to our project delivery process and eliminates several variables for those seeking to bid on the project.

The decision to move forward with the Progressive Design Build Procurement will allow ODOT and KYTC to meet our goal of having a Design Build Team selected and project groundbreaking in 2023, with construction activities expected to begin in earnest in 2024.

As always, we welcome any questions or feedback you have. Please don't hesitate to reach out to us via our contact information noted below.

Stacee/Stefan signatures ...

**Brent Spence Bridge Corridor Project
Project Advisory Committee Summary
August 21, 2023**

Brent Spence Bridge Corridor Project Project Advisory Committee Meeting Summary August 21, 2023

Introduction

The Brent Spence Bridge (BSB) Corridor Project Advisory Committee (PAC) meeting was held on August 21, 2023 from 10:00 am to 11:30 am. The meeting was held at the Ohio-Kentucky-Indiana Regional Council of Governments (OKI) building located at 720 Pete Rose Way, Suite 420, Cincinnati, Ohio. Invitations to the meeting were sent to PAC members via email on July 25, 2023 (see Attachment). Attendees at the meeting included PAC members or their designated representatives and members of the project team from the Federal Highway Administration (FHWA), the Kentucky Transportation Cabinet (KYTC), the Ohio Department of Transportation (ODOT), the design-build team, and other consultants. The PAC meeting was open to the general public, although no members of the public attended. A list of attendees is included in Attachment 2.

Presentation

The meeting began with opening remarks by Stacey Hans (KYTC) and an introduction of Tom Arnold as ODOT project manager for the construction phase and members of the Walsh-Kokosing design-build team. The remarks were followed by a presentation by Tom Arnold (ODOT) and Stacey Hans. A copy of the presentation is included in Attachment 3. Major topics addressed in the presentation include:

- Project history;
- Purpose and need;
- Construction phases and schedules;
- Project overview, including discussion of the new companion bridge and a flyover animation;
- Progressive design-build, including description, request for proposal process, contract objectives, selected design-build team, schedule, and innovation process;
- Public outreach update, including on-going public outreach activities and diversity and inclusion opportunities; and
- Environmental update, including potential environmental impacts, proposed mitigation and enhancement measures, and schedule.

Comments and Questions

No members of the general public attended the PAC meeting, and no public comments were received. The following comments and questions were provided by PAC members after the presentation:

- Brent Cooper – Northern Kentucky Chamber of Commerce: Are there opportunities to advance construction and build the project sooner?



-
- Several factors influence the construction schedule, such as the need to maintain traffic during construction. KYTC and ODOT will continue to explore opportunities to reduce schedule during the progressive design-build process.
 - Tom West – City of Covington, Economic Development Director: Has the project team considered reflective sound from noise walls to make sure they don't increase noise on the opposite side of the highway?
 - The noise analyses fully considered how noise walls would impact the overall noise and acoustic environment. KYTC is also exploring the use of transparent noise walls to preserve views in Covington based on neighborhood feedback.
 - Woody Keown – National Underground Railroad Freedom Center: Is ODOT aware of erosion along the bank of the Ohio River (on the Ohio side) that was recently addressed in a Park Foundation meeting/presentation? Has ODOT connected with the Parks Foundation?
 - The project team is not aware of these activities. Additional information can be sent to Tom Arnold at ODOT.
 - Brent Cooper – Northern Kentucky Chamber of Commerce: Please confirm the groundbreaking will occur next year.
 - The groundbreaking for the overall project (heavy dirt activity) will be next year. There are several on-going project activities that are occurring now and will continue, including right-of-way acquisition in Kentucky and Ohio. KYTC is also looking at an opportunity for a pilot project with transparent noise walls in the near future.
 - Elizabeth Pierce – Cincinnati Museum Center: The museum center is interested in more information about the timing for the construction of the Ezzard Charles Drive bridge, which provides access to the center. The Museum Center is also interested in partnering with the project team to showcase various aspects of the project, such as the engineering and science involved or the environmental impacts.
 - The Ezzard Charles Drive bridge will be reconstructed during Phase II of the project, which is scheduled to start in 2026. KYTC and ODOT welcome the opportunity to work with the Museum Center to showcase the project.
 - Gary Moore – Boone County Fiscal Court Judge Executive: Will there be dedicated truck/freight lanes on the cut-in-the-hill? Will the project accommodate transit?
 - The project will not include dedicated truck/freight lanes, but I-71/I-75 will be six lanes on the cut-in-the-hill, which will allow traffic to flow better and experience fewer slowdowns. The project will not preclude future transit opportunities and improvements. The additional lanes will improve traffic flow for existing transit that uses I-71/I-75. The shoulders will be built full depth, which would allow bus on shoulder if that is implemented in the future.



-
- Sharmili Reddy – Planning and Development Services of Kenton County: Has the project team considered a virtual option for the upcoming public meetings?
 - Because the upcoming meetings are open house format and there are not formal meeting proceedings to broadcast, a virtual option is not being offered. The exhibits and handouts for the meetings are posted on the project website. KYTC and ODOT are also preparing a pre-recorded presentation that will play at the open house meetings, which will also be posted on the project website when it is finalized.
 - Nathan Alley – Sierra Club: Please confirm the environmental schedule. Sierra Club recently received information that the environmental documents would be available in February 2024.
 - The dates shared at today's meeting are the most current. KYTC and ODOT are targeting late 2023 for the environmental documents to be made available for public review and comment.

Exhibits

Exhibits prepared for the upcoming open house project status update meetings were available for review by attendees, see Attachment 4. Members of the project team were present to answer any questions. Photographs from the PAC meeting are provided in Attachment 5.



Attachment 1: Invitations

Subject: Brent Spence Bridge PAC Meeting -- August 21, 2023
Location: OKI Regional Council of Governments; 720 East Pete Rose Way; Suite 420

Start: Mon 8/21/2023 10:00 AM
End: Mon 8/21/2023 12:00 PM
Show Time As: Tentative

Recurrence: (none)

Meeting Status: Not yet responded

Organizer: Mark Becherer

Required Attendees



Optional Attendees



Dear Advisory Committee Member:

The next meeting of the Advisory Committee will be held from 10:00 AM – Noon on August 21, 2023, at:

OKI Regional Council of Governments
720 East Pete Rose Way
Suite 420
Cincinnati, Ohio 45202

The purpose of the Advisory Committee Meeting is to provide an overall project update; including the current project status, a discussion of the progressive design build process, a review of the overall project schedule, updates on the upcoming public meetings, and provide an opportunity for questions and comments. We ask that you share the information provided at the meeting and continue to act as liaisons between your interested groups and the Project Team.

We look forward to seeing you on Monday, August 21, 2022, at OKI Regional Council of Governments. Please email Mark Becherer a [REDACTED] to confirm your attendance. If you are unable to attend, we invite you to send another representative from your organization in your place.

If you are sending another representative; or if there is a new contact or representative for your organization; please provide the name and contact information with your response. You can also call me at [REDACTED] with any questions. Thank you for your involvement on the Advisory Committee.

Sincerely,

Mark Becherer
Vice President

[REDACTED]
HNTB CORPORATION

 **100+ YEARS OF INFRASTRUCTURE SOLUTIONS**



Dear Advisory Committee Member:

The next meeting of the Advisory Committee will be held from 10:00 AM – Noon on August 21, 2023, at:

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We look forward to seeing you on Monday, August 21, 2022, at OKI Regional Council of Governments. Please email Mark Becherer at mbecherer@hntb.com to confirm your attendance. If you are unable to attend, we invite you to send another representative from your organization in your place.

If you are sending another representative; or if there is a new contact or representative for your organization; please provide the name and contact information with your response. You can also call me at 414.559.8900 with any questions. Thank you for your involvement on the Advisory Committee.

Sincerely,

Mark Becherer, PE
Vice President
HNTB Corporation

Advisory Committee Meeting Agenda August 21, 2023

1. Welcome and Introductions
2. Current Project Status
3. Progressive Design Build Status & Process
4. Current Project Overall Timeline/Schedule
5. Project Status Update Public Meeting
6. Public Comments

This is a working meeting of the Project Team and the Advisory Committee. As such, participation from the general public during the meeting will be limited to the portion of the agenda designated for public comment.



Attachment 2: Attendees

BRENT SPENCE BRIDGE CORRIDOR PROJECT

Project Advisory Committee Meeting ~ August 21, 2023

First Name	Last Name	Agency	Checked In
Andrew	Aiello, Chief of Staff	SORTA/Metro	✓
Nathan	Alley	Sierra Club	✓
Tom	Arnold	ODOT, District 8	✓
Mark	Becherer	HNTB	✓
Eric	Beck	Hamilton County Engineer	✓
Leland	Bennett	SouthBank Partners	
Caroline	Blackburn	Cincinnati Bengals	
Katie	Blackburn	Cincinnati Bengals	✓
Keith	Blake	Stand in for West End Community Council	
John	Brazina	City of Cincinnati Dept. of Transportation & Engineering Dept.	✓
Laura N.	Brunner, President & CEO	Port of Greater Cincinnati Development Authority	✓
Nan	Cahall	Senator Portman's Office	
Tammy	Campbell	ODOT, District 8	
Markiea	Carter	City of Cincinnati Community and Economic Development	
Phil	Castellini	Cincinnati Reds	
Brent	Cooper	Northern Kentucky Chamber of Commerce	✓
Lee	Crume	Northern Kentucky Tri-Ed	
Brendon	Cull, President & CEO	Cincinnati USA Regional Chamber	
Mour	Diop	FHWA - KY	
Gina	Douthat	TANK, Deputy General Manager	✓
Stephanie	Dumas	Hamilton County Commissioners	
Cindy	France	Cincinnati Park Board	
Rob	Franxman	Boone County Engineer	
Kathleen	Fuller	ODOT	



BRENT SPENCE BRIDGE CORRIDOR PROJECT

Project Advisory Committee Meeting ~ August 21, 2023

First Name	Last Name	Agency	Checked In
Charles "Chip"	Gerhardt	Government Strategies Group	
Jason	Gloyd	Governor's Southwest Ohio Regional Liaison	✓
Chris	Griffin	West End Community Council	
Joel	Gross	Cincinnati Park Board	
Stacey	Hans	KYTC, District 6	✓
Mayor Dave Jill Bailey	Hatter	City of Ft. Wright, KY	✓
Jodi	Heflin	HNTB	✓
Mayor Jude	Helman	City of Ft. Mitchell, KY	
Nick	Hendrix	Kenton County Engineer	
Steve	Hill	Lewisburg Neighborhood Association	
Adam	Johnson	FHWA Ohio	✓
Matt	Jones	Cincinnati Business Committee	
Katherine	Keough-Jurs	City of Cincinnati City Planning	✓
Woody	Keown	National Underground Railroad Freedom Center	✓
Alexis	Kidd	West End Community Council	
Sherry	Kish	HNTB	
Kris	Knochelmann, Judge Exec.	Kenton County Fiscal Court	
Steve	Leeper, President & CEO	3CDC	
Tim	Long	FHWA - OH	✓
Michael	Loyselle	FHWA Kentucky	
Pete	Metz	Cincinnati USA Regional Chamber	✓
Jill P.	Meyer	Cincinnati USA Regional Chamber	
Mayor Joseph U.	Meyer	City of Covington	✓
Gary	Moore, Judge Exec.	Boone County Fiscal Court	✓

BRENT SPENCE BRIDGE CORRIDOR PROJECT

Project Advisory Committee Meeting ~ August 21, 2023

First Name	Last Name	Agency	Checked In
David	Okum	Hamilton County Regional Planning Commission	✓
Steve	Pendery, Judge Exec.	Campbell County Fiscal Court	
Elizabeth	Pierce, President & CEP	Cincinnati Museum Center	✓
Lorrie	Platt	OKI	
Mark	Policinski	OKI	✓
Mayor Aftab	Pureval	City of Cincinnati	
Kevin	Randall	HNTB	
Mimi	Rasor	RASOR	✓
Shannon	Ratterman	Center for Great Neighborhoods	✓
Sharmili	Reddy, AICP	Planning and Development Services of Kenton County	✓
Alicia	Reece, President of the Board	Hamilton County Commissioners	
Charlie	Rowe	ODOT	
Joe	Shields	City of Park Hills, KY	
Keith	Smith	ODOT, District 8	
Ken	Smith	City of Covington	
Amy	Spiller, President	Duke Energy, OH-KY	
Stefan	Spinosa	ODOT, District 8	✓
Jeffrey L.	Stec, J.D., Director	Citizens for Civic Renewal	
Scott	Stone	FHWA Ohio	✓
Gary	Valentine	KYTC State Highway Engineer's Office	✓
Will	Weber	SouthBank Partners	
Melissa	Wegman	Queensgate Business Alliance	✓
Jack	Weiss	Cincinnati Bulk Terminals, LLC	✓
Tom	West	City of Covington, Economic Development Director	✓




Attachment 3: Presentation



1

Welcome

- Meeting purpose
 - Share updates on the Brent Spence Bridge (BSB) Corridor Project
 - Keep the PAC informed to allow you to be a conduit to keep the community informed
- Agenda
 - Project history and overview
 - Progressive design-build process
 - Public Outreach and Environmental update



2

Project History

2000	
2004	← 2004 ODOT & KYTC begin BSB Corridor Project
2005	Alternatives Development & Evaluation
	Impact Assessment
2010	Public & Stakeholder Input
2012	← 2012 Environmental Approval - Selected Alternative I
2015	Design Refinements
	Cost/Impact Reductions
	Enhancements
2022	← 2022 Refined Alternative I - Concept I-W

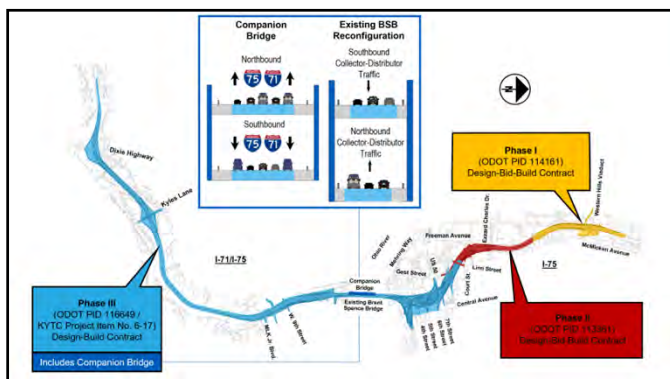
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Purpose and Need

- Improve traffic flow and level of service
- Improve safety
- Correct geometric deficiencies
- Maintain connections to key regional and national transportation corridors



4

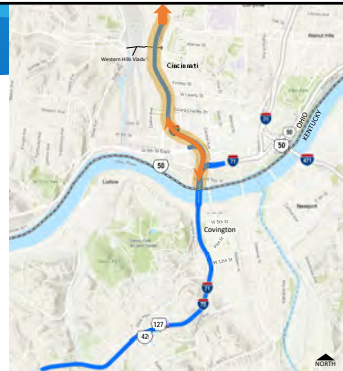


5

Project Description

Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into the Mill-Creek Expressway-Hopple Street Interchange Project




6

Project Description

Ohio

- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E




7

Project Description

Kentucky

- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges




8

Project Description

Kentucky

- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane




9

Project Description

Brent Spence Bridge

- Rehabilitate and reconfigure existing bridge
 - Three lanes each deck
 - Inside/outside shoulders
 - Carry local traffic




10

Project Description

Brent Spence Bridge

- New double-decker companion bridge
 - 5 lanes each deck
 - Carry through (interstate) traffic



11

Project Description

New Companion Bridge

- Innovative and cost-effective design
- Arch or Cable-Stayed bridge type
- Iconic and aesthetically pleasing
- On-going coordination with the project Aesthetics Committee



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Progressive Design-Build

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Progressive Design-Build Process

Progressive Design-Build

- Selection based on qualifications and pricing approach
- Offsets construction market uncertainties
 - Inflation
 - Supply chain
 - Availability of materials

BRENT SPENCE BRIDGE CORRIDOR PROJECT
REQUEST FOR PROPOSALS (RFP)
PROGRESSIVE DESIGN-BUILD CONTRACT
SOUTH SIDE BRIDGE RAMP PROJECT - PHASE 2 RFP
2020 CONSTRUCTION PROJECT #104
FEBRUARY 17, 2022
TEAM KENTUCKY

17

17

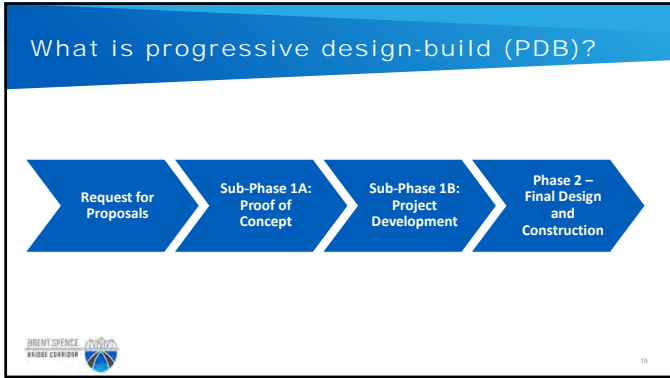
Progressive Design-Build Process

Progressive Design-Build

- Prices materials closer to construction
- Allows for innovation concepts
- Provides more opportunities for outreach
 - Local agency coordination
 - Traffic and incident management
 - Aesthetics

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- ### Procurement Highlights
- What's in the RFP?
- Instructions to Offerors
 - Contract
 - Technical Requirements
 - Contract Objectives
 - Higher level design-build scope
 - Innovation Period
 - Diversity and Inclusion Outreach Plan (DIOP)
 - Limited Negotiations
- IBRENT SPENCE
KROGER CORRIDOR


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- ### Contract Objectives
- Maximize the Project scope within the programmed funding amounts through innovation, design optimization and effective risk mitigation;
 - Build the Project with a context sensitive design that fits within the community;
 - Maximize the public investment in the Project by minimizing the footprint;
 - Minimize the footprint of the interstate system to maximize potential developable space;
 - Improve neighborhood connectivity across the interstate;
 - Minimize traffic disruption during construction, with minimal detours or diversion of traffic to local streets;
 - Provide opportunities for Workforce Development and DBE utilization;
 - Provide strong aesthetic value along the Project corridor;
 - Achieve effective project delivery;
 - Minimize physical intrusion and impact;
 - Create best environmental outcomes;
 - Design for sustained quality of life;
 - Improve the local road aesthetics when crossing the interstate; and
 - Open the new companion bridge to traffic by July 15, 2029.
- IBRENT SPENCE
KROGER CORRIDOR

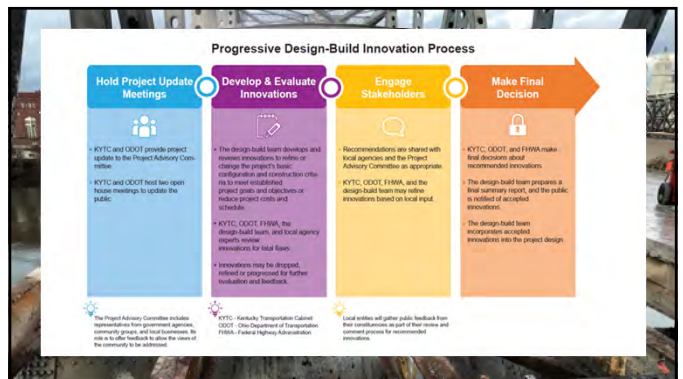
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- ### Progressive Design-Build Team Selection
- Contractors – Joint Venture
- Walsh Construction Company II, LLC
 - Kokosing Construction company, Inc
- Design Consultants
- AECOM Technical Services, Inc
 - Lead design firm
 - Lead designer Kentucky segment
 - Jacobs Technical Services, Inc.
 - Lead designer Ohio Segment
 - Independent companion bridge engineer
 - Parsons Engineering Group, Inc
 - Independent design quality firm
 - WEB Ventures, LLLC
 - Diversity, inclusion, and outreach firm
- 
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- ### Draft Schedule
- August: Preconstruction Notice to Proceed
 - 60-day Innovation Period
 - Spring of 2024: Proof of Concept
 - Spring of 2025: Phase 1 Complete
- 
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Develop & Evaluate Innovations

- The design-build team develops and reviews innovations to refine or change the project's basic configuration and construction criteria to meet established project goals and objectives or reduce project costs and schedule.
- KYTC, ODOT, FHWA, the design-build team, and local agency experts review innovations for fatal flaws.
- Innovations may be dropped, refined or progressed for further evaluation and feedback.

25

Develop & Evaluate Innovations

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- Innovations may be dropped, refined or progressed for further evaluation and feedback.

Engage Stakeholders

- Recommendations are shared with local agencies and the Project Advisory Committee as appropriate.
- KYTC, ODOT, FHWA, and the design-build team may refine innovations based on local input.

26

Develop & Evaluate Innovations

- The design-build team develops and reviews innovations to refine or change the project's basic configuration and construction criteria to meet established project goals and objectives or reduce project costs and schedule.
- KYTC, ODOT, FHWA, the design-build team, and local agency experts review innovations for fatal flaws.
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Engage Stakeholders

- Recommendations are shared with local agencies and the Project Advisory Committee as appropriate.
- KYTC, ODOT, FHWA, and the design-build team may refine innovations based on local input.

Make Final Decision

- KYTC, ODOT, and FHWA make final decisions about recommended innovations.
- The design-build team prepares a final summary report, and the public is notified of accepted innovations.
- The design-build team incorporates accepted innovations into the project design.

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Design & Construction

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BRENT SPENCE BRIDGE CORRIDOR

Public Outreach & Environmental Update

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Public Outreach Since Last PAC Meeting

Neighborhood Meetings

- Thirteen neighborhood meetings
- Project open house in both Ohio and Kentucky

In addition:

- Held over 75 meetings with individuals/groups, communities, and organizations
- Received over 630 Public Comments (neighborhood meetings and website)

Upcoming Project Status Update Open House

- August 23 in Kentucky
- August 24 in Ohio

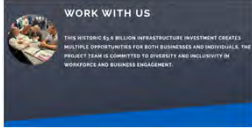

BRENT SPENCE BRIDGE CORRIDOR

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Landmark Diversity and Inclusion Opportunity

Historic federal investment in our region with guidelines and requirements for:

- Disadvantaged Business Enterprise (DBE) participation
- Workforce development

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Landmark Diversity and Inclusion Opportunity

- Diversity & Inclusion Outreach Committee
- DBE outreach events
- Workforce development events



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Environmental Process Update

Potential Environmental Impacts

- Right-of-way
- Parks
- Historic properties
- Noise
- Wetlands, streams, rivers, and floodplains
- Threatened and endangered species
- Temporary construction impacts




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Environmental Process Update

Mitigation Measures

- Noise walls
- Park improvements
- Management of temporary construction impacts
- Investments in historic resources
- Off-site improvements for wetlands, streams, and threatened and endangered species




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Environmental Process Update

Enhancement Measures

- Aesthetic treatments
- Pedestrian and bicycle improvements
- Noise/visual screening walls
- Separating highway runoff
- Land for potential redevelopment
- Workforce development and training




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Environmental Process Update

- Supplemental EA available for public review – late 2023
- Public hearings – late 2023
- Expected environmental approval – early 2024




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Comments?

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THANK YOU!

For more detailed information or to provide feedback visit:
www.brentspencebridgecorridor.com



BRENTSPENCEBRIDGECORRIDOR.COM

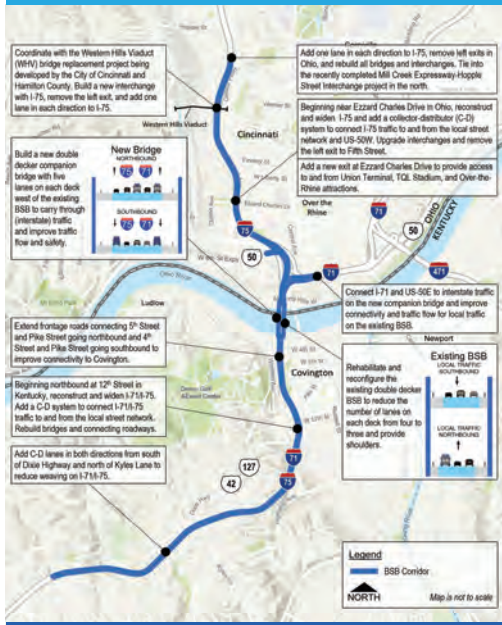
 

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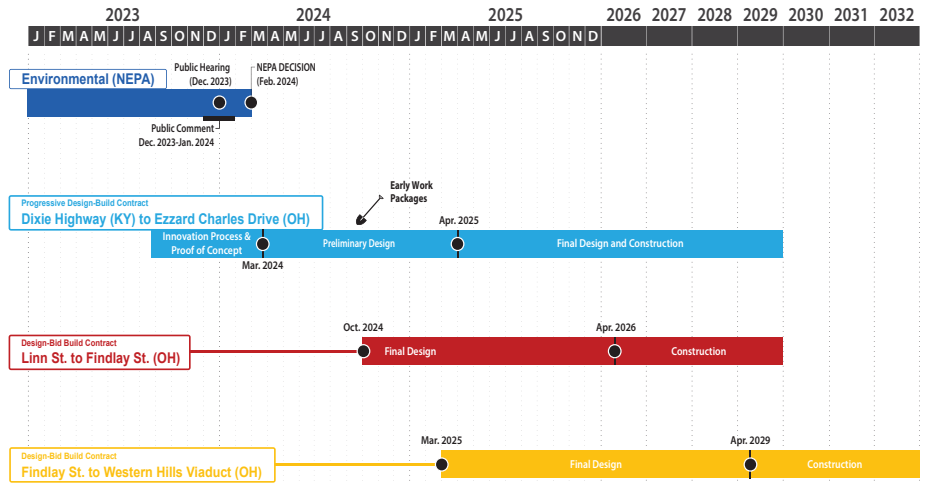
Attachment 4: Exhibits

Brent Spence Bridge Corridor Project Proposed Improvements



KYTC Item No. 6-17 | ODOT PID 89068

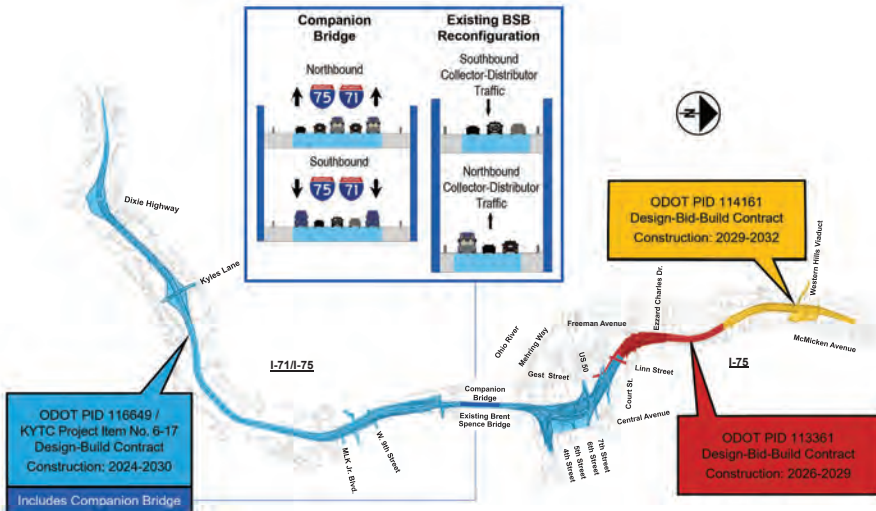
Project Schedule



Brent Spence Bridge Corridor Project

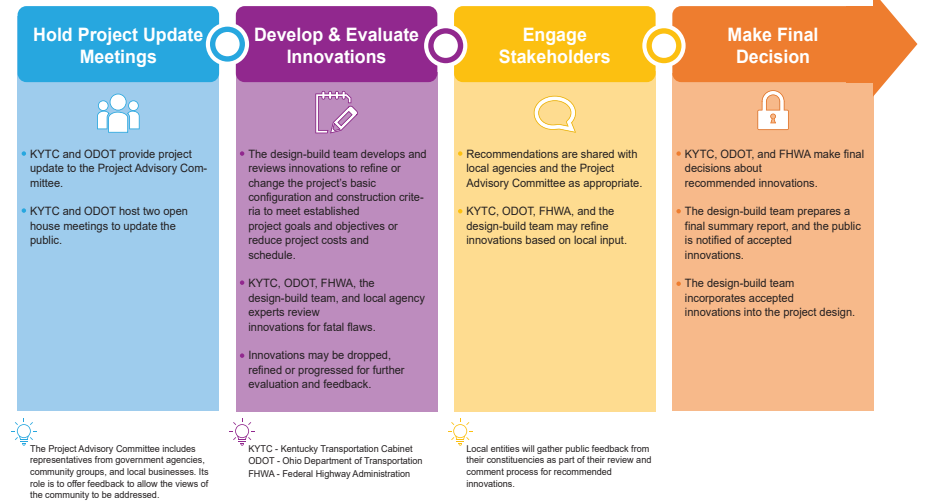


KYTC Item No. 6-17 | ODOT PID 89068

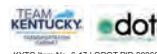


The Brent Spence Bridge Corridor Project will be delivered in three phases, which are shown in blue, red, and yellow above.

Progressive Design-Build Innovation Process



Brent Spence Bridge Corridor Project Construction Phases



KYTC Item No. 6-17 | ODOT PID 89068

Brent Spence Bridge Corridor Project



KYTC Item No. 6-17 | ODOT PID 89068

Enhancing the Community and Offsetting Potential Impacts

Environmental commitments are being developed to enhance the community and address permanent and temporary impacts.

Corridor-wide aesthetic enhancements

New and improved pedestrian and bicycle connections on local streets that are parallel to or cross I-71/I-75

Reduced flooding and combined sewer overflows by separating interstate stormwater runoff and improving existing drainage systems in Kentucky and Ohio

10 acres of land opened up for potential redevelopment in downtown Cincinnati

Disadvantaged business enterprise participation, on-the-job training, and workforce development during the progressive design-build contract

Mitigating potential noise impacts and proposing additional noise/visual screening walls above and beyond state policy

Measures to manage potential temporary traffic, dust, noise, and air quality impacts during construction, including coordination with local agencies and the public

Minimization and mitigation for potential wetland and stream impacts

Avoidance, minimization, and mitigation for potential impacts to threatened and endangered species

Mitigation for potential impacts to the Goebel Park Complex, Queensgate Playground and Ball Field, Lewisburg Historic District, and Longworth Hall



Potential Impact Summary

Environmental Resource	Original (2012) Impacts	Current Potential Impacts
Land Use	53.4 acres ¹	51.2 acres
Neighborhood and Community Cohesion	<ul style="list-style-type: none"> Minor impacts Pedestrian/bicycle access at limited, select locations 	<ul style="list-style-type: none"> Impacts avoided Enhanced pedestrian/bicycle access and improved walkability on local streets that cross I 71/I-75 and at intersections
Goebel Park Complex	<ul style="list-style-type: none"> 2.59 acres (2.4 acres new land returned to park) 360 feet of walking trail Basketball court and parking lot Proximity impacts to outdoor pool 	<ul style="list-style-type: none"> 2.84 acres² (2.23 acres new land returned to park) 360 feet of walking trail Basketball court and associated resources Proximity impacts to outdoor pool
Queensgate Playground and Ball Field	0.9 acres	0.72 acres
Relocations	-80 residential units ³ -30 commercial ⁴	4 residential units 27 full, 1 partial commercial
Historic Longworth Hall	204 feet removed	204 feet removed
Lewisburg Historic District	21 full acquisitions 7 partial acquisitions	3 full acquisitions 8 partial acquisitions (strip right-of-way)
Low-income and minority populations	Minor Impacts	Reduced impacts and increased benefits
Wetlands	1.38 acres – no high quality	2.38 acres – no high quality
Streams and Rivers	3,340+ feet – no exceptional	1,368 feet – no exceptional
Floodplains	New pier construction	Same as original
Threatened/Endangered Species Habitat	52 acres (typical for 1 bat species)	90 acres (typical for 3 bat species)
Noise	Impacts throughout Kentucky and in some locations in Ohio	Currently under study
Air Quality	Currently under study	Currently under study
Construction Impacts	Normal temporary impacts ⁵	Normal temporary impacts ⁵

1. Total includes 22.01 acres of property owned by the City of Cincinnati that was not quantified in the original impacts.

2. Increased impacts due to the extension of Simon Kenton Way and new stormwater facilities. Additional mitigation will occur to offset these impacts.

3. Total includes estimated number of relocated households. The original impacts counted apartment buildings as one unit and would have relocated a substantially greater number of households.

4. Total includes commercial tenants relocated in Longworth Hall. The original impacts counted the demolition of 204 feet of Longworth Hall as one commercial relocation.

5. Construction of the project may result in temporary noise, air quality, and access impacts.



Brent Spence Bridge Corridor Project



KYTC Item No. 6-17 | ODOT PID 89068



Brent Spence Bridge Corridor Project



KYTC Item No. 6-17 | ODOT PID 89068



Brent Spence Bridge Corridor
(Looking Northeast from Kentucky – Cable Stayed Option)



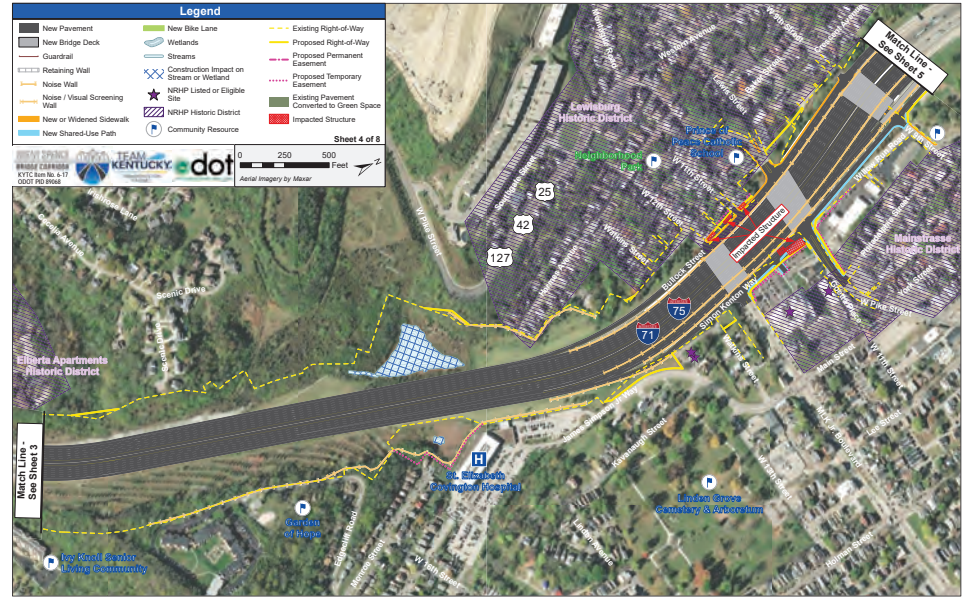
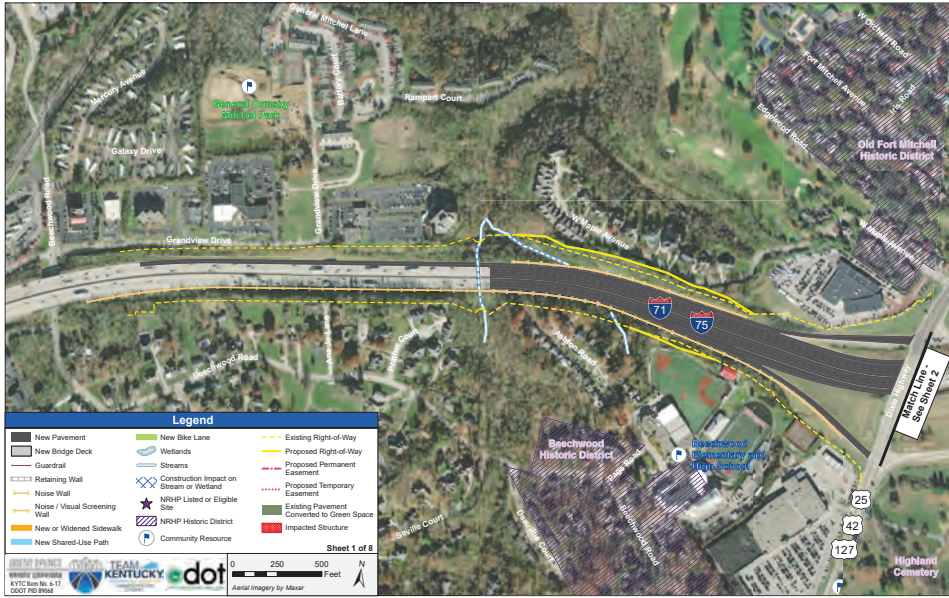
KYTC Item No. 6-17 | ODOT PID 89068



Brent Spence Bridge Corridor
(Looking Northeast from Kentucky – Arch Bridge Option)



KYTC Item No. 6-17 | ODOT PID 89068



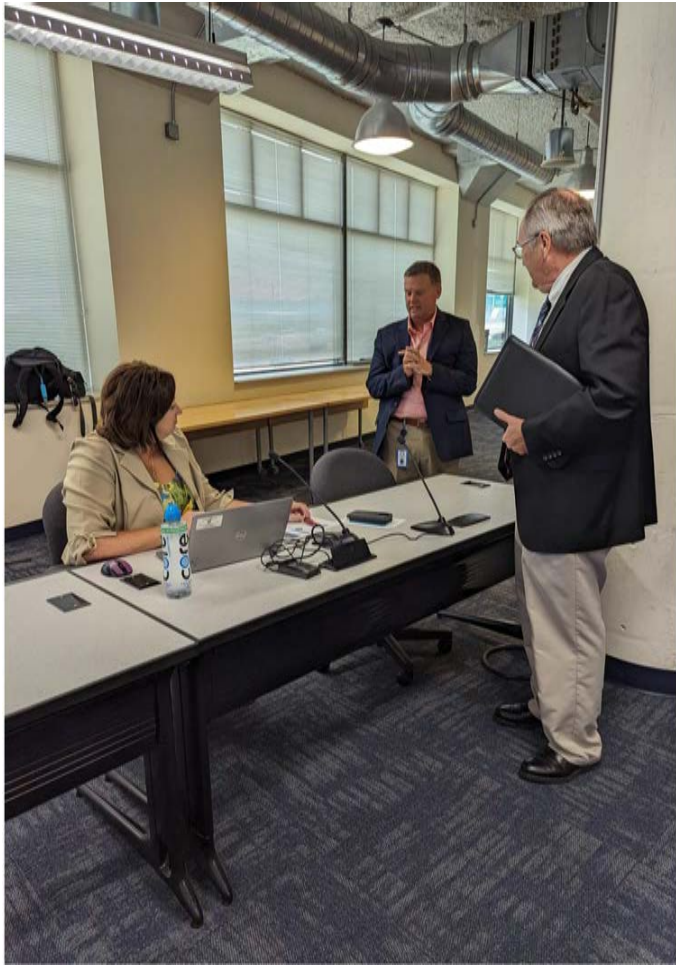






Attachment 5: Photographs





**Appendix C:
Aesthetic Committee**



**Brent Spence Bridge Corridor Project
Meeting Summary - Covington Subcommittee
June 28, 2022**

**Covington Aesthetic Sub-Committee Meeting No.1
June 28, 2022 – (KYTC D-6 office, 3-5 pm)**

1. Welcome and Introductions

Following welcoming comments by Gary Valentine, the below listed attendees made self-introductions

Emily Wolff - Advisory Committee Member
Ben Oldiges - Advisory Committee Member
Steven Hill - Advisory Committee Member
Sarah Allan – representing Susan Smith, Advisory Committee Member
Mayor Joseph Meyer, City of Covington KY
Stacey Hans -KYTC
Gary Valentine – KYTC
David Reed – QK4
Glen Kelly- QK4

2. Purpose of Subcommittee

Purpose - Assist KYTC in the development of “I-71/75 Covington Area Streetscape Guidelines”. The Guidelines will be a component of the overall Brent Spence Bridge (BSB) Project procurement documents for final improvements made to impacted areas of Covington and adjacent areas in Kentucky.

Goals of the guidelines will include protection and enhancement of the unique and historic character of downtown Covington including incorporation of the following critical elements of design:

- Coordinated corridor aesthetic features and treatments.
- Landscape buffering and integration.
- Pedestrian safety and mobility
- Complete street concepts and linkage to existing trail networks where possible

3. Project History and Overview

Information presented at the June 7, 2022 Industry Forum, held at the Northern Kentucky Convention Center in Covington, was used to provide an overview of the Brent Spence Bridge project history and the currently proposed work. This information is available on the [Brent Spence Procurement website](#) at the link labeled June 7th Forum Information

6/13/2022: Industry Forum Information

6/13/2022 3:48 PM

Thank you to all the companies, organizations, and individuals who participated in the June 7th, 2022 BSB Industry Forum.

ODOT and KYTC is sharing the following information: General Session PowerPoint, Attendee Lists, and the various visual displays.

Link: [June 7th Forum Info](#)

A recording of the General Session presentation will be posted soon.

KYTC noted the primary focus of the BSB project is to address the significant traffic capacity and safety issues resulting from the multiple movements and transitions from local to through interstate traffic. Construction of local system adjacent to the interstate travel ways is the foundation of the overall BSB project. This local traffic system, with associated interfaces with local streets, will cross the Ohio River on the existing bridge. Interstate traffic only will be accommodated on the new double deck river crossing.

4. Opportunities

To trigger discussion on concerns and values, KYTC briefly presented constructed elements of I-65 Ohio River Crossing completed in Louisville, Ky and Jeffersonville In. These included: underpass treatments, gateways, pedestrian facilities, lighting, signing, landscaping, and retaining wall treatments. Documents/guidelines included in Construction Procurement Documents for Aesthetic and Landscape treatments were also briefly discussed.

5. Covington Values

- Opportunities to create improved bike and pedestrian environments along parallel roads and intersecting side streets.
- Lighting is very important throughout the overhead bridge crossings.
- Concern for mitigating the loss of tree canopy within new footprint of the interstate - especially alongside the park. Solutions should be developed to coordinate with the existing park master plan. KYTC noted that there have been recent efforts to minimize impacts.

- Concern for the treatment and environment created within underpasses.
- Concern for those areas where footprint is close to existing assets (park, pool on east side and houses on the west side). Some previous geotechnical work (by SD1) may be available.
- Gateway opportunities- CW Bailey Bridge intersection. Opportunities to coordinate solutions and partner in implementation.
- Concern for noise attenuation. Noise walls and quiet pavement solutions represent the typical treatments and solutions.
- Traffic calming on connecting local streets are also a concern to eliminate high speeds within the urban fabric.

6. Next Steps

Develop and provide to Advisory committee

- Available details of underpass options and examples, including dimensions (width and heights).
- Complete Street Options (specifically along Bullock and Jillian's/Simon Kenton)
- Lighting option examples

Information exchange targeted mid-July

Next meeting – Tentatively, August 10 at 2:30

**Brent Spence Bridge Corridor Project
Meeting Summary - Ohio Subcommittee
June 28, 2022**

BURGESS & NIPLE

BRENT SPENCE BRIDGE CORRIDOR PHASES I AND II PID Nos. 113361 & 114151

Meeting: Aesthetic Committee Meeting

Date: Tuesday, June 28, 2022

Attendees:

Charlie Rowe	ODOT D8
John Otis	ODOT D8
Stefan Spinosa	ODOT D8
Keith Smith	ODOT D8
Heather McColeman	ODOT Central Office
Jon Brunot	Burgess & Niple
Steve Anslinger	Burgess & Niple
Abby Cueva	EMH&T
Betty Hull	Razor Marketing
Angie Strunc	City of Cincinnati
James Noyes	Hamilton Co. Regional Planning Commission
Jim Beitz	Cincinnati History Library and Archives
Michael Schuster	Michael Schuster Associates
Krutarth Jain	American Institute of Architects – Cincinnati Chapter
Katie Eagan	Cincinnati USA Regional Chamber of Commerce
Cindy France	City of Cincinnati – Park Planning
Jaret Lundy	American Society of Civil Engineers Students - UC
Omar Mohamed	American Institute of Architects Students - UC

Discussion Items:

- The Project overview was provided for Phase I and Phase II limits of the BSB corridor
- The Aesthetic Committee has been reconvened with new members to review and provide feedback on the proposed Aesthetic treatments for these two projects phases
- Since the previous aesthetic meetings ODOT has developed an Aesthetic Design Checklist which will be followed for these projects
- An explanation was provided between Baseline and Enhanced aesthetic treatments
- A review of the Stage 1 Aesthetic Design Checklist was provided including pictures of similar treatments
- A review of the schedule was provided and committee members were asked to provide feedback by the end of July
- The following feedback was provided in the meeting:
 - Angie Strunc – The City has interest in translucent vandal fence panels for Ezzard Charles and treatments similar to Long St and Spring St bridges in Columbus

BURGESS & NIPLE

- Michael Schuster – Basically the aesthetic elements presented were clean and look fine except the vandal fence may need to be something other than chain link
- Stefan noted that there is not much room for landscaping areas in this corridor. If the City identifies locations for landscaping during plan development then ODOT will work with the City to incorporate those into the plans with the understanding that the City will provide maintenance of all landscaping.
- Jon told the committee members that he would send them the following information
 - The PowerPoint presentation
 - The Aesthetic Design Checklist
 - A link to ODOT's Aesthetic Design Guidelines
 - The list of the current Aesthetic Committee members

Attachments:

- Presentation
- Aesthetic Design Checklist
- Follow-Up Comments

BRENT SPENCE BRIDGE CORRIDOR
PHASE I AND PHASE II

AESTHETIC DESIGN
COMMITTEE
MEETING

June 28, 2022

1 | Brent Spence Bridge Corridor Phase I and Phase II



Good afternoon and thank you for attending today's Aesthetic Design Committee meeting. I am Jon Brunot with Burgess & Niple and I am the Project Manager for Phase II. With me today is

- Stefan Spinosa – the ODOT Project Manager for the BSB Corridor
- Charlie Rowe – the ODOT Project Manager for Phase II
- John Otis – the ODOT Project Manager for Phase I

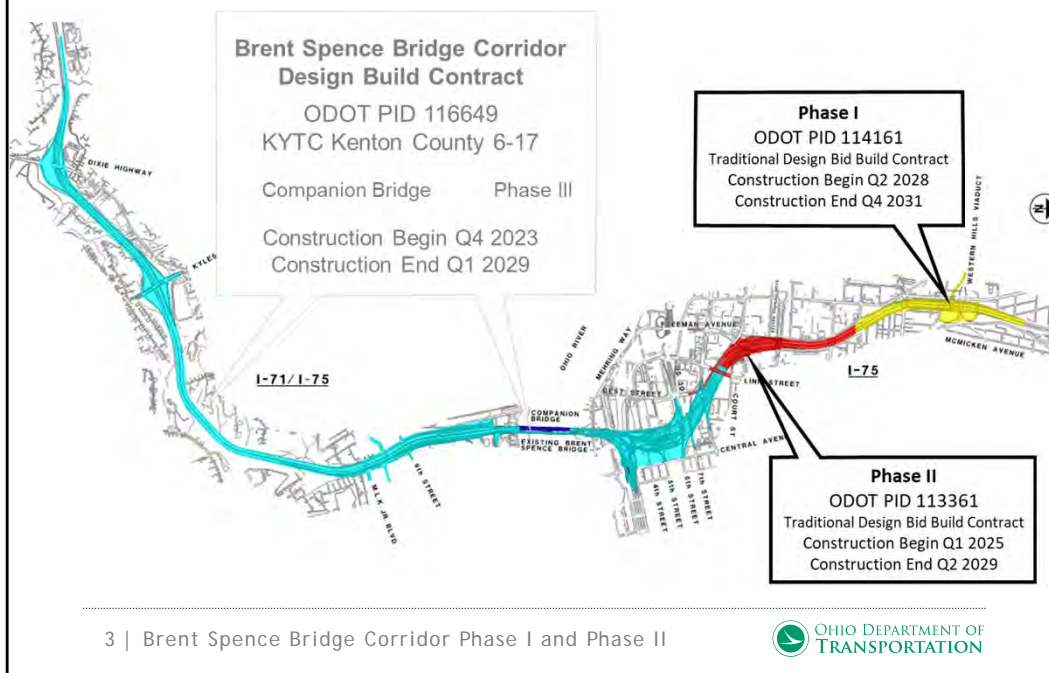
BRENT SPENCE BRIDGE CORRIDOR PHASE I AND PHASE II

AGENDA

- Project Overview
- Aesthetic Process
- Project Aesthetic Design Checklist Review
- Schedule
- Comments/Input

- Today we will:
 - Provide a Project Overview
 - Describe the Aesthetic Process
 - Review the Project Aesthetic Design Checklist
 - Review the Schedule
 - Provide time for your input and comments

PROJECT OVERVIEW - PHASE I AND PHASE II



- Describe three projects limits, schedule, timelines
- Today's focus is on the red and yellow sections of the project;
- There will be further outreach coordination with the City and Aesthetics Committee on the other areas of the project and the bridges over the Ohio River.

BRENT SPENCE BRIDGE CORRIDOR AESTHETIC COMMITTEE



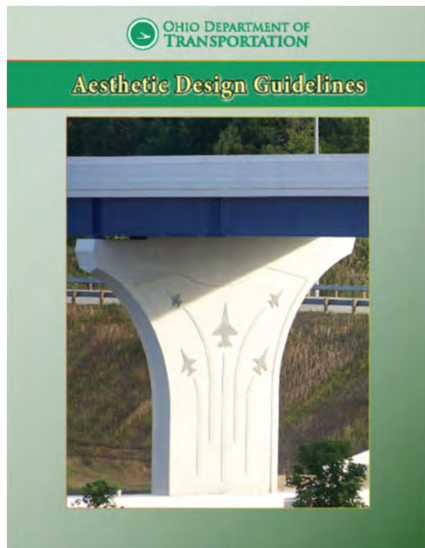
- 2005 Project Aesthetic Committee (PAC) instituted for the BSB Corridor project
- Assist KYTC and ODOT with development of Aesthetic Design Guidelines
- Provide input on aesthetic design elements used in the corridor

4 | Brent Spence Bridge Corridor Phase I and Phase II



- The Aesthetic Committee was formed to provide assistance on the project corridor's vision.
- An Aesthetic Charter was written that detailed the framework and process at the time for this project.
- During preliminary engineering the effort included input on the river crossing bridge type selection as well as general thoughts on the corridor as a whole.
- One of the commitments we made was that as we move in to further design and eventually to construction we would re-engage the committee to continue the earlier collaboration.
- We are at point in time where we are starting to include actual details for the portions of the project from Linn St. through the WHV interchange.
- Previous efforts by the committee identified similar focus on patterns, color, and texture elements similar to what has been constructed by other projects along I-75 in recent years.
- We've met with City staff to get ideas to refine the previous discussion and we are here today to share the current thoughts we have and to get feedback

ODOT AESTHETIC PROCESS



- Projects following ODOT's Aesthetic Design Process
- Baseline verses Enhanced Treatments
- Completion of Aesthetic Design Checklist

5 | Brent Spence Bridge Corridor Phase I and Phase II



- Since the completion of the Aesthetic effort as part of the Brent Spence project preliminary engineering, ODOT has developed and put into use an Aesthetic design process.
- This process mimics what the original project charter included,
- It also provides more direction on what is typically part of the project costs ODOT covers--baseline treatments; and how we will review, develop, implement, enhanced treatments.
- Baseline treatments are implementation of standard ODOT engineering and construction specifications. Typically consist of those pattern, color, and texture treatments.
- Enhanced treatments are usually incorporated into ODOT projects through stakeholder/public involvement efforts like we are doing for this project. Aesthetic Enhancements are implemented in addition to (or in place of) baseline treatment.
- I am going to turn the discussion over now to Jon Brunot. He will discuss the Aesthetic Design Checklist and how today's effort will be used to finalize it.
- Once completed the checklist will aid ODOT in Finalizing the design requirements for the northern section of the BSB corridor projects.

AESTHETIC DESIGN CHECKLIST

AESTHETIC DESIGN CHECKLIST
(to be completed by Design Team)

Preliminary Engineering (PE) Phase Environmental Engineering (EE) Phase Final Engineering (FE) Phase

Project Name: _____ ODOT File: _____
 ODOT CDS and PID: _____ Consultant File: _____
 District: _____ Local Agency File: _____
 County / Municipality: _____
 Project Description: _____
 Anticipated RFP Path: _____ Anticipated CE Level: _____

Is this project exempt from the aesthetic design process? (see Aesthetic Strategy CHAIRS) Yes No
 If "Yes", EISAC Completion of this item is not required. If "No", proceed to next section.

What types of aesthetic treatments are included in the plan? (check both boxes if applicable)
 Substantial flow via this treatment (include routing dates and reference applicable agency correspondence): _____ Retaining Structures

Is there a specific aesthetic theme or color plan that is being followed? Yes No
 If "Yes", reference color or theme plan or other document.

*** NOTE: The next section summarizes proposed aesthetic treatments for the project in some cases (preliminary stage 1 plans) or proposed aesthetic treatment may not be shown in this current plan but will be included in a future set.**

Bridge Treatments

Aesthetic Element	Proposed Treatment	Alternative Baseline	Alternative Bid Item	Use Plan (Specify Substantial Baseline or Enhancements)
Vertical Fencing	Baseline	Enhanced	NA	Yes
Retaining Walling	Baseline	Enhanced	NA	Yes
Diaphragm	Baseline	Enhanced	NA	Yes
Deck Fencing	Baseline	Enhanced	NA	Yes
Beam Orders	Baseline	Enhanced	NA	Yes
Accessories	Baseline	Enhanced	NA	Yes
Flat Curb	Baseline	Enhanced	NA	Yes
Plan Curb	Baseline	Enhanced	NA	Yes
Change/Support	Baseline	Enhanced	NA	Yes
Lighting	Baseline	Enhanced	NA	Yes
Other	Baseline	Enhanced	NA	Yes

Use Specific Proposed Alternative Baseline Treatments:
 Use Specific Proposed Alternative Bid Items:

Lighting Treatments

Aesthetic Element	Proposed Treatment	Alternative Baseline	Alternative Bid Item	Use Plan (Specify Substantial Baseline or Enhancements)
Highway Lighting	Baseline	Enhanced	NA	Yes
Decorative Lighting	Baseline	Enhanced	NA	Yes
Street Lighting	Baseline	Enhanced	NA	Yes
Bridge Lighting	Baseline	Enhanced	NA	Yes
Other	Baseline	Enhanced	NA	Yes

Use Specific Proposed Alternative Baseline Treatments:
 Use Specific Proposed Alternative Bid Items:

Traffic Signal Treatments

Aesthetic Element	Proposed Treatment	Alternative Baseline	Alternative Bid Item	Use Plan (Specify Substantial Baseline or Enhancements)
Flare and Arms	Baseline	Enhanced	NA	Yes
Signal Heads	Baseline	Enhanced	NA	Yes
Other	Baseline	Enhanced	NA	Yes

Use Specific Proposed Alternative Baseline Treatments:
 Use Specific Proposed Alternative Bid Items:

Retaining Wall, Noise Wall, and Longitudinal Barrier Treatments

Aesthetic Element	Proposed Treatment	Alternative Baseline	Alternative Bid Item	Use Plan (Specify Substantial Baseline or Enhancements)
Retaining Wall	Baseline	Enhanced	NA	Yes
Concrete Modular Barrier	Baseline	Enhanced	NA	Yes
Concrete Barrier Barrier	Baseline	Enhanced	NA	Yes
Guardrail	Baseline	Enhanced	NA	Yes
Noise Wall (Steel)	Baseline	Enhanced	NA	Yes
Noise Wall (Wood)	Baseline	Enhanced	NA	Yes
Noise Wall (Other)	Baseline	Enhanced	NA	Yes
Other	Baseline	Enhanced	NA	Yes

Use Specific Proposed Alternative Baseline Treatments:
 Use Specific Proposed Alternative Bid Items:

Landscaping Treatments

Aesthetic Element	Proposed Treatment	Alternative Baseline	Alternative Bid Item	Use Plan (Specify Substantial Baseline or Enhancements)
Plantings	Baseline	Enhanced	NA	Yes
Soil Erosion	Baseline	Enhanced	NA	Yes
Other	Baseline	Enhanced	NA	Yes

Use Specific Proposed Alternative Baseline Treatments:
 Use Specific Proposed Alternative Bid Items:

Signage Treatments

Aesthetic Element	Proposed Treatment	Alternative Baseline	Alternative Bid Item	Use Plan (Specify Substantial Baseline or Enhancements)
Highway Signage	Baseline	Enhanced	NA	Yes
Community Signage	Baseline	Enhanced	NA	Yes
Other	Baseline	Enhanced	NA	Yes

Use Specific Proposed Alternative Baseline Treatments:
 Use Specific Proposed Alternative Bid Items:

6 | Brent Spence Bridge Corridor Phase I and Phase II



- Thanks Stefan
- As Stefan mentioned we met with the City to developed an initial project Aesthetic Design Checklist that I will review with you today.
- In completing the checklist we focused on similar patterns, colors and textures already constructed along the I-75 corridor
- All the treatments selected in the initial aesthetic design checklist are either ODOT baseline or City standard baseline treatments
- Based on feedback and input we receive from you we will complete the final Aesthetic Design Checklist that will set the aesthetic elements to be included in the final design of these two Project Phases.
- The aesthetic design checklist treatments that I will present include
 - Bridge Treatments
 - Lighting treatments and
 - Retaining wall and noise walls

AESTHETIC DESIGN CHECKLIST REVIEW

Bridge Treatments

Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Vandal Fencing	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Pedestrian Railing	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Parapets	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Deck Fascia	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Beams/Girders	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Abutments	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Pier Caps	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> Yes	
Pier Columns	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> Yes	
Drainage/Scuppers	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Utilities	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	

List Specific Proposed Alternative Baseline Treatments: City Standard pedestrian railing
 List Specific Proposed Enhanced Treatments:
 List Specific Proposed Alternate Bid Items:

- Aesthetic Bridge Treatments include vandal fencing, parapets, beams, abutments and piers

AESTHETIC DESIGN CHECKLIST REVIEW

- Vandal Fence



8 | Brent Spence Bridge Corridor Phase I and Phase II



- The Vandal Fence proposed for the projects will consist of an ODOT standard Straight Black chain link Fence - 14' from the top of walk similar to what is pictured here and used in the corridor

AESTHETIC DESIGN CHECKLIST REVIEW

○ Parapets



9 | Brent Spence Bridge Corridor Phase I and Phase II



- The bridge parapets will use a Texas Classic Rail formliner with solid windows - a similar treatment used in the I-75 corridor
- The treatment will be on both sides of the parapet for bridges over I-75 with sidewalks
- And only on the exterior side of I-75 mainline bridges. The interior side will be a standard single slope concrete barrier

AESTHETIC DESIGN CHECKLIST REVIEW

- Parapets



10 | Brent Spence Bridge Corridor Phase I and Phase II



- The overhead bridge parapets will also include end treatments with the bridge identification name, construction completion date and rustification design features – The city will assist in determining the final design of the parapet end treatments

AESTHETIC DESIGN CHECKLIST REVIEW

- Beams & Girders



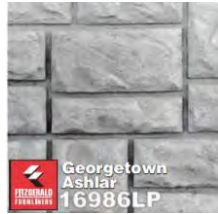
11 | Brent Spence Bridge Corridor Phase I and Phase II



- The project will include the use of both steel and concrete girder bridges
- Steel girders will be painted to match the color of the Hopple Street and Monmouth Street bridges (shown on the left)
- Concrete girders will include a Federal Standard concrete sealer color similar to what is shown on the right

AESTHETIC DESIGN CHECKLIST REVIEW

o Abutments/Piers



12 | Brent Spence Bridge Corridor Phase I and Phase II



- o Bridge abutments will include a concrete formliner with an Ashlar stone pattern similar to the texture shown below
- o Piers will be standard cap and column type piers with tapered end caps and round columns

AESTHETIC DESIGN CHECKLIST REVIEW



13 | Brent Spence Bridge Corridor Phase I and Phase II



- This is the recently completed Sheppard Lane bridge with similar proposed bridge treatments including
 - Abutment formliner
 - Cap and column piers
 - Texas Classic Railing
 - Straight Vandal Fence

AESTHETIC DESIGN CHECKLIST REVIEW

Lighting Treatments

Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Highway Lightning	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Interchange Lighting	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Street Lighting	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Bridge Lighting	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	

List Specific Proposed Alternative Baseline Treatments: City standard roadway steel tapered mast arm with LED fixture and bridge mounted straight steel pole post with LED fixture

List Specific Proposed Enhanced Treatments:

List Specific Proposed Alternate Bid Items:

- Lighting Aesthetic Elements include Highway and Interchange lighting and Local Street and Bridge Lighting

AESTHETIC DESIGN CHECKLIST REVIEW

- Highway Lighting



Median Lighting



Tower Lighting

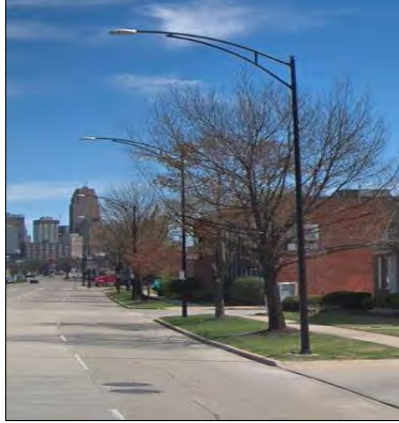
15 | Brent Spence Bridge Corridor Phase I and Phase II



- ODOT Standard highway median and tower lighting will be used matching the existing I-75 corridor

AESTHETIC DESIGN CHECKLIST REVIEW

- Street Lighting



Queensgate/West End Standard Lighting

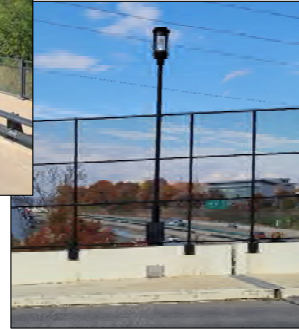
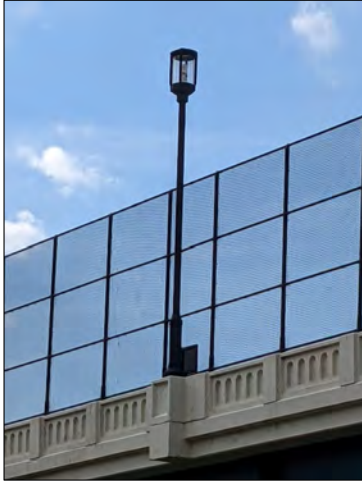
16 | Brent Spence Bridge Corridor Phase I and Phase II



- Local street lighting will match the existing Queensgate/West End Standard which is a black Steel Tapered Pole with a Curved Truss Mast Arm

AESTHETIC DESIGN CHECKLIST REVIEW

- Bridge Lighting



17 | Brent Spence Bridge Corridor Phase I and Phase II



- Overhead bridges will include a Straight Steel pole with Post top LED Luminaire matching the existing Hopple, Monmouth and West 8th Street bridges

AESTHETIC DESIGN CHECKLIST REVIEW

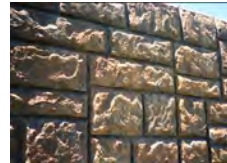
Retaining Wall, Noise Wall, and Longitudinal Barrier Treatments

Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Retaining Walls	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Concrete Shoulder Barrier	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Concrete Median Barrier	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Guardrail	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Noise Wall Panels	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Noise Wall Panel Caps	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Noise Wall Posts	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
List Specific Proposed Alternative Baseline Treatments:				
List Specific Proposed Enhanced Treatments:				

Retaining Walls and Noise Walls Aesthetic Treatments

AESTHETIC DESIGN CHECKLIST REVIEW

- Retaining Walls



19 | Brent Spence Bridge Corridor Phase I and Phase II



- Retaining walls will match the bridge abutments
Ashlar stone formliner and will include a concrete cap at the top of the wall
- Concrete wall caps will overhang the wall with chamfered top and bottom edges

AESTHETIC DESIGN CHECKLIST REVIEW

- Noise Walls



20 | Brent Spence Bridge Corridor Phase I and Phase II



- Noise walls will also use an Ashlar stone formliner with straight concrete top caps and concrete vertical posts
- Posts will extend 1" beyond the face of the panel and with chamfered side edges

SCHEDULE

- Phase I PID 1144161
 - Stage 1 Plans - 12/06/2022
 - Stage 2 Plans - 05/30/2024
 - Stage 3 Plans - 05/29/2025
 - Final Submittal - 12/24/2025
 - Sale Date - 10/01/2027
- Phase II PID 113361
 - Stage 1 Plans - 07/06/2022
 - Stage 2 Plans - 05/16/2023
 - Stage 3 Plans - 01/23/2024
 - Final Submittal - 06/18/2024
 - Sale Date - 01/01/2025

- Here are the proposed schedule commitment dates for each phase
- We are currently completing Stage 1 design plans
- Final submittal date is June of 2024 for Phase II and December of 2025 for Phase I with Sale dates in 2025 and 2027 respectively

COMMENTS

- At this time we would like to open it up to hear your feedback and comments or answer any questions you may have



AESTHETIC DESIGN CHECKLIST
(to be completed by Design Team)

<input checked="" type="checkbox"/> Preliminary Engineering (PE) Phase		<input type="checkbox"/> Environmental Engineering (EE) Phase		<input type="checkbox"/> Final Engineering (FE) Phase	
Project Name:	HAM IR 75 1.05 & HAM-75-1.95	ODOT PM:	Stefan Spinosa		
ODOT CRS and PID:	HAM-75-1.05, PID 113361 HAM-75-1.95, PID 114161	Consultant PM:	TBD		
District:	8	Local Agency PM:	Bryan Williams		
County / Municipality:	City of Cincinnati				
Project Description:	<p>HAM-75-1.05 PID 113361 is the middle portion in Ohio of the Brent Spence Bridge Corridor project. Work includes the following:</p> <ul style="list-style-type: none"> Reconstruct and widen I-75 from just north of the Linn St. overpass to the northern limits of the bridge over Findlay St. Replace the Linn St. overpass with 1-75 and reconstruct Gest Street from Freeman Avenue to US 50. The reconstruction of Gest Street will eliminate the roadway connection from Gest St. to Linn St. The pedestrian access from Gest to Linn St. shall be replaced. Replace the Ezzard Charles Drive overpass over I-75, reconstruct portions of Western Ave., cul-de-sac West Court St., and construct new I-75 ramps to and from Freeman Ave., Western Ave., and Ninth St. <p>HAM-75-1.95 PID 114161 is the northern end in Ohio of the Brent Spence Bridge Corridor project. Work includes the following:</p> <ul style="list-style-type: none"> Reconstruct and widen I-75 from Findlay to just south of Marshall Ave. Construct a new interchange on I-75 to connect to the new Western Hills Viaduct (WHV) 				
Anticipated PDP Path:	5	Anticipated CE Level:	EA/FONSI		

Is the project exempt from the aesthetic design process? (see Aesthetic Strategy Checklist)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If "Yes", STOP . Completion of this form is not required. If "No", proceed to next section.	

What types of aesthetic treatments are included in the plans? <i>Check both boxes if applicable.</i>	<input checked="" type="checkbox"/> Baseline <input checked="" type="checkbox"/> Enhanced
Summarize how was this determined (include meeting dates and reference applicable agency correspondence): Aesthetic treatments determined through ongoing coordination and meetings between consultant, ODOT, City of Cincinnati and the Aesthetic Committee	

Is there a specific aesthetic theme or corridor vision that is being followed?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
If "Yes", summarize what it is and how it was determined: Rivers and Hills; see Aesthetic Report completed under PID 75119.	

** NOTE: The next sections summarize proposed aesthetic treatments for the project. In some cases (particularly Stage 1 plans), a proposed aesthetic treatment may not be shown in the current plan set but will be included in a future set.*

Bridge Treatments

Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
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Vandal Fencing	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Pedestrian Railing	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Parapets	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Deck Fascia	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Beams/Girders	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Abutments	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Pier Caps	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> Yes	
Pier Columns	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> Yes	
Drainage/Scuppers	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Utilities	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
List Specific Proposed Alternative Baseline Treatments: City Standard pedestrian railing						
List Specific Proposed Enhanced Treatments:						
List Specific Proposed Alternate Bid Items:						

Lighting Treatments

Aesthetic Element	Proposed Treatment			Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Highway Lighting	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Interchange Lighting	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Street Lighting	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Bridge Lighting	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
List Specific Proposed Alternative Baseline Treatments: City standard roadway steel tapered mast arm with LED fixture and bridge mounted straight steel pole post with LED fixture						
List Specific Proposed Enhanced Treatments:						
List Specific Proposed Alternate Bid Items:						

Traffic Signal Treatments

Aesthetic Element	Proposed Treatment			Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Poles/Mast Arms	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Signal Heads	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
List Specific Proposed Alternative Baseline Treatments: City standard mast arms and signal heads						
List Specific Proposed Enhanced Treatments:						
List Specific Proposed Alternate Bid Items:						

Retaining Wall, Noise Wall, and Longitudinal Barrier Treatments

Aesthetic Element	Proposed Treatment			Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Retaining Walls	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Concrete Shoulder Barrier	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Concrete Median Barrier	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Guardrail	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Noise Wall Panels	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Noise Wall Panel Caps	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Noise Wall Posts	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
List Specific Proposed Alternative Baseline Treatments:						
List Specific Proposed Enhanced Treatments:						
List Specific Proposed Alternate Bid Items:						

Landscaping Treatments



Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Woody Plantings	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Seed Mixes	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
List Specific Proposed Alternative Baseline Treatments: List Specific Proposed Enhanced Treatments: List Specific Proposed Alternate Bid Items:				

Signage Treatments

Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Highway Signage	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Community Signage	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
List Specific Proposed Alternative Baseline Treatments: Community signage to meet City standards List Specific Proposed Enhanced Treatments: List Specific Proposed Alternate Bid Items:				

Roadway/Sidewalk Treatments

Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Right-of-Way Fencing	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Sidewalks	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Buffers (Tree Lawns)	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Utilities	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Crosswalks	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Medians	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Islands	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
List Specific Proposed Alternative Baseline Treatments: 5' wide x 5" thick concrete sidewalks with 4'-5" wide tree lawn meeting City standards. Crosswalks, medians and islands for local streets to meet City standards List Specific Proposed Enhanced Treatments: List Specific Proposed Alternate Bid Items:				

Special Treatments*

Aesthetic Element	Proposed Treatment*	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Planters	<input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Benches/Tables	<input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Trash Receptacles	<input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Rest Rooms/Shelters	<input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Kiosks/Monuments	<input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Interpretive Signage	<input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Decorative Wall	<input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Logos/Lettering	<input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
List Specific Proposed Alternative Baseline Treatments: List Specific Proposed Enhanced Treatments: List Specific Proposed Alternate Bid Items:				

* All special treatments are considered "enhancements".

General

<input type="checkbox"/>	Construction drawings are to scale and large enough to adequately depict aesthetic treatments.
--------------------------	--



<input type="checkbox"/>	Drawings adequately show or describe surface treatments (texture and color), transitions/connections between various elements with details that indicate textural patterns and relief depths.
<input type="checkbox"/>	The plans have been reviewed for potential proprietary restrictions (if federal funds are being used).
<input type="checkbox"/>	The plans have been reviewed for potential bid issues (such as local specs that conflict with ODOT specs, and poorly-defined or conflicting standards of acceptance). <i>Describe any potential issues here:</i>
<input type="checkbox"/>	Funding guidelines/alternate bid considerations for aesthetic enhancements have been discussed with stakeholders. Maintenance requirements for enhanced aesthetic treatments have been discussed with stakeholders.
<input type="checkbox"/>	Potential long-term maintenance issues and life-cycle costs have been discussed with stakeholders.
<input type="checkbox"/>	<i>Describe any potential issues here:</i>
<input type="checkbox"/>	Participation Agreement(s) completed/included.
<input type="checkbox"/>	Aesthetic Funding Assessment Form completed/included. <i>List any agreements or outstanding issues that still need to be secured/resolved:</i>
<i>Sign and date below and include this form, the Aesthetic Funding Assessment Form, and any Participation Agreements with the design plan review submittal.</i>	

Stage 1 (PE Phase)			Stage 2 (EE Phase)		Stage 3 (FE Phase)	
Design Team	Signature	Date	Signature	Date	Signature	Date
Consultant PM:						
ODOT PM:						

City of Cincinnati
Department of Transportation and Engineering (DOTE)
Follow-Up Aesthetic Design Committee Comments

Cincinnati DOTE offers the following comments:

1. *The City of Cincinnati intends to apply for federal grant funding through the BIL - Reconnecting Communities Pilot (RCP) grant opportunity for enhancements along the BSB corridor. In particular, the application will be for enhancements to city streets crossing I-75 throughout the corridor, both above and below the mainline. The grant type will be for capital construction.*
2. *Above mainline street enhancements may include bridge treatment of translucent panels in lieu of vandal fencing, planters or tree boxes, benches, and artistic expression on the parapets. Post top lighting should be moved to the curb, not on the outer edge of the bridge with these enhancements. These enhancements may be similar to Columbus' Long St. and Spring St. city streets crossing I-71.*
3. *Above mainline city streets are:*
 - a. *Linn St. (PID 113361)*
 - b. *Ezzard Charles (PID 113361)*
 - c. *9th St. / 8th St. Viaduct (PID 116649)*
 - d. *7th St. & 6th St. (PID 116649)*
4. *For below mainline streets, ODOT to provide Cincinnati DOTE with proposed baseline features including proposed sidewalk width, location of pier columns, abutment/pier wall locations, proposed under bridge lighting and any adjacent fence locations for further review and comment. The city anticipates preferred treatments such as continuing the typical road and walk section, street lighting, under bridge lighting and vertical walls instead of sloped embankments. (PID 113361)*
5. *Below mainline city streets are:*
 - a. *Liberty St. (PID 114161)*
 - b. *Findlay St., Bank St., Central Ave/Harrison Ave (PID 114161)*
 - c. *3rd St. (PID 116649)*

**Brent Spence Bridge Corridor Project
Meeting Summary - Ft. Wright/Ft. Mitchell Subcommittee
August 10, 2022**



Groundbreaking by Design.

MEETING MINUTES

Project: Brent Spence
Purpose: Project Urban Aesthetics
Place: Kenton Co. Court's Conference Room
Meeting Date: August 10, 2022
Prepared By: David Reed
Attendees: Dave Hatter Mayor - Ft. Wright
Jill Bailey Ft. Wright Administrator
Jude Hehman Mayor - Ft. Mitchell
Kris Knochelmann Kenton Co. Judge Executive
Sharma Lee Kenton Co.
Nick Hendrix Kenton Co. Public Works
Scott Gunning Kenton Co. Administrator
Laura Tinfelde Kenton Co. Planning and Development Services
Gary Valentine KYTC – CO
Stacee Hans KYTC – D6
Glen Kelly Qk4
David Reed Qk4

1. Introductions/Project History

- a. Introductions were made. Human Nature, Inc's. team members will be assisting Qk4 in the visioning process, but were not in attendance.
- b. A history summary was provided by Stacee. Alternative I is what was originally selected. A Mega Project Grant, programmed for \$2.7 billion – has not been awarded yet, but is anticipated to be successful to allow the project to move forward. Grant awards will be made in October and December of 2022. This project is in competition for funding along with five or six other projects in the country and it is speculated that each project may receive some level of funding from the grant process.

2. Current Design Details and Impacts

- a. Main revision of Alternative I is within the bridge itself, where local traffic was shifted to existing bridge, and through traffic shifted to new bridge. Issues with incident management (emergency services response) is being studied and may incorporate movable barrier gates in the median wall.
- b. The group was most interested in the location where decision making for local/through and I-71/I-75 will be critical – and specifically, the impacts to the Dixie Highway and Kyle's Lane interchanges. These two interchanges will be linked by a collector/distributor corridor and separated from the higher speed through lanes. The introduction of this new pattern will be augmented with enhanced signage.

- c. Interstate signage and concerns regarding nomenclature available for local businesses for each ramp was also discussed, in addition to new project lighting and the impact it will have on residents/businesses. Lighting of the interstate will be fixed and dictated by interstate standards, but transition zones beyond the interstate will need to be coordinated with municipal partners.

3. Aesthetic Guidelines

- a. Efforts have begun to explore aesthetic guidelines which could include “gateway” type improvements at each city’s exit ramps. Judge Knochelmann requested the vision along the entire I-71/I-75 corridor be coordinated and cohesive – making it clear that you have arrived to Kentucky.

4. Gateway Opportunities

- a. Representatives of Ft. Wright shared their desire to create gateway treatments at the Kyles Lane interchange ramps ramps and emphasized how important it is for these areas to be aesthetically pleasing. They also identified that the city is willing to pay for some of the improvements. Ft. Mitchell echoed the same level of interest and commitment. The consulting team supports these improvements and their inclusion in the project, and would like to begin by reviewing draft gateway improvement plans available.
- b. In addition to gateway and signage ideas, the consulting team is also interested in pedestrian fencing for plan inclusion. Human Nature will be assisting in this effort. Since structures and roadway are being reconstructed, costs will be handled by the project, with commitments from municipalities to maintain landscape and streetscape elements. Evendale, OH is an example of preferred landscape aesthetics.
- c. Areas where duplicate fencing exists along the interstate will be examined to offer more practical patterns of right-of-way boundary control and maintenance opportunities by the municipalities.

5. Sound Barriers

- a. Noise studies are ongoing to verify they offer a reduction in predicted noise levels and the optimum locations to construct. KYTC is committed to being a better neighbor and will construct these as community enhancements regardless of the cost effectiveness if they reduce predicted noise and the community desires.
- b. If an existing noise wall is impacted by construction, the noise wall will be reconstructed.

6. Right of Way Acquisition

- a. Right of Way work has begun and has been reduced in scope through value engineering efforts. Affected property owners south of 12th Street have been contacted with the current focus on appraisals, with offers made to five or six property owners thus far. One home is in Ft. Wright, which will be acquired and demolished as part of this project.

7. Drainage Issues

- a. The group noted drainage issues within the outside bend of the interstate (northbound east side) where skid abrader surfacing has been installed. The group prefers this pavement treatment be maintained in the future with final pavement installation/restoration.

8. Traffic Issues

- a. Beechwood School was discussed, but will not likely be able to be addressed within the scope of the project.
- b. Concerns were raised about crumbling pavements, medians etc. on Kyles Lane. These concerns will be addressed within the project limits.
- c. Wright's Summit Pkwy is currently a right in only from Kyles Lane, and there is a preference is for a right in/right out option to improve traffic flow within the Wright Summit Properties, including the development of the three current vacant parcels fronting the interchange. This is currently being evaluated by KYTC. Development is at an impasse until this option is resolved.
- d. Maintenance of traffic and phasing will determine the limits of buildable units and whether the two interchanges will be constructed in tandem, or separately.
- e. The construction of the project is expected to be from November 2023 to the end of 2029 and will be developed in multiple phases.

9. Action Items and Next Steps

- a. Both Ft. Wright and Ft. Mitchell would like to have access to some information and graphic materials to help educate interested residents, such as any available boards and graphics which could be provided to each community so residents could view them at municipal buildings, or on the KYTC website.
- b. It was requested that each City appoint one representative to attend meetings to keep conversations efficient and productive. The Mayor indicated that at this time Jill Bailey would be that person for the city.

End of Meeting Notes

**Brent Spence Bridge Corridor Project
Meeting Summary - Covington Subcommittee
August 10, 2022**



MEETING MINUTES

Groundbreaking by Design.

Project:	Brent Spence	
Purpose:	Urban Aesthetics/Guidelines	
Place:	KYTC – District 6	
Meeting Date:	August 10, 2022	
Prepared By:	David Reed	
Attendees:	Mayor Joseph Meyer	City of Covington
	Ben Oldiges	Advisory Committee Member
	Emily Wolff	Advisory Committee Member
	Sarah Allen	Advisory Committee Member (representing Susan Smith)
	Steven Hill	Advisory Committee member
	Diana Martin	RL Record LLC
	Rick Record	RL Record LLC
	Nicole DiNovo	Human Nature
	Gary Wolnitzek	Human Nature
	Gary Valentine	KYTC - CO
	Stacey Hans	KYTC – D6
	Glen Kelly	Qk4
	Lindsay Hoskins	Qk4
	David Reed	Qk4

1. Introductions

David opened by introducing new team members (Lindsay, Gary, and Nicole), followed by room introductions, and Gary Valentine provided a brief introduction and recap of progress to date.

2. Information Exchange Materials

- a) The group reviewed information that was exchanged during the month of July; seeking direct input on preferred design parameters for impacted city streets and expand underpass areas.
- b) The three (3) page list of Advisory Committee considerations (compiled by Rick Record) was cross-referenced to the list of agenda items with the plan for discussion to touch on most items listed.
- c) Review of Example Aesthetic Guidelines
 - These represent a good sampling of other project examples, although each a little different
 - Louisville Bridges (Aesthetics and Landscape)
 - I-69 (2nd Street) Henderson - very prescriptive down to types of materials used, including landscape plant material and pavement types and scoring patterns.
 - The group will work together collaboratively to develop a similar set of guidelines, specific to this project, and tailored to the needs of the city.

- Further evaluation and comment was requested: What is the Committee's opinion of these documents and this format; is it effective? What did you like or not like? What would you like to see included in this effort?
 - (1) KY 351 (69 project) was intended to be a gateway into downtown Henderson, matched type of lights city already had in place; and a variety of pedestrian crossing options, details and examples of street furnishings were provided.
 - (2) Louisville's document focused more on architectural elements; it was more broad, had different areas/zones; and a specific landscape document was developed to explore the use of native plant material and plant communities.

- d) Review of City Master Plan Documents
 - The group reviewed these three documents as valuable to the project and Aesthetic Guidelines. All documents provide a good background/formula for our use including two specific examples.
 - (1) Pike Street example provides solutions for narrow street and sidewalk corridors.
 - (2) 3rd street recommendations overlap a portion of this project (between Main and Crescent).

We would like to confirm that these are still the ideas that you're interested in pursuing, or would you like for them to be modified in some specific way?
 - Bike Trails
 - (1) The group understands that bicycle mobility is a strong goal of the city and the guidelines will incorporate opportunities for bike paths to be included and the bike trail network expanded.
 - (2) The group looked at route options transposed from City's plan, which has a lot of east/west movement. The group examined ramps and connections with local streets, and the opportunity to connect bike traffic with the River Front Park. The group also discussed options of where and how to introduce bike and pedestrian crossings under the interstate and to the riverfront (on grade or elevated options).
 - (a) 9th Street is currently the safest place to bike from Devou Park due to limited vehicular conflicts.
 - (b) An Art Park is being created on the river side of flood wall at C.W. Bailey Bridge, where preference is to go through levee gates instead of over the levee. A street artist will be developing floodwall murals and the area will be developed into an event space. This Economic Development initiative - inspired by graffiti and display urban art, not permanent art – will be supplemented by food trucks and festival activity.
 - (c) 3rd Street connection is less dangerous due to limited vehicular conflicts. Still need walkability improvements for 4th and 5th Streets.
 - (d) The IRS site is focusing on walkability and is a pedestrian friendly plan; 3rd Street will be re-established from Madison Avenue to Johnson.
 - (e) Does 3rd or 4th have enough width to add tree wells? Group will investigate.
 - (f) 4th Street will have to be reconstructed completely with traffic calming measures, and complete street features.
 - (g) Gateway opportunities were discussed, where work is ongoing at 4th and Main.
 - (3) New patterns for I-71/I-75 were discussed, with the overall goal of separating through traffic (express system) and local traffic trying to exit/enter the system (collector-distributor systems) networks.
 - (a) Existing bridge will only serve local system.
 - (b) New bridge will serve through traffic.
 - (c) Local restaurants are concerned with how that will impact them – for instance, how will drivers be informed to get off at 12th to get on local system?
 - (d) Collector/Distributor system needs to be attractive, free flowing,
 - (e) Concern for noise increase due to expanded interstate footprint and mass.
 - (f) KYTC has hired UK to do research for quiet pavement; FHWA doesn't recognize pavement as a form of mitigation, but it is being investigated.

- Goebel Park
 - (1) Master Plan identified a number of features and amenities. The park will be impacted and may drive the need for significant improvements to be considered. The group agreed this is an opportunity to explore improvements recommended from this study and find new use patterns.
 - (2) Roadway designers are exploring options to lower Jillians/Simon Kenton Way down to surface level to allow the reconstructed combined sewer to be located under the surface street.
 - (3) The group recommended the development of a new conceptual masterplan for Goebel Park, to allow implementation of initial improvements and a coordinated plan for the city to implement future improvements.

3. Current Design, Disturb Limits and Impacts

a) Design Overview

- Sampled cross-section illustrations provide examples of how landscaping can be provided along both sides, but ultimately these buffer plantings are subject to design preferences of the advisory group.
- Impacted Street Corridors
 - (1) The group would like to extend 3rd, 4th and 5th Street streetscape recommendations east to Main Street.
 - (2) The streetscape recommendations should examine capacity, movement, conflict points with bikes/pedestrians, and not just aesthetics.
 - (3) 12th Street is already connected and improved; although, it would be good to explore bike lanes on both north and south sides.
 - (4) Pike Street should be extended to Main if possible and extended west to the St. Johns School crossing.
 - (5) 9th Street treatments should explore bike connectivity.
- Tree Canopy Impacts
 - (1) Current city canopy guidelines were reviewed, and more detailed information will be provided on restoration areas/numbers.
 - (2) The group noted hillside stabilization concerns from clearing all the trees at Goebels Park. (Gary Valentine will follow-up with Geotechnical team members.)
 - (3) Solutions in this hillside area should acknowledge the new townhomes constructed along Crescent.

4. Preferred Design Parameters

a) Existing and Proposed Corridor Dimensions

- Concern for proximity of the new Jillians Corridor in relation to residences and pool.
- The design of underpass areas is critically important – what does the “Greenbook” offer in the way of design guidelines?
 - (1) Vehicular and pedestrian lighting will be important within underpass areas; lighting direction and temperature are important too.
- All surface cross-street intersections will likely be signalized and actuated. There is concern for children in the areas of all street crossings.

b) Underpass Conditions and Preferences

- Parking within the underpass area between Pike and 12th Street is in high demand and should remain.
- The group was encouraged to think of how this environment will be different than typical streetscape - decorative pavement opportunity for murals on abutment walls, decorative fencing between columns were all discussed.
- The group is interested in the potential for park space in underpass zones. (Sawyer Point Park is an example).
 - (1) City would need to agree to maintain/police area; and the group was concerned if this area would be an attractive area for homeless.

- (2) Areas of higher, wider overpasses may be suitable for picnic areas, bike stations or other similar recreation functions.

5. Next Meeting and Preferred Areas of Focus

- a) The designers will develop more detailed plan view concepts and renderings of impacted corridors, typical sections for each of those corridors, rendered in graphical format, including 3-dimensional images of proposed underpass areas.
- b) The September meeting could be moved to the First Financial Bank (6th and Madison) where the 2nd floor is available for public use (Stacey to coordinate with Mayor's office).
- c) If there are any comments following the meeting, please forward those along to Stacey and Gary.

End of Meeting Notes

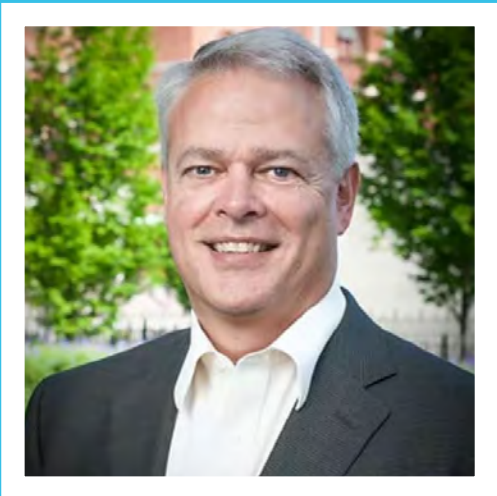
COVINGTON STREETSCAPE & PUBLIC REALM DESIGN GUIDELINES

BRENT SPENCE
BRIDGE CORRIDOR





Lindsay Hoskins
Qk4



Gary Wolnitzek
Human Nature

INTRODUCTION OF NEW TEAM MEMBERS

COVINGTON STREETSCAPE &
PUBLIC REALM DESIGN GUIDELINES

BRENT SPENCE
BRIDGE CORRIDOR



Landscape Design Guidelines and Concepts
for the **Kennedy Interchange**
Louisville, Kentucky

August 2006



**OHIO RIVER
CROSSING**
HENDERSON: SECTION 1

I-69 ORX KY 351 Streetscape Improvements
I-69 Ohio River Crossing Project – Section 1
Henderson, KY

REVIEW OF EXAMPLE AESTHETIC GUIDELINES

Aesthetic Design Guidelines

The Louisville-Southern Indiana
Ohio River Bridges Project





Section 1-Kennedy Interchange

Prepared for
Kentucky Transportation Cabinet

Prepared by
Rosales Gottemoeller and Associates
+ Kentucky Transportation Associates

COVINGTON STREETSCAPE &
PUBLIC REALM DESIGN GUIDELINES

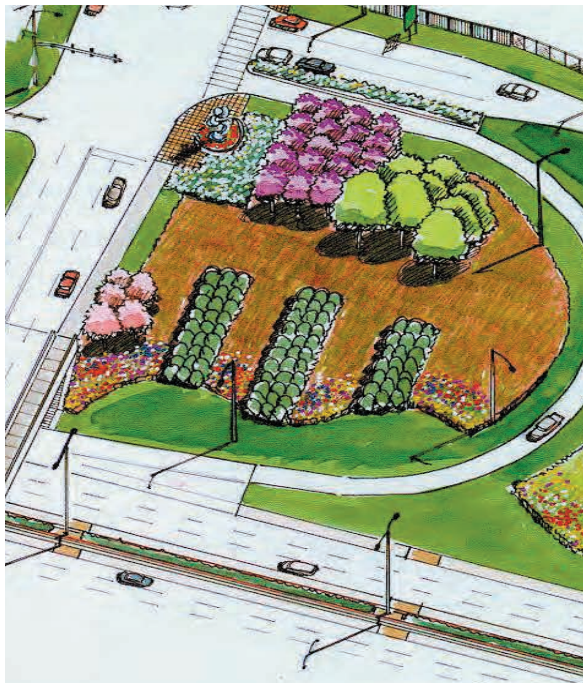
BRENT SPENCE
BRIDGE CORRIDOR



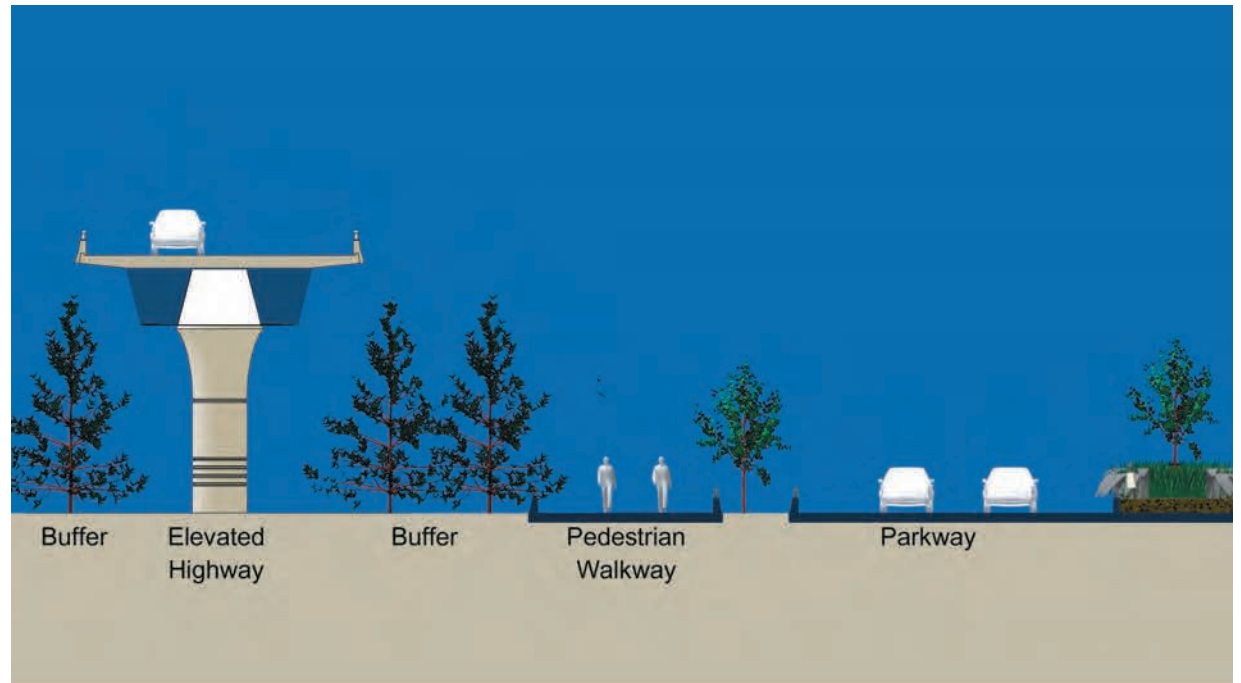
LOUISVILLE BRIDGES - AESTHETICS AND LANDSCAPE



A.4- Extension of Park into Interchange Zone



A.5- Example of Landscape Treatments



A.6- Example of Circulation and Buffer Zones

LOUISVILLE BRIDGES - AESTHETICS AND LANDSCAPE

Landscape Design Guidelines and Concepts for the Kennedy Interchange Louisville, Kentucky

August 2006



I-69 - 2ND STREET

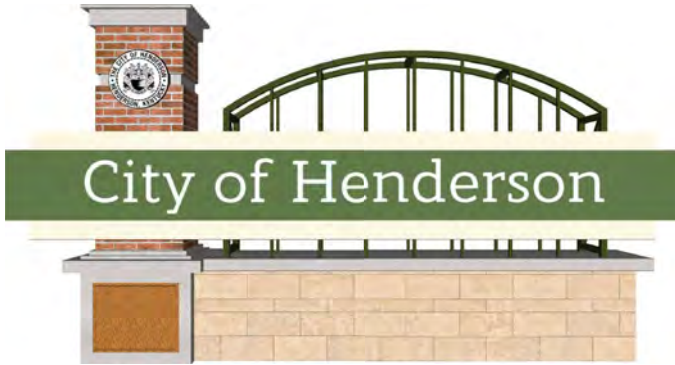


City Gateway Signage - Plan View

Type Family: Modum Regular

Brick	Powdercoated Alum. (R-99 G.121 B.75)
Limestone	Powdercoated Alum. (R-57 G.66 B.19)
Limestone Veneer	Powdercoated Alum. (R-211 G.205 B.189)

Materials, Colors & Finishes



City Gateway Signage - Front View



City Gateway Signage - Side Views

Aesthetic Design Guidelines

The Louisville-Southern Indiana Ohio River Bridges Project



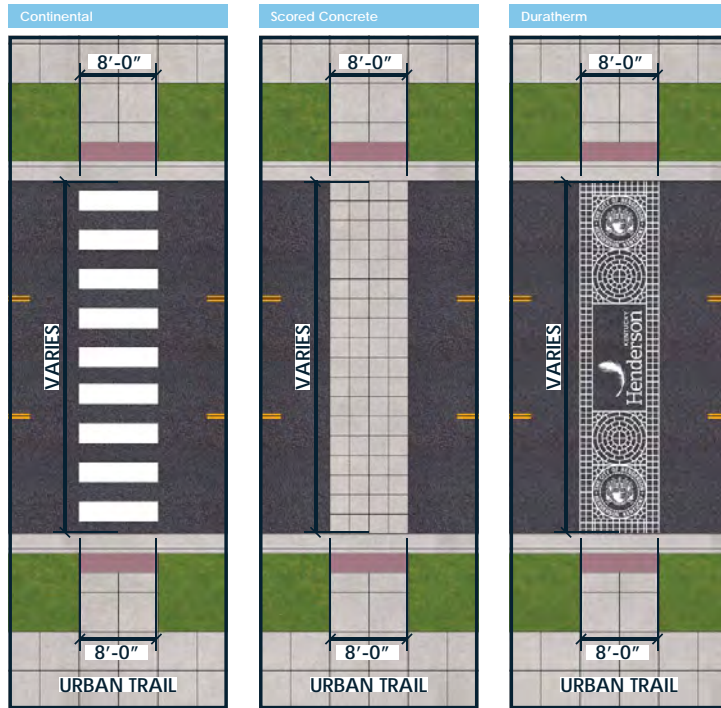



Section 1-Kennedy Interchange

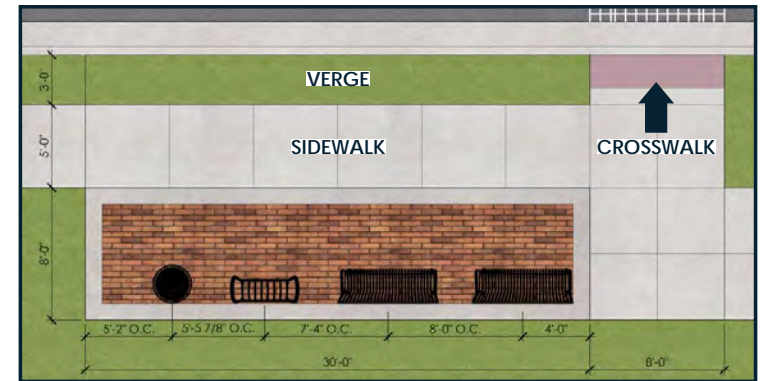
Prepared for
Kentucky Transportation Cabinet

Prepared by
Rosales Gottemoeller and Associates
+ Kentucky Transportation Associates

February 21, 2006



Crosswalk Typologies - Plan View



Pedestrian Node - Plan View



Pedestrian Node - Perspective View



REVIEW OF CITY MASTER PLAN DOCUMENTS

COVINGTON STREETScape &
PUBLIC REALM DESIGN GUIDELINES

BRENT SPENCE
BRIDGE CORRIDOR

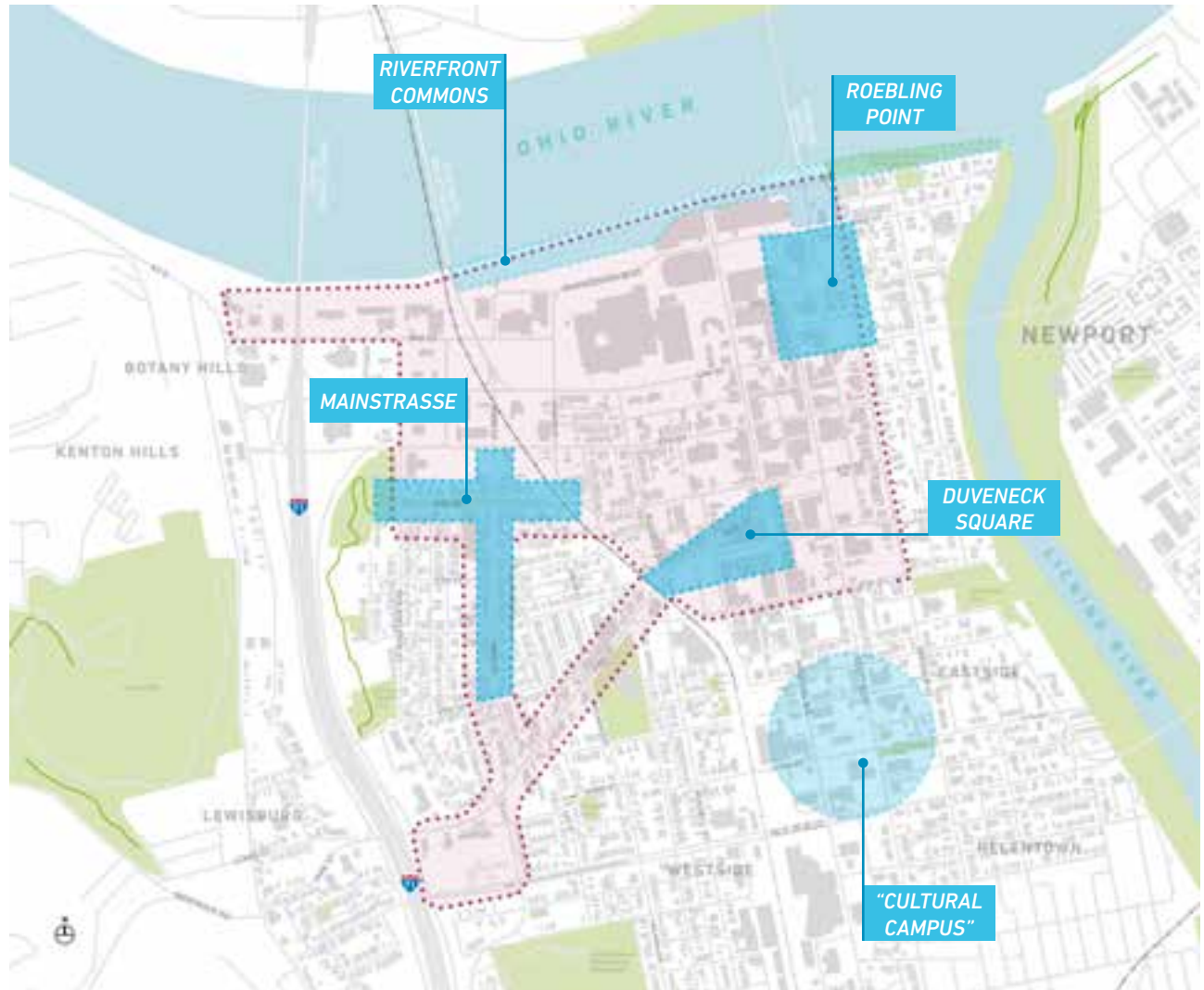


PURPOSE AND INTENT

The recommendations of this plan are guided by the knowledge and opinions of key stakeholders and city staff from Economic Development, Engineering, Public Works, Historic Preservation, Community Development and Urban Forestry.

The design guidelines contained in Section 3 are intended to achieve the following objectives:

- Build upon the findings and recommendations of prior planning initiatives
- Develop a more cohesive, aesthetically-pleasing, and vibrant downtown streetscape.
- Address the poor condition of sidewalks and lack of compliance with the Americans with Disabilities Act (ADA)
- Develop a more “Complete” street network with a stronger balance between the needs of motorists, transit riders, cyclists and pedestrians.
- Ensure that the unique identity of and sense of place within the Pike Street Corridor, MainStrasse, Roebling Point and Duveneck Square districts are celebrated and maintained within the adopted guidelines.
- Mitigate the impacts of utility infrastructure and include strategies and standards that allow for the incremental deployments of smart technologies such as fiber-optic and WIFI distribution, intelligent street lighting and connected roadway and traffic signal technologies.
- Minimize uncertainty around expectations for the replacement and development of sidewalk and streetscape improvements and bolster significant private-sector investments in Covington’s Historic downtown area



INTRODUCTION

This section includes design standards for each of the following categories of the streetscape design including:

Standard Streetscape Elements: The section begins by outlining the City's accepted standards for streetscape design elements such as paving, furnishings, lighting and street trees.

Geometric Layout: Street-specific standards are then categorized by major street corridors (such as Main and Madison) or grouped together based on similar form and function (6th and 8th Streets). Each section includes an existing section diagram of current conditions on the street, followed by a prototypical section of what future improvements should look like.

Sidewalk Standards: Design standards for the public realm along each street include specific dimensions and functions for each component within the public realm. A perspective illustration calls out dimensional standards and an accompanying plan view portrays prototypical patterning and placement of streetscape elements.

Material Palette: Specific materials, furnishings and planting treatments are then listed for each component. Materials that are listed represent the standard for aesthetics and performance. Other materials may be substituted, but they will be required to meet the performance standards of the guidelines listed herein and will be subject to City approval.



VIBRANT STREETScape ACTIVITY ALONG MAIN STREET IN MAINSTRASSE

CITY-WIDE DESIGN STANDARDS

MATERIAL PALETTE



1. Concrete Sidewalk

Standard concrete; light buff color
Light to medium broom finish,
perpendicular to traffic flow Saw-cut
joints, no edge marks



2. Container Planters

Round or square fiberglass planters,
black finish
30" diameter (or width) minimum size,
low-profile
Style to match existing planters along
Madison Avenue
Locations to be prioritized near
intersection plazas or key pedestrian
areas where budget allows.
Alternate: Size & style may vary
per district character and agreed
maintenance responsibilities amongst
adjacent proprietors.



3. Benches

Transitional-style backed steel slat
bench with intermediate armrests
6' length, black gloss finish.
Mount to pavement per manufacturer's
recommendations
Exception:
Maintain existing historical steel
benches (if present), such as on 6th
Street in George Steiner Park.



4. Trash/ Recycling Receptacles

City standard steel receptacles, pair
trash and recycling where demand
requires and service is available.
Locate at corner intersection plazas
where demand requires, maintain
clear pedestrian through-ways in all
instances.
Black gloss finish.



5. Decorative Street Light

Pedestrian scale decorative street light
Duke Energy Deluxe Acorn style
luminare
LED 50 watt fixture
12' Fluted tapered steel or aluminum
pole, black automotive finish
Locate 2' from face of curb.
Exception:
Current Madison Avenue standard is to
be replaced over time on a per-block
basis with the above.



6. Street Tree Well

Upright deciduous tree, see appendix
for approved species.
Locate within sidewalk bump-outs or
in amenity zone when sidewalk width
meets or exceeds 8'.
Install perennial and ground-cover
underplantings.
Alternate: Cast iron tree grates where
minimum 4'-0" pedestrian clear zone
necessitates use. Center hole must be
capable of expansion as tree growth
requires.

STREET TREES & LANDSCAPE DESIGN STANDARDS



7. Future Street Tree Plantings

A healthy urban forest is an integral
component of an appealing
streetscape environment. In addition to
ecological benefits, a canopy of
trees contributes to the comfort,
beauty and walkability of the urban
environment and consequently yields
tangible social and economic benefits.
While there is tremendous potential
to increase the amount of green
space along Covington streets and
sidewalks, it is important to note
that not all streets have sidewalk
spaces which are wide enough to
accommodate street tree plantings.

In these areas the best opportunity to
introduce street trees is in front yards,
screens and buffers associated with
parking areas and private properties.
Future plantings should only be
located where space is sufficient
and should promote continuity with
existing plantings that have not
exceeded their useful lifespan. Trees
should be located to avoid conflicts
with overhead utilities and obstructed
views to and from buildings. All trees
to be planted within the public right-
of-way shall be approved by the City of
Covington Urban Forester.



8. Increased Soil Volume for Tree Health

Trees in pavements typically are confined
to small areas of soil often lacking in
water, nutrients, oxygen and adequate
room for proper root growth. Soils
under sidewalks are highly compacted
to meet engineering standards required
to support pavements; therefore, trees
in this environment live a stunted and
shortened life, generally living only 7-10
years. With better soil conditions, life
expectancy can be greatly increased to
upwards of 60 years. CU-Structural Soil
and Silva Cells are two options that both
support pavements and encourage deep
root growth. The investment in soil for a

healthy tree is paid back by fulfilling the
functions for which it was planted, which
may include shade, noise reduction,
pollution reduction, wildlife habitat and
the creation of civic identity.
Application: Structural soils have been
successfully employed for many years
and are easily integrated into standard
tree well applications. Silva cells are a
newer, more expensive technology that
shows greater promise for long-term
tree health and development and can
be utilized in prioritized locations such
as festival streets where larger planting
areas can occur.



9. Bioretention Planters

Rain Gardens, Bio-retention Cells
and Storm Water Planters utilize a
series of landscaped or turf covered
catchment areas designed to capture,
cool, cleanse and infiltrate stormwater
runoff from urban streets. These
systems are an effective method of
integrating landscaping and
stormwater management into the
urban area. During rainfall events,
stormwater runoff is directed into the
catchment area, is allowed to collect,
and then infiltrate into the soil. With
intense rainfall events, the remaining
excess water will either flow back to

the street gutter, entering the next
downstream catchment in the series,
or can be diverted to underground
storage chambers. After traveling
throughout the entire series, any
remaining stormwater is directed to
the storm sewer system or directed
into swales or stream channels.
Application: Generous Sidewalk widths
along Madison, Main, Seventh and
potentially Scott & Greenup Streets
provide excellent potential for the
integration of various types of bio-
retention and catchment areas.

PIKE STREET OVERVIEW

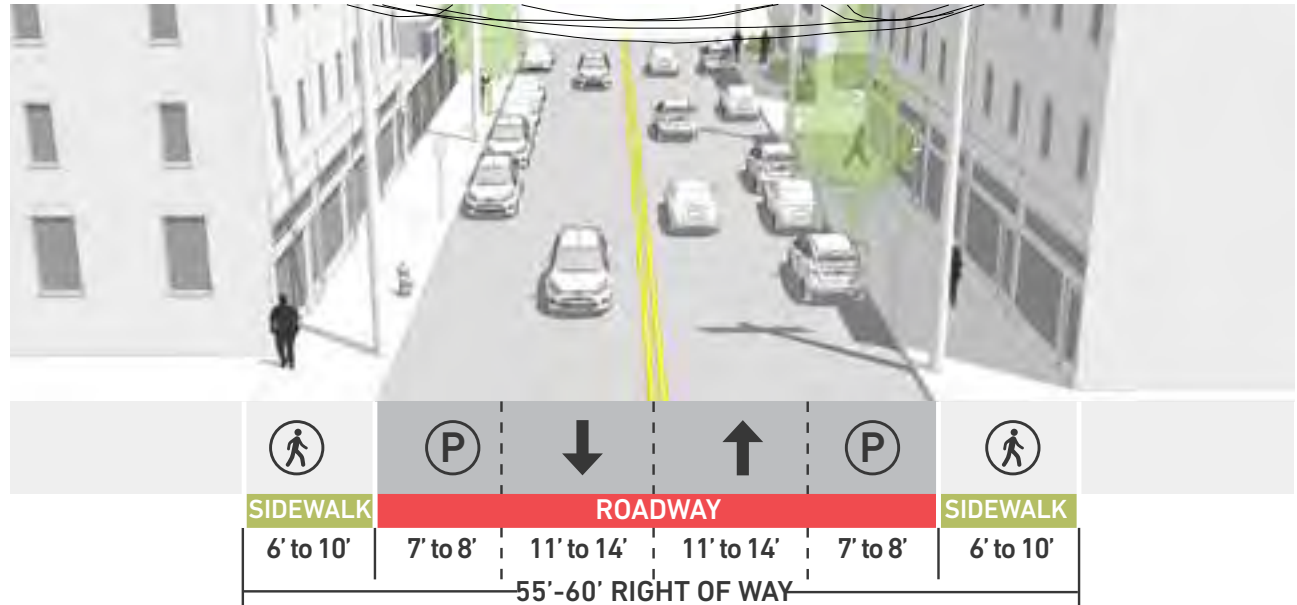
Pike Street features widely varied right-of-way and streetscape configuration extending from I-75 to Madison Avenue. Electric utility service crossings create a cluttered appearance to the street to a much greater extent than distribution poles along the curb. Incremental sidewalk replacement projects should include utility conduits that support the phased elimination of electric service crossings and address non-compliant sidewalk cross-slopes. Future lighting should employ post-top LED fixtures in place of existing cobra-head lighting. Future tree plantings should utilize tree wells with adequate soil volume to support the development of a healthy tree and more substantial canopy. Future improvements in bicycle mobility could include the establishment of sharrow lanes.

Note: Lane configuration and sidewalk widths may vary slightly from the sections seen at right. These sections are meant to be typical.

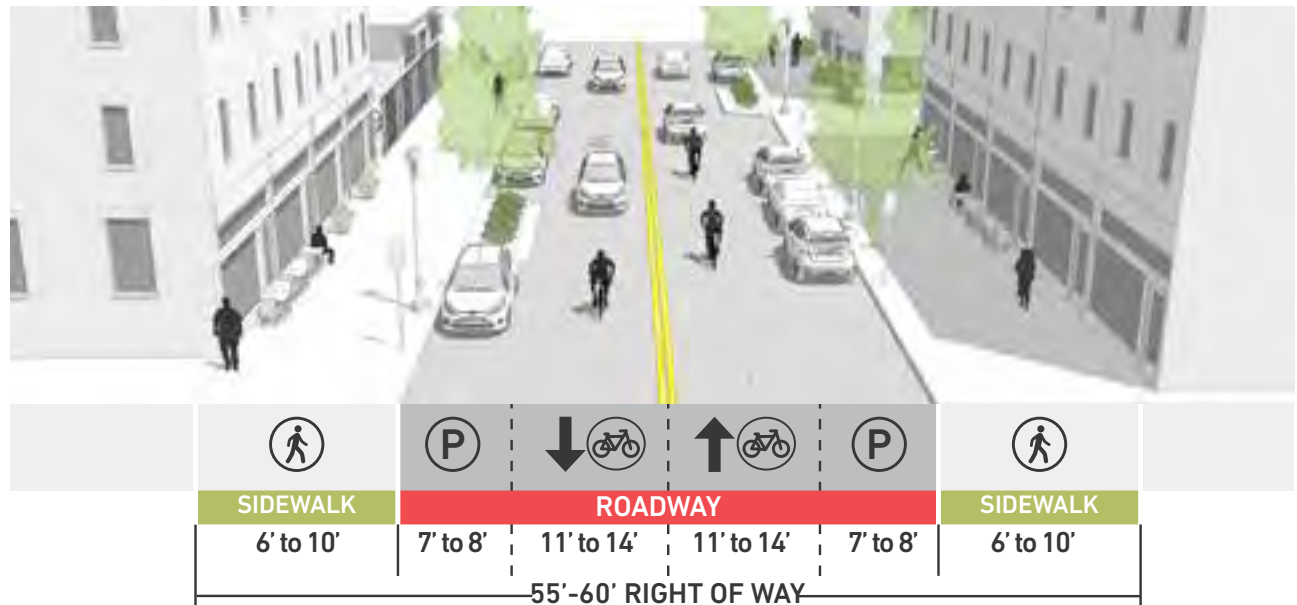


PIKE STREET

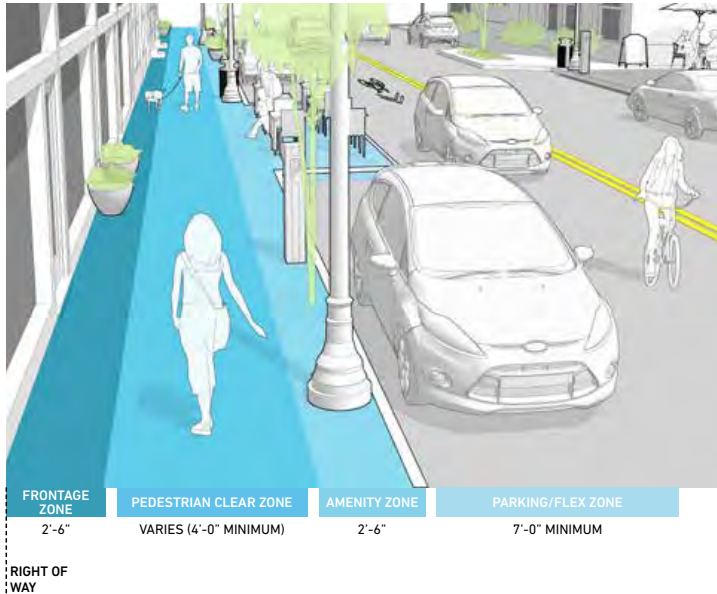
EXISTING SECTION



POTENTIAL SECTION



SIDEWALK STANDARDS



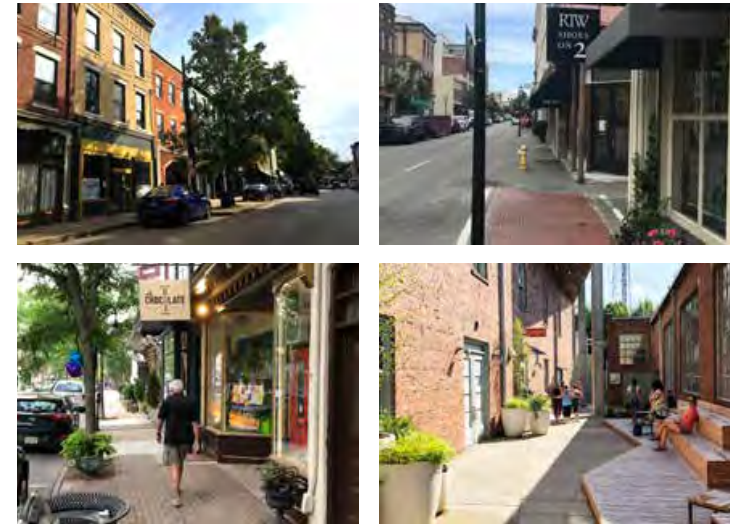
TYPICAL PLAN

The proposed design standard establishes fixed dimensions for the sidewalk frontage and amenity zones which will provide consistency along the widely varying spaces between the back of curb and storefront sidewalk. Bump-outs should be strategically placed to support retail and dining venues and provide space for any proposed street tree plantings and/or placemaking elements.

The character of site furnishings and placemaking elements should reflect the art-focused "bohemian" aesthetic of the corridor.

1. Decorative Post Lamp
2. Cafe Tables
3. Street Tree Well at Bump-Out
4. Bump-out
5. Container Planters
6. Concrete Sidewalk

MATERIAL PALETTE



Standard Street Elements:

For the following street elements and their use on Pike Street refer to the city standards:

- Concrete Sidewalk
- Trash / Recycling Receptacles
- Street Tree Well
- Decorative Street Light
- Container Planters
- Wayfinding Elements

1. Street Character:

Pike Street's historic architecture and unique building geometries provide a great opportunity to promote diversity of materials and a more eclectic, art-centric appearance.

Although sidewalk conditions vary, ample room exists for a vibrant and inviting public realm through attractive storefront signing and displays, container planters, sandwich boards, outdoor seating and tree plantings at bump-outs or select locations.

FRONTAGE ZONE

WIDTH - 2'-6"

PURPOSE - BUILDING ENTRY

FURNISHINGS & AMENITIES - ENTRY STEPS, CONTAINER PLANTINGS, 2-TOP SEATING, SANDWICH BOARDS

PEDESTRIAN CLEAR ZONE

WIDTH - VARIES (4'-0" MINIMUM)

PURPOSE - PRIMARY ACCESSIBLE PATHWAY

AMENITY ZONE

WIDTH - 2'-6"

PURPOSE - PLACEMENT OF TYPICAL STREETScape INFRASTRUCTURE

FURNISHINGS & AMENITIES - LIGHT POLES, WAYFINDING & SIGNAGE, PARKING METERS, UTILITIES

STREET TREES - TREE WELL

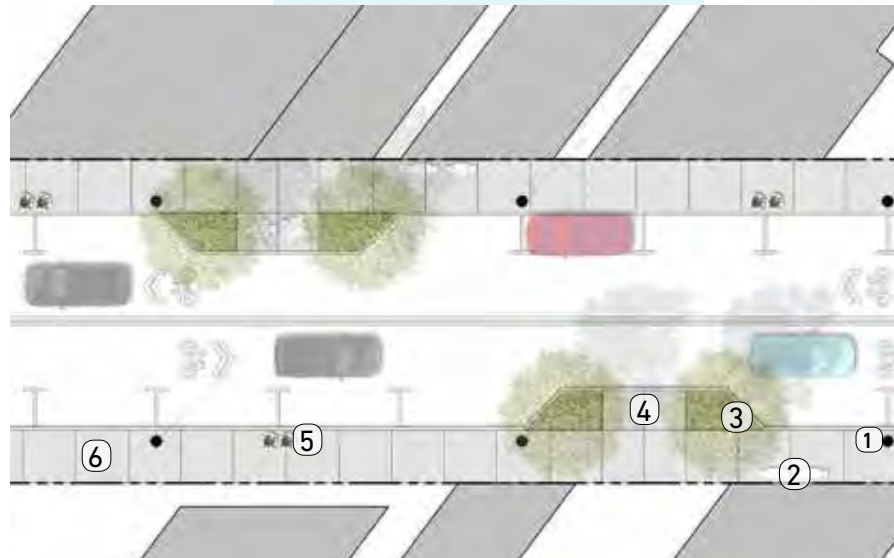
PARKING / FLEX ZONE

WIDTH - 7'-0" MINIMUM

PURPOSE - ON-STREET PARKING OR CURB EXTENSIONS

FURNISHINGS & AMENITIES - OUTDOOR DINING, VALET PICK-UP/ DROP-OFF

STREET TREES - TREE WELL

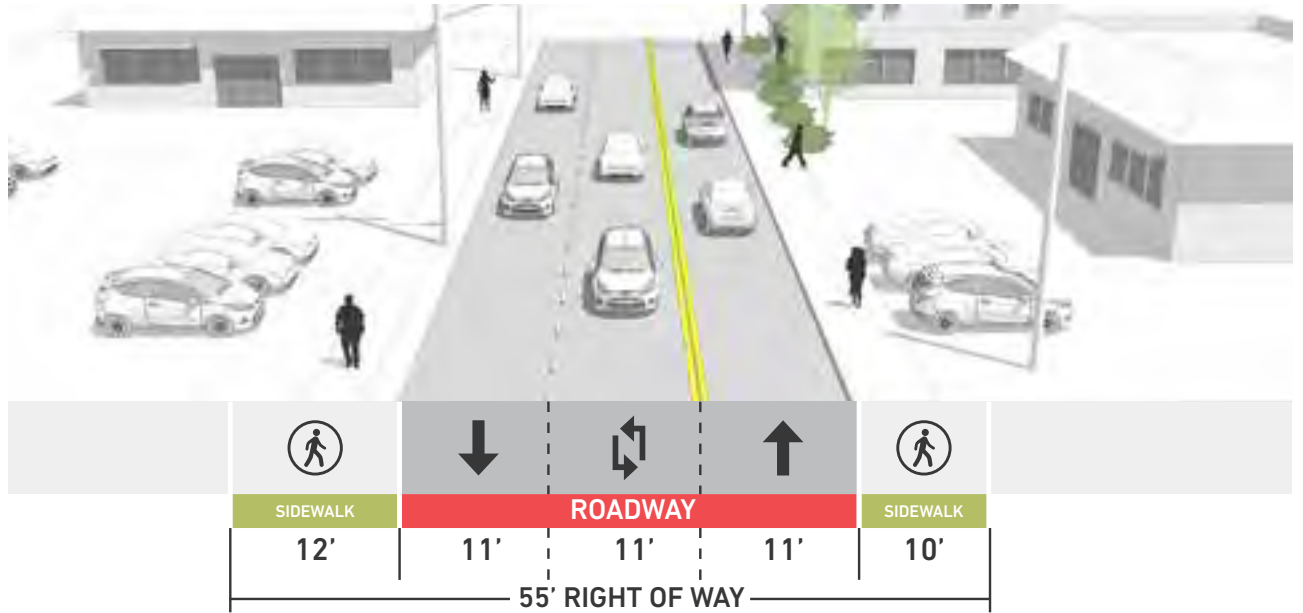


WEST 3RD STREET OVERVIEW

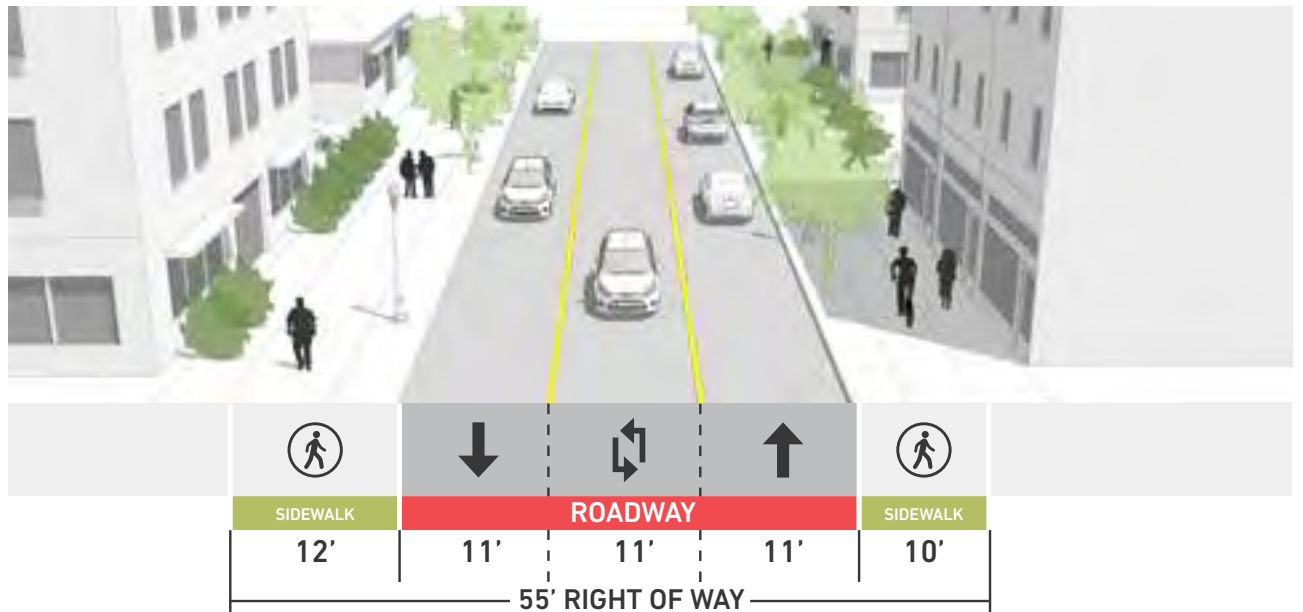
Future improvements to 3rd Street should follow the recommendations of the 2015 meet NKY plan. Future improvements should include parking area buffers, street trees and post-mounted street lighting in order to create a more pedestrian-focused, walkable environment between the City's Riverfront Hotels and Convention Center Area.

Note: Lane configuration and sidewalk widths may vary slightly from the sections seen at right. These sections are meant to be typical.

EXISTING SECTION

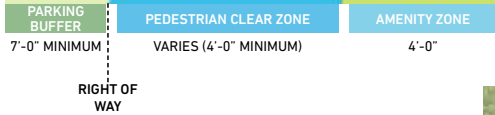
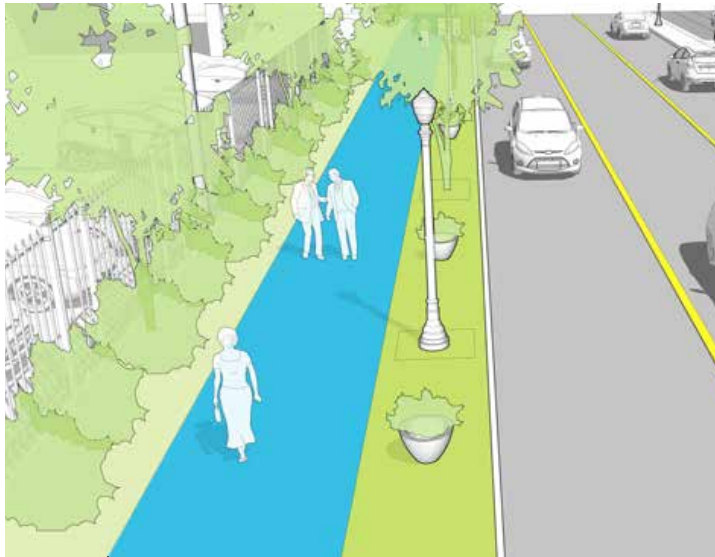


POTENTIAL SECTION



3RD STREET

SIDEWALK STANDARDS



PEDESTRIAN CLEAR ZONE

WIDTH - VARIES (4'-0" MINIMUM)

PURPOSE - BUILDING ENTRY, PRIMARY ACCESSIBLE PATHWAY

AMENITY ZONE

WIDTH - 4'-0"

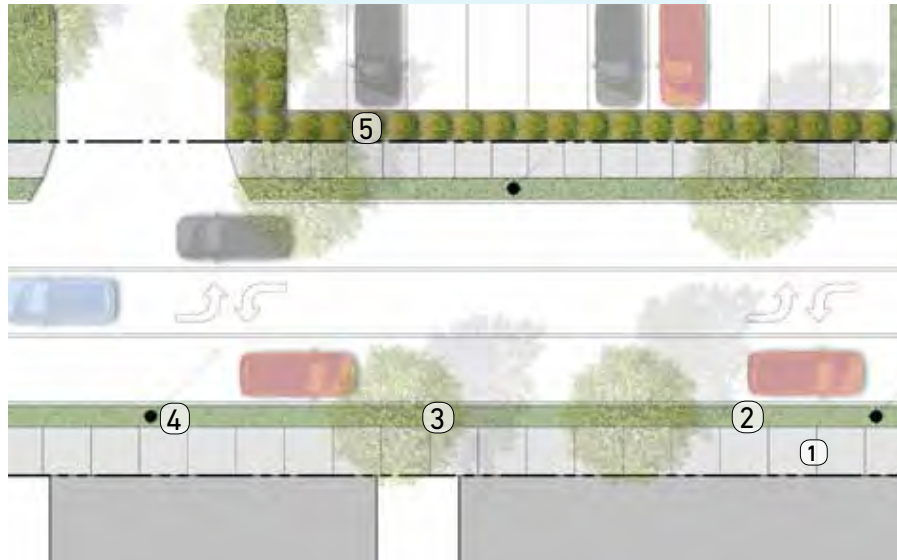
PURPOSE - PLACEMENT OF STREETSCAPE ELEMENTS
FURNISHINGS & AMENITIES - LIGHT POLES, WAYFINDING & SIGNAGE
STREET TREES - TREE WELL

TYPICAL PLAN

The proposed design standard establishes fixed dimensions for the parking area buffers and amenity zones which can provide consistency along the widely varying sidewalk widths moving east to west.

Landscaping and street tree planting standards should be established at sufficient density to mitigate the impacts of current parking and industrial properties and support the transition of the street's tenancing over time.

1. Concrete Sidewalk
2. Tree Lawn
3. Street Tree
4. Decorative Street Light
5. Landscape Buffer



MATERIAL PALETTE

Standard Street Elements:

For the following street elements and their use on West 3rd Street refer to the city standards:

- Concrete Sidewalk
- Trash / Recycling Receptacles
- Decorative Street Light
- Container Planters
- Wayfinding Elements



1. Low Density Streetscape

A more suburban style of development with a mixture of buildings and parking lots should provide a continuous tree lawn, pedestrian scale lighting and wayfinding elements.



2. Vehicular Use Area Buffer:

Parking lots and vehicular use area buffers should include evergreen and deciduous plantings that reduce the visual impact of parked cars and provides seasonal interest. See the city's vehicular use area perimeter landscaping, screening and fencing standards, 7'-0" minimum width.

BICYCLE MOBILITY

Over the past decade the level of interest around bike mobility has shown significant growth and now represents a multitude of groups across the Northern Kentucky/ Greater Cincinnati region. Planned and ongoing efforts include Riverfront Commons across the six Northern Kentucky river city communities, the Licking River Greenway Trail, CROWN (Cincinnati Riding or Walking Network), RedBike and Ride the Cov to name a few.

Options for improvements to bicycle mobility in downtown Covington are limited by the widely varied right-of-way and street widths which exist throughout the study area street network. Recognizing that there is not an opportunity for the widening of streets, a combination of safe & proven design solutions will need to be employed to provide continuous and inter-connected bike routes.

The map at right identifies bicycle-compatible opportunities across the study area street network. Utilizing data from traffic counts, crash/ accident reports, existing lane configuration and right-of-way measurements, the highlighted routes outline the primary streets on which potential bicycle infrastructure could occur. Precedence was given to routes that connect the three major districts within the downtown study area as well as connections to potential destinations, adjacent communities and existing/ proposed trail networks.

It is important to recognize that the formal designation of bike lanes carries with it the understanding that the responsible agencies encourage and support the designation in promoting safe and accessible passage.

More work and discussion with City of Covington Staff and Kentucky Transportation Cabinet (KyTC) must be done during subsequent engineering phases to determine the most appropriate applications for downtown.





SHARROW LANES



DESIGNATED BIKE LANE



BUFFERED BIKE LANE



MULTI-PURPOSE TRAIL

Shared-use trails such as the Indianapolis Cultural Trail (above) are designed for pedestrians and cyclists alike and can drive economic activity along the corridor as they attract a diversity of users.

POTENTIAL BIKE FACILITIES

Sharrows Lanes

Sharrows are short-hand for “shared lane pavement markings” to indicate that motorists and cyclists share the same travel lane. Sharrows are accepted practices for higher volume streets where dedicated bike lanes cannot be used because of demands for on-street parking or the number of travel lanes. Based on analysis of Covington’s street network these could potentially be proposed on the following streets: Main, Madison, 6th, 8th, Russell, Washington, Bakewell, Johnson and Pike.

One-way Bike Lanes

While One-way bike lanes are not currently in use in other parts of the city they may provide a viable means of creating a dedicated bike lane that could connect the northern and southern areas of the downtown core. Based on limitations of pavement width, right-of-way, and a current desire to maintain the existing number of travel lanes, Scott and Greenup are strong candidates for one-way, dedicated bike lanes.

Designated Bike Lanes

This type of lane relies on roadway markings to demonstrate the space allocated for a bicyclist. A six-foot wide lane is most desirable, but three-feet is an acceptable minimum width. Designated lanes have been shown to increase cyclist comfort and serve as a visual cue to drivers to be on the lookout for cyclists.

Sheltered (Buffered) Bike Lanes

In Sheltered lanes bicyclists are segregated from the vehicular carriageway by a median or other grade-separating device. The model has been used extensively in Europe where it has been successful at promoting bicycle commuting among novice cyclists. More space, typically eight feet, is needed to implement this type of lane, meaning significant changes would need to be planned and accommodated for if their use were to be considered in Covington.

GOEBEL PARK COMPLEX OVERVIEW



FEATURES & AMENITIES

- **Two Playgrounds**
 - **Goebel Park:** Two Large Structures (Separate Age 2-5 and Age 5-12), Swings (5 Belt, 1 Adaptive, 2 Toddler), Spring Rider, Seesaw
 - **Sergeant First Class Jason Bishop Memorial Park:** Small Play Structure (Age 2-5), Ladder Climber
- **Basketball Courts (2)**
- **Picnic Shelters**
- **Gazebo**
- **Swimming Pool**
- **Walking Path (Paved) - 0.8 Miles**
- **Carroll Chimes Clock Tower**
- **Goebel Goats**
(Used for Ground Maintenance)
- **Pollinator Garden**
- **Storage Building**
- **Grill**
- **Monuments and Dedication Plaques**
- **Open Space**
- **Benches**
- **Picnic Tables**
- **Bike Racks**
- **Portable Toilets**
- **Trash Receptacles**
(Some Decorative)
- **Parking Lots**



PARK ISSUES IDENTIFIED THROUGH SITE ASSESSMENTS AND PUBLIC INPUT

PARK IMPROVEMENT RECOMMENDATIONS

Lack of park and wayfinding signage

Provide wayfinding and trailhead signage at multiple locations to assist visitors in location features; Develop interpretive signage throughout the site (will require a signage plan)

Limited accessibility (ADA) & Lack of walkways; Paved access to park amenities is needed; Missing railings on stairs throughout complex

Add paved access to facilities, including ADA walkways/ramps and stairs

Age and condition of playground equipment, limited play value and not accessible at Sergeant First Class Jason Bishop Memorial Park

Replace playground at SFC Jason Bishop Memorial/Kenny Shields side and include age 2-5 and 5-12 equipment; Improve Clock Tower to make it functional again

Condition of shelters and nearby asphalt slab at Kenny Shields Park - potentially unsafe; Graffiti along wall

Demolish and replace the shelter on the Kenny Shields side in a more visible location; Repave all parking lots and pave the gravel lots (Kenny Shields); Renovate existing shelters near the main playground and add a restroom to promote increased park use for planned events, extended visits

Condition of basketball courts at Kenny Shields Park

Renovate or replace basketball courts (Kenny Shields)

Obsolete and deteriorating swimming pool; limited or deteriorating recreational options

Consider a sprayground as a long-term replacement of the pool if it becomes unsustainable to operate; Improve existing park trail; Add outdoor fitness equipment; Seek a location for pickleball courts to meet the growing need for these facilities, especially for those age 50+; Develop a neighborhood feature based on the preferences of the nearby residents as determined through neighborhood outreach; Add a food truck pad and support infrastructure for program concessions and revenue; Consider adding permanent outdoor games (Foosball, Ping Pong, Corn Hole) in a central gathering area near seating and the future food truck pad

Condition of drinking fountain

Add support amenities including drinking fountains with bottle fillers, Wi-Fi hotspots, trees, landscaping, site furnishings, and entrance signage (multiple locations)

Strong desire for a dog park and a disc golf course

Develop a dog park to meet the strong demand for this type of facility in this portion of the city; Add a disc golf course throughout the site, utilizing underutilized areas while avoiding conflict with the other users

Requests for improved security

Add site/security lighting and cameras

DESIGN OVERVIEW

Sampled Cross-Section Illustrations

IMPACTED STREET CORRIDORS

3rd, 4th, 5th, 9th, Pike, 12th,
Jullians, Bullock

CURRENT DESIGN, DISTURB LIMITS AND IMPACTS

TREE CANOPY IMPACTS

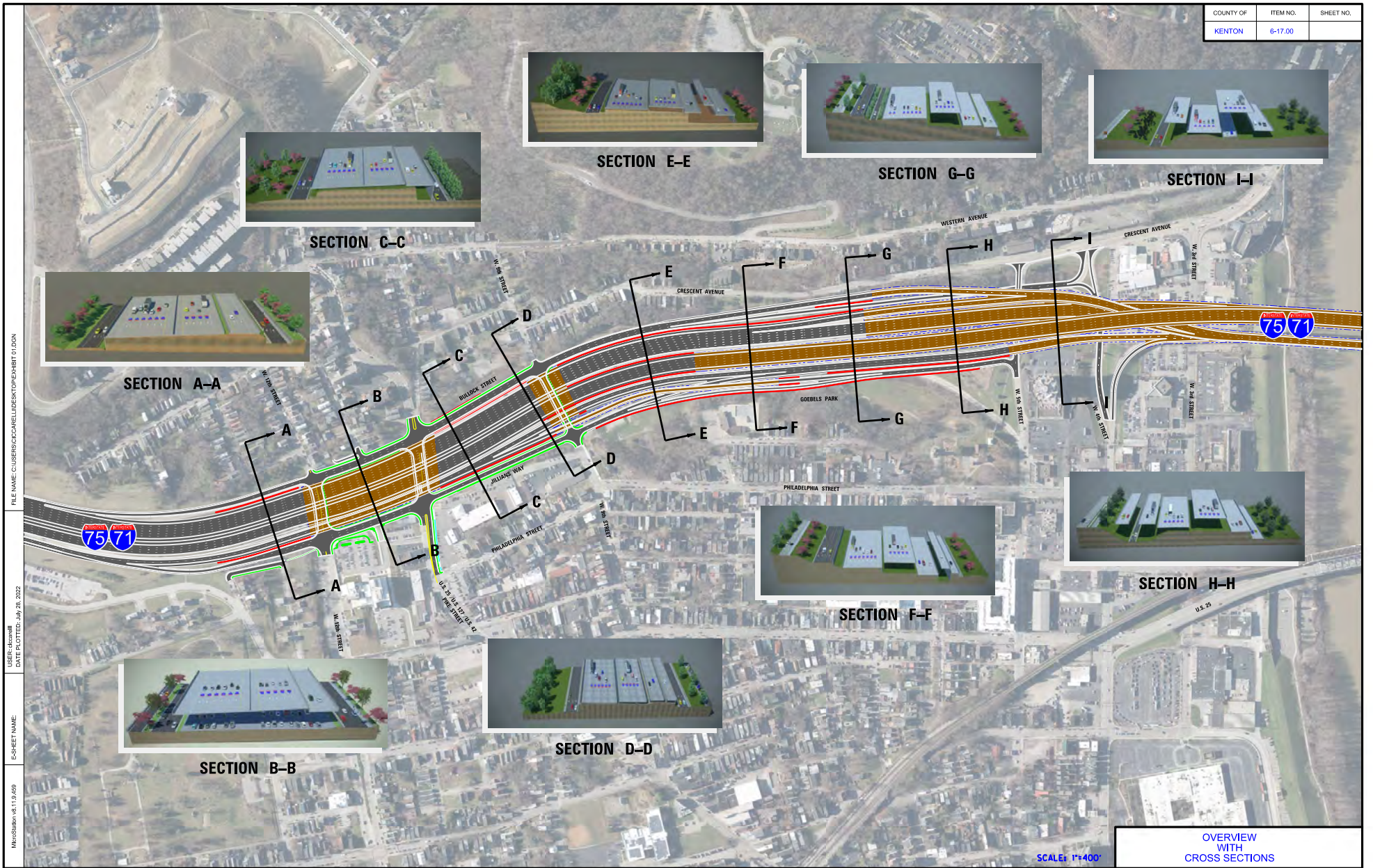
COVINGTON STREETScape &
PUBLIC REALM DESIGN GUIDELINES

BRENT SPENCE
BRIDGE CORRIDOR



OVERVIEW WITH CROSS SECTIONS

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KENTON	6-17.00	



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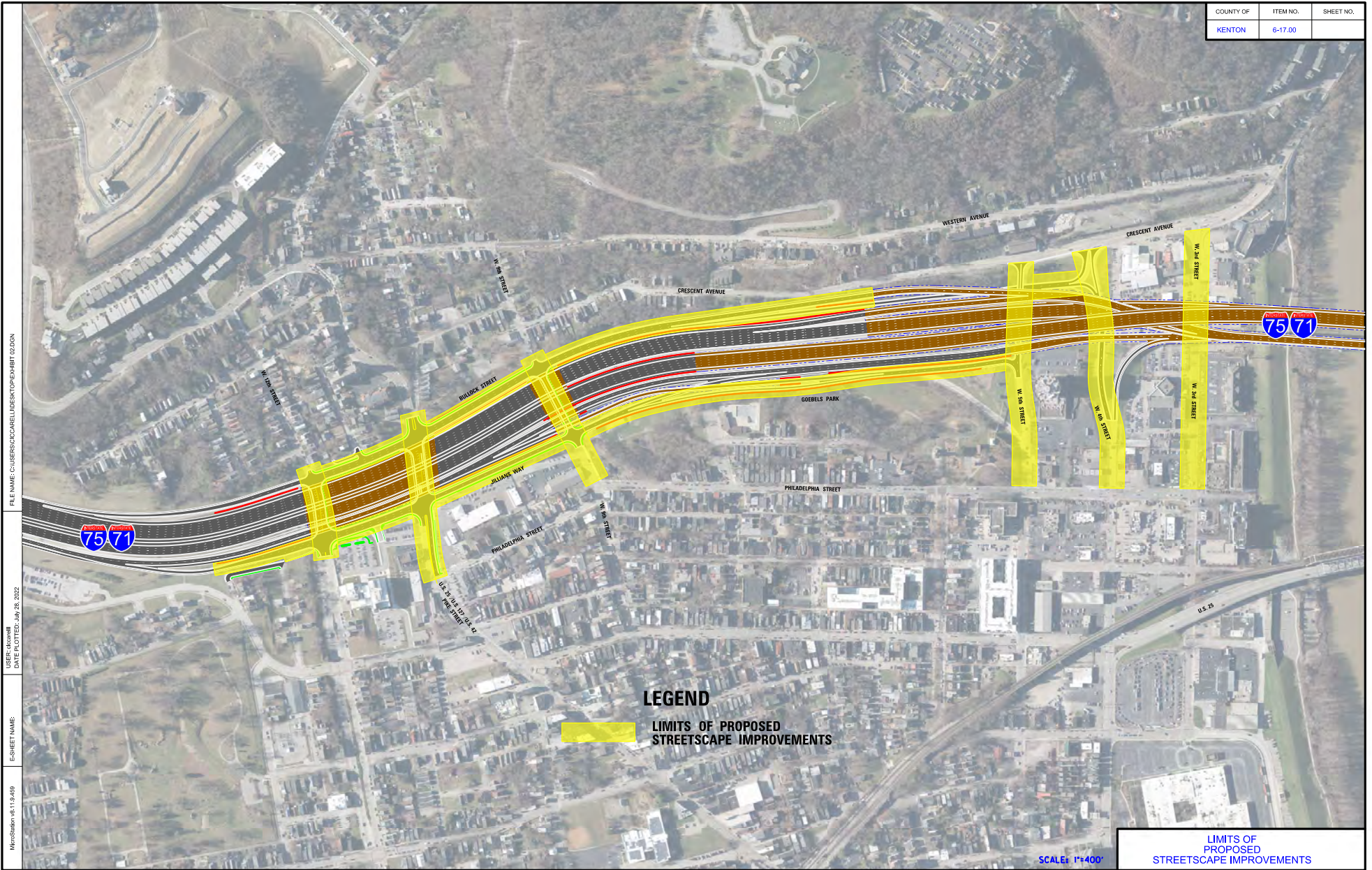
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DATE PLOTTED: July 26, 2022

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LIMITS OF PROPOSED STREETSCAPE IMPROVEMENTS

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KENTON	6-17.00	



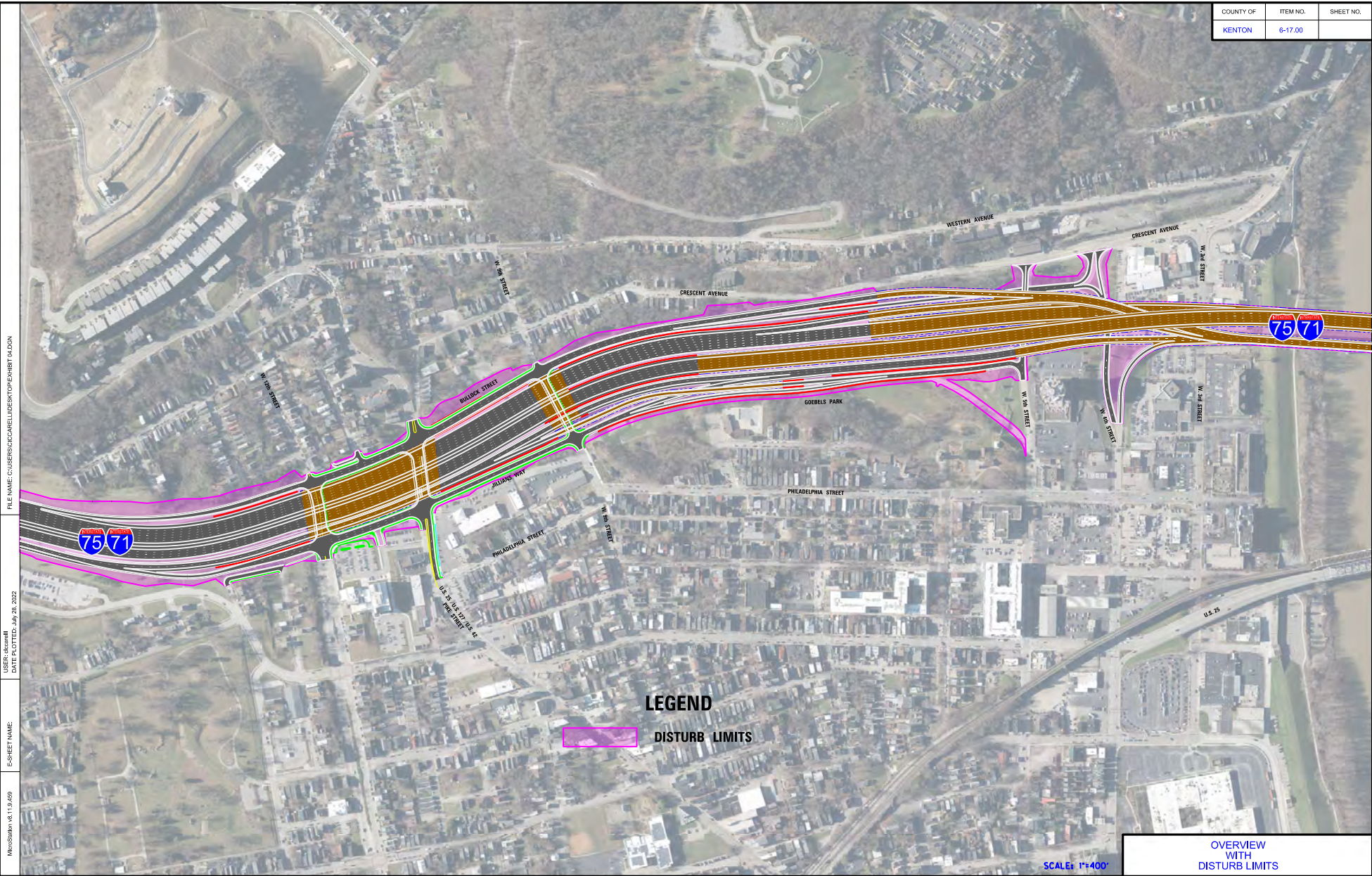
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DATE PLOTTED: JULY 28, 2022

E-SHEET NAME:

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OVERVIEW WITH DISTURB LIMITS



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 DATE PLOTTED: July 26, 2022
 E-SHEET NAME:
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LIMITS OF PROPOSED TREE IMPACTS

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KENTON	6-17.00	



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**EXISTING AND PROPOSED
CORRIDOR DIMENSIONS**

**UNDERPASS CONDITIONS
AND PREFERENCES**

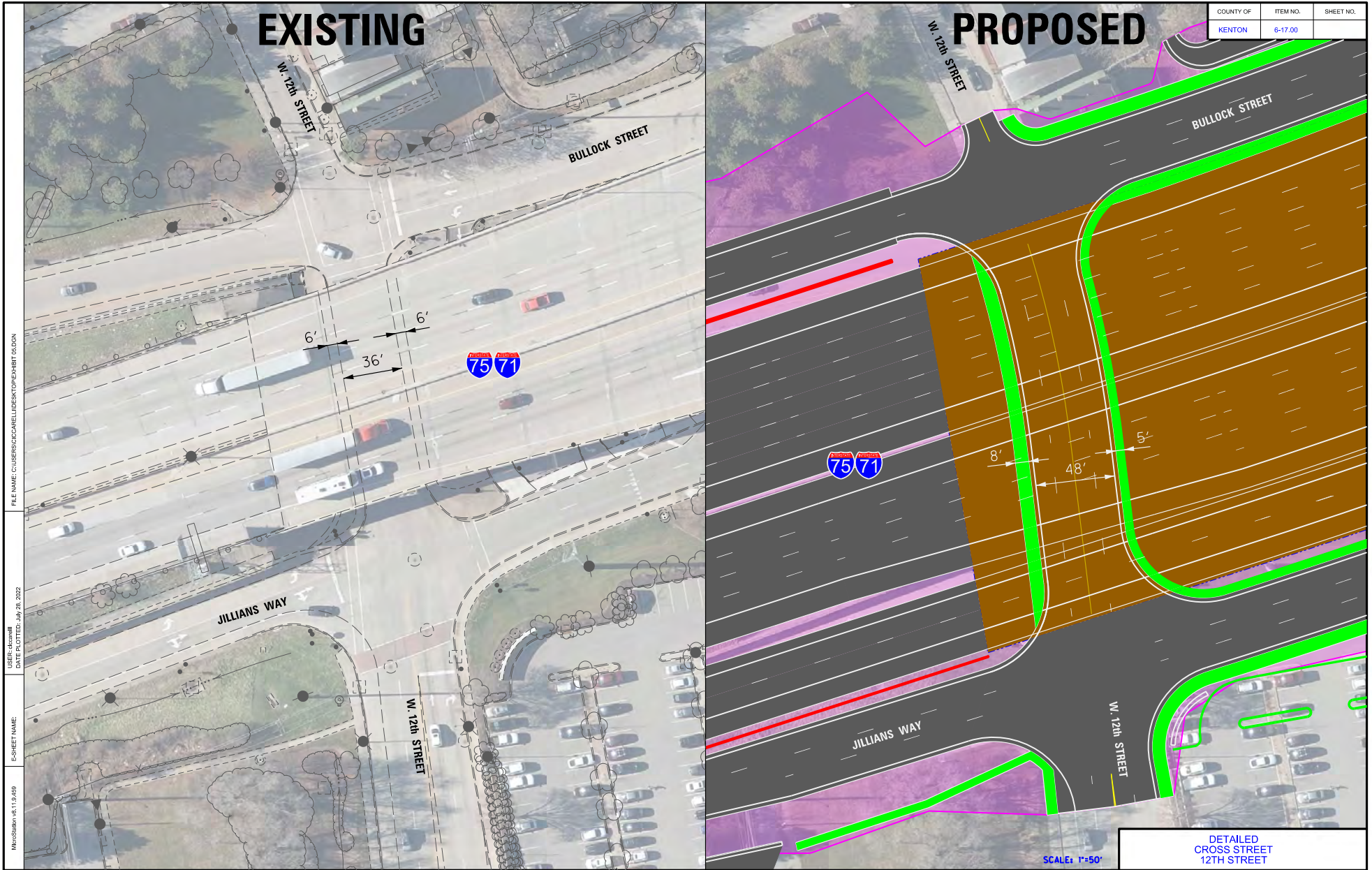
**PREFERRED DESIGN
PARAMETERS**

COVINGTON STREETScape &
PUBLIC REALM DESIGN GUIDELINES

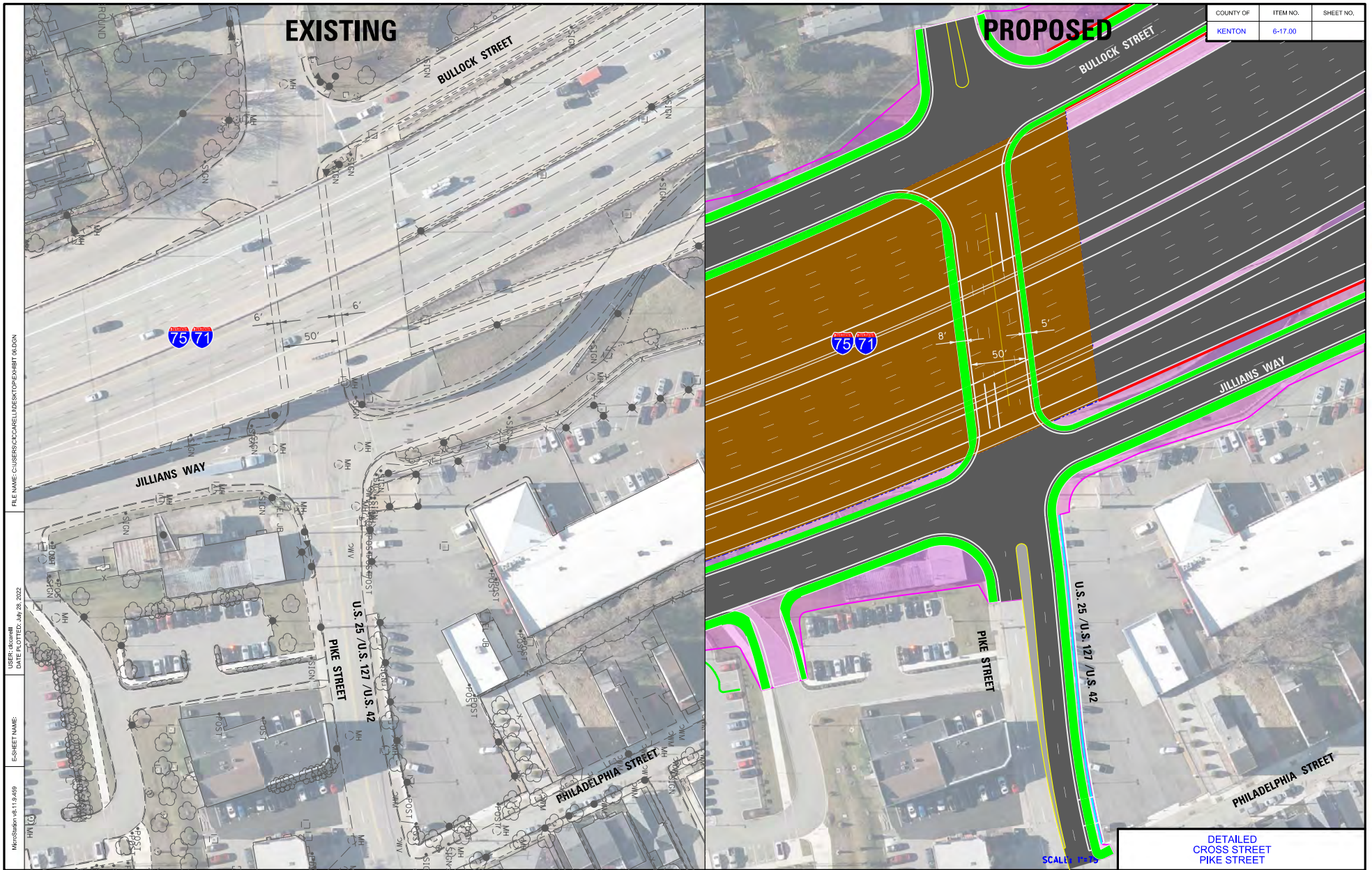
BRENT SPENCE
BRIDGE CORRIDOR



12TH STREET



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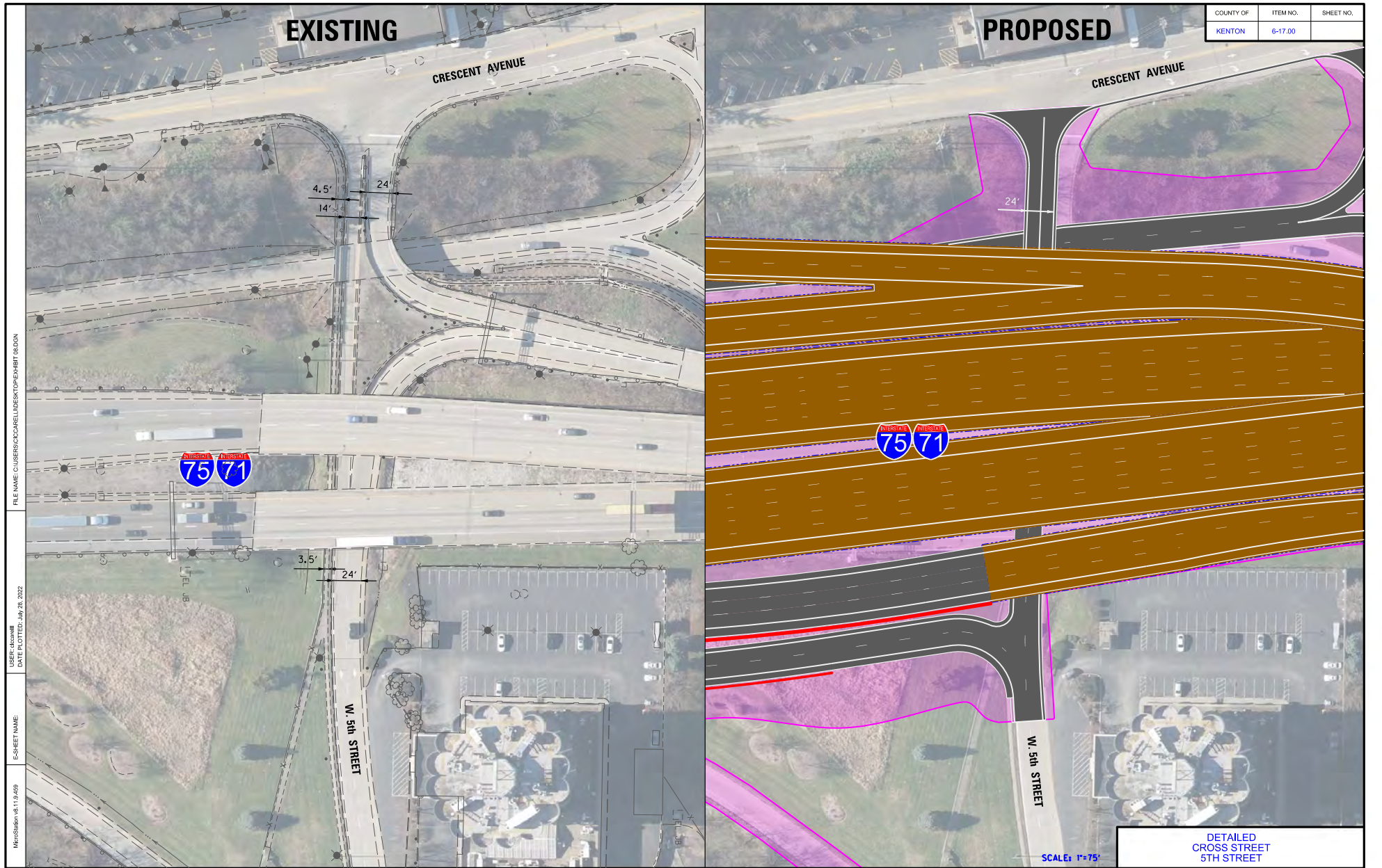


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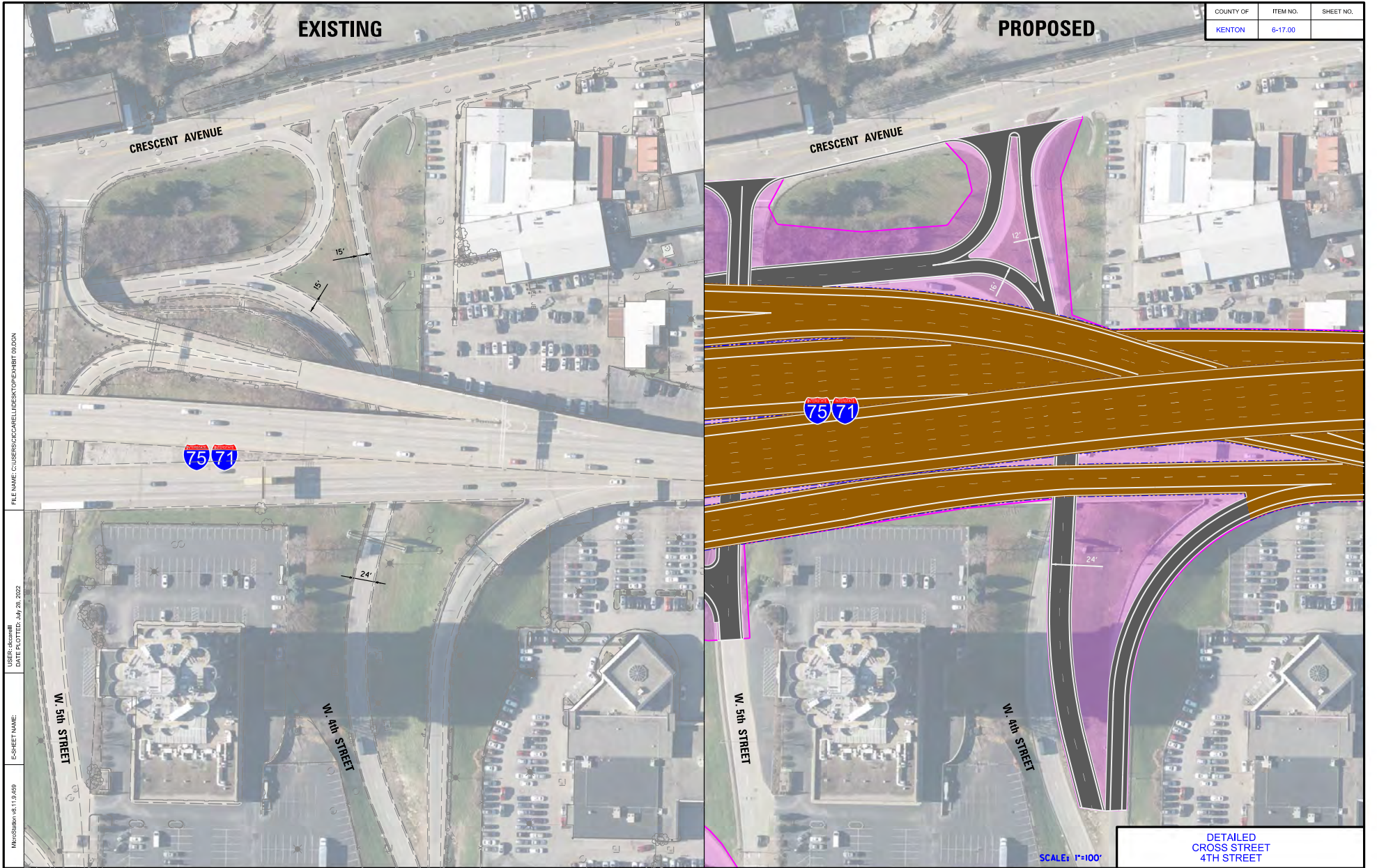
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DATE PLOTTED: JULY 28, 2022

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4TH STREET



COUNTY OF	ITEM NO.	SHEET NO.
KENTON	6-17.00	



DETAILED
CROSS STREET
3RD STREET

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DATE PLOTTED: JULY 28, 2022

SHEET NAME:

6062456.rvt 11/2/2022

COVINGTON STREETScape & PUBLIC REALM DESIGN GUIDELINES

NEXT MEETING
DAY, DATE

PREFERRED AREAS OF FOCUS

HERE •

HERE •



BRENT SPENCE
BRIDGE CORRIDOR



From: rrecord@rrecord.com <rrecord@rrecord.com>
Sent: Monday, August 22, 2022 9:49 AM
To: Valentine, Gary (KYTC) <gvalentine@ky.gov>
Cc: Hans, Stacey D (KYTC-D06) <Stacey.hans@ky.gov>; Joseph Meyer <jumeyer@covingtonky.gov>
Subject: some follow up from aesthetics committee Covington, and pavement 411 - BSB Corridor Project

****CAUTION** PDF attachments may contain links to malicious sites. Please contact the COT Service Desk ServiceCorrespondence@ky.gov for any assistance.**

Gary,

Attached are some rough notes from a group download session 8/17, as promised in follow up to the August 10 meeting at KYTC. This will give your team some guidance on how the four committee members are thinking.

I should add that one member has since expressed, in addition to the attached, concern for number of lanes and understanding why so many are needed, in particular on C-D/parallel local road elements (*...we need to get a solid idea of what is being proposed for the 'connector roads' and how they will impact the two sides of the interstates* [meaning number of lanes and overall project footprint/width]...[and] *the enormity of the intersections*). Just passing this along...

Gary, I heard you mention last meeting that you had asked UK to dig a little on possible pavement design for various mitigation areas, and that is great! Couple years ago, I sat in on a TRB webinar (slides attached FYI) on OGFC function and benefit in high-speed systems. A key slide for the BSB corridor is #20. Among the presenters, afterward I talked with the Austin guys (Hazlett and Barrett) and I know Scott Taylor well from various TRB committees. From an initial emphasis on WQ and water volume dispersion, more work has been done on noise (and now safety) benefits, and somewhere along the line I reviewed a couple research papers on that. All these benefits are quantifiable, within ranges, as performance outcomes. I do have somewhere a collection of articles/papers on PFC/OGFC and will pass them along if I can locate. Also, last time I heard I believe TnDOT was heading toward essentially 100% PFC/OGFC for uniform high-speed systems, for safety and environmental (noise, water) benefits.

Thanks,

Rick R.

Coordination session Covington Aesthetics Committee Members
Wednesday August 17, 2022
Farny Room – City of Covington

The group met for a brief period to collect thoughts and reactions from the August 10 meeting at KYTC D6. The idea for today is to compile some consolidated direction that can be provided back to KYTC to keep their work on target and with best use of effort.

Broad reactions/thoughts on overall from August 10:

- The KYTC team had a good command of what the project involves from their view.
- Pool/park area issues were surprising.
- Not clear on how much flexibility there is, how changeable.
- Height/scale issues not clear so far.
- Good information but had more questions going away than coming in.
- Not clear on how local/surface streets ideas would actually work.
- The 3D model they are working on should help.
- No information on emergency response issues or changes.
- How parallel roadways to local streets would actually work.

A little deeper dive into 3 categories for things heard/presented August 10 (flip chart sheets):

1. What works (or might?)
2. What does not work?
3. What information needs, questions, ideas?

Group worked through various concepts and parts of project covered August 10 (but not everything came up or was discussed; things that 'stood out' were emphasis for today). Slide deck provided by KYTC from Aug 10 was put up on screen where needed to look at or discuss an item, as well as 'project preliminary exhibits' (7 pages total) showing general configuration relative to community, including though-lanes, local lanes/C-D, and service roads, and access points; KYTC will be sending along requested detailed preliminary plans shown August 10 soon.

What works (or might?)

1. *9th-12th bridge rework [existing pier pattern]*
2. *Land back to parks [at 5th Street ramp removal]*
3. *Surface street improvements [extending away from project east and west]*
4. *Multi-modal trails on C/D [roads]*
5. *Opportunities "under" [reuse/better use of land under freeway]*

What does not work?

1. *Park and pool [outcome]*
2. *Height and scale*
3. *"Long" underpasses*

What information needs, questions, ideas?

1. *EMS/response*
2. *3D picture*
3. *Vibration/noise [especially historic districts/structures; remedy question]*
4. *Do we need 9th to 5th connection [park issues]*
5. *Do we connect at 9th at all? [traffic distribution and impacts]*
6. *Clarify access at 5th southbound [proposed]*
7. *Next meeting date?*

Open Graded Pavements: A Primer with Emphasis on Water Quality Benefits

Thursday, September 19, 2019
2:00-3:30 PM ET

The Transportation Research Board has met the standards and requirements of the Registered Continuing Education Providers Program. Credit earned on completion of this program will be reported to RCEP. A certificate of completion will be issued to participants that have registered and attended the entire session. As such, it does not include content that may be deemed or construed to be an approval or endorsement by RCEP.



Purpose

To describe open graded pavement designs for highways.

Learning Objectives

At the end of this webinar, you will be able to:

- Describe PFC mix design and function
- Identify water quality benefits of PFC by pollutant of concern
- Apply PFC for water quality at a transportation agency

Open Graded Pavements: A Primer with Emphasis on Water Quality Benefits

Darren Hazlett, P.E.
University of Texas at Austin
Center for Transportation Research

Permeable Friction Course – Open Graded Friction Course

- Types of Pavement
- PFC
 - Properties
 - Drawbacks
 - Benefits
 - Specifications to insure desired performance
 - PFC in action

Types of Pavement/Surfaces

- Concrete
- Hot Mix Asphalt
- Seal Coat



Types of Hot Mix Asphalt Pavement

- Dense Graded
- Open Graded

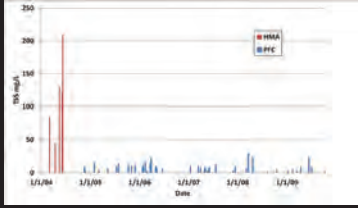
They have different aggregate gradations.



Historical

- Called **Plant Mix Seal**.
- In Texas, originated in **mid to late 1980s**.
- The first specifications in the 1990s used:
 - conventional asphalt binders (**no polymers, no lime, no fibers, no asphalt-rubber**), and
 - no tests for **durability or drain-down**.
- The binder drained down and the top of the mix, with little asphalt remaining, raveled off. **Performance was bad**.
- In the 2000's additives and performance tests were introduced which greatly improved performance of these mixtures.

TSS Temporal Trend

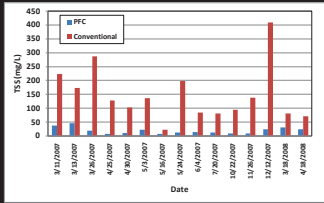


Water Quality at TX1

Constituent	Conventional Asphalt	PFC	Reduction %	p-value
TSS	118	8.8	92	0.016
Total P	0.13	0.07	48	0.047
Total Copper	27	13	50	0.010
D. Copper	6	10	-77	0.045
Total Lead	13	1	91	0.025
Total Zinc	167	29	83	0.002
Dissolved Zinc	47	22	53	0.139



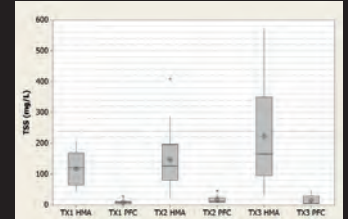
Paired Samples – TX2



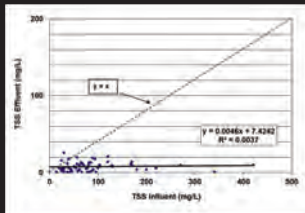
Water Quality at TX2

Constituent	Conventional Asphalt	PFC	Reduction %	p-value
TSS	148	18	88	<0.000
Total P	0.15	0.05	63	0.006
Total Copper	90	13	57	<0.000
D. Copper	6.3	9.0	-44	0.015
Total Lead	11	1.3	88	<0.000
Total Zinc	130	21	84	<0.000
Dissolved Zinc	18	11	40	0.043

Original Texas Sites



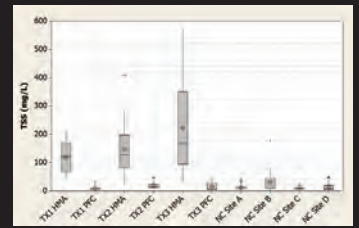
Sand Filter Performance



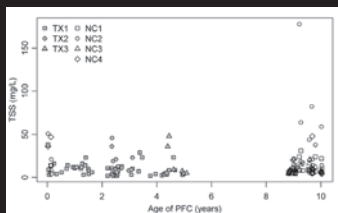
European Studies

Study	TSS Conventional	TSS PFC
Berbee et al. (median)	194	17
Pagotto et al. (wgt mean)	68	13
Pagotto et al. (subset)	46	8.7

Texas & North Carolina



Water Quality Persistence



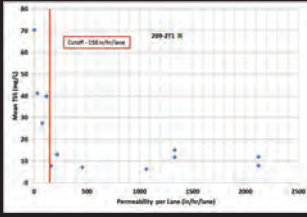
Caltrans Study Objectives

- Identify 10 locations where there were paired installations of conventional asphalt and a thin lift overlay
- Evaluate the performance of:
 - Open graded friction course (OGFC)
 - Rubberized hot mix asphalt – gap graded (RHMA-G)
 - Rubberized hot mix asphalt – open graded (RHMA-O)

Caltrans Paired Sites



Maintenance Indicator



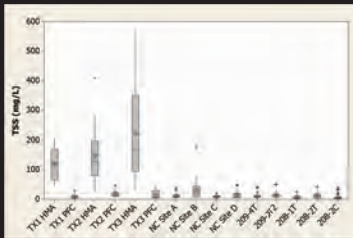
The Problematic Site (209-2T1)



OGFC Failure



TX NC CA Comparison



Sand Filter Comparison

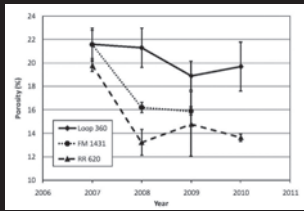
Constituent	OGFC	Sand Filter	Test	p-value
TSS (mg/L)	8	6.2	2-Sample t	0.202
Total P (mg/L)	0.07	0.165	2-Sample t	< 0.001
Total N (mg/L)	1.0	1.37	2-Sample t	< 0.001
Total Cu (µg/L)	7.1	8.5	2-Sample t	0.02
Dissolved Cu (µg/L)	4.4	6.9	2-Sample t	< 0.001
Total Zn (µg/L)	16.5	31	2-Sample t	0.075
Dissolved Zn (µg/L)	7.9	21	2-Sample t	0.001

Concentrations reported are medians

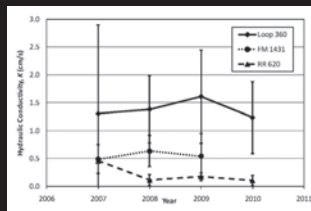
A-R PFC on CRCP
IH 35 San Antonio, Fall 2002



Porosity Testing



Permeability Testing



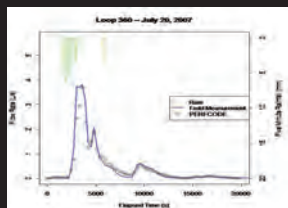
Darcy's Law

$$Q = KA \frac{dH}{dL} \quad q = K \frac{dH}{dL} \quad v = \frac{q}{n} = \frac{K}{n} \frac{dH}{dL}$$

Where:

- Q = Discharge
- K = Hydraulic Conductivity
- A = Cross-sectional Area
- q = Darcy Flux
- v = Water Velocity
- n = Porosity

Model Calibration



Darcy's Law

$$Q = KA \frac{dH}{dL} \quad q = K \frac{dH}{dL} \quad v = \frac{q}{n} = \frac{K}{n} \frac{dH}{dL}$$

$$n = 0.15, K = 1.0 \text{ cm/s}, dH/dL = 0.02$$

$$v = \frac{1.0}{0.15} \times 0.02 = 0.13 \text{ cm/s} = 16 \text{ ft/hr}$$

PFC/OGFC Summary

- Runoff from PFC/OGFC is much cleaner than that from conventional pavement and comparable to the discharge from other approved BMPs
- Will treat at least 450 inches of rain without maintenance
- Provides treatment at all rainfall intensities
- Ideal method to retrofit existing highways for water quality

Questions?

Pavement Mix Design

Sieve Size	1 inch		3/8 inch		INDOT PS 70	INDOT A-R Max	NCDOT P 78M	NCDOT P 57	NCDOT FC L/No. of	NCDOT FC-2
	Max Limits Proposed Gradation	Max Limits Proposed Gradation	Max Limits of Proposed Gradation	Max Limits of Proposed Gradation						
1.5"	100	100	100	100	100	100	100	100	100	100
1"	95-100	95-100	100	100	100	100	100	100	100	100
3/4"	85-96	85-96	100	100	100	100	100	100	100	100
3/8"	55-71	55-71	80-100	80-100	80-100	80-100	80-100	80-100	80-100	80-100
2"	78-89	78-89	80-100	80-100	80-100	80-100	80-100	80-100	80-100	80-100
1 1/2"	30-35	28-37	20-30	1-10	0-4	20-45	10-29	25-45	15-25	15-25
No. 4 (1/4")	0-45	7-18	7-18	1-10	0-4	1-15	1-10	1-15	1-10	1-10
No. 10	0-10	0-10	0-10	-	-	-	-	-	-	-
No. 20	0-5	0-5	0-5	0-5	0-4	0-4	0-5	0-5	0-4	0-4

Pavement Mix Design

Sieve Sizes	Caltrans 1/2 inch Max	NCDOT PADC P 78M	TxDOT A-R Mix
1.5"			
1"			
3/4"	100	100	100
3/8"	95-100	95-100	95-100
2"	78-89	75-100	50-80
No. 4 (1/4")	28-37	20-45	0-8
No. 8	7-18	3-15	0-4
No. 16	0-10	-	-
No. 200	0-3	1-3	0-4

Today's Participants

- Scott Taylor, *Michael Baker International*, staylor@mbakerintl.com
- Michael Barrett, *University of Texas, Austin*, mbarrett@mail.utexas.edu
- Darren Hazlett, *University of Texas, Austin*, darren.hazlett@austin.utexas.edu

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**Brent Spence Bridge Corridor Project
Meeting Summary - Covington Subcommittee
October 20, 2022**



Project:	Brent Spence	<i>Groundbreaking by Design.</i>
Purpose:	Urban Aesthetics/Guidelines	
Place:	First Financial Bank Conference Room (6 th and Madison, downtown Covington)	
Meeting Date:	October 20, 2022	
Prepared By:	David Reed	
Attendees:	Mayor Joseph Meyer	City of Covington
	Ben Oldiges	Advisory Committee Member
	Emily Wolff	Advisory Committee Member
	Sarah Allen	Advisory Committee Member (representing Susan Smith)
	Steven Hill	Advisory Committee member
	Diana Martin	RL Record LLC
	Andrew Wilhoite	City of Covington
	Nicole DiNovo	Human Nature
	Gary Wolnitzek	Human Nature
	Gary Valentine	KYTC - CO
	Stacey Hans	KYTC – D6
	Glen Kelly	Qk4
	Lindsay Hoskins	Qk4
	David Reed	Qk4

1. Introductions

Gary Valentine provided a brief introduction and project update, reporting that the current Progressive Design Build project delivery method will target a final Contractor selection by June of 2023, and will allow the process of refining Urban Aesthetic Guidelines to be extended to a similar timeframe. Gary presented a current summary project overview, which included a review of the existing roadway design details, and highlighted the proposed surface roads, collector/distributor network, interstate ramps, and mainline interstate corridors. Gary also presented a series of 3-dimensional renderings, depicting images of the proposed project corridor from varied aerial perspectives, highlighting proposed views within downtown Covington. Mr. Valentine spoke briefly about noise walls - as some of the images depicted views with and without proposed noise walls – and indicated that there would be a separate emphasis on noise wall presentations and ample opportunity for the public to weigh in on the issue of appropriateness. Where supported by study analysis, noise walls will be budgeted for inclusion, but the community will ultimately determine what noise walls are built.

2. Streetscape Details

David Reed provided an overview of the proposed streetscape conditions for each of the downtown street corridors, including the impacted areas of 3rd, 4th, 5th, and 9th, Pike and 12th Streets. These streetscape concepts were detailed to

reflect a continuation of the City's Streetscape Guidelines and dimensioned the functional areas beyond the roadway travel lanes, including amenity zones, principal pedestrian zones, and buffer zones for each of the subject corridors. These concepts also included recommendations for how roadway intersections and pedestrian crosswalks could be improved to offer enhanced striping and surfacing treatments, with areas of raised crosswalks proposed to offer traffic calming and create a much safer pedestrian and bicycle crossing environment. A detailed example of this was highlighted for the Pike and Lewis intersection, where an accessible pedestrian ramp was proposed as a replacement for the existing conditions defined by a steep concrete stairway which the committee identified as hazardous and unsightly. This proposed ramp would connect to the existing crosswalk serving the St. Johns School and provide a safe and accessible route for parents and students to access the lower Lewis Street corridor and neighborhood.

The committee questioned whether street trees could be accommodated in both the buffer zones and amenity zones, and Mr. Reed responded indicating that trees would need to be supported by a minimum of 3-foot planting zone-width in either the buffer or amenity zone.

Mr. Reed also provided an overview of proposed bike path improvements, which were focused on expanding the existing bike path network and exploring the completion of linkages identified in the City's bike and trail master plan. The primary focus of the proposed improvements included a new north-south multi-use path, which would extend north from 12th Street and provide a linkage to the Riverfront Commons Trail. This proposed improvement would include nodes of connectivity at 12th Street, 9th Street at the south end of Goebel Park, the Goebel Park pool area, and at the north end of Goebel Park which is the gateway to the Mainstrasse area. This new corridor would also provide opportunities for future bike path network expansions with connections at 9th Street to extend further west toward Devou Park; and connections along 5th Street to be extended further east to Bakewell, and further north toward the Riverfront Commons Trail. The northern portion of the proposed bike path would be supported by a road-diet segment of Crescent Avenue, where travel lanes would be reduced to create a new bike path corridor along the east side of the roadway, and where intersections at 3rd, 4th and 5th would be reconstructed to minimize crosswalk length and offer a safer route for cyclists and pedestrians. The connection to Riverfront Commons Trail would occur at the end of the existing floodwall between Western Avenue and Spring Street, where a maintenance access gate currently exists.

The committee questioned why the bike path could not be extended east along 5th Street, and Mr. Reed responded indicating that initial plans would include a path along 5th Street up to Philadelphia, but that any continuation east along 5th Street or north along Bakewell would need to be supported by travel lane modifications or right of way expansion, given the limited space available along the existing streetscape in these areas.

3. Goebel Park Concepts

Gary Wolnitzek provided an overview of the impacts to Goebel Park, and offered two alternative conceptual plans for consideration, which were based upon feedback from an earlier September meeting with Parks staff and other committee members. The overview included an illustration of the changes in height which would occur as part of the project – where new northbound lanes would be constructed above the existing interstate structures and increase the scale of the interstate where adjacent to Goebel Park. The expanded footprint of the interstate – primarily along the new portion of Jillians Way – was also illustrated, where project improvements around the existing pool would be as close as 50 feet. This “pinch point” would allow for the construction of the proposed 10' bike path, with some areas of landscape buffering remaining on either side of the bike path.

4. Alternative Recreational Use Areas Under the Interstate

Gary Wolnitzek also presented a series of exhibits which depicted possibilities for expanded recreational use and enhanced visual aesthetics for areas under the interstate – within the open space areas immediately west and north of Goebel Park. These concepts were oriented around a theme focused on the Willow Run Creek corridor which once existed within the footprint of the interstate (now within a sub-surface combined sewer facility along the interstate corridor). Five primary use areas were defined, including Riverfront Commons, Riverlink Park, 5th Street Sports Park, 9th Street Gateway, and the 12th and Pike Community Connector area. Unique concepts for each area were proposed

and featured a variety of improved aesthetics and potential use areas and functions. The Commons and Riverlink areas north of 5th Street represent larger open space areas beneath the interstate, and were envisioned as more transitional use areas, characterized by passive use areas connected by an interior pathway network and dominated by larger areas of landscape enhancement; whereas the 9th, Pike and 12th Street areas represent much smaller areas under the interstate where gateway and community focused use areas are supported by areas of expanded decorative pavement, where a flexible use area can be created to serve a variety of possible community functions as well as to support community gateway features and other welcoming opportunities. The central area south of 5th Street and immediately adjacent and to the west of Goebel Park was presented as a large area for active ball courts and sports venues, offering relocation and expansion of facilities impacted within Goebel Park, as well as offering alternative locations for parking, restrooms and possible relocation of parks maintenance and service functions which currently occupy valuable land use areas within Goebel Park. Overall, these concepts were presented as alternative opportunities for areas under the interstate which are typically underutilized or community eyesores, and images were focused on depicting the difference between generic areas void of character and aesthetic value, contrasted with richly colored and textured open spaces of practical use, improved aesthetics, and higher community value.

5. Community Connector, Gateways and Greenspace Analysis

Gary Wolnitzek wrapped up the presentation with a discussion of the Community Connector area at 12th and Pike, an overview of Gateway opportunities, and a summary of the Greenspace Analysis which had been prepared to evaluate the project impacts on existing tree canopy. More detail was provided regarding the details of the existing parking lot between 12th and Pike which serves the County Building, and the flexible open space opportunities that exist at each end of the lot. Gateway opportunities throughout the downtown area along the corridor were identified for future use and consideration by the city. A greenspace analysis was presented which quantified the areas of canopy loss, estimated how this canopy loss could be mitigated through new tree streetscape plantings, and also identified areas of possible greenspace expansion where trees and green infrastructure (sustainable stormwater infiltration features) could be introduced along the corridor as part of the project.

6. Next Steps

The presenting team agreed to provide the review committee with a copy of the presentation immediately following the meeting (completed next day).

The review committee is planning to meet internally on or about November 2; and would like to schedule another group meeting on November 16, where they will share their review comments and define the areas of further discussion preferred (a meeting has been scheduled for Wednesday, November 16, at 2:30 pm, at the First Financial Bank Conference Room).

End of Meeting Notes

**Brent Spence Bridge Corridor Project
Meeting Summary - Ft. Wright/Ft. Mitchell Subcommittee
November 16, 2022**



Groundbreaking by Design.

MEETING MINUTES

Project: Brent Spence
Purpose: BSB Aesthetics Ft. Wright & Ft. Mitchell
Place: First Financial Bank Conference Room (6th and Madison, downtown Covington)
Meeting Date: November 16, 2022
Prepared By: David Reed
Attendees: Edwin King City of Ft. Mitchell
Dave Hatter City of Ft. Wright
Jill Cain Bailey City of Ft. Wright
Laura Tenfelde Planning Development Services
John Stanton Kenton County
Sean Buckley Human Nature
Nicole DiNovo Human Nature
Gary Valentine KYTC - CO
Stacey Hans KYTC – D6
Glen Kelly Qk4
David Reed Qk4

1. Introductions

Introductions were made by attendees around the room. Stacey Hans provided a project overview and reviewed the role of the committee and the purpose of the meeting.

- The overview of the proposed changes affecting the Dixie Highway and Kyles Lane interchanges included a review of the interstate lanes, new collector/distributor lanes, and existing ramp modifications.
- The latest soundwall information was also provided along with a conceptual rendering of the soundwalls affecting the subject area. Stacey noted that the intent was to maximize the extents of soundwalls, based on locations that were both feasible and reasonable (standard criteria). The details of the soundwalls are still in progress, and ultimately each of the cities, and the impacted property owners would have an opportunity to provide input on the final locations and limits of the soundwalls to be constructed. Separate meetings are planned for each community – Ft. Mitchell on November 30, and Ft. Wright on December 1.

2. Aesthetic Concepts

Nicole DiNovo presented a series of images exploring possible aesthetic improvements to the interchanges along with research and inspirational images for bridge designs and the creation of iconic gateways.

a. Fencing Concepts

- Typical fencing for areas for interstate-controlled access right of way is chain link.
- There is a desire for a more aesthetic fencing, but ease of maintenance is a concern.
- Material is desired to be vinyl or aluminum without the need to continuously paint the fence.
- Mowing around the fence is a concern as well so a fence type that is lifted off the ground with space for a mower or trimmer is ideal.
- Nicole provided a series of fencing images for consideration.
- Gary Valentine indicated his support of aesthetic fencing alternatives but made it clear that KYTC cannot provide maintenance for anything other than chain link, and that while specialty fencing could be constructed as part of the project, the maintenance would become the responsibility of the municipality.
- In addition to fencing, Jill Cain Bailey expressed a desire for lighting on the bridge surface and whether decorative light poles could be included in concepts. Gary indicated that he would research what lighting would typically be provided on the interchange bridges and ramps.

b. Landscaping and Gateway Concepts

- Nicole presented landscape comments for the interchange areas, including improvements offered by each of the two municipalities. Nicole encouraged inclusion of these features and a desire to further expand and embellish these solutions and find ways to offer a consistent overall aesthetic, such as bolder simplifications of the masonry features proposed, and the inclusion of larger areas of landscape plantings to help unify the two interchange areas.
- John Stanton expressed the importance of creating cohesion with landscaping and signage between Covington, Ft. Wright, and Ft. Mitchell.
- John asked if improvements or repairs to the terraced reforestation project at the cut-in-the-hill would be part of the project. KYTC stated that was an SDI project and is not impacted by the interstate project. John indicated that he would speak to SDI about repairs or concerns independently.
- Edwin King reiterated Ft. Mitchells' desire to manage invasive species along the roadside and remove the proliferation of volunteer Bradford Pears within the corridor.
- Gary Valentine provided an explanation of the dual fences that exists along the right-of-way area bordering Highland Cemetery, indicating that this was now an area of excess right-of-way, and if the community is interested the transfer of land ownership in this area could be discussed. This area was the right of way of the old interstate and is no longer needed.
- Overall, the comments were for a desire to have a cohesive design for northern Kentucky gateways and signage for Covington, Ft. Mitchell and Ft. Wright. Examples were brought up like an "Unbridled Spirit" sign on the Devou Park Hill or a Bourbon Trail identity tying signage together. It was clarified that both of those sites were outside the project area. However, the design team did look at gateway opportunities with Covington within the project area.
- The cities seemed consistent in their desire to maintain a more historic character on bridges and gateways, in reference to Civil War fortress features and possible use of each city's crest as presented by Nicole.

- Ft. Mitchell shared some flexibility and potential interest in a more modern fenestration solutions.
- Ft. Wright was clear in their dislike for anything contemporary like the cut metal or metal fins look and that this type of imagery was too abstracted for their taste.
- Ft. Wright really likes the Evandale gateway and mentioned the abutment and walls near Wright Patterson Airforce Base.
- Overall, the design team are on the right track in terms of general concepts and the intent is for us to “shoot for the moon and then work backwards” to find a balance between aesthetics and cost.

3. Next Steps

More input from Ft. Mitchell and Ft. Wright to help to move concepts further, and members of the communities represented agreed that they would consider the options presented and share them with others in their respective groups (Mayor Hatter mentioned an upcoming city caucus meeting). The next meeting to be scheduled would occur in late January or early February of 2023, with the intent of being complete with the aesthetic recommendations by June, 2023.

End of Meeting Minutes

**Brent Spence Bridge Corridor Project
Meeting Summary - Aesthetic Committee
January 31, 2023**



Brent Spence Bridge Corridor Project Aesthetics Committee Meeting Summary January 31, 2023

Introduction

The Brent Spence Bridge (BSB) Corridor Aesthetics Committee meeting was held on January 31, 2023 from 10:00 am to 11:45 am. The meeting was held at the Ohio Department of Transportation ARTIMIS building located at 508 West Third Street. Invitations to the meeting were sent to Project Advisory Committee (PAC) and Aesthetics Committee members via email on January 6, 2023 (see Attachment 1). Attendees at the meeting included Aesthetics Committee members, PAC members, the project team, and other stakeholders. A list of attendees is included in Attachment 2.

Presentation

The meeting format included a formal presentation by Mark Becherer and Jodi Heflin (HNTB). The project team responded to questions posed by attendees members both during and after the presentation.

A summary of the presentation is provided below. A copy of the presentation is included in Attachment 3.

1. Introductions

- a. Mark Becherer (HNTB) opened the meeting, and attendees shared their names and the organization they represent.

2. Project Overview

- a. Overview of key project elements in Ohio and Kentucky, including the existing BSB and the new companion bridge.
- b. The project will be constructed in three phases. Phases I and II (northern two miles in Ohio) are traditional design-bid-build projects. Phase III, which includes the new companion bridge, is following a progressive design-build process.
- c. Overview of progressive design-build.
- d. A draft RFP for design-build teams was released in January. The final RFP will be released in February. The design-build team will be announced in May. Public hearings will be held in July/August, and the supplemental Environmental Assessment will be finalized in October. Groundbreaking on Phase III will occur in late 2023, with substantial construction beginning in 2024 and completion estimated for 2029.

3. Aesthetic Committee Overview and History

- a. The Aesthetic Committee was established in 2005 as a subcommittee to the Project Advisory Committee to provide input on aesthetics for the overall corridor, the new companion bridge, and the existing BSB.



-
- b. Six meetings were held, and a survey was completed from 2005 to 2010.
 - c. Aesthetic Design Guidelines were developed for the project in 2011.
 - d. The project was evaluated in an Environmental Assessment, and the Federal Highway Administration (FHWA) issued a Finding of No Significant Impact (FONSI) in 2012. The project team is currently preparing a supplemental Environmental Assessment to give updates on what has changed since 2012.
 - e. The FONSI selected two potential bridge types for the new companion bridge:
 - i. Arch bridge, simply supported with inclined arch ribs
 - ii. Cable-stayed bridge, two towers with vertical legs/towers
 - f. The FONSI also stipulated that the bridge height would be at least 300 feet above the Ohio River but no higher than 420 feet. This was to ensure the bridge was visible because the existing bridge was to remain and to protect the viewshed in historic areas surrounding the bridge.
 - g. Renderings of both bridge types from 2012 were shared. The renderings showed a middle support which will not be required because the width of the bridge has been reduced.
4. Aesthetic Committee Recent Activities
- a. The Aesthetics Design Guidelines identified a “Rivers and Hills” corridor theme. Since then, it was decided that the Ohio River is a prominent boundary, and both states can work independently to develop aesthetic plans while coordinating to make sure aesthetic treatments in the corridor do not clash.
 - b. ODOT has been meeting with a subcommittee to discuss aesthetics in the Ohio portions of the corridor. One meeting was held in June, and another will be held later today. ODOT is following the process set out in their statewide Aesthetic Design Guidelines.
 - c. KYTC has held three meetings with a Covington subcommittee and two meetings with a subcommittee for Fort Wright, Fort Mitchell, and Kenton County. KYTC has hired QK4 and Human Nature to help develop aesthetic guidelines for the Kentucky portions of the corridor.
 - d. Subcommittees are providing feedback about aesthetic treatments for piers, abutments, retaining walls, and noise walls; lighting; streetscapes; landscapes; and city gateways.
5. Ohio River Bridges
- a. The design has not progressed sufficiently to make decisions on the bridges. The Aesthetics Committee will be engaged in the future to provide feedback on items such as bridge lighting, color, tower/pier texture and color, railing, fence, roadway lighting, and gateways between Ohio and Kentucky.
 - b. The design team is interested in opening up some of the decisions about the new companion bridge. The required elevations are set at no less than 300 ft and no more than 420 ft above the river. These will not change. However, more flexibility is required for the bridge type to allow the design-team to pursue innovative and cost-effective designs to the greatest extent possible. For the arch bridge, the design team would like to remove the requirements for it to be simply supported with inclined ribs and just state that it will be an “arch bridge.” For the cable-stayed



bridge, the design team would like to remove the requirements for two vertical towers and just state that it will be a “cable-stayed” bridge.

- c. ODOT and KYTC will still work with the design-build team to ensure an iconic, aesthetically pleasing bridge is ultimately built.
- d. Recent renderings of the arch bridge type were shared.

Discussion

The following comments and input were provided during and after the presentation:

- Lighting of the bridge will be critically important. Clarification was requested as to why only up-lighting would be permitted.
 - Lighting is anticipated to be a key element in the bridge aesthetics. The entire bridge can be lit, but lights cannot shine down into the navigation channel of the Ohio River because they would be a safety concern for barge traffic. The lighting on the bridge will need to take into account considerations for the navigation channel, the roadway, and safety.
- In favor of making the allowable bridge types more flexible so that the designers are not locked into a design.
 - Once the design-build team (DBT) is selected, their first task will be to develop a proof of concept. The DBT will propose a bridge type, the project team will make the selection on the bridge type, and then the DBT and the project team will present options to the Aesthetics Committee.
- Does Ohio have a separate consultant for aesthetics?
 - No. In Ohio, ODOT is following its statewide aesthetics process. KYTC does not have an established process, so they’ve hired a consultant to help develop aesthetic guidelines in Kentucky.
- Stefan Spinosa (ODOT project manager) asked if there was any opposition to opening up the bridge types.
 - No opposition was offered.
- Stefan Spinosa asked if there was feedback on the shape of the towers if a cable-stayed bridge was selected.
 - The towers should have a unique design, be beautiful, and serve as a prominent gateway between Ohio and Kentucky.
- Creating a signature bridge is very important, particularly because this is the entrance to Kentucky and Ohio.
- Will there be time to get feedback on aesthetics with such an aggressive schedule?
 - The DBT will be working quickly. There will need to be quick turnaround for Aesthetic Committee feedback, but it will be incorporated into the process and project schedule.



Next Steps

- The project team will distribute minutes of this meeting.
- The project team will keep the public informed through e-newsletters and other communications.
- The minor revisions to the bridge types will be addressed in the supplemental EA, and the public will be invited to give feedback during the public hearing process.
- Once completed, updated renderings of the cable-stayed bridge will be distributed to the committee.
- Meetings will be scheduled with the DBT when the design has progressed to the appropriate stage.
- Questions can be emailed to Mark Becherer: mbecherer@hntb.com.

Comments

Members of the Aesthetics Committee were provided 30 days to comment on the meeting summary. The following comments were received (see Attachment 4):

1. Comment:

Thanks for taking time to listen to my comments at the meeting in January regarding the view of the bridge from multiple points. Possibly a view shot from Covington Landing and our Riverfront might be a better perspective.

I have provided two photos from the web that represent concepts that might be considered by the aesthetics and design committee for our new bridge which are statement structures. Both of the designs below brings into context the existing construction of the Brent Spence bridge structure and integrates our John A. Roebling Suspension Bridge the ICON of Cincinnati.

I feel the arch is an odd contrast to our existing bridge. The two column suspension is quite plain and do not make an entry to our communities statement.



Response:

An extensive bridge type selection process was undertaken by the project team that included public involvement and input from the Aesthetics Committee. The arch and cable-stayed bridge types were identified through that process. The Executive Summary of the process can be viewed here: <https://brentspencebridgecorridor.com/wp-content/uploads/2022/10/Bridge-Type-Selection-Executive-Summary-1.pdf>.



As the project progresses, ODOT and KYTC will work with the design-build team and the Aesthetics Committee to ensure the new companion bridge will be an iconic, aesthetically pleasing structure. Additional renderings of the proposed bridge from different views will be prepared during the design-build phase of the project.



[Attachment 1: Invitations](#)

From: Mark Becherer
Sent: Thursday, January 5, 2023 2:17 PM
To: [REDACTED]
Cc: [REDACTED]
Subject: Brent Spence Bridge Corridor Ohio Aesthetic Committee Meeting

All – Thank you for agreeing to join or continue serving as a member of the Brent Spence Bridge Corridor Aesthetic Committee. As the project continues to move forward; most recently with the \$1.6B grant award; we look forward to reconvening this committee to provide assistance and input on the project corridor’s vision. The overall goal of the aesthetic committee is to provide recommendations to the project team on aesthetic treatments/vision and to communicate decisions back to their respective agencies and constituents.

The overall committee includes all on this email. Subcommittees; as needed; will also be formed. The first subcommittee formed is for the Ohio corridor and approaches. In the next day or two you will be receiving a hold the date invite or invites to the entire committee meeting, and for those involved an Ohio sub-committee meeting. The corridor wide meeting has both in-person and virtual options. The Ohio sub-committee meeting is planned to be in person, with a virtual option also being provided if needed. We are currently looking at the morning of January 31 for the overall committee meeting and the afternoon of the same day for the Ohio subcommittee meeting. More details; including a draft agenda; will be in the invites.

Don’t hesitate to give me a call or email if questions. We look forward to continue partnering with you in developing and delivering this exciting investment in Southwest Ohio and Northern Kentucky.

Thanks.

Mark Becherer
Vice President

HNTB CORPORATION

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Subject: Brent Spence Bridge Corridor Aesthetic Committee Meeting
Location: ODOT Artimus Building 508 W 3rd Street Cincinnati, Oh

Start: Tue 1/31/2023 10:00 AM
End: Tue 1/31/2023 11:30 AM

Recurrence: (none)

Meeting Status: Accepted

Organizer: Mark Becherer

Required Attendees



Optional Attendees:rcod

All – As per the email you received yesterday this is an invite to a corridor wide Aesthetic Committee Meeting for the Brent Spence Bridge. The meeting is intended to be a virtual meeting. The project team will be in person at the ODOT Artimus Building if anyone prefers joining in person. Again, thank you for agreeing to participate on this committee. A draft agenda is attached.

Let me know if any questions.

Thanks.

Mark Becherer
Vice President

HNTB CORPORATION

Microsoft Teams meeting

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Attachment 2: Attendees

Angie Strunc, City of Cincinnati Department of Transportation & Engineering

Michael Schuster, Michael Schuster and Associates

Jain Krutarth, Champlin Architecture

Katie Eagan, Cincinnati USA Regional Chamber

Mark Becherer, HNTB

Jodi Heflin, HNTB

Andrew Wilhoite, City of Covington

Joshua Wice, Planning and Development Services of Kenton County

Stefan Spinosa, ODOT

Stacey Hans, KYTC

Bryan Williams, City of Cincinnati Urban Planning

David Okum, Hamilton County Regional Planning Commission

Eric Beck, Hamilton County

Mark Policinski, Ohio-Indiana-Kentucky Regional Council of Governments

Matt Jones, Cincinnati Business Committee

Jack Weiss, Cincinnati Bulk Terminals

Heather McColeman, ODOT

John Seibert, University of Cincinnati

Tommy Arnold, ODOT

Keith Smith, ODOT

Tammy Campbell, ODOT

Robert Koehler, Ohio-Indiana-Kentucky Regional Council of Governments

Gary Valentine, KYTC

Adam Johnson, FHWA

Jeremy Worley, Northern Kentucky Tri-ED

Timothy Long, FHWA



Kristopher Stone, Boone County Arboretum

Mimi Rasor, Rasor Communications

Amy Clay, Rasor Communications

Blake Finley, Cincinnati Bengals

Joseph Smithson, ODOT

Scott Stone, FHWA,



Attachment 3: Presentation

BRENT SPENCE
BRIDGE CORRIDOR



AESTHETIC COMMITTEE MEETING
BRENT SPENCE BRIDGE CORRIDOR &
RIVER CROSSING

January 31, 2023



INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.
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Agenda

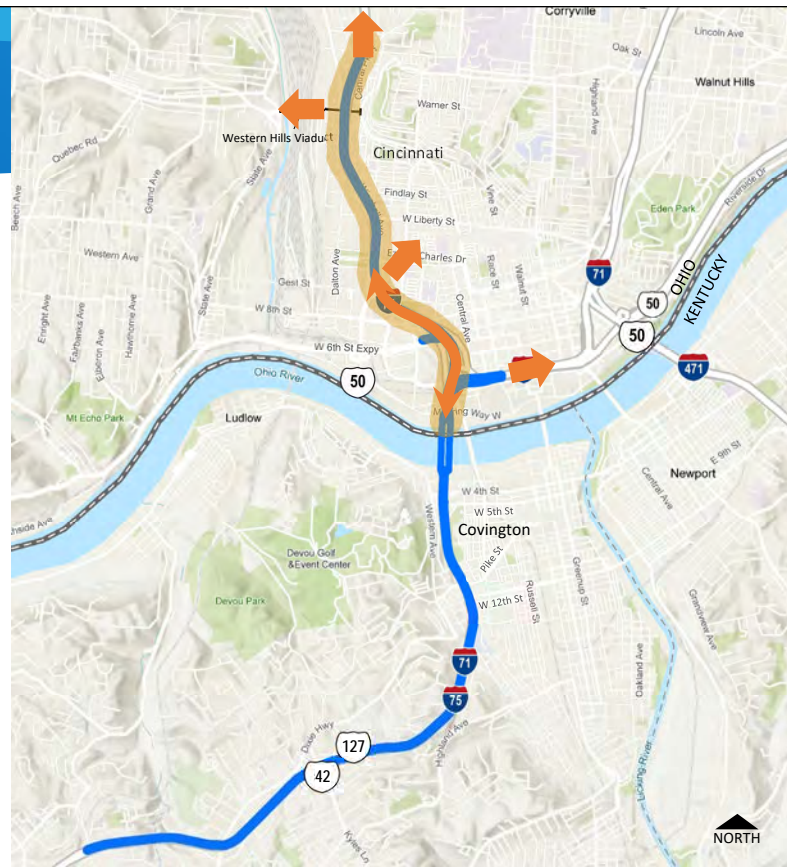
- Introductions
- Overview
- History
- Recent activities
- Ohio River bridges
- Schedule
- Comments and input



Overview

Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



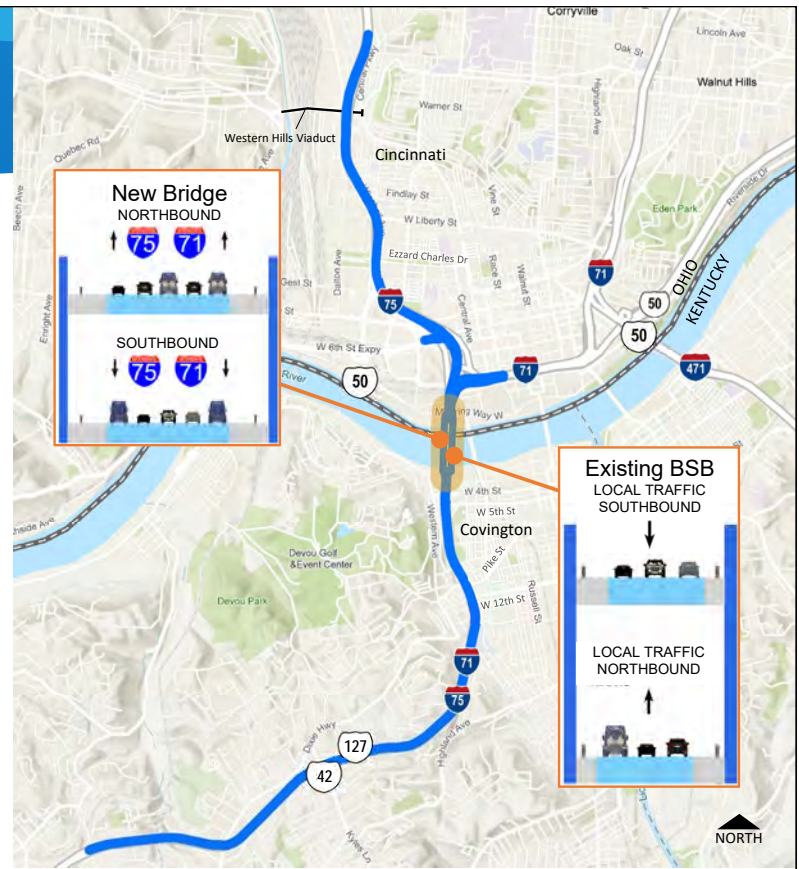
Note: Individual project elements will come in with click (marked by #).

In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. (#) Beginning near Ezzard Charles Drive, a collector-distributor system will be added to connect I-75 traffic to and from the local street network and US-50 West. (#) In the north, it will tie into the recently completed Mill Creek Expressway-Hopple Street Interchange project. (#) The project will rebuild the I-75 interchange at the Western Hills Viaduct and tie into the new bridge replacement project being developed by the City of Cincinnati and Hamilton County. (#) A new northbound exit will be built at Ezzard Charles Drive to improve access to Union Terminal, TQL Stadium, and Over-the-Rhine. Lastly, it will connect to I-71 and US-50 East.

Overview

Brent Spence Bridge

- New double-decker companion bridge
 - 5 lanes each deck
 - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
 - Three lanes each deck
 - Increased inside/outside shoulders
 - Carry local traffic



Note: Bridge details will come in with click (marked by #).

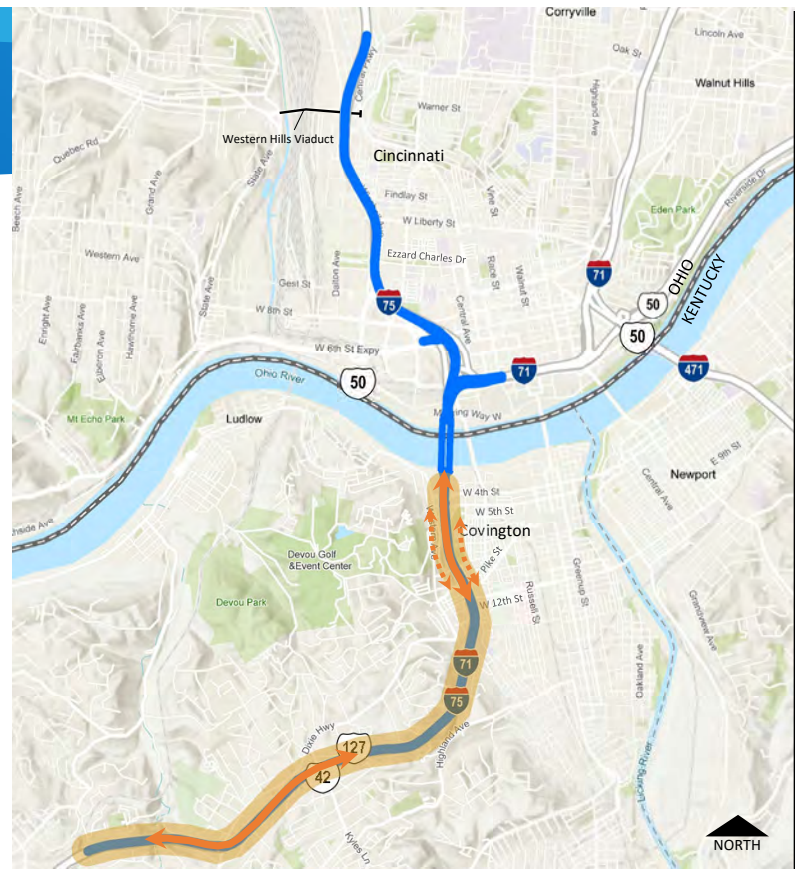
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

Overview

Kentucky

- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles Lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

Overview

Progressive Design-Build

- Selection based on qualifications and pricing approach
- Offsets construction market uncertainties
 - Inflation
 - Supply chain
 - Availability of materials
- Prices materials closer to construction
- Provides more opportunities for outreach
 - Local agency coordination
 - Traffic management plan
 - Maintenance of traffic
 - Incident management plan
 - Aesthetics

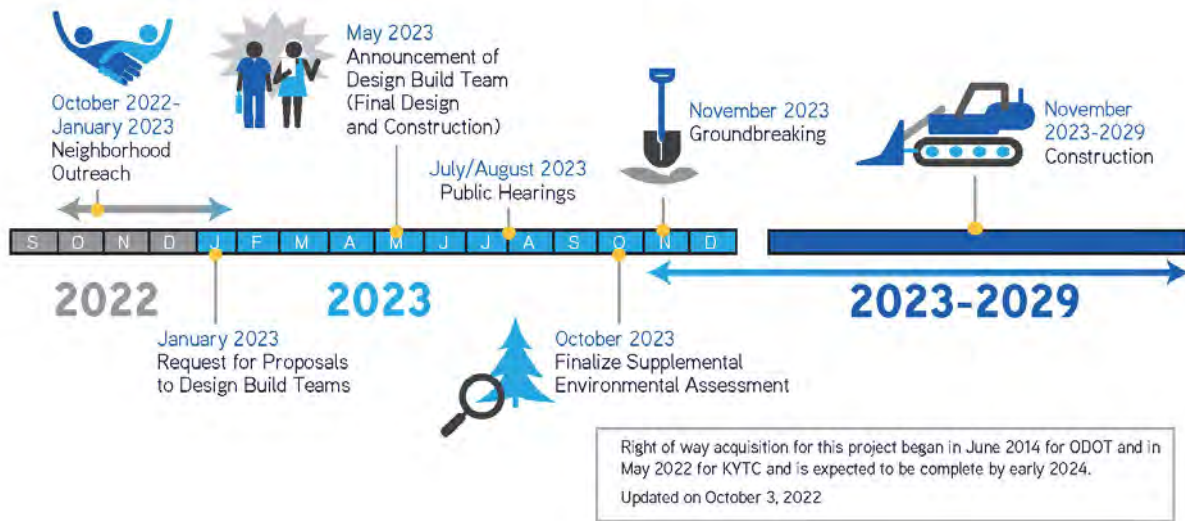
7

The decision to move forward with the Progressive Design Build Procurement will allow ODOT and KYTC to meet our goal of having a Design Build Team selected and project groundbreaking in 2023, with construction activities expected to begin in earnest in 2024.

The Design-Build Team will be selected based on qualifications while considering their pricing approach rather than qualifications and just a fixed or lowest bidder price. It also brings more certainty to the project delivery process and eliminates several uncertainties for those seeking to bid on the project, such as the effects of inflation, supply chains, and availability of materials. It also prices materials closer to construction allowing for more accurate cost estimates.

PDB allows the project team to work collaboratively with the Design Build Team to identify cost-effective solutions that meet the goals of the project and community. Opportunities will be available for local communities and agencies to provide feedback as the Project Team finalizes details for items such as traffic management, maintenance of traffic, incident management, aesthetic design treatments, streetscapes, landscapes, and gateways.

Schedule



This slide shows the project schedule from today to the completion of construction. The next meetings for the Project Advisory Committee and the full Aesthetic Committee are anticipated in July/August, before the public hearings.

Overview

Aesthetics committee

- Established in 2005
- Subcommittee of the Project Advisory Committee
- Provide local input on design and aesthetic appearance
 - Overall corridor
 - Main span of the new companion bridge
 - Rehabilitated Brent Spence Bridge
- Community and agency representatives
- Focus on pattern, color, texture, shape, lighting, and landscaping



BRENT SPENCE BRIDGE PROJECT AESTHETIC COMMITTEE CHARTER



The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are equally aware of the community's desire to provide for an aesthetically pleasing corridor through the City of Cincinnati, Ohio and Covington, Kentucky. Because ODOT and KYTC also believe that transportation projects can be attractive as well as safe and efficient, the Brent Spence Aesthetic Committee shall be established. The States are looking for a context sensitive solution that involves a collaborative, interdisciplinary approach in which citizens and agencies are part of the planning and design team. Context sensitive solutions ask questions first about the need and purpose of the transportation project, and then address equally safety, mobility, and preservation of scenic, aesthetic, historic, environmental, and other community values. The Aesthetic Committee is tasked to provide assistance to the transportation agencies and the project Advisory Committee in achieving a desirable result.

The Aesthetic Committee shall provide assistance and input on the project corridor's vision, and shall be guided by the following general tenets:

- The committee shall provide aesthetic guidelines and recommendations. Design decisions shall be made by the agencies and advisory committee.
- Decisions need to be financially feasible, and capable of being implemented.
- Safety shall not be compromised.
- All design standards with regards to lighting, signing, and geometry shall be followed.
- Bridge structure types will be selected in accordance with current ODOT and KYTC requirements. The Aesthetic Committee will provide input on the aesthetic treatments of the selected alternatives.
- Aesthetic treatments shall focus on pattern, color, texture, shape, lighting, and landscaping as opposed to adding extraneous elements solely for the sake of appearance.
- Funding considerations shall include initial costs and future maintenance costs.
- Aesthetic improvements can be achieved with minimal response in appropriated construction cost, typically a cost of 1% of the total construction cost is allowed for aesthetic treatment.
- The States shall have final authority over inclusion of aesthetic treatment for funding. Additional sources of funding may be developed by the committee to supplement the funding provided by the states.

Committee Membership and Roles:

The committee shall be made up of representatives from various community groups and organizations from both States. In addition, the Transportation agencies and the project consultant (Project Team) shall also be represented. The size of the committee is limited to twenty-five members to facilitate productive meetings. The membership list for this committee is attached. The Aesthetic Committee shall be a sub-committee to the project Advisory Committee. Because of this structure, the Aesthetic Committee shall be chaired by an individual representing one of the members of the Advisory Committee. The Project Team has selected The City of Cincinnati Architect to chair this committee. The Advisory Committee Membership list is also attached to this charter.

The Aesthetic Committee Members shall be responsible for developing the vision for the project and associated goals, developing methods to reach consensus on the aesthetic vision, provide recommendations to the project team on aesthetic treatments, communicate decisions back to their respective agencies/committees, and identify project issues and community values. The Project Team shall be responsible for developing the schedule for the project as well as determining specific goals for aesthetic committee input. The Project Team is also tasked with ensuring communication between the Aesthetic Committee and Advisory Committee as required. In addition, the Project Team will be responsible for documentation of meetings, recommendations, and decisions of all issues with respect to the committee and project.

Decision Making Process:

The Aesthetic Committee shall operate by consensus whenever possible. Consensus does not necessarily mean agreement or active support by each member. Those not objecting are not necessarily indicating that they favor a decision, but merely that they see "no better" in the absence of someone, a super majority of three-quarters of the members present is required for approval of an administrative recommendation. Participation in the aesthetic committee is limited to its members. All meetings are open to the public, and non-members shall attend as observers and may be invited to offer comments, if time allows. All actions and recommendations shall be taken by the Project Team to the Advisory Committee for concurrence. Final decisions on actions and recommendations shall be made by ODOT and KYTC.

In 2005, KYTC and ODOT instituted an Aesthetics Committee to provide local input on the design and aesthetic appearance of the corridor, the main span of the new Ohio River Bridge, and the rehabilitated Brent Spence Bridge structure. The aesthetic committee is comprised of community and agency representatives from Kentucky and Ohio to collaborate with the design team to develop context sensitive design solutions for the project. Aesthetic treatments shall focus on pattern, color, texture, shape, lighting, and landscaping as opposed to adding extraneous elements solely for the sake of appearance. These expectations were formalized in an Aesthetic Committee Charter.

History

- 6 meetings
 - December 16, 2005
 - August 29, 2006
 - September 25, 2009
 - January 29, 2010
 - April 15, 2010
 - September 20, 2010
- Survey
 - November 9, 2010
 - Design themes
 - Design elements



Between 2005 and 2010, there were six meetings of the aesthetics committee. In addition, the project team conducted a survey of the aesthetics committee to gather input on design themes and design elements for the corridor. However, participation was limited, and only three responses were received.

History

Aesthetic Design Guidelines

- February 2011
- Established aesthetic design goals
 - Minimize impacts on surrounding area and enhance economic development
 - Establish a new gateway between Kentucky and Ohio
 - Emphasize simplicity and clarity of design
 - Additional goals for design treatments
- “Rivers and Hills” corridor theme
- No decisions on design elements



11

The results of the collaboration with the aesthetics committee were documented in an Aesthetic Design Guidelines report in February 2011. The design guidelines established overarching aesthetic design goals for the corridor (see bullets). They also established goals for the design treatments such as creating a cohesive corridor, using simple and timeless designs, using metaphoric theme open to multiple interpretations, considering maintenance and resistance to build-up of dirt and salt in aesthetic design elements, and using green infrastructure for storm water remediation. The guidelines included general feedback and ideas for design elements, but no decisions or final recommendations were made regarding the structures themselves or their forms, sizes, materials, styles, textures, colors and finishes.

History

Finding of No Significant Impact (FONSI)

- August 9, 2012
- Two bridge types
 - Arch bridge | simply supported with inclined arch ribs
 - Cable-stayed bridge | two towers with vertical legs/towers
- Height requirements
 - At least 300 feet above the Ohio River
 - No higher than 420 feet above the Ohio River

City of Covington. Based on evaluation of comments received and coordination with the Cities of Cincinnati and Covington, Alternative I was refined.

Selected Alternative

The selected alternative is a revised version of EA Alternative I. Based upon public comments and coordination between the ODOT, KYTC and Cities of Cincinnati and Covington following the public hearing process, EA Alternative I was refined to include the following items:

- In Kentucky, the C-D roadway northbound and Jillians Way will be modified to include a new KY 5th Street exit ramp. The C-D roadway will be widened to accommodate the new exit ramp and a section of Jillians Way will be reduced in width due to the changes in traffic volumes. The proposed section of Jillians between KY 9th and KY 5th streets will be removed to accommodate the new exit ramp from the C-D Roadway.
- In Ohio, the exit ramp from the C-D roadway southbound will connect to the OH 3rd Street intersection at the north end of the Clay Wade Bailey Bridge.

Selected Ohio River Bridge

Based on the comparative analysis completed during the Bridge Type process with respect to construction cost, constructability, construction time, maintenance and durability, major rehabilitation feasibility, maintenance of traffic, and aesthetics and the public comments received as part of the public hearing process, two bridge alternatives can be considered as part of the Selected Alternative:

- Alternative 1, Arch Bridge: simply supported arch with inclined arch ribs
- Alternative 2, Cable-stayed Bridge: two towers, vertical legs/tower

Regardless of the bridge type that is ultimately selected for construction, the following requirements must be achieved to:

1. The highest point of the bridge shall be at least 300' +/- above the Normal Pool Elevation of the Ohio River (EL. 456.36').
2. The lowest point of the bridge shall be less than 420' +/- above the Normal Pool Elevation of the Ohio River (EL. 456.36').
3. The Minimum Provided Underclearance shall be no lower than that provided by the existing Brent Spence Bridge.
4. The bridge main span shall provide sufficient length to insure that substructure units are outside of the main span piers of the existing Brent Spence Bridge.
5. If a double deck design is provided, a twenty-five foot (25') minimum vertical clearance shall be provided above the bottom deck roadway surface.

The alternative in the EA identified as the Selected Alternative I satisfies the purpose and need of the project and is the Selected Alternative for the project. The Interchange Modification Study (IMS) is being amended to include these refinements. Construction of the Selected Alternative will impact the following:

- 32.06 total acres of new right-of-way including 54 displacements - 40 residential and 14 commercial. Alternative I in the EA required a total of 31.31 acres with the same displacements.
- Approximately 3,340 linear feet of intermittent streams, 1.28 acres of wetlands, and habitat for the Indiana bat and running buffalo clover. No change from EA impacts.
- One hazardous material site recommended for a Phase I Environmental Site Assessment (ESA) and 11 hazardous material sites recommended for Phase II ESA investigations. No change from EA impacts.
- Two National Register of Historic Places (NRHP) listed properties: Lorainburg Historic District in Kentucky and Longworth Hall in Ohio. No change from EA impacts.

Finding of No Significant Impact for The Brent Bridge Replacement/Rehabilitation Project.
ODOT/IMP 10/15/12/KYTC Project Item No. 6-17

3

KYTC and ODOT prepared an Environmental Assessment for the project, which culminated in a Finding of No Significant Impact, which was signed by FHWA on August 9, 2012. The FONSI included specific requirements for the bridge type – a simply supported arch bridge with inclined ribs or a two-tower cable-stayed bridge with vertical legs/towers. The bridge also had to be at least 300 ft above the Normal Pool Elevation of the Ohio River and less than 420 ft above the normal pool elevation of the Ohio River.

History

Cable-stayed (2012 rendering)



13

Rendering from the March 2011 Bridge Type Selection Report showing a two-tower cable-stayed bridge with vertical legs/towers.

History

Tied arch (2012 rendering)



14

Rendering from the March 2011 Bridge Type Selection Report showing a tied arch bridge simply supported with inclined ribs.

Recent Activities

- Full aesthetics committee
 - New companion bridge
 - Existing Brent Spence Bridge
 - Corridor-wide
- Aesthetics subcommittees for each state
 - Moved away from “Rivers and Hills”
 - Ohio River serves as a boundary
 - Separate aesthetic treatments in each state



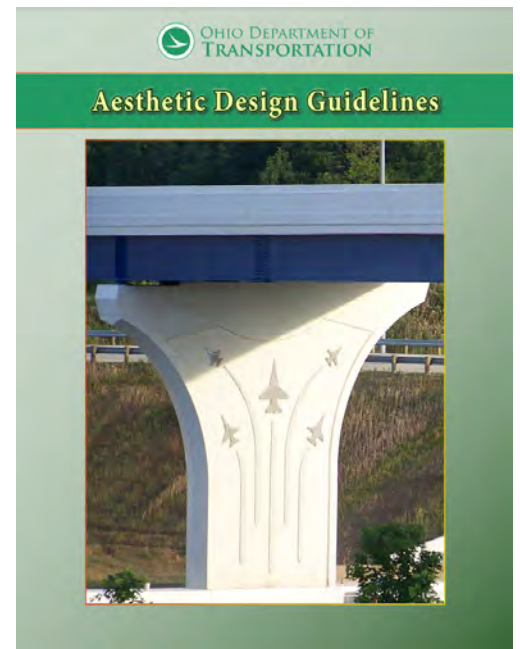
15

The full aesthetics committee will focus primarily on the new companion bridge and the existing Brent Spence Bridge and any corridor-wide aesthetics details. Today, there is not a continuing interest in the “Rivers and Hills” corridor theme, and both states have moved away from this theme. It has been determined that the Ohio River services as a boundary that would allow each state to work independently with subcommittees to develop aesthetic plans. However, the states will be coordinate their activities and be cognizant of each other to make sure that aesthetic treatments in the corridor do not clash.

Recent Activities

Ohio subcommittee

- June 29, 2022 | Phases I and II
- January 31, 2022 | Phase III Ohio approaches
- Following ODOT's Aesthetic Design Guidelines
- Developed Aesthetics Design Checklist



ODOT met with the Ohio subcommittee in June 2022 to finalize plans for Phases I and II of the project. The Ohio subcommittee will meet later this afternoon to discuss Phase III. ODOT is following its statewide Aesthetics Design Guidelines, which were developed after 2012. It involves completing an Aesthetics Design Checklist that defines the standard and enhanced aesthetic treatments that will be incorporated into the project. ODOT has also closely coordinated the Ohio aesthetics with the City of Cincinnati, and has incorporated their feedback into the checklist.

Recent Activities

Kentucky subcommittees

- June 28, 2022 | Covington
- August 10, 2022 | Ft. Wright, Ft. Mitchell, Kenton County
- August 10, 2022 | Covington
- October 20, 2022 | Covington
- November 16, 2022 | Ft. Wright, Ft. Mitchell, Kenton County



KYTC met with the different subcommittees focused on Covington, Ft. Wright, and Ft. Mitchell in June, August, October, and November. The City of Covington also led a meeting in November to provide ideas and perspectives. Although Ft. Wright and Ft. Mitchell were not original members of the aesthetic committee, KYTC has been engaging them through the subcommittee process. KYTC has retained consultants (Qk4 and Human Nature) to help develop the aesthetic plans for Kentucky.

Recent Activities

On-going aesthetic subcommittee decisions

- Aesthetic treatments
 - Piers
 - Abutments
 - Retaining walls
 - Noise walls
- Lighting
- Streetscapes
- Landscapes
- City gateways



Above are design elements in the corridor that will be addressed primarily through aesthetic subcommittees as we move through the progressive design-build phase. In Ohio, most of these decisions for Phases I and II have been made. Many decisions on Phase III will be discussed this afternoon, although coordination will be on-going throughout the design process. Kentucky will continue to work with subcommittees to develop aesthetic plans as the project progresses through the design-build process.

Ohio River Bridges

- Elevations
 - Set requirement based on viewshed
 - 300 to 420 feet above the Ohio River
- Bridge type
 - Flexibility desired
 - Arch
 - Remove requirement for simply supported with inclined ribs
 - Cable-stayed
 - Remove two-tower, vertical legs requirement
 - Documented in Supplemental EA and updated FONSI



19

The design team is interested in opening up some of the decisions about the new companion bridge. The required elevations are set at no less than 300 ft and no more than 420 ft above the river. The minimum elevation was set to ensure the new bridge could be better seen because the existing bridge will remain. The maximum elevation was set due to viewshed issues in historic areas. These will not change. However, more flexibility is required for the bridge type to allow the design-team to pursue innovative and cost-effective designs to the greatest extent possible. For the arch bridge, the design team would like to remove the requirements for it to be simply supported with inclined ribs and just state that it will be an “arch bridge.” For the cable-stayed bridge, the design team would like to remove the requirements for two vertical towers and just state that it will be a “cable-stayed” bridge. ODOT and KYTC will still work with the design-build team to ensure an iconic, aesthetically pleasing bridge is ultimately built.

The design team would like to hear your feedback on revising the bridge types. ODOT and KYTC are currently preparing a Supplemental Environmental Assessment for the project, which will culminate in an updated FONSI. If the aesthetic committee agrees with these changes, they will be reflected in those documents, which will set the requirements for the design-build teams.

Ohio River Bridges

Future aesthetics decisions

- Lighting
 - Up-lighting only
 - No down-lighting due to navigation channel
 - Coordination between new companion and existing Brent Spence bridges
- Bridge color
- Tower/pier texture and color
- Cables (for cable-stayed option)
 - White due to design considerations
- Railing
- Fence
- Roadway and aesthetic lighting
- State gateways



20

Above are design elements that will be open for input from the aesthetics committee as we move through the progressive design-build phase. Decisions on these items will be deferred until the design-build team is selected, the decision about the bridge type has been made, and the design has progressed sufficiently to begin making decisions. Although, the design team would like to hear any preliminary thoughts you have on those items today.



Before wrapping up, here's a rendering of what the project might look like with an arch bridge.



Comments + Input

First, the design team would like to ask if you have any comments or concerns with revising the FONSI bridge types. Another goal is to hear any preliminary input on other aesthetic design elements.

BRENT SPENCE
BRIDGE CORRIDOR



THANK YOU!

BRENTSPENCEBRIDGECORRIDOR.COM





Attachment 4: Comments

From: Dave Okum

Sent: Saturday, March 4, 2023 4:31:58 PM

To: Mark Becherer

Subject: RE: Brent Spence Bridge Corridor Aesthetic Committee Meeting

Mark thanks for taking time to listen to my comments at the meeting in January regarding the view of the bridge from multiple points. Possibly a view shot from Covington Landing and our Riverfront might

Be a better perspective.

I have provided two photos from the web that represent concepts that might be considered by the aesthetics and design committee for our new bridge which are statement structures.

Both of the designs below brings into context the existing construction of the Brent Spence bridge structure

and integrates our

[John A. Roebling Suspension Bridge](#) the ICON of Cincinnati.

I feel the arch is an odd contrast to our existing bridge. The two column suspension is quite plain and do not make

an entry to our communities statement.





Respectfully,

David Okum

Hamilton County Regional Planning Commission representative.

**Brent Spence Bridge Corridor Project
Meeting Summary - Ohio Subcommittee
January 31, 2023**



Brent Spence Bridge Corridor Project

Ohio Aesthetics Subcommittee

Meeting Summary

January 31, 2023

Introduction

The Brent Spence Bridge (BSB) Corridor Ohio Aesthetic Subcommittee meeting was held on January 31, 2023 from 1:00 pm to 2:00 pm. The meeting was held at the Ohio Department of Transportation (ODOT) ARTIMIS building located at 508 West Third Street. Invitations to the meeting were sent to PAC members via email on January 6, 2023 (see Attachment 1). Attendees at the meeting included Aesthetics Subcommittee members and the project team. A list of attendees is included in Attachment 2.

Presentation

The meeting format included a formal presentation by Mark Becherer and Jodi Heflin (HNTB). The project team responded to questions posed by attendees both during and after the presentation. A summary of the presentation is provided below. A copy of the presentation is included in Attachment 3.

1. Introductions
 - a. Mark Becherer (HNTB) opened the meeting, and attendees shared their names and the organization they represent.
2. Project Overview
 - a. Overview of key project elements in Ohio and Kentucky, including the existing BSB and the new companion bridge.
 - b. The project will be constructed in three phases. Phases I and II (northern two miles in Ohio) are traditional design-bid-build projects. Phase III, which includes the new companion bridge, is following a progressive design-build process. The Ohio Subcommittee previously met for Phases I and II. This meeting is for the portions of Phase III in Ohio.
 - c. Overview of progressive design-build.
 - d. A draft RFP for design-build teams was released in January. The final RFP will be released in February. The design-build team will be announced in May. Public hearings will be held in July/August, and the supplemental Environmental Assessment will be finalized in October. Groundbreaking on Phase III will occur in late 2023, with substantial construction beginning in 2024 and substantial completion estimated for 2029.



3. Aesthetics Design Checklist

- a. An overview of the aesthetics checklist and the difference between the baseline treatments and enhanced treatments was shared. Baseline treatments match ODOT standards. Enhanced treatments are incorporated based on input from stakeholders, like this committee.
- b. The project team stated that they met with city staff in November, went over aesthetic features of the corridor, and developed a draft aesthetics design checklist. Comments were received from the City and incorporated into the checklist being presented at this meeting.
- c. The presenters stated that this meeting was to go over that revised checklist and that many of these items presented are the basic standards and features, but where enhancements are inserted they would be gone over in more detail.
- d. The proposed translucent screen walls were described and highlighted.
- e. The proposed planters, lighting post, and fixtures were noted. The City might provide more options utilizing their own funding sources.
- f. Trees are not being recommend for bridges due to maintenance issues.
- g. The lighting and fence posts on the overpass bridges will be Cincinnati standards.
- h. The Fifth Street Bridge will not have pedestrian facilities.
- i. All of the other bridges will have a 10-foot shared use path with buffers to provide protection for pedestrians.
- j. The longitudinal parapets will use Texas classic form liner.
- k. Fencing on the bridges will have translucent lighted walls and not the basic fencing.
- l. The color of the steel girders under the bridges will be painted to match the Hopple Street Bridge.
- m. The concrete girders will have a beige color.
- n. Bridge abutments and vertical walls will have an ashlar stone pattern.
- o. Piers will have the standard tapered end caps with round columns.
- p. A rendering of the Findlay Street underpass was shown as example of the form liners with ashlar stone pattern, underpass lining, and vertical abutments.
- q. Street lights will follow Cincinnati standards for local streets.
- r. Lighting on the bridges will be placed close to the curb and will be the same as existing fixtures.
- s. Traffic signals will match Cincinnati standards.
- t. The ashlar stone pattern will be used on all form liners and retaining walls.
- u. For landscaping, ODOT will provide the planters and mulch. The City will be responsible for planting and ongoing maintenance.
- v. Areas of significant reclaimed land, approximately 9.5 acres, will be opened up for potential redevelopment. There are no plans for special landscaping or features in these areas. They will be seeded and mulched only.
- w. Highway and wayfinding signage will be replaced in accordance with Cincinnati standards if impacted by the project.



-
- x. Local sidewalks will be five feet in width, and all trees will meet Cincinnati standards as shown on an example rendering of Ezzard Charles Drive (Ezzard Charles Drive is not in the project area).

Discussion

The following comments and input were provided during and after the presentation:

- The project team went through the design elements (PowerPoint slides) on a topic by topic basis to ensure comments on all design elements were considered.
- Bridge elements
 - The examples of existing translucent fences have white lighting. Can this lighting color be modified or have changing colors?
 - Stefan Spinosa from ODOT stated that these were enhanced treatments. Extra costs would be the responsibility of the City of Cincinnati.
 - Can both versions be bid at the same time to see the cost difference?
 - For the design-bid-build contracts, both options can be bid. For the progressive design-bid contract, ODOT can work with the design-build team to get a price for each.
 - A comment from the City was made that this would help them work through projected costs to see what versions might be supported.
 - Can the colors of the lights be changed via a program or software?
 - Lighting can be programmable and changed, and the City will have maintenance responsibilities. The City and ODOT will need to establish guidelines/agreements regarding lighting to maintain safety along the corridor (i.e., flashing lights could be a distraction and safety concern on I-75).
- Planters
 - The Cincinnati Parks Board commented that the planters on bridges need to provide provisions for watering. They either need irrigation or access/place to have employees safely water the planters.
 - The planters will have irrigation built into the design.
- Longitudinal and parapet treatments
 - No comments.
- Beams and girder
 - Does the paint color on steel beams under the overpasses have to be the same as Hopple Street? Can ODOT provide a specific color name or number?
 - The final decision about color can be made during the design phase and in consultation with the City. Darker colors make inspection more difficult, because it is more difficult to



see cracks. Color options can be explored further once the determination is made about whether the bridges will be steel or concrete.

- Ashlar Stone on Abutments
 - No comments.
- Piers
 - No comments.
- Lighting
 - Stefan reiterated that the local city streets and overpasses will use the City's local street standards and example masts and fixtures from the presentation. Lighting fixtures and masts within the highway right-of-way will be ODOT standard lighting types.
- Signals, walls and barriers,
 - No comments.
- Landscaping
 - A question was asked if planters can be selected by the City or if there was room for discussion on this design?
 - There is flexibility in the design for the planters, but decisions would have to be made quickly for the northern two sections of the project as these are currently in design.
 - The width of the planters is set because of the need to provide buffers, but the shape and length could have some flexibility. Also, planters will not be movable. They are integrated into the structure in order to have the irrigation system, and they cannot be swapped out at a later date.
 - There is not a lot of information yet, but future designs may accommodate small areas of additional landscaping in later phases. If new areas are discovered, the project team will work with this subcommittee on more detailed plans.
- Wayfinding or other signage
 - No comments.
- Sidewalk buffers
 - No comments.
- Schedule
 - The materials from the meeting will be distributed to the attendees, and attendees will have 30 days to provide any additional comments to the project team. The group was asked if they had any comments regarding the schedule. No comments were offered.
- Stefan stated that the project team will continue to engage this group and will coordinate any changes through the Aesthetics Committee.



-
- Members of the committee asked for a digital copy of the presentation from the meeting, A copy will be forwarded along with the meeting summary.

Next Steps

- The project team will distribute a summary of this meeting.
- Subcommittee members will have 30 days to provide comments back to ODOT.
- Once the comment period is over, ODOT will finalize the Aesthetic Design Checklist.

Comments

The following comments were received during the 30-day comment period (see Attachment 4):

1. Comment:

The City of Cincinnati DOTE has one point of clarification regarding lighting. Central Ave. and east of Central Ave. have our “CBD Lighting Standard” which is different than our “Standard Roadway Lighting”. This PID includes both types of fixtures. On the checklist Lighting Treatments: Replace CBD and Non-CBD STREET LIGHTING section to:

- Standard roadway lighting: City standard tapered steel pole with bracket arm and LED fixture.
- CBD lighting: East of and including Central Avenue (also includes 3rd Street east of Clay Wade Baily Bridge) as shown in Exhibit A.

Poles, arms, bases, fixtures, and other incidental attachments to street lighting shall be black in color.

Response:

The Aesthetic Design Checklist was revised as requested.



Attachment 1: Invitations

From: Mark Becherer
Sent: Thursday, January 5, 2023 2:17 PM
To: [REDACTED]
Cc: [REDACTED]
Subject: Brent Spence Bridge Corridor Ohio Aesthetic Committee Meeting

All – Thank you for agreeing to join or continue serving as a member of the Brent Spence Bridge Corridor Aesthetic Committee. As the project continues to move forward; most recently with the \$1.6B grant award; we look forward to reconvening this committee to provide assistance and input on the project corridor’s vision. The overall goal of the aesthetic committee is to provide recommendations to the project team on aesthetic treatments/vision and to communicate decisions back to their respective agencies and constituents.

The overall committee includes all on this email. Subcommittees; as needed; will also be formed. The first subcommittee formed is for the Ohio corridor and approaches. In the next day or two you will be receiving a hold the date invite or invites to the entire committee meeting, and for those involved an Ohio sub-committee meeting. The corridor wide meeting has both in-person and virtual options. The Ohio sub-committee meeting is planned to be in person, with a virtual option also being provided if needed. We are currently looking at the morning of January 31 for the overall committee meeting and the afternoon of the same day for the Ohio subcommittee meeting. More details; including a draft agenda; will be in the invites.

Don’t hesitate to give me a call or email if questions. We look forward to continue partnering with you in developing and delivering this exciting investment in Southwest Ohio and Northern Kentucky.

Thanks.

[Mark Becherer](#)
Vice President

HNTB CORPORATION

 100+ YEARS OF INFRASTRUCTURE SOLUTIONS

Subject: Brent Spence Bridge Corridor Ohio Aesthetic Subcommittee Meeting
Location: ODOT Artimus Building 508 W 3rd Street Cincinnati, Oh

Start: Tue 1/31/2023 1:00 PM
End: Tue 1/31/2023 2:00 PM

Recurrence: (none)

Meeting Status: Accepted

Organizer: Mark Becherer

Required Attendees

Optional Attendees

All – As per the email you received yesterday this is an invite to the Ohio subcommittee aesthetic committee meeting for the Brent Spence Bridge. The meeting is being offered both in person (project team’s preference) and virtual. Again, thank you for agreeing to participate on this committee. A draft agenda is attached.

Let me know if any questions.

Thanks.

Mark Becherer
Vice President
Cell (414)559.8900

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Attachment 2: Attendees

In-person attendees:

Jodi Heflin, HNTB

Mark Becherer, HNTB

Stefan Spinosa, ODOT

Tom Arnold, ODOT

Adam Johnson, FHWA

Jocelyn Hinshaw, American Structurepoint

Matthew O'Rourke, American Structurepoint

Stacey Hans, KYTC

Katie Eagan, KYTC

Mimi Rasor, Rasor Communications

Amy Clay, Rasor Communications

Virtual attendees (not also in-person):

Angie Strunc, City of Cincinnati Department of Transportation & Engineering

Tammy Campbell, ODOT

Joel Gross, Cincinnati Park Board

Jain Krutarth, Champlin Architecture

Timothy Long, FHWA

Michael Loyselle, FHWA

Scott Stone, FHWA,

Heather McColeman, ODOT

John Seibert, University of Cincinnati

Keith Smith, ODOT

Joseph Smithson, ODOT



Attachment 3: Presentation

BRENT SPENCE
BRIDGE CORRIDOR



OHIO AESTHETICS SUBCOMMITTEE MEETING
BRENT SPENCE BRIDGE CORRIDOR PHASE III
OHIO APPROACHES

January 31, 2023



INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.
brentspencebridgecorridor.com



Agenda

- Introductions
- Aesthetics committee
- Aesthetics process
- Aesthetics design checklist review
- Schedule
- Comments and input



2

Today we will:

- Review the purpose of the aesthetic committee
- Describe the aesthetic process
- Review the project aesthetic design checklist
- Review the schedule
- Provide time for input and comments

BSB Corridor Aesthetic Committee

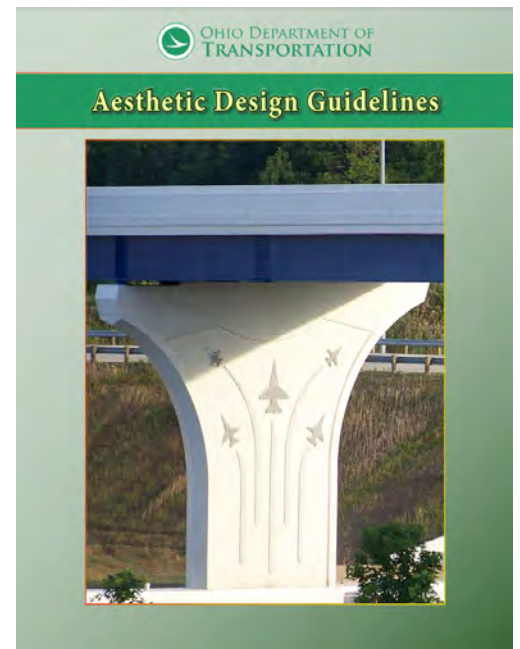
- Project aesthetic committee formed in 2005
- Assist KYTC and ODOT with development of Aesthetic Design Guidelines (2011)
- Provide input on aesthetic design elements
- Ohio subcommittee



- The project aesthetic committee was formed to provide assistance on the project corridor's vision.
- An Aesthetic Charter was written that detailed the framework and process at the time for this project.
- During preliminary engineering, the effort included input on the river crossing bridge type selection as well as general thoughts on the corridor as a whole.
- One of the commitments we made was that as we move into further design and eventually into construction, we would re-engage the committee to continue the earlier collaboration. We are doing this through a subcommittee focused specifically on Ohio.
- As mentioned earlier, we've already met to discuss details of the project from Linn St. through the WHV interchange. This meeting will focus on the remainder of the project in Ohio.
- Previous efforts by the committee identified patterns, color, and texture elements similar to what has been constructed by other projects along I-75 in recent years.
- We've met with City staff to get ideas to refine the previous discussion. We are here today to share the current thoughts we have and to get feedback

Aesthetic Process

- Following ODOT's aesthetic design process in Ohio
- Baseline verses enhanced treatments
- Aesthetic Design Checklist



- Since the completion of the aesthetic effort as part of the Brent Spence Bridge Corridor Project preliminary engineering, ODOT has developed and put into use an aesthetic design process. This process mimics what the original project charter included. It also provides more direction on what part of the project costs that ODOT typically covers - called "baseline treatments" - and how we will review, develop, and implement enhanced treatments.
- Baseline treatments include standard ODOT engineering and construction specifications. They typically consist of pattern, color, and texture treatments.
- Enhanced treatments are usually incorporated into ODOT projects through stakeholder/public involvement efforts like we are doing for this project. Aesthetic enhancements are implemented in addition to (or in place of) baseline treatments.
- The aesthetic design checklist will aid ODOT in finalizing the design requirements for the Phase III of the BSB Corridor Project.

Aesthetic Design Checklist

- Initial coordination with City staff
- Draft Aesthetic Design Checklist
- Revised to reflect City feedback
- Ohio aesthetic subcommittee feedback

Ohio Department of Transportation
Aesthetic Design Guidelines | 2016

AESTHETIC DESIGN CHECKLIST
(to be completed by Design Team)

<input type="checkbox"/> Preliminary Engineering (PE) Phase		<input checked="" type="checkbox"/> Environmental Engineering (EE) Phase		<input type="checkbox"/> Final Engineering (FE) Phase	
Project Name:	Brent Spence Bridge	ODOT PM:	Stefan Spinosa		
ODOT CRS and PID:	Brent Spence Bridge Corridor PID 116549	Consultant PM:	Mark Becherer		
District:	8	Local Agency PM:	Bryan Williams		
County / Municipality:	City of Cincinnati				
Project Description:	Ohio portion of Phase III of the BSB Corridor Project that includes the following: <ul style="list-style-type: none"> • Reconstructing and widening of approximately 1 mile of I-75 and connectors to I-71 and US-50 from the Ohio River to Linn Street; • Building a new I-75 exit at Ezzard Charles Drive; • Constructing a C-D system to connect I-71/I-75 traffic to and from the local street network between the Ohio River and Ezzard Charles Drive in Ohio; and • Connecting interstate improvements and the C-D system to the existing Brent Spence Bridge and the proposed Companion Bridge over the Ohio River. 				
Anticipated PDP Path:	5	Anticipated CE Level:	EA/FONSI		
Is the project exempt from the aesthetic design process? (see Aesthetic Strategy Checklist) <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No					
If "Yes", STOP. Completion of this form is not required. If "No", proceed to next section.					
What types of aesthetic treatments are included in the plans? Check both boxes if applicable.					<input checked="" type="checkbox"/> Baseline <input checked="" type="checkbox"/> Enhanced
Summarize how was this determined (include meeting dates and reference applicable agency correspondence): Aesthetic treatments determined through ongoing coordination and meetings between consultant, ODOT, City of Cincinnati, and the Aesthetic Committee.					
Is there a specific aesthetic theme or corridor vision that is being followed? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No					
If "Yes", summarize what it is and how it was determined: An original concept of Rivers and Hills was identified during preliminary engineering/NEPA phase, see Aesthetic Report completed under PID 75119. This concept was removed from consideration as part of the 2022 Aesthetic Committee coordination for PIDs 113361 and 114161.					

* NOTE: The next sections summarize proposed aesthetic treatments for the project. In some cases (particularly Stage I plans), a proposed aesthetic treatment may not be shown in the current plan set but will be included in a future set.

- ODOT conducted some initial coordination with the City to develop a draft Aesthetic Design Checklist.
- The City provided review comments, and the checklist was revised to address their comments.
- The checklist focuses on providing similar patterns, colors and textures already constructed along the I-75 corridor.
- The checklist prepared for the first mile in Ohio is similar to and coordinated with those completed for the other areas of the BSB in Ohio.
- All the treatments selected in the initial aesthetic design checklist are either ODOT baseline or City standard baseline treatments
- The final Aesthetic Design Checklist will incorporate feedback from this subcommittee and will set the aesthetic elements to be included in the final design for Phase III of the project in Ohio.

Checklist Review – Bridge

Bridge Treatments

Aesthetic Element	Proposed Treatment			Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Vandal Fencing	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Pedestrian Railing	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Parapets	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Deck Fascia	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Beams/Girders	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Abutments	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Pier Caps	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Pier Columns	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Drainage/Scuppers	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Utilities	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	

6

For bridges, proposed alternative baseline treatments will be provided for vandal fencing, pedestrian railing, parapets, and additional multimodal and aesthetic features.

Checklist Review – Bridge

- Translucent screen wall vandal fence
 - Also serve as pedestrian railing
- Planters (trees not recommended)
- Post lighting (placement)



The 6th Street, 7th Street, and 9th Street bridges will include a translucent screen wall with interior lighting for the vandal protection fencing similar to the Spring Street Bridge in Columbus. As an enhancement, the City of Cincinnati may include colored and/or integral graphic panels. If these are included, they will be considered enhanced with funding from the City. Pedestrian railing along local streets will match City of Cincinnati standard pedestrian railing. In locations with translucent screen walls, they will serve as the pedestrian railing.

A photo of the Spring Street Bridge in Columbus is shown as an example. At this time, trees are not recommended because of maintenance issues associated with trees on bridges. Instead, we are envisioning landscaping similar to what is currently on the bridges over Fort Washington Way. In addition, the post lighting will be similarly placed, but the fixtures will match Cincinnati standards, which are shown on another slide.

Checklist Review – Bridge

- Shared-use path
- Buffers
- Planters
- Lighting



6th Street



7th Street



9th Street



This is a view of what Findlay Street will look like when the project is built. Although this particular location is in Phase I, it shows what bridge walls will look like with the ashlar stone treatment and what the bridge parapets will look like with the Texas Classic Rail relief. The abutments will be square, not sloped. Also notice the rebuilt sidewalks and underpass lighting.

Checklist Review – Bridge

- Longitudinal parapets



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Longitudinal bridge parapets will include a 'Texas Classic Rail' form liner on the outside and inside faces where pedestrian sidewalk is present. This is a similar treatment to what is used throughout the I-75 corridor.

Checklist Review – Bridge

- Parapet end treatments



Overhead bridge parapets will include end treatments with the bridge identification name, construction completion data and rustification design features. The City will assist in determining the final design of the parapet end treatments.

Checklist Review – Bridge

- Beams and girders



Steel girder example



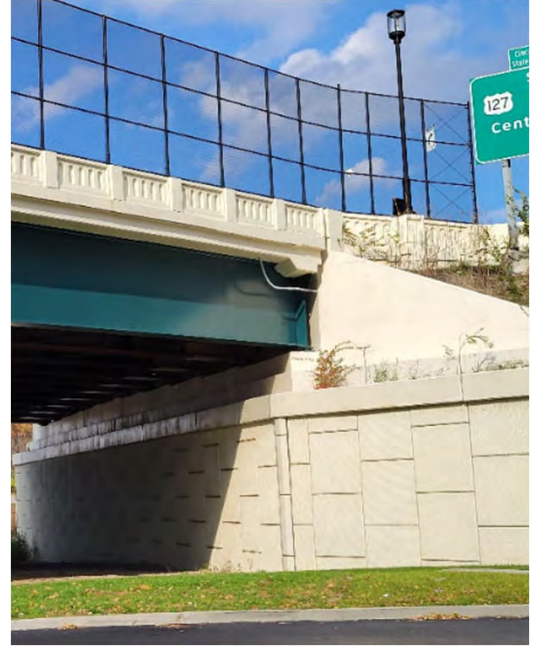
Concrete girder example

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- The project will use both steel and concrete girder bridges.
- Steel girders will be painted to match the color of the Hopple Street and Monmouth Street bridges as shown on the left.
- Concrete girders will include a Federal standard concrete sealer color similar to what is shown on the right.
- Translucent screening will be installed instead of chain link fencing.

Checklist Review – Bridge

- Abutments



Bridge abutments will include a concrete form liner with an Ashlar Stone, a pattern similar to the texture shown. Abutments will include vertical walls instead of sloping embankments.

Checklist Review – Bridge

- Piers



Piers will be standard cap and column type piers with tapered end caps and round columns.

Checklist Review - Lighting

Lighting Treatments

Aesthetic Element	Proposed Treatment			Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Highway Lighting	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Interchange Lighting	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Street Lighting	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Bridge Lighting	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	

15

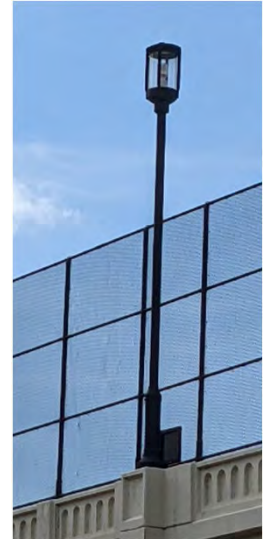
- Highway and Interchange lighting will be ODOT standard for the I-75 corridor
- Local street lighting will match City of Cincinnati standards
- Underpass lighting will meet City of Cincinnati standards

Checklist Review – Lighting

- Street and bridge lighting



Street lighting example



Bridge lighting example

16

- Local street lighting will match City of Cincinnati steel roadway tapered mast arm with LED fixture and bridge mounted straight steel pole post with Gardco LED fixture.
- Bridge post lighting will be placed close to the curb as shown on the bridge slides. The bridge fixture will Sun Valley to match other overpasses on I-75 corridor.
- Poles, arms, bases, fixtures, and other incidental attachments to lighting will be black.

Checklist Review – Signals

Traffic Signal Treatments

Aesthetic Element	Proposed Treatment			Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Poles/Mast Arms	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Signal Heads	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	

Poles, mast arms, and signal heads will match City of Cincinnati standards.

Checklist Review – Walls/Barriers

Retaining Wall, Noise Wall, and Longitudinal Barrier Treatments

Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Retaining Walls	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	Ashlar Stone Pattern
Concrete Shoulder Barrier	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Concrete Median Barrier	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Guardrail	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Noise Wall Panels	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Noise Wall Panel Caps	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Noise Wall Posts	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	

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Concrete shoulder and median barrier will be standard ODOT barriers. Retaining walls will have the Ashlar Stone pattern discussed earlier for bridges.

Checklist Review – Landscape

Landscaping Treatments

Aesthetic Element	Proposed Treatment			Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Woody Plantings	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Seed Mixes	<input checked="" type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline	<input type="checkbox"/> Enhanced	<input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	



19

The project will provide City of Cincinnati standard planting soil and mulch in the planters for future use by the City of Cincinnati for plantings. The City will also maintain planters.



The City does not anticipate additional landscape improvements for the interim condition in areas of significant reclaimed land (such as between West 3rd St. and West 6th St, which is shown in the upper middle portion of this rendering.

Checklist Review – Signage

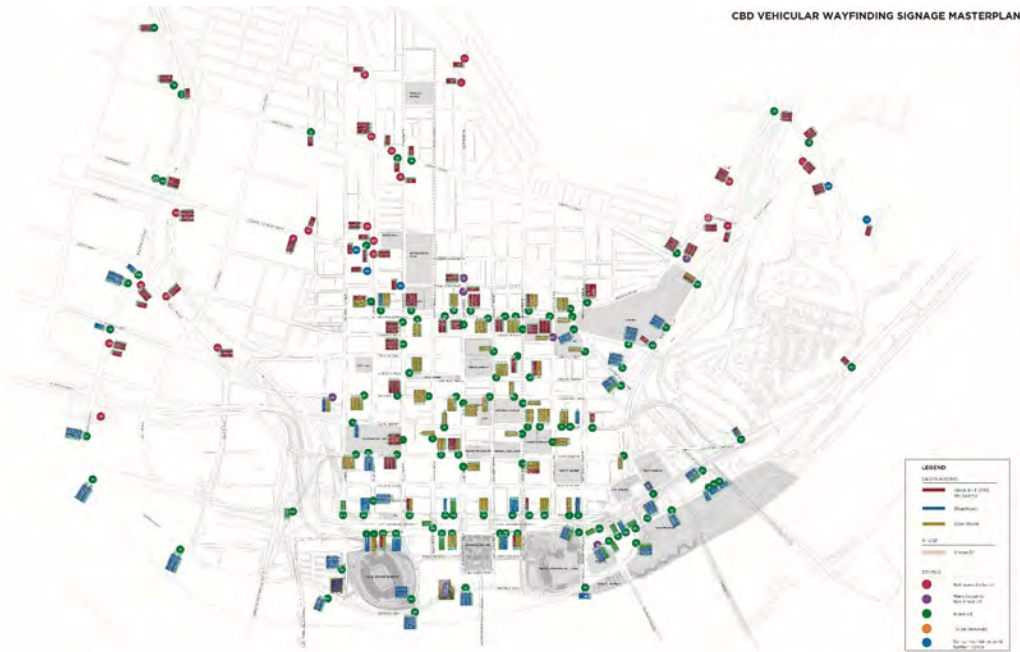
Signage Treatments

Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Highway Signage	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Community Signage	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	

21

- Existing City of Cincinnati wayfinding signs impacted by the project will be reinstalled per City of Cincinnati Standards unless it conflicts with the OMUTCD.
- The project will include the salvage, installation, and/or replacement in-kind of wayfinding signs impacted, either physically or by changes in circulation pattern.
- Further coordination with the City is required to finalize wayfinding sign accommodations.

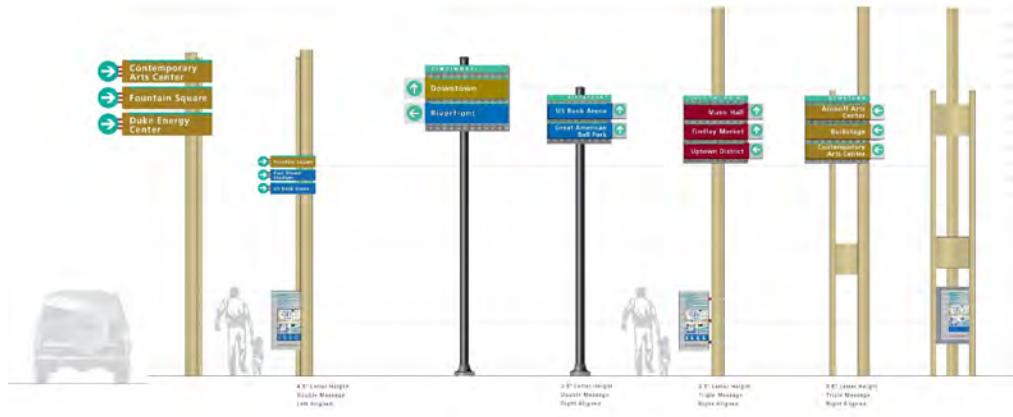
Checklist Review – Signage



22

The city provided masterplan mapping of City wayfinding for determination of signs and/or routes that will be affected, as well as a list of existing community signs that will need to be relocated. Only those impacted by the BSB project are covered in this effort.

Checklist Review – Signage



The city provided details for wayfinding signs that may need to be relocated.

Checklist Review – Signage



Examples of wayfinding signs that may be relocated by the project.

Checklist Review – Roadway & Sidewalk

Roadway/Sidewalk Treatments

Aesthetic Element	Proposed Treatment	Alternate Baseline?	Alternate Bid Item?	List Plan Sheets (Alternative Baseline or Enhancements)
Right-of-Way Fencing	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Sidewalks	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Buffers (Tree Lawns)	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Utilities	<input checked="" type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Crosswalks	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Medians	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Islands	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes	
Other:	<input type="checkbox"/> Baseline <input type="checkbox"/> Enhanced <input type="checkbox"/> N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes	

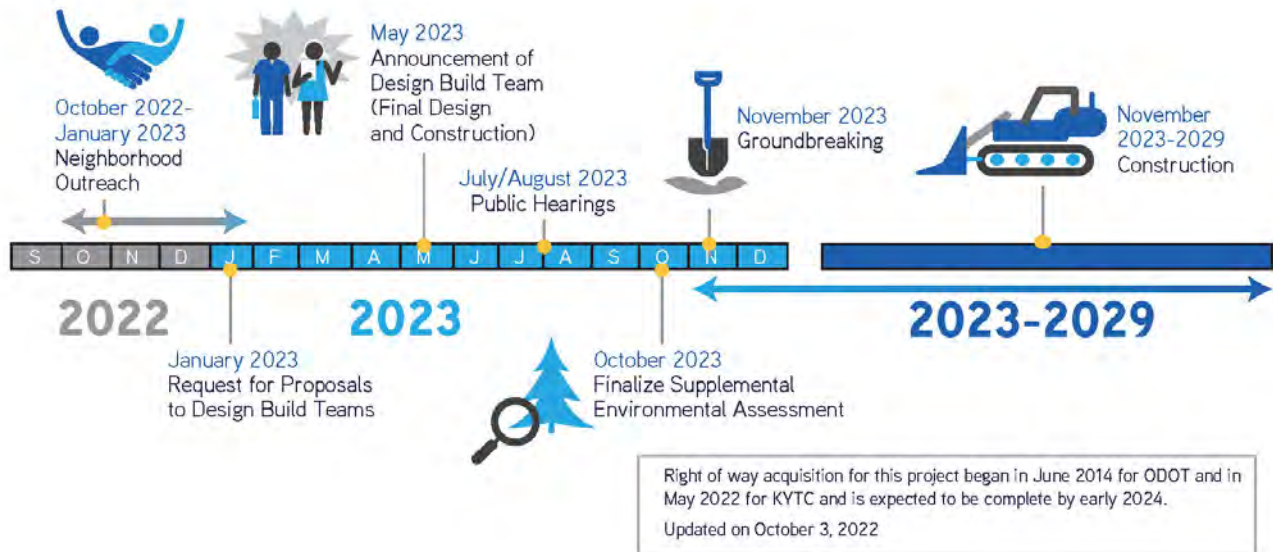
25

- Local street sidewalks will be 5'-0" minimum width and 5" depth concrete sidewalks.
- 4'-5" width tree lawns will be provided along local street sidewalks.
- Crosswalks, medians, and islands will meet City of Cincinnati standards.



- This view shows what Ezzard Charles Drive will look like. Although it is in Phase II, notice the planters, screen wall, and decorative lighting on the bridge.
- The bridge lights and traffic signal supports will be black and meet City standards.

Schedule



This slide shows the project schedule.



Comments + Input

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THANK YOU!

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Attachment 4: Comments

Mark,

Thank you for the opportunity to comment on the 2/9/23 Aesthetic Design Checklist for PID116649. The City of Cincinnati DOTE has one point of clarification regarding lighting. Central Ave. and east of Central Ave. have our “CBD Lighting Standard” which is different than our “Standard Roadway Lighting”. This PID includes both types of fixtures.

On the checklist Lighting Treatments: Replace CBD and Non-CBD STREET LIGHTING section to:

- Standard roadway lighting: City standard tapered steel pole with bracket arm and LED fixture.
- CBD lighting: East of and including Central Avenue (also includes 3rd Street east of Clay Wade Baily Bridge) as shown in Exhibit A.

Poles, arms, bases, fixtures, and other incidental attachments to street lighting shall be black in color.

Sincerely,

Angie Strunc, RA

Development, Permits & Urban Design – Division Manager
City of Cincinnati | Department of Transportation & Engineering
801 Plum Street, Suite 450 | Cincinnati, OH 45202
(513) 352-3310
Angie.strunc@cincinnati-oh.gov

**Brent Spence Bridge Corridor Project
Meeting Summary - Ft. Wright/Ft. Mitchell Subcommittee
March 29, 2023**



Groundbreaking by Design.

MEETING MINUTES

Project: Brent Spence
Purpose: BSB Aesthetics Ft. Wright & Ft. Mitchell
Place: Kenton County Government Building, Fiscal Court Room
Meeting Date: March 29, 2023
Prepared By: David Reed
Attendees: Mayor Jude Hehman City of Ft. Mitchell
Edwin King City of Ft. Mitchell
Mayor Dave Hatter City of Ft. Wright
Jill Cain Bailey City of Ft. Wright
Nicole DiNovo Human Nature
Gary Wolnitzek Human Nature
Stacey Hans KYTC – D6
Gary Valentine KYTC - CO
David Reed Qk4

1. Introductions

A brief introduction was made by Gary Valentine, who provided an update on the project status and turned the presentation over to Gary Wolnitzek.

- The overview of the proposed changes affecting the Dixie Highway and Kyles Lane interchanges included a review of the interstate lanes, new collector/distributor lanes, existing ramp modifications, and the reconstruction of new bridges for both Dixie Highway and Kyles Lane.
- The overview also included locations for the latest soundwall locations affecting the subject area. Stacey noted that the intent was to maximize the extents of soundwalls, A series of typical sections depicting optional shoulder treatments were provided as illustration, with details to be determined as part of final design. Of the sections provided, Section B represented the most likely condition to be expected in this area, which located soundwalls immediately behind the barrier walls.

2. Aesthetic Concepts

Gary provided an outline for each of the two interchanges, which included the architectural treatments for bridges and abutments, interior and perimeter landscape recommendations, alternative bridge or abutment signage options, and other minor aesthetic elements such as bridge fencing, perimeter fencing and soundwall patterns and textures. Gary also provided a series of historic photos which were used as the source of inspiration for the selection of brick and limestone material types, as well as the selection of architectural forms and geometries, where the civil war history of the area and previous fortress features offered unique character elements to support the aesthetics proposed. Gary also noted that there were three options generated for each interchange, ranging from traditional to more contemporary.

a. Ft. Wright – Kyles Lane

- Option 1 included the use of brick abutments (precast panels to mimic brick color and texture), bridge parapet mounted signage, and a simple but light and subtle fencing above the bridge parapet. Landscape concepts included the planting of the expanded abutment wall geometry, as well as tree plantings within the interior of the interchange (beyond clear zone dimensions) and along the perimeter of the interchange adjacent to right of way. Decorative fencing along the right of way perimeter was also depicted. The landscape ground plain offered a combination of mown turf within the maintained clear zones, and interior areas of higher and more colorful meadow zones outside the clear zone limits.
- Option 2 was similar but more simplified and utilized stone abutment detailing and signage mounted to the face of the abutment walls facing approaching traffic.
- Option 3 was further simplified and reflected more contemporary detailing of the bridge and abutment walls.
- Each option depicted how the bridge and signage could be illuminated outside daylight hours; and how the new bridge decks would support a separated zone for pedestrians on one side, and a slightly wider zone for combined pedestrian and bicycle use on the other side, each side separated from vehicular travel lanes by a curb and railing behind the curb.
- The ramp intersections also included the use of signage markers or pilasters which were a carryover from original concepts offered by the city.
- Comments from the Fort Wright group members included a preference toward the more traditional options presented and favored the idea of both interchanges offering a similar coordinated aesthetic theme, to offer continuity along this portion of the corridor. Ft. Wright members also favored bridge mounted signage (as opposed to abutment signage) but wanted to see how signage would look if centered across the entire length of the bridge and included “City of...” (like the Evendale example offered as precedent image). Ft. Wright members noted that light fixtures on the bridge should be coordinated with the bridge character (traditional vs. contemporary) and other lighting within the city; and Ft. Wright also asked about the possible use of transparent sound walls - Gary Valentine indicated transparent barriers are being considered in the Covington area of the corridor at key locations due to the interstate being elevated through this area.
- Ft. Wright also expressed interest in landscaping the center medians within Kyles Lane in the areas beyond the ramp intersections. These areas outside of the ramp terminals are not a part of the Brent Spence Corridor project, and would need to be handled separately. Ft. Wright acknowledged this condition and their willingness to utilize their own funds to support features like these which may not be supported by project funding.

b. Ft. Mitchell – Dixie Highway

- Gary presented a similar set of options for the Dixie Highway bridge and interchange, which again ranged from traditional, to intermediate, to more contemporary.
- Option 1 included a traditional brick abutment with brick and stone towers at each end of the bridge.
- Option 2 included the use of stone in the bridge, abutments, and tower features, with abutment signage shown as free-standing elements atop the abutment wall..
- Option 3 was further towards the contemporary, with the use of more modern architectural abstractions of the fortress forms and geometries.
- All options included a long retaining wall extension beyond the primary abutment wall on the west side of the bridge, where proposed interstate widening would make an already steep embankment steeper, and where the use of additional retaining wall would help manage the grade change and provide additional landscape planting area between the abutment and adjacent ramps. The northbound ramp in this area was also highlighted to receive width reductions in response to earlier public comment, making the pedestrian crosswalk much shorter and safer.
- All options for the Dixie interchange offered similar landscape solutions as the Kyles Lane examples, with lawn within maintained clear zone and meadow areas beyond supporting tree plantings. One area of tree plantings was noted as the “Freedom Grove” area – a previous planting initiative - which was recommended for preservation and expansion as part of the overall landscape plan. The perimeter landscape buffer area adjacent to the cemetery to the northeast was identified as an area of excess right of way which could be donated to local government for public use, or sold as excess property to an adjoining owner.
- Ft. Mitchell representatives preferred the character of stone over brick, and liked the tower features which anchored the ends of the bridge section. Fr. Mitchell was also aware and cautious about the introduction of features which would require their additional maintenance responsibilities. For this reason, they were less inclined to push hard for aesthetic fencing and accept the long-term maintenance required – Ft. Wright representatives agreed with this position. Both communities also favored the introduction of meadow plantings in landscape areas outside the clear zones and favored the more traditional Options 1 and 2. Ft. Wright also favored the bridge tower features included in the Ft. Mitchell options, and seemed to prefer the images supporting a mixture of brick and stone.
- Both communities would like to have the presentation available to share with their respective council or caucus groups for review and further input. It was agreed that Human Nature would edit the presentation to modify the alternatives to only include the more traditional Options 1 and 2, and to further modify the Ft. Wright options to reflect more of the Ft. Mitchell architectural character (primarily the use of tower features which was absent in the Ft. Wright options). Human Nature representatives also agreed that they would be available to present the revised images to each of the community groups and identified the evenings of April 17 and 19th as the preferred dates for presentation.
- Mr. Reed reminded the group that the goal is not to develop final solutions at this time, but rather to generate aesthetic guidelines and optional features that are strongly supported by each community for the ultimate Design-Build Team to use as aesthetic examples to develop their final designs solutions. Further, it is the intent that the communities will continue to be involved in the development of final design details as part of the next phase of the design-build process – which will likely result in actual construction during the 2025-2026 timeframe.

3. Next Steps

The presentation will be revised and re-presented at each of the April community meetings.

End of Meeting Minutes

Appendix D:
Stakeholder and Public Outreach Activities



**Stakeholder and Public Involvement
Summary of Outreach Efforts**

Date	Begin	End	Location	Stakeholders/Organizations Represented	Topics Discussed	Issues/Concerns Raised
12/5/2012			Cincinnati	ACI Greater Miami Valley	Project Update	None
1/10/2013			OKI	OKI Board of Directors	Project Update	None
1/30/2013	6:00 PM	7:00 PM	2951 Sydney Ave.	Zone 2 Community Council Meeting; Camp Washington Community Council; Millvale Residents & Community Council; North Fairmount Community Council	Project Update	None
2/14/2013			OKI		Project Update	None
3/6/2013			Columbus, OH	ACEC of Ohio	Project Update	None
3/11/2013			BCEO	Butler County TID	Project Update	None
3/12/2013			City Hall	Cincinnati T&I Committee	Project Update	None
4/25/2013			Cincinnati, oh	Amer. Soc. Prof. Engineers	Project Update	None
1/24/2014			Blue Ash, OH	Blue Ash Rotary Club	Project Update	None
2/25/2014			Cincinnati, OH	OLLI	Project Update	None
3/17/2014			Hyde Park, OH	Hyde Park Center; Community	Project Update	None
1/1/2022	9:30 AM	10:30 AM	Zoom Meeting	Bridge Forward	Bridge Forward Concept	Project Design Concept
2/2/2022	8:00 AM	9:00 AM	Zoom Meeting	KYTC, Bridge Forward	Met with Brian Boland of Bridge Forward	He presented Bridge Forward's proposed roadway design for the I-75 corridor. Most of the presentation focused on work within Ohio
2/9/2022	10:00 AM	11:00 AM	MS Teams meeting	KYTC, NKY Chamber	Met with Brent Cooper, NKY Chamber, for overall project update that he could share with Chamber members.	No concern, overall project update.
3/3/2022	9:30 AM	10:30 AM	KYTC District 6	KYTC, City of Covington, SD1	Covington officials and Sanitation District #1 attended a meeting to discuss Willow Run Combined Sewer.	Repeated CSO events in Goebel Park and within the Willow Run Drainage Basin. Interstate runoff flows into this system.
3/4/2022	10:30 AM	12:00 PM	City Hall	City of Cincinnati, DOTE	Project Update and Coordination	None
3/7/2022	10:00 AM	11:30 AM	Western & Southern	Business Leaders / Chamber	Project Update	None
3/15/2022	3:00 PM	4:30 PM	Cincinnati Art Museum	Cinti. Regional Business Committee	Project Update	None
3/13/2022	4:00 PM	5:00 PM	Columbus, OH	ASHE National Conference	Project Update Presentation	None
5/11/2022	9:00 AM	10:00 AM	MS Teams meeting	KYTC, Kenton County Engineer	Met with Kenton County Engineer for project update and status.	No concern, just overall project update.
5/13/2022			Cincinnati, OH	ASHE Trikovalley	Project Update	None
5/23/2022	1:00 PM	2:00 PM	OKI Conf Room	KYTC, ODOT, Ohio/Kentucky ITE	Overall project update to Joint Ohio/Kentucky Institute of Transportation Engineers (ITE) meeting	No concern, overall project update.
6/6/2022	2:00 PM	3:00 PM	KYTC District 6/Virtual	KYTC, City of Covington	Project update and impacts discussion with City of Covington officials.	Continued conversation regarding drainage in Goebel Park and traffic diversion during construction.
6/7/2022	9:00 AM	4:00 PM	NKY Convention Center	Industry and DBE Firms	Project Overview and DBE Networking Opportunity	None
6/10/2022	10:00 AM	11:15 AM	Cincinnati, OH	Senator Portman and Local Agency Officials	Project Update	None
6/13/2022	4:00 PM	4:30 PM	Queen City Club	Cinti. Regional Business Board	Project Update	None
6/28/2022	2:00 PM	4:00 PM	Virtual Meeting	Project Aesthetics Committee	PID 113361 and PID 114161 Aesthetics	None
6/29/2022	10:00 AM	12:00 PM	OKI	Project Advisory Committee Meeting	Project Update	None
7/12/2022	2:30 PM	3:30 PM	KYTC District 6	KYTC, City of Covington, SD1	City of Covington officials (and their consultant engineers), Sanitation District #1 to discuss options for drainage in Goebel Park.	No concerns, KYTC consultant presented options to address flooding frequency and will work with VS Engineering (City Engineer) to confirm results.

**Stakeholder and Public Involvement
Summary of Outreach Efforts**

Date	Begin	End	Location	Stakeholders/Organizations Represented	Topics Discussed	Issues/Concerns Raised
7/28/2022	8:30 AM	11:00 AM	Queen City Club	Business Leaders / Chamber	Project Update NEPA Discussion	Need for continued coordination and project evaluation.
7/28/2022	5:00 PM	7:00 PM	312 Walnut St.	ULI BSB Panel Discussion	Project Update	Project Design Concept
8/1/2022	1:00 PM	2:00 PM	City Hall	Cincinnati Staff	BSB / City Coordination	None
8/24/2022	12:00 PM	8:00 PM	Kenton County Govt Building	KYTC, Property Owners	Meeting with various property owners within Covington to discuss upcoming Right of Way process.	Understanding impact to their particular property, timeline of process.
8/25/2022	9:00 AM	5:00 PM	Kenton County Govt Building	KYTC, Property Owners	Meeting with various property owners within Covington to discuss upcoming Right of Way process.	Understanding impact to their particular property, timeline of process.
9/2/2022	11:30 AM	12:00 PM	KYTC District 6	KYTC, Property Owner	Met with Gus McKinley to discuss project impacts to his property.	Access to his adjoining property that was shared with Design team.
9/6/2022	2:15 PM	3:00 PM	Galt House, Louisville	KYTC, ACEC Partnering Conference	Project update at the 2022 ACEC-KY/FHWA/KYTC Partnering Conference	No concern, overall project update.
10/3/2022	10:00 AM	10:30 AM	MS Teams meeting	KYTC, City of Covington, SD1	Continued coordination for Willow Run, potential location of outfall through the levee	Capacity of SD#1 pumping station
10/25/2022	1:55 PM	2:20 PM	Hyatt Regency, Lexington	KYTC, Midwest Bridge Preservation Partnership Conference	Overall project update to the Midwest Bridge Preservation Partnership Conference	Confirm project compliance with Build America, Buy America (BABA)
11/7/2022	12:00 PM	2:00 PM	3457 Montgomery Rd.	Diversity and Inclusion Outreach Committee	Committee kick-off; project update	None
11/10/2022			OKI	OKI Board of Directors	Project Update	None
11/15/2022	7:00 PM	9:00 PM	2515 Clifton Ave.	CUF Community Council	Neighborhood Outreach; Project Update	None
11/16/2022	11:00 AM	1:00 PM	The Phoenix, Cincinnati	KYTC, ODOT, ACEC Ohio Southwest Chapter	Joint (ODOT/KYTC) Overall project update to ACEC OH Southwest Chapter	No concern, overall project update.
11/16/2022	2:30 PM	4:30 PM	First Financial Bank	KYTC, City of Covington	Project coordination meeting with City of Covington	Aesthetics, procurement, overall project details.
11/30/2022	10:30 AM	11:30 AM	MS Teams meeting	KYTC, City of Covington	Met with City of Covington Officials to discuss project impact to recreational uses within Goebel Park	Park facilities (current and future use), replacement activities and locations.
12/7/2022	9:00 AM	4:00 PM	Anderson Pavilion 8 E Mehring Way Cincinnati, OH 45202	DBE Networking Event	Project Overview, Procurement Update, DBE Certification Information, Networking Opportunity	None
12/12/2022	3:30 PM	4:00 PM	225 E. Sixth Street Cincinnati, OH 45202	Cincinnati Bar Association	Mentioned BSB as part of a discussion of how ODOT incorporates environmental justice into the project development process	None
1/5/2023	12:00 PM	1:00 PM	2350 Auburn Avenue	Mt. Auburn Chamber of Commerce	Project Update; Presentation	The group was appreciative of the presentation and offered no comments or concerns about the project. Thirteen people attended in-person and several others attended virtually.
1/24/2023	12:00 PM	1:00 PM	LFUGC, Lexington	KYTC, ASHE Bluegrass Chapter	Project Update to ASHE Bluegrass Chapter	No concern, overall project update.
1/25/2023	1:00 PM	3:00 PM	Virtual Meeting	Diversity and Inclusion Outreach Committee	DBE Goal Overview; 2023 Outreach Efforts	The group asked questions about the methodology used to calculate the DBE goal and target for PID 116649.
2/6/2023	1:00 PM	2:30 PM	Allied Construction Industries	Diversity and Inclusion Outreach Committee	DBE Goal Methodology	The group asked many questions, and ODOT, KYTC, and DBE Goal Methodology Consultant, BBC, provided answers.

**Stakeholder and Public Involvement
Summary of Outreach Efforts**

Date	Begin	End	Location	Stakeholders/Organizations Represented	Topics Discussed	Issues/Concerns Raised
2/10/2023	9:00 AM	11:00 AM	Cincinnati City Hall	Bridge Forward, Cincinnati DOTE	Bridge Forward Concept; Traffic Development and Analysis, Progressive DB Contract Process	Project Design Concept
2/27/2023	2:00 PM	3:30 PM	Cincinnati City Hall	Bridge Forward, Cincinnati DOTE	City Land Use and Zoning, Revive Cincinnati Study (Cincinnati Planning Study), US 50 Connections into BSB Study Limits.	None
3/7/2023	11:00 AM	4:00 PM	Raddison Hotel Covington, KY	DBE Networking Event	DBE Networking Opportunity	None
3/15/2023	7:45 AM		Wegman Company	Queensgate Business Alliance	Project Update; Presentation	MOT during construction, desire for an iconic new companion bridge, desire not to preclude the extension of a bike trail along the river westward along Mehring Way.
3/17/2023	11:00 AM	12:00 PM	Cincinnati City Hall	Bridge Forward, Cincinnati DOTE	Working session to discuss Bridge Forward goals in more detail	Bridge Forward presented a concept for the proposed work in Ohio.
3/22/2023	10:00 AM	11:30 AM	Queen City Club	Cincinnati DOTE, Cincinnati Regional Business Committee	Project Update; Process for On-going Engagement	None.
3/22/2023	12:30 PM	1:30 PM	Moerlein Lager House	ASCE Lunch Meeting	Project Overview; Presentation	40-50 people in attendance, invited to speak at additional events, Q&A with no substantial issues raised.
3/28/2023	10:00 AM	11:00 AM	Cincinnati City Hall	City of Cincinnati Council CEI Committee Meeting	Project Overview; Key Project Details	Cooperating agencies; Workforce development, DBE participation, Bridge Forward concepts
3/28/2023	1:00 PM	2:00 PM	Hamilton County Office	Hamilton County Commissioners	Project Overview; Key Project Details; Next Steps	Cooperating agencies; workforce development; DBE participation; Bridge Forward concepts
4/13/2023	10:30 AM		OKI Boardroom	OKI Board of Directors	Project Update	The board meeting was well attended, and several questions were posed regarding DBE, Justice 40, comments from Bridge Forward, and Hamilton County request to be a cooperating agency.
4/17/2023			One-On-One Meetings	Hamilton County Commissioners	Project Update	The project team met with each Hamilton County Commissioner and discussed a number of topics, including DBE participation, workforce development, and the project design.
4/26/2023	8:30 AM	1:00 PM	Virtual Meeting	Tri-State DBE Training	Project Overview; Key Project Details	Approximately 100 people in attendance. No questions were asked.
5/1/2023 to 5/3/2023	Varied		Butler County Fair Grounds	Allied Construction Industries Construction Career Event (Adult Career Fair on 5/1; High School Students on 5/2)	Project Information	Approximately 3,000 high school and middle school students attended.
5/2/2023	2:00 PM	4:00 PM	AACOC HQ 2303 Gilbert Ave Cincinnati, Ohio 45206 Main Conference RM	Meeting with Greater Cincinnati and Northern Kentucky African American Chamber of Commerce staff and member businesses, ODOT	Diversity and Inclusion Outreach Committee; DBE goals and engagement; project overview; and workforce development	None.
5/22/2023	10:00 AM	2:00 PM	Sharonville Convention Center	Hamilton County Small Business	BSB Breakout Session and Table with ODI	Approximately 900 people registered for this event. ODOT had a table as well with ODI staff available throughout the all day event. ODOT also made a BSB presentation during a breakout session.

**Stakeholder and Public Involvement
Summary of Outreach Efforts**

Date	Begin	End	Location	Stakeholders/Organizations Represented	Topics Discussed	Issues/Concerns Raised
5/22/2023			West End Neighborhood	ODOT, City of Cincinnati	ODOT Director Jack Marchbanks walked with Cincinnati Mayor Aftab Pureval to tour neighborhoods around the I-75 corridor. The tour included locations along Linn Street, West Court Street, Ezzard Charles Drive, Winchell Avenue, Wade Walk, and West Liberty Street.	None.
5/23/2023	12:00 PM	1:00 PM	Virtual Meeting	Metropolitan Sewer District Collaboration Meeting	Project Team meeting	ODOT and MSD team met to continue collaboration on addressing storm water in the BSB Corridor.
5/23/2023	2:00 PM	4:00 PM	ODOT ARTIMIS Building	Bridge Forward, Cincinnati DOTE	Working session to discuss Bridge Forward concepts	The project team discussed the April concept presented by Bridge Forward and the progressive design build process.
5/24/2023	8:00 AM	12:00 PM	Queen City Club	ODOT, KYTC, HNTB, FHWA, Cincinnati DOTE, Hamilton County, Cincinnati Chamber, CBC, CRBC, and other local businesses and groups.	Project presentation and discussion	Provided project update including a discussion of schedule, upcoming activities and the progressive design build process. Shared, discussed, and compared the I-W concept and the current Bridge Forward concept.
5/25/2023	1:00 PM	1:30 PM	Phone	ODOT, Greater Cincinnati and Northern Kentucky African American Chamber of Commerce	General discussion of Diversity and Inclusion Outreach Committee	None.
5/30/2023	10:30 AM	12:00 PM	Enquirer Building	Cincinnati Enquirer Editorial Board and Enquirer reporters, ODOT	Q&A session regarding the project in general and Bridge Forward.	None.
6/1/2023	3:30 PM	4:30 PM	City Hall	ODOT, Cincinnati DOTE, Cincinnati Councilmember Meeka Owens and staff	General project discussion	None.
6/5-7/28			Camp OT'Y OKWA	Middle school summer camp includes topics related to transportation	Workforce development	None.
6/6/2023	3:00 PM	4:00 PM	City Hall	ODOT, Cincinnati DOTE, Councilmember Seth Walsh and staff	General project discussion	None.
6/7/2023	2:30 PM	4:00 PM	Jurgensen Headquarters	ODOT, MACC Tech	Discussion of workforce development	None.
6/7/2023	8:30 AM	9:30 AM	Receptions Event Centers 5975 Boymel Dr Fiarfield, OH 45014	Traffic Engineering Workshop event attended by local governments, consultants, and contractors	Keynote	None.
6/8/2023	10:00 AM	3:00 AM	Greater Cincinnati Foundation 720 Pete Rose Way Cincinnati, OH	Cincinnati We Mean Business Event	Project Presentation	Approximately 900 people registered for this event. ODOT had a table as well with ODI staff available throughout the all day event. ODOT also made a BSB presentation during a breakout session.
6/8/2023				ODOT, Urban League, AFL/CIO	Workforce development; Building Futures Program	None.
6/14/2023	7:00 PM	8:30 PM	Kenton County Govt Building	KYTC, Benefited Receptors on Crescent Avenue	Noise wall public meeting	Attendees expressed the desire for a transparent noise wall.
6/15/2023	9:00 AM	10:30 AM	ODOT ARTIMIS Building	Bridge Forward, Cincinnati DOTE	Bridge Forward concept	None.
6/15/2023	3:00 PM	5:30 PM	Braxton Brewery	ACI CEO Leadership Forum	Panel Discussion about BSB Corridor Project	None.

**Stakeholder and Public Involvement
Summary of Outreach Efforts**

Date	Begin	End	Location	Stakeholders/Organizations Represented	Topics Discussed	Issues/Concerns Raised
6/21/2023	9:00 AM	4:00 PM	Ohio Expo Center Kasich Hall 717 E. 17th Ave. Columbus, OH	Ohio MBE Diverse Expo	BSB Workshop and table	None.
6/23/2023	12:00 PM	5:00 PM	Greater Cincinnati Foundation 720 Pete Rose Way Cincinnati, OH	Workforce Development Day - Hispanic Chamber, African American Chamber and American Marketing Association	ODI table with BSB handout	None.
6/27/2023	2:30 PM	3:30 PM	City Hall, Centennial II, 805 Central Ave., Cincinnati, Ohio	Councilman Mark Jeffreys	BSB Overview	None.
6/28/2023	9:00 AM	11:00 AM	City Hall, Centennial II, 805 Central Ave., Cincinnati, Ohio	City of Cincinnati DBE Certification Workshop	ODOT DBE certification presentation with remarks on BSB Corridor Project	None.
6/29/2023	1:00 PM	2:00 PM	Virtual Meeting	African American Chamber of Commerce; Gregory Park and a member of business	Discussion of DBE Goal and opportunities for area businesses	None.
7/20/2023	5:00 PM	7:00 PM	Esoteric Brewing Co. 918 E. McMillan St. Cincinnati, OH	The Exchange Networking Event	Diversity and inclusion opportunities	None.
7/25/2023	8:30 AM	9:30 AM	Virtual Meeting	Kaskaskia Engineering Group	Cost estimate for Bridge Forward Concept	None.
7/25/2023	2:30 PM	3:30 PM	City Hall, Centennial II, 805 Central Ave., Cincinnati, Ohio	Councilman Jeff Cramerding	BSB Overview	None.
7/27/2023	10:00 AM	11:30 AM	2303 Gilbert Ave 2303 Gilbert Avenue Cincinnati, OH 45206	Cincinnati DBE Certification Training Event: Learn How to Become DBE Certified and Potential Opportunities with DBE Certification	ODOT Office of Diversity and Inclusion staff participated in a workshop hosted by the Greater Cincinnati and Northern Kentucky African American Chamber of Commerce	None.
8/9/2023	4:00 PM	5:00 PM	Virtual Meeting	City of Cincinnati Advisory Committee (Cincinnati DOTE staff and committee members, including community representatives)	City of Cincinnati discussion of BSB project	None.
8/16/2023	10:30 AM	11:00 AM	Milwaukee Convention Center	MAASTO Conference	BSB Presentation focusing on bi-state coordination and DBE and workforce development project approach	None.
8/17/2023	11:00 AM	11:30 AM	Virtual Meeting	Freedom Center	Potential collaboration opportunities related to local business engagement, youth programming (STEM), and opportunities related to a historic marker for the West End	None.
8/21/2023	1:30 PM	2:30 PM	Queen City Club	Cincinnati Regional Business Committee	ODOT update	None.
8/21/2023	3:30 PM	4:00 PM	Queen City Club	Cincinnati Business Committee	ODOT update	None.
8/22/2023	10:00 AM	11:30 AM	Ohio-Kentucky-Indiana Regional Council of Governments	Diversity and Inclusion Outreach Committee	BSB update and draft Diversity and Inclusion Outreach Plan	None.
8/28/2023	1:00 PM	1:30 PM	Hamilton County Office	Transportation Improvement District Meeting	BSB update	None.

**Stakeholder and Public Involvement
Summary of Outreach Efforts**

Date	Begin	End	Location	Stakeholders/Organizations Represented	Topics Discussed	Issues/Concerns Raised
9/5/2023	10:30 AM	11:00 AM	Virtual Meeting	Bridge Forward	Bridge Forward presented a new concept to the project team	
9/6/2023	9:30 AM	10:00 AM	Virtual Meeting	Cincinnati Bengals	Update on right-of-way acquisition	None.
9/8/2023	2:00 PM	2:30 PM	Virtual Meeting	Sierra Club Meeting	Project approach to storm water	None.
9/12/2023	11:00 AM	11:45 PM	Virtual Conference	USDOT Great Lakes Region Small Business Contracting Symposium	Panel discussion of small business opportunities on BIL projects, including BSB	None.
9/12/2023	2:30 PM	4:00 PM	Cincinnati Museum Center	Cincinnati Museum Center	Collaboration opportunities related to youth programming (STEM) and historic marker related to West End	Positive feedback on potential collaboration opportunities.
9/13/2023	8:30 AM	11:00 AM	ODOT Central Office	ITS Midwest Fall Workshop	BSB Presentation	None.
9/19/2023	9:30 AM	10:00 AM	Virtual Meeting	Freedom Center	Youth programming opportunities (STEM)	None.
9/21/2023	1:00 PM	2:30 PM	2220 Victory Pkwy, East Walnut Hills, 45206	UC Osher Lifelong Learning Institute	BSB Presentation	None.
9/21/2023	4:00 PM	6:00 PM	Braxton Brewery	AACC Exchange	DBT introduction	None.
9/27/2023	3:00 PM	5:00 PM	ACI Office	Allied Construction Industries Contractor Event	DBT introduction	None.
9/29/2023	9:30 AM		Duke Energy Convention Center	City of Cincinnati DEI's Business Enterprise EXPO	BSB presentation	None.
10/2/2023	3:00 PM		Cincinnati City Hall	Cincinnati Department of Transportation & Engineering, Cincinnati Councilman Reggie Harris and staff	Project process and timeline and the City's and Council's role in the remaining decisions.	None.
10/6/2023	9:00 AM		Virtual Meeting	Cincinnati Councilmember Meeka Owens and Department of Transportation & Engineering	Project update	None.
10/11/2023	10:00 AM	11:30 AM	Alloy Development	DBE Workshop/Update	BSB Presentation	None.
10/11/2023	4:00 PM	5:00 PM	Virtual Meeting	City of Cincinnati Advisory Committee	City of Cincinnati Discussion of BSB project	None.
10/13/2023	11:30 AM	12:30 PM	Cincinnati City hall	State legislators and Cincinnati leadership	Presentation and Q&A on BSB project	None.
10/17/2023	3:30 PM	5:00 PM	Columbus Convention Center	Ohio Transportation Engineering Conference (OTEC)	BSB presentation on progressive design-build	None.
10/19/2023	10:00 AM	1:00 PM	Greater Cincinnati Foundation	SORTA/Metro	Larry Brown talked with businesses about DBE Certification and was a judge for a Pitch Contest for SORTA/Metro's 'Meet the Primes' event	None.
10/19/2023	3:00 PM	4:00 PM	Virtual Meeting	Sierra Club	Various topics	None.
10/19/2023	4:00 PM	5:00 PM	Virtual Meeting	Cincinnati Advisory Committee Meeting	The City shared a comment related to providing a street grid on the east side of I-75 and as well as an intersection with Gest Street and 9th Street	None.
10/24/2023	2:00 PM	4:00 PM	Virtual Meeting	ABCD Central OH Chapter	BSB presentation	None.
10/24/2023	10:00 AM	1:00 PM	In-Person	City of Springfield, OH	Larry Brown presented a DBE Certification Workshop and ODOT Project update	None.
10/25/2023	5:00 PM	7:00 PM	Mad Tree Brewing 3301 Madison Road	Ohio Valley Region of the Design-Build Institute of America	Introduction of the design-build team	None.
10/26/2023	10:00 AM	11:00 AM	Virtual Meeting	CLG Forum	BSB presentation	None.
11/1/2023	Lunch		ACI Luncheon	Allied Construction Industries members	BSB presentation	None.

**Stakeholder and Public Involvement
Summary of Outreach Efforts**

Date	Begin	End	Location	Stakeholders/Organizations Represented	Topics Discussed	Issues/Concerns Raised
11/1/2023	10:00 AM	11:00 AM	African American Chamber 2303 Gilbert Avenue Cincinnati, OH	African American Chamber and City of Cincinnati	Larry Brown presented a DBE Certification Workshop and Lab	None.
11/15/2023	10:00 AM	11:30 AM	Webinar	State of Ohio	How to do business with the State of Ohio using Ohio Buys to find ODOT contracts	None.
11/27/2023	1:00 PM	1:30 PM	Hamilton County Office	Regularly scheduled monthly TID Meeting	Regular BSB update	None.
12/1/2023			Doubletree Guest Suites Hotel in Blue Ash	Cincinnati MENSA	BSB project update	None.
12/4/2023	1:30 PM	3:00 PM	Kenton County Government Center	Diversity and Inclusion Outreach Committee	Project and subcontracting update	DIO Committee provided feedback that subcontracting opportunities should be available to the public.
12/13/2023	10:00 AM	11:30 AM	Webinar	ODOT/CVG Airport/SORTA-Metro	How to use your DBE certification in the transportation industry. Larry Brown presented on DBE Certification & doing business with ODOT, CVG Airport and SORTA/Metro present on how to do business with them using DBE certification	None.
12/14/2023	4:00 PM	4:30 PM	Virtual	Cincinnati Advisory Committee Meeting	BSB status update	None.
1/25/2024	8:00 AM	10:00 AM		Dayton Chamber of Commerce	BSB overview, focusing on potential subcontracting opportunities.	None.

**Appendix E:
E-Newsletters**



CONNECT

E-NEWSLETTER

BRENT SPENCE
BRIDGE CORRIDOR
Moving the Economy, Creating Jobs



Map My Commute

Use this tool to find out what your commute will look like once the project is complete.

CONTINUE READING →

Governors Beshear and Kasich Sign Memo of Understanding

The current status of the Brent Spence Bridge is not a one-state issue: The Brent Spence Bridge Corridor includes roadway in Northern Kentucky as well as Ohio, and motorists from both states cross the bridge every single day.

CONTINUE READING →

What is the Value for Money Study?

ODOT and KYTC are conducting a Value for Money (VfM) study to explore financing and delivery options for a new Brent Spence Bridge Corridor.

CONTINUE READING →

Check out our new website!

The Brent Spence Bridge Corridor website has a new look and new, updated information.

[More >](#)

Tolling: Myth vs. Reality

The fact of the matter is, adequate sources of revenue do not exist to replace and refurbish the Brent Spence Bridge Corridor. [More >](#)

Speakers Bureau

A member of the Brent Spence Bridge Corridor team is available to speak with your business or community. To connect with an expert or request a presentation, [contact us.](#)

Brent Spence Bridge Corridor

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CONNECT

E-NEWSLETTER

BRENT SPENCE
BRIDGE CORRIDOR
Moving the Economy, Creating Jobs



Digging Deeper: The Value for Money Study

ODOT and KYTC are conducting a Value for Money study to explore financing and delivery options for a new Brent Spence Bridge Corridor. Find out more about what it is and how it will affect the project.

CONTINUE READING →

So What's a P3, Anyway?

A public-private partnership, or P3, is one of the delivery options for the Brent Spence Bridge Corridor. Find out exactly what a P3 is and how it can help deliver this project faster.

CONTINUE READING →

Why Tolls?

Because tolling must be part of this project if the Brent Spence Bridge is to be built before the year 2040, it's important to understand what modern tolling looks like. Here's a hint: It's not your father's toll booth.

CONTINUE READING →

Texas A&M Transportation Institute Releases 2012 Urban Mobility Report

Each year, the Texas A&M Transportation Institute publishes its Urban Mobility Report, a study of urban congestion problems. Cincinnati has been a part of this study since 1982.

CONTINUE READING →

Did You Know?

The Brent Spence Bridge has been part of the Cincinnati skyline since 1963. Learn more about the history of the bridge [here](#) or visit our [Facebook page](#) to see historical photos of the bridge during construction.

Project Team Appears on Impact Cincinnati

The Brent Spence Bridge was the topic of WVXU's Impact Cincinnati on Thursday, February 21. [Listen to the Podcast >](#)

Speakers Bureau

A member of the Brent Spence Bridge Corridor team is available to speak with your business or community. To connect with an expert or request a presentation, [contact us.](#)

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Why the Gas Tax Alone Isn't Enough

The Brent Spence Bridge is a project of regional and national significance - which is why many believe Congress should find the money to pay for its replacement. After all, isn't the federal gas tax collected to pay for infrastructure projects like this one?

CONTINUE READING →

A Look at the Bridge Design Selection Process

KYTC and ODOT began the Bridge Design Selection Process in 2009 with some specific requirements in mind.

CONTINUE READING →

Local Congestion Will Increase Without a New Brent Spence

Today, the bridge carries approximately double the number of vehicles for which it was designed, creating congestion that leads to an average of 3.6 million hours and 1.6 million gallons of gas wasted per year.

CONTINUE READING →

Brent Spence Bridge Featured on Fox 19 Morning News

KYTC discussed changes to the Brent Spence Bridge Corridor on WXIX's Fox 19 Morning News on February 27. [Watch here.](#)

Project Team Appears on CET's Focus.

The Brent Spence Bridge was the topic of conversation on CET's Focus on March 1. [Watch here.](#)

Brent Spence Bridge on Issues

ODOT and KYTC gave a project update to WLWT's Issues on March 3. [Watch here.](#)

Get an In-Depth Update

Brent Spence Bridge Corridor representatives are available to provide a project overview and update to your business, professional association or community interest group. [Make your request.](#)

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BRENT SPENCE
BRIDGE CORRIDOR



The Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT) are working together to prepare for future funding opportunities related to the recent passage of the federal Infrastructure Bill.

We know that funding to support travel along the Brent Spence Bridge Corridor is top of mind with many.

The Brent Spence Bridge Corridor covers eight miles from the Western Hills Viaduct interchange in Ohio to Dixie Highway (US 25) in Kentucky. The Brent Spence Bridge has long been recognized as the centerpiece of the corridor, carrying I-75 and I-71 traffic across the Ohio River and connecting two states that are critical to the movement of people, goods, and services.

We expect that a funding package that could be of benefit to Kentucky and Ohio for funding a new companion bridge and supporting updates to the eight-mile corridor, would entail a grant process that would require an application and competitive review process before any funding was awarded for construction. Funding to build a new companion bridge – not a replacement structure – and support corridor-wide improvements will not immediately become available.

Ohio and Kentucky are in the process of reviewing the design and engineering study that was completed several years ago. As part of that study, multiple concepts were identified as potential options for a new companion bridge. These alternatives are currently being reevaluated and opportunities to reduce the cost and footprint of a new bridge are being considered. Current and future traffic volumes also are being examined. This work is expected to be completed by the summer of 2022.



KYTC recently completed a routine maintenance project to clean and paint the Brent Spence Bridge. Crews also completed other tasks, including cleaning drains, installing new signage, and replacing the interstate emblems on the roadway surface.

ODOT is evaluating various improvements between the Western Hills Viaduct interchange and the bridge to support the connections that would be needed for more efficient travel on the Ohio-side of the corridor.

Brent Spence Bridge Quick Facts

The Brent Spence Bridge is safe and structurally sound. It is part of an important eight-mile corridor from the Western Hills Viaduct interchange in Ohio to Dixie Highway in Kentucky.



Source: Kentucky Transportation Cabinet

Current plans call for the construction of a new companion bridge – not a replacement – to provide an additional river crossing to the west of the existing structure. New funding would also support improvements to the current bridge and the interstate network throughout the entire eight-mile corridor.

The bridge was designed to carry 80-100,000 vehicles per day. It currently carries twice that volume. During peak travel times, the number of vehicles crossing the bridge significantly exceeds what each lane can efficiently handle without travel delays.

The bridge was reconfigured in 1985 to provide an additional travel lane on each deck.

- Before: three 12-foot lanes + approximately five-foot shoulders
- Now: four 11-foot lanes + one-foot shoulders

Motorists who cross the bridge often do so with limited time to move to their intended lane. This contributes to congestion and overall safety concerns on the bridge itself. For example, a northbound driver who enters I-71/75 northbound from the 4th St. on-ramp in Covington and wants to continue north to I-75 in Ohio has to cross multiple lanes of travel – on the bridge. A southbound driver from I-71 in Ohio who wants to exit at 5th St. in Covington confronts the same challenge. One of the primary goals of building a new companion structure to the west of the existing bridge is to separate local and through traffic.

The bridge was built in 1963 and is maintained by KYTC. Multiple routine maintenance projects have taken place throughout the history of the bridge to ensure it remains viable and safe for long-term use, including the recent project to clean and paint the bridge, as well as a project in 2017 to replace the concrete deck on both the north and southbound travel lanes.

A Bi-State Management Team is hard at work on various activities to position the Brent Spence Bridge Corridor for future improvements.

The Bi-State Management Team is fully engaged and working to complete tasks to position the project for funding opportunities and delivery of the Brent Spence Bridge Corridor improvements and new companion bridge over the Ohio River.

Current activities include:

- Researching and identifying potential options for a Project governing structure for the two states including:
 - Analyzing the risks and opportunities of each structure.
 - Determining the legal process and necessary actions.
 - Benchmarking strategies and structures used in other projects across the United States.
- Preparing a Financial Plan Update for the Project including:
 - Evaluating the capacity and potential use of State funding for both Ohio and Kentucky.
 - Evaluating the capacity and potential use of Federal and non-traditional funding.
 - Preparation of a technical memorandum with a recommended funding/financing structure for an expeditious delivery of the Project.
- Updating traffic projections, models, and forecasts including:
 - Updating existing traffic forecasts to current conditions.
 - Preparing an updated Traffic Demand Model to provide a base for future forecasts and concept modeling.
 - Preparing a Travel Forecasting Technical Memorandum summarizing the baseline assumptions and process for performing the travel forecasts.
 - Performing demand model analysis of the Project concepts.
- Updating design concepts and Project cost estimates including:
 - Updating Project cost estimates for the currently preferred alternative.
 - Evaluating additional design concepts to reduce impacts and costs.
 - Developing Project cost estimates for the additional design concepts.
- Reassembling the [Project Advisory Committee](#) with a potential meeting in Spring 2022.

January 2022 E-Newsletter

Stay Connected

The website will undergo a makeover in early 2022 to become even more user-friendly. If you have comments on what changes you'd like to see to the website, submit a comment.

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Boletín electrónico del corredor del puente Brent Spence, enero



BRENT SPENCE BRIDGE CORRIDOR



El Gabinete de Transporte de Kentucky (KYTC) y el Departamento de Transporte de Ohio (ODOT) están trabajando juntos para prepararse para futuras oportunidades de financiación relacionadas con la reciente aprobación del proyecto de Ley de infraestructura federal.

Sabemos que la financiación para apoyar los viajes a lo largo del corredor del puente Brent Spence es una prioridad para muchos.

El corredor del puente Brent Spence cubre ocho millas desde el intercambio del viaducto de Western Hills en Ohio hasta Dixie Highway (US 25) en Kentucky. El puente Brent Spence ha sido reconocido durante mucho tiempo como la pieza central del corredor, que transporta el tráfico de la I-75 y la I-71 a través del río Ohio y conecta dos estados que son fundamentales para el



través del río Ohio y conecta dos estados que son fundamentales para el movimiento de personas, bienes y servicios.

Lo esperable es que un paquete de financiamiento que podría ser de beneficio para Kentucky y Ohio para financiar un nuevo puente complementario y respaldar las actualizaciones del corredor de ocho millas, conlleve un proceso de subvención que requeriría una solicitud y un proceso de revisión competitivo antes de que se otorgue cualquier financiamiento para construcción. Los fondos para construir un nuevo puente complementario, no una estructura de reemplazo, y apoyar las mejoras en todo el corredor no estarán disponibles de inmediato.

Ohio y Kentucky están en proceso de revisar el estudio de diseño e ingeniería que se completó hace varios años. Como parte de ese estudio, se identificaron varios conceptos como posibles opciones para un nuevo puente complementario. Actualmente se están reevaluando estas alternativas y se están considerando oportunidades para reducir el costo y el impacto de un nuevo puente. También se están examinando los volúmenes de tráfico actuales y futuros. Se espera que este trabajo esté terminado para el verano de 2022.



KYTC completó recientemente un proyecto de mantenimiento de rutina para limpiar y pintar el puente Brent Spence. Los equipos también completaron otras tareas, incluida la limpieza de desagües, la instalación de nuevos letreros y el reemplazo de los emblemas interestatales en la superficie de la carretera.

ODOT está evaluando varias mejoras entre el intercambio del viaducto de Western Hills y el puente para respaldar las conexiones que serían necesarias para un viaje más eficiente en el lado de Ohio del corredor.

Datos breves acerca del puente Brent Spence

El puente Brent Spence es seguro y estructuralmente sólido. Es parte de un importante corredor de ocho millas desde el intercambio del viaducto de Western Hills en Ohio hasta la autopista Dixie en Kentucky.



Fuente: Gabinete de Transporte de Kentucky

Los planes actuales exigen la construcción de un nuevo puente

Los automovilistas que cruzan el puente a menudo lo hacen con tiempo

Los planes actuales exigen la construcción de un nuevo puente complementario, no un reemplazo, para proporcionar un cruce de río adicional al oeste de la estructura existente. Los nuevos fondos también apoyarían las mejoras al puente actual y la red interestatal en todo el corredor de ocho millas.

El puente fue diseñado para transportar de 80,000 a 100,000 vehículos por día. Actualmente transporta el doble de ese volumen. Durante las horas pico, la cantidad de vehículos que cruzan el puente excede significativamente lo que cada carril puede manejar de manera eficiente sin demoras en el viaje.

El puente se reconfiguró en 1985 para proporcionar un carril de circulación adicional en cada plataforma.

- Antes: tres carriles de 12 pies + arcenes de aproximadamente cinco pies
- Ahora: cuatro carriles de 11 pies + arcenes de un pie

Los automovilistas que cruzan el puente a menudo lo hacen con tiempo limitado para moverse a su carril previsto. Esto contribuye a la congestión y a las preocupaciones generales de seguridad sobre el puente. Por ejemplo, un conductor que va en dirección norte, que ingresa a la I-71/75 en dirección norte desde la rampa de acceso de 4th St. en Covington y quiere continuar hacia el norte hasta la I-75 en Ohio, tiene que cruzar varios carriles de circulación en el puente. Un conductor que va en dirección sur desde la I-71 en Ohio, que quiere salir en 5th St. en Covington, enfrenta el mismo desafío. Uno de los objetivos principales de construir una nueva estructura complementaria al oeste del puente existente es separar el tráfico local del tráfico de paso.

El puente fue construido en 1963 y es mantenido por KYTC. Se han llevado a cabo múltiples proyectos de mantenimiento de rutina a lo largo de la historia del puente para garantizar que siga siendo viable y seguro para su uso a largo plazo, incluido el proyecto reciente para limpiar y pintar el puente, así como un proyecto en 2017 para reemplazar la cubierta de hormigón en los carriles que van hacia el norte y el sur.

Un equipo de administración biestatal está trabajando arduamente en varias actividades para posicionar el corredor del puente Brent Spence para futuras mejoras.

El equipo de administración biestatal está totalmente comprometido y trabajando para completar las tareas para posicionar el proyecto para oportunidades de financiamiento y la entrega de las mejoras del corredor del puente Brent Spence y el nuevo puente complementario sobre el río Ohio.

Estas son las actividades que se están realizando:

- Investigar e identificar opciones potenciales para una estructura de gobierno del Proyecto para los dos estados. Esto incluye lo siguiente:
 - Analizar los riesgos y oportunidades de cada estructura.
 - Determinar el proceso legal y las acciones necesarias.
 - Evaluar las estrategias y estructuras utilizadas en otros proyectos en los Estados Unidos.
- Preparar una actualización del plan financiero para el proyecto que incluya lo siguiente:
 - Evaluar la capacidad y el uso potencial de los fondos estatales para Ohio y Kentucky.
 - Evaluar la capacidad y el uso potencial de fondos federales y no tradicionales.
 - Preparar un memorándum técnico con una estructura de financiamiento recomendada para una entrega expedita del proyecto.
- Actualizar proyecciones, modelos y pronósticos de tráfico. Se incluye lo siguiente:

- recomendada para una entrega expedita del proyecto.
- Actualizar proyecciones, modelos y pronósticos de tráfico. Se incluye lo siguiente:
 - Actualizar las previsiones de tráfico existentes a las condiciones actuales.
 - Preparar un modelo de demanda de tráfico actualizado para proporcionar una base para futuros pronósticos y modelos de conceptos.
 - Preparar un memorándum técnico de previsión de viajes que resuma los supuestos de referencia y el proceso para realizar las previsiones de viajes.
 - Realizar un análisis del modelo de demanda de los conceptos del Proyecto.
 - Actualizar conceptos de diseño y estimaciones de costos del proyecto. Estos incluyen lo siguiente:
 - Actualizar las estimaciones de costos del Proyecto para la alternativa preferida actualmente.
 - Evaluar conceptos de diseño adicionales para reducir impactos y costos.
 - Desarrollar estimaciones de costos del proyecto para los conceptos de diseño adicionales.
 - Reagrupación del [Comité asesor del proyecto](#) con una posible reunión en la primavera de 2022.

Manténgase conectado

El sitio web se someterá a un cambio de imagen a principios de 2022 para que sea aún más fácil de usar. Si tiene comentarios sobre los cambios que le gustaría ver en el sitio web, envíe un comentario.

Visite el sitio web para obtener actualizaciones


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BRENT SPENCE
BRIDGE CORRIDOR



May 2022 E-Newsletter

The Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT) are working together to pursue every available federal dollar.

On February 28, 2022, Kentucky Governor Andy Beshear and Ohio Governor Mike DeWine executed a Memorandum of Understanding (MOU) further solidifying the strong partnership between the two states. The MOU authorized the two state departments of transportation to move full steam ahead on project planning and to take the necessary steps to obtain project funds through federal grant applications as part of the federal Bipartisan Infrastructure Law.



"For decades, the backups on the Brent Spence Bridge have frustrated drivers, hindered economic development and slowed supply chain deliveries. Today, a solution is in reach, and we are committed to aggressively working together to secure this funding to help us fix this transportation nightmare once and for all. Not only will this project improve quality of life for drivers in Ohio and Kentucky but keeping this major transportation network open and moving will also have a significant positive impact on our national economy and national security."

-Governor Mike DeWine

"With today's signing, the Commonwealth of Kentucky and the State of Ohio are aligning our efforts to make this project a reality. This memorandum spells out our obligations and positions us to quickly apply for these federal dollars, which will allow us not only to build this new bridge, but to do it without tolls. And just as important: This project shows what we in government can do when we embrace cooperation and progress and simply do what is best for our people."

-Governor Andy Beshear

As part of one of the largest infrastructure funding packages ever created in the United States, the Brent Spence Bridge Corridor project team will pursue federal funding through multiple grant opportunities including the Multimodal Projects Discretionary Grant, which includes funding set aside for projects that are so large that traditional funding and grant mechanisms are not sufficient. For more information on these grant opportunities, please see [Bipartisan Infrastructure Law](#). To read the IJA language, please visit [Infrastructure Investment and Jobs Act](#).

To solidify the success of obtaining this funding, both states have made efforts to advance state funding and state projects surrounding the Brent Spence Bridge Corridor. Kentucky's legislature recently passed the budget bill that includes funding to fulfill state match requirements for large projects. The State of Ohio is actively developing its financing strategy that will be included as a commitment to the grant application. The grant application is due May 23, 2022.

May 2022 E-Newsletter

Get Involved



Show your support for the Brent Spence Bridge Corridor grant application by signing or submitting a letter of support. The deadline is May 31, 2022. Individuals, organizations and businesses can email a signed letter to info@brentspencebridgecorridor.com.

You may also add your signature to an existing letter of support via the website:

[View the letter of support](#)

The Bi-State Management Team is hard at work on various activities to position the Brent Spence Bridge Corridor for future improvements.

The Bi-State Management Team is fully engaged and working to complete tasks to position the project for funding opportunities and delivery of the Brent Spence Bridge Corridor improvements and new companion bridge over the Ohio River.

Opportunities to reduce the cost of the preferred alternative are being evaluated. The project goals to improve safety and ease congestion remain unchanged and can be accomplished through providing additional capacity by building a new companion structure west of the existing bridge that will separate local and through traffic. The preferred alternative – with potential adjustments to design – meets that objective.

The table below shares updates on the project status.

	Preparing a Financial Plan Update for the Project Including: <ul style="list-style-type: none">• Determining the capacity and potential use of state funding for both Ohio and Kentucky.• Evaluating the capacity and potential use of federal funding, including federal grant programs.• Preparing a technical memorandum with a recommended funding/financing structure for an expeditious delivery of the Project.
	Preparing Grant Applications for Federal Funding Opportunities: <ul style="list-style-type: none">• Identifying the appropriate grants to pursue.• Gathering data and letters of support.• Preparing a grant application for a federal grant.
	Updating Traffic Projections, Models, and Forecasts Including: <ul style="list-style-type: none">• Performing demand model analysis of the project concepts.• Performing an operational analysis of the value engineering concepts.• Preparing a Travel Forecasting Technical Memorandum summarizing the baseline assumptions and process for performing the travel.
	Updating Design Concepts and Project Cost Estimates Including: <ul style="list-style-type: none">• Updating project cost estimates for the preferred alternative.• Continuing to evaluate value engineering design concepts to reduce impacts and costs.• Developing spot improvements to accommodate traffic modeling and forecasts.
	Updating the Approved Environmental Document: <ul style="list-style-type: none">• Assessing changes that have occurred in the project corridor since the last re-evaluation.• Performing additional surveys in the project corridor.• Updating the environmental document for coordination with and review by FHWA.
	Other Updates <ul style="list-style-type: none">• Project Advisory Committee meeting in late June.

May 2022 E-Newsletter

Timeline

The graphic below shows the anticipated timeline for the Brent Spence Bridge Corridor and companion bridge activities.



May 2022 E-Newsletter

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Boletín electrónico del corredor del puente Brent Spence, mayo de 2022



BRENT SPENCE
BRIDGE CORRIDOR



El Gabinete de Transporte de Kentucky (KYTC) y el Departamento de Transporte de Ohio (ODOT) están trabajando juntos para aprovechar cada dólar federal disponible.

El 28 de febrero de 2022, el gobernador de Kentucky, Andy Beshear, y el gobernador de Ohio, Mike DeWine, firmaron un Memorando de Entendimiento (MOU) que consolida aún más la sólida

El 28 de febrero de 2022, el gobernador de Kentucky, Andy Beshear, y el gobernador de Ohio, Mike DeWine, firmaron un Memorando de Entendimiento (MOU) que consolida aún más la sólida asociación entre ambos estados. El MOU autorizó a los dos departamentos de transporte estatales a avanzar en la planificación del proyecto y a tomar las medidas necesarias para obtener fondos del proyecto a través de solicitudes de subvenciones federales como parte de la Ley de Infraestructura Bipartidista federal.



"Durante décadas, los atascos en el puente Brent Spence han frustrado a los conductores, obstaculizado el desarrollo económico y ralentizado las entregas de la cadena de suministro. Hoy hay una solución viable, y estamos comprometidos a trabajar enérgicamente juntos para asegurar esta financiación que nos ayude a solucionar esta pesadilla del transporte de una vez por todas. Este proyecto no solo mejorará la calidad de vida de los conductores en Ohio y Kentucky, sino que mantener abierta y en movimiento esta importante red de transporte también tendrá un impacto positivo significativo en nuestra economía nacional y seguridad nacional".

-Gobernador Mike DeWine

"Con la firma de hoy, los estados de Kentucky y Ohio están alineando sus esfuerzos para hacer realidad este proyecto. Este memorándum detalla nuestras obligaciones y nos posiciona para solicitar rápidamente dólares federales, que nos permitirán no solo construir este nuevo puente, sino hacerlo sin peajes. E igual de importante: este proyecto muestra lo que podemos hacer en el gobierno cuando adoptamos la cooperación y el progreso y simplemente hacemos lo que es mejor para nuestra gente".

- Gobernador Andy Beshear

Como parte de uno de los paquetes de financiamiento de infraestructura más grandes creados en los Estados Unidos, el equipo del proyecto del corredor del puente Brent Spence buscará financiamiento federal a través de múltiples oportunidades de subvenciones, incluida la Subvención discrecional para proyectos multimodales, que incluye fondos reservados para proyectos que son tan grandes que los mecanismos tradicionales de financiación y subvenciones no son suficientes. Para obtener más información sobre estas oportunidades de subvenciones, consulte la [Ley de infraestructura bipartidista](#). Para leer el lenguaje IJJA, visite la [Ley de inversión en infraestructura y empleos](#).

Para solidificar el éxito de la obtención de estos fondos, ambos estados se han esforzado por promover los fondos estatales y los proyectos estatales relacionados con el corredor del puente Brent Spence. La legislatura de Kentucky aprobó recientemente el proyecto de ley de presupuesto que incluye fondos para cumplir con los requisitos estatales de contrapartida para grandes proyectos. El estado de Ohio está desarrollando activamente su estrategia de financiación que se incluirá como compromiso en la solicitud de subvención. La solicitud de subvención vence el 23 de mayo de 2022.

Involúcrese



Muestre su apoyo a la solicitud de subvención del corredor del puente Brent Spence firmando o enviando una carta de apoyo. La fecha límite es el 31 de mayo de 2022. Las personas, organizaciones y empresas pueden enviar una carta firmada por correo electrónico a info@brentspencebridgecorridor.com.

También puede agregar su firma a una carta de apoyo existente a través del sitio web:

[Vea la carta de apoyo](#)




El equipo de administración biestatal está trabajando arduamente en varias actividades para posicionar el corredor del puente Brent Spence para futuras mejoras.

El equipo de administración biestatal está totalmente comprometido y trabajando para completar las tareas para posicionar el proyecto para oportunidades de financiamiento y la entrega de las mejoras del corredor del puente Brent Spence y el nuevo puente complementario sobre el río Ohio.

Se están evaluando oportunidades para reducir el costo de la alternativa preferida. Los objetivos del proyecto para mejorar la seguridad y aliviar la congestión permanecen sin cambios y se pueden lograr proporcionando capacidad adicional mediante la construcción de una nueva estructura complementaria al oeste del puente existente que separará el tráfico local y de paso. La alternativa preferida, con posibles ajustes al diseño, cumple con ese objetivo.

La siguiente tabla comparte actualizaciones sobre el estado del proyecto.

	Preparación de una actualización del plan financiero para el proyecto que incluya: <ul style="list-style-type: none">• Determinar la capacidad y el uso potencial de los fondos estatales para Ohio y Kentucky.• Evaluar la capacidad y el uso potencial de los fondos federales; incluidos los programas de subvenciones federales.• Preparar un memorándum técnico con una estructura de financiación/financiamiento recomendada para una entrega expedita del proyecto.
	Preparación de las solicitudes de subvención para las oportunidades de financiación federal: <ul style="list-style-type: none">• Identificar las subvenciones apropiadas a solicitar.• Reunir datos y cartas de apoyo.• Preparar una solicitud de subvención federal.
	Actualización de proyecciones, modelos y pronósticos de tráfico que incluye: <ul style="list-style-type: none">• Realizar un análisis del modelo de demanda de los conceptos del proyecto.• Realizar un análisis operativo de los conceptos de ingeniería valor.• Preparar un memorándum técnico de previsión de viajes que resuma los supuestos de referencia y el proceso para realizar las previsiones de viajes.
	Actualización de conceptos de diseño y estimaciones de costos del proyecto, que incluyen: <ul style="list-style-type: none">• Actualizar las estimaciones de costos del Proyecto para la alternativa preferida.• Continuar evaluando los conceptos de diseño de ingeniería de valor para reducir impactos y costos.

	<ul style="list-style-type: none"> • Actualizar las estimaciones de costos del Proyecto para la alternativa preferida. • Continuar evaluando los conceptos de diseño de ingeniería de valor para reducir impactos y costos. • Desarrollar mejoras puntuales para adaptar el modelado y las previsiones de tráfico.
	<p>Actualización del documento medioambiental aprobado:</p> <ul style="list-style-type: none"> • Evaluar los cambios que se han producido en el corredor del proyecto desde la última reevaluación. • Realizar inspecciones adicionales en el corredor del proyecto. • Actualizar el documento medioambiental para su coordinación y revisión por parte de FHWA.
	<p>Otras actualizaciones</p> <ul style="list-style-type: none"> • Reunión del Comité asesor del proyecto a fines de junio.

Línea de tiempo

El siguiente gráfico muestra el cronograma anticipado para las actividades del corredor del puente Brent Spence y el puente complementario:





2024-2028

Manténgase conectado

Hay varias maneras de mantenerse conectado con el proyecto del corredor del puente Brent Spence.

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August 2022 E-Newsletter



Second Federal Funding Request Submitted



Earlier this month, Ohio Governor Mike DeWine and Kentucky Governor Andy Beshear announced that a second federal funding application was submitted jointly by the two states to support bridge and roadway improvements along the eight-mile Brent Spence Bridge Corridor from the Western Hills Viaduct in Ohio to Dixie Highway in Kentucky.

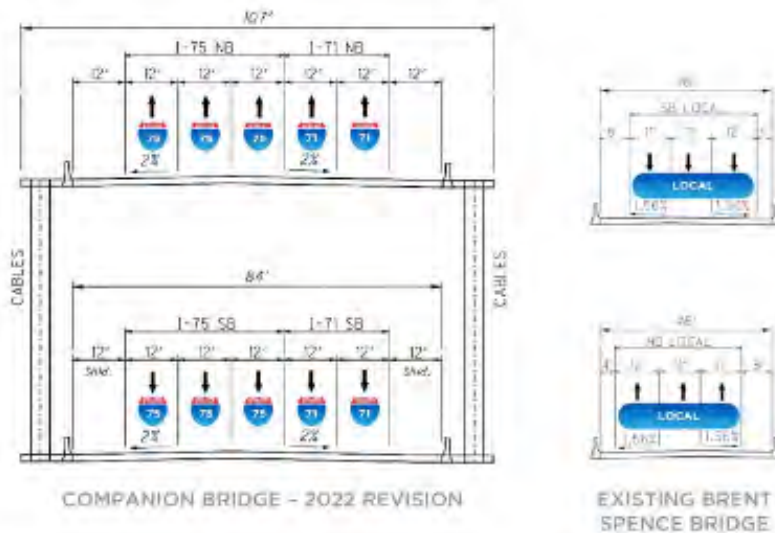
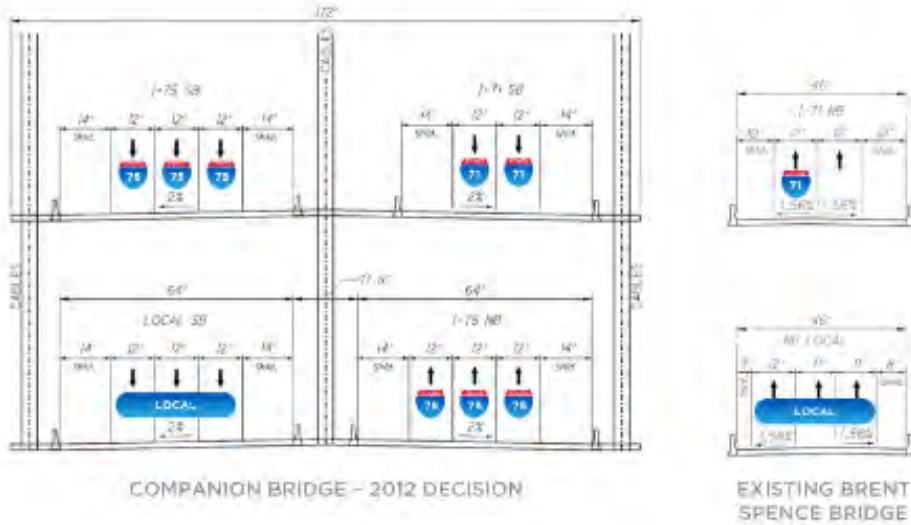
The new funding request was made to the [Bridge Investment Program](#) and is in addition to the still-pending [May 2022 funding request](#) made to the [Multimodal Projects Discretionary Grant Program](#) (MEGA). Ohio and Kentucky articulated in both applications that a total of \$1.66 billion in federal grant funding is needed regardless of which discretionary grant awards funds to the project.

The states are applying to multiple Bipartisan Infrastructure Law (BIL) grant programs to give themselves the best chance of receiving maximum funding, a strategy that is in line with the [pledges of Governors DeWine and Beshear to pursue every federal dollar available for this bridge project](#).

[READ THE ANNOUNCEMENT](#)

New Plans for BSB Corridor Focus on Smaller Footprint

Based on significant community engagement, as well as a thorough technical analysis, the footprint of the new bridge that will be built to the west of the existing Brent Spence Bridge has been dramatically reduced from the alternative approved in 2012.



Initial plans included two 14-foot shoulders on each deck of the companion bridge and expanded shoulder widths on the existing bridge. Revised plans show the new bridge at almost half the size of the 2012 footprint.

[New illustrations](#) show widened emergency shoulders on the existing Brent Spence Bridge to safely stow stalled vehicles, and 12-foot shoulders on the new companion bridge. Additionally, interstate and local traffic are separated – the new companion structure will carry I-71/75 traffic; local traffic will use the existing Brent Spence Bridge.

Plan Revisions and Ongoing Work Reflect Commitment to Public Engagement

Plans to significantly reduce the footprint of the companion bridge that were approved in 2012 are a direct result of ongoing dialogue with local municipalities and key stakeholders within the project area.

Local partners have been integral to the planning process for more than a decade. Their feedback and involvement in the public engagement process in 2012 helped to shape the plans that were approved at that time. Without a practical funding plan, project-related conversations slowed down until passage of the Bipartisan Infrastructure Law (BIL) created a viable path forward for the project.

With a mutual commitment to working together to improve the Brent Spence Bridge Corridor and investing in local communities, the project team has had multiple meetings with local partners like the City of Covington and the City of Cincinnati, as well as several other municipalities throughout the project area, to gather feedback, address concerns, and keep the project moving forward.

The project team has also engaged an [Advisory Committee](#) to guide decision-making throughout the project development process. This group met in June of this year and continues to receive updates from the project team, with additional meetings to be held in the coming months.

Standing side-by-side with our local partners, ODOT and KYTC expect to have a Design Build Team selected and project groundbreaking in 2023, with construction activities anticipated to begin in earnest in 2024.

ICYMI: Brent Spence Bridge Featured in the *Cincinnati Enquirer*

NEWS

Five things to know as Brent Spence Bridge project marks another milestone

After close to two decades of discussion and planning, Ohio and Kentucky took another giant step this week on plans to build a companion bridge next to the [Brent Spence Bridge](#).



The Brent Spence Bridge - built in 1963 and maintained by the Kentucky Transportation Cabinet - transports about 160,000 vehicles a day over the Ohio River. That is twice the volume it was designed for. Kareem Elgazzar/The Enquirer

Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

[Visit the project website](#)

[Sign up for updates](#)

[Submit a comment](#)

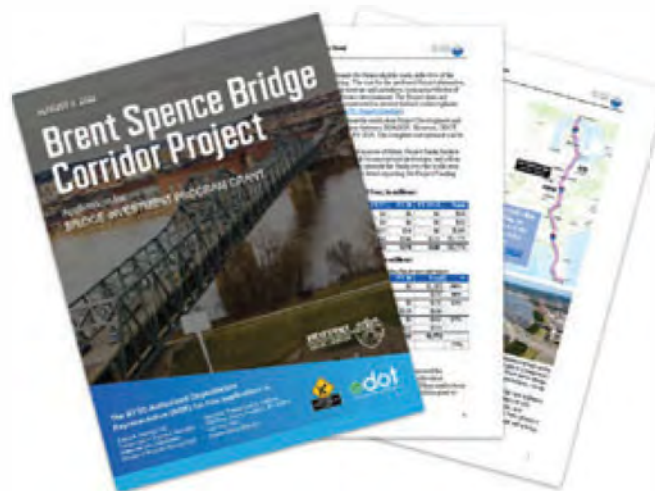
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Boletín electrónico del corredor del puente Brent Spence, agosto de 2022



INVIRTIENDO EN LAS COMUNIDADES LOCALES.
HACIENDO CRECER LA ECONOMÍA ESTADOUNIDENSE.

Segunda solicitud de financiamiento federal presentada



A principios de este mes, el gobernador de Ohio, Mike DeWine, y el gobernador de Kentucky, Andy Beshear, anunciaron que los dos estados presentaron conjuntamente una segunda solicitud de financiamiento federal para apoyar las mejoras de puentes y carreteras a lo largo del corredor del puente Brent Spence, de ocho millas, desde el vialdueto de Western Hills, en Ohio, hasta la autopista Dixie, en Kentucky.

La nueva solicitud de financiamiento se hizo al [Programa de inversión en puentes](#) y se

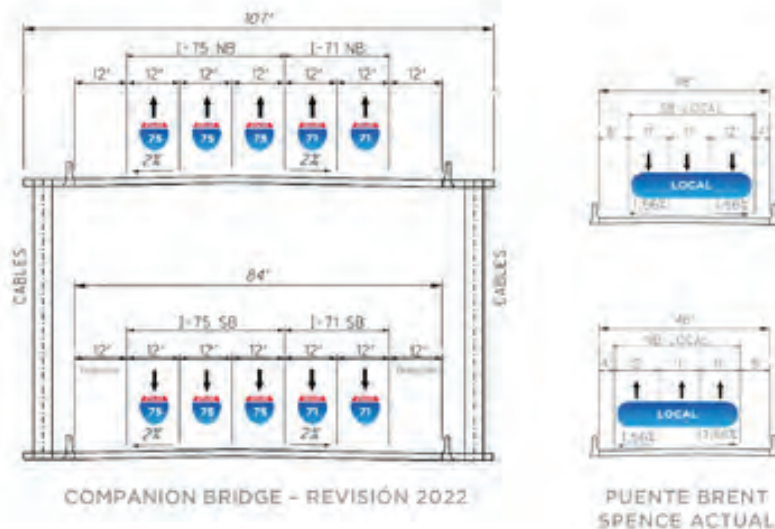
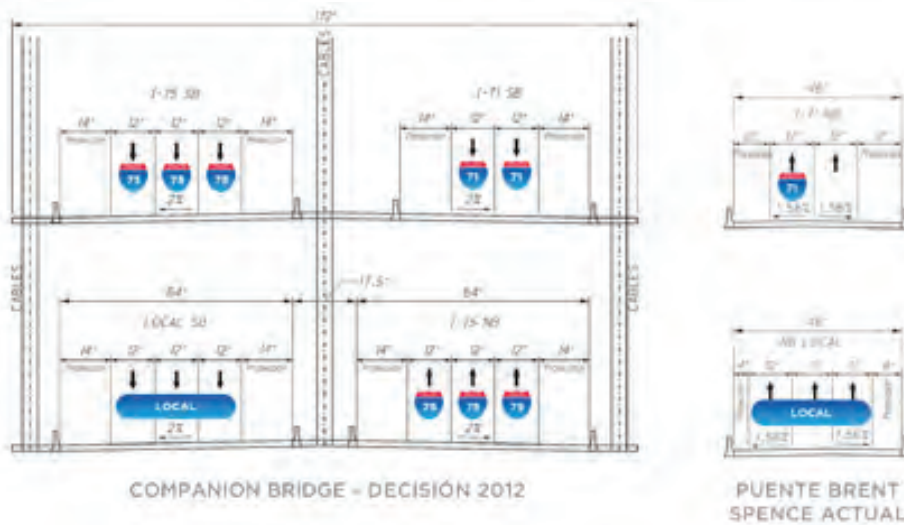
suma a la solicitud de financiamiento aún pendiente, de [mayo de 2022](#), realizada al [Programa de Subvenciones Discrecionales para Proyectos Multimodales \(MEGA](#), por sus siglas en inglés). Ohio y Kentucky expresaron en ambas solicitudes que se necesita un total de \$1,660 millones en fondos de subvenciones federales, independientemente de qué subvenciones discrecionales otorguen fondos al proyecto.

Los estados están solicitando múltiples programas de subvenciones de la Ley de Infraestructura Bipartidista (BIL) para tener la mejor oportunidad de recibir la máxima financiación, una estrategia coherente con las promesas de los gobernadores DeWine y Beshear de buscar cada dólar federal disponible para este proyecto de puente.

LEA EL ANUNCIO

Los nuevos planes para el corredor BSB se centran en un impacto más pequeño

En base a la participación significativa de la comunidad, así como a un análisis técnico exhaustivo, el impacto del nuevo puente que se construirá al oeste del puente Brent Spence existente se ha reducido drásticamente con respecto a la alternativa aprobada en 2012.



Los planes iniciales incluían dos arcenes de 14 pies en cada cubierta del puente complementario y anchos de arcén ampliados en el puente existente. Los planos revisados muestran que el nuevo puente tiene casi la mitad del tamaño de impacto que en 2012.

Las [nuevas ilustraciones](#) muestran arcenes de emergencia ampliados en el puente Brent Spence existente para guardar de manera segura los vehículos averiados, y arcenes de 12 pies en el nuevo puente complementario. Además, el tráfico interestatal y local están separados: la nueva estructura complementaria transportará el tráfico I-71/75; el tráfico local utilizará el puente Brent Spence existente.

Las revisiones del plan y el trabajo en curso reflejan el compromiso con la participación pública

Los planes para reducir significativamente el impacto del puente complementario, que se aprobaron en 2012, son el resultado directo del diálogo continuo con los municipios locales y las partes interesadas clave dentro del área del proyecto.

Los socios locales han sido parte integral del proceso de planificación durante más de una década. Sus comentarios y participación en el proceso de participación pública en 2012 ayudaron a dar forma a los planes que se aprobaron en ese momento. Sin un plan de financiamiento práctico, las conversaciones relacionadas con el proyecto se desaceleraron hasta que la aprobación de la Ley de Infraestructura Bipartidista (BIL) creó un camino viable para el proyecto.

Con un compromiso mutuo de trabajar juntos para mejorar el corredor del puente Brent Spence e invertir en las comunidades locales, el equipo del proyecto ha tenido varias reuniones con socios locales, como la ciudad de Covington y la ciudad de Cincinnati, así como con varios otros municipios a lo largo del proyecto, para recopilar comentarios, abordar inquietudes y hacer que el proyecto avance.

El equipo del proyecto también ha contratado a un [Comité asesor](#) para guiar la toma de decisiones a lo largo del proceso de desarrollo del proyecto. Este grupo se reunió en junio de este año y continúa recibiendo actualizaciones del equipo del proyecto, con reuniones adicionales que se llevarán a cabo en los próximos meses.

Junto con nuestros socios locales, ODOT y KYTC esperan tener un equipo de diseño y construcción seleccionado y el inicio del proyecto en 2023, y se prevé que las actividades de construcción empiecen a comienzos del 2024.

ICYMI: El puente Brent Spence aparece en el *Cincinnati Enquirer*

NEWS

Five things to know as Brent Spence Bridge project marks another milestone

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Manténgase conectado

Hay varias maneras de mantenerse conectado con el proyecto del corredor del puente Brent Spence.

[Visite el sitio web del proyecto](#)

[Regístrese para recibir novedades](#)

[Envíe un comentario](#)

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Public involvement guides everything we do

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are fully committed to robust public involvement to gain insights and receive feedback as part of the project development process.

ODOT and KYTC have engaged in thorough public involvement since the inception of the Brent Spence Bridge Corridor project and have worked closely with local partners to incorporate changes to the project that have already significantly reduced local impacts while investing in underserved populations on both sides of the river.

The National Environmental Policy Act of 1969 (NEPA) requires projects funded with federal money to adhere to strict guidelines that support decision-making and ensure that agencies assess the environmental effects of their proposed actions prior to making decisions. Agencies also are required to provide opportunities for the general public, as well as those who are impacted by potential projects, to review and comment on their evaluations.

We've added some new information to our website to help you learn more about this process.

[Read more on our website](#)

Teamwork makes the dream work – or the Corridor work, as the case may be

In a strong show of support for the Brent Spence Bridge Corridor project, KYTC Secretary Jim Gray, ODOT Director Jack Marchbanks, Cincinnati Mayor Aftab Pureval, and several other members of the project team met with various representatives from the Federal Highway Administration to provide an update on project planning and current work.



The Brent Spence Bridge Corridor project is ready to go. After funding is secured, the project team is prepared to break ground in late 2023.



The project team continues to work hand-in-hand with local officials to identify every opportunity to reduce the footprint of the project and invest in changes that support and connect communities throughout the corridor.

We want to be good neighbors and create better connections

The Brent Spence Bridge Corridor project brings the opportunity to address shared community priorities, including:

- Reconnecting communities through multi-modal and pedestrian-friendly improvements
- Returning public land for local use
- Separating local and through traffic to improve safety and support better access to the Covington and Cincinnati business districts
- Honoring local requests to separate highway runoff from the combined sewer system
- Investing in much-needed quality of life amenities in urban communities within the corridor
- Providing aesthetic features that establish a more dynamic sense of place in urban neighborhoods
- Completing and investing in necessary bike, pedestrian, and roadway connections in both Cincinnati and Covington that will enhance connections between the east and west sides of the freeway and will accommodate the recent RAISE and INFRA investments in the City of Cincinnati
- Continuing public engagement as project plans are refined
 - Upcoming one-on-one meetings with directly-affected communities are being scheduled now
 - Additional neighborhood meetings to be held later this year
 - Two public hearings to be held in 2023
 - BrentSpenceBridgeCorridor.com provides project information and opportunity to submit feedback

Recently, ODOT and KYTC received some public comments related to the possibility of lowering and covering I-75. The project team carefully considered these comments. The analysis shows that this concept would be unsafe and impractical to construct.

You can read more here:

[Response to public comment – WORKING POSITION PAPER: REDESIGN OF THE BRENT SPENCE BRIDGE PROJECT](#)

[Response to public comment – BRENT SPENCE BRIDGE PROJECT – RECONNECTING CINCINNATI WESTWAY DESIGN IMPROVEMENTS](#)

Stay Connected

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[Visit the project website](#)

[Sign up for updates](#)

[Submit a comment](#)

[Vea este correo electrónico en su buscador](#)

Boletín electrónico del corredor del puente Brent Spence, octubre de 2022



La participación pública guía todo lo que hacemos

El Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC) están totalmente comprometidos con una sólida participación pública para obtener información y recibir comentarios como parte del proceso de desarrollo del proyecto.

ODOT y KYTC se han involucrado a fondo en la participación pública desde el inicio del proyecto del corredor del puente Brent Spence y han trabajado en estrecha colaboración con socios locales para incorporar cambios al proyecto que ya han reducido significativamente los impactos locales mientras invierten en poblaciones desatendidas en ambos lados del río.

La Ley de Política Ambiental Nacional de 1969 (NEPA) requiere que los proyectos financiados con dinero federal se adhieran a pautas estrictas que respalden la toma de decisiones y garanticen que las agencias evalúen los efectos ambientales de sus acciones propuestas antes de tomar decisiones. También se requiere que las agencias brinden oportunidades para que el público en general y quienes se ven afectados por proyectos potenciales revisen y comenten sus evaluaciones.

Hemos agregado información nueva a nuestro sitio web para ayudarlo a obtener más información sobre este proceso.

[Lea más en nuestro sitio web](#)

El trabajo en equipo hace que el sueño funcione... o el corredor, según sea el caso.

En una fuerte muestra de apoyo al proyecto del corredor del puente Brent Spence, el secretario de KYTC, Tim Gray, el director de ODOT, Jack Mastromarino, y el

En una fuerte muestra de apoyo al proyecto del corredor del puente Brent Spence, el secretario de KYTC, Jim Gray, el director de ODOT, Jack Marchbanks, el alcalde de Cincinnati, Aftab Pureval, y varios otros miembros del equipo del proyecto se reunieron con varios representantes de la Administración federal de carreteras para brindar una actualización sobre la planificación del proyecto y el trabajo actual.



El proyecto del corredor del puente Brent Spence está listo para lanzarse. Una vez asegurada la financiación, el equipo del proyecto está preparado para comenzar a construir a fines de 2023.



El equipo del proyecto continúa trabajando junto con los funcionarios locales para identificar cada oportunidad de reducir el impacto del proyecto e invertir en cambios que apoyen y conecten a las comunidades a lo largo del corredor.

Queremos ser buenos vecinos y crear mejores conexiones

El proyecto del corredor del puente Brent Spence brinda la oportunidad de abordar las prioridades comunitarias compartidas, que incluyen las siguientes:

- Reconectar comunidades a través de mejoras multimodales y amigables para los peatones.
- Devolver terrenos públicos para uso local.
- Separar el tráfico local y directo para mejorar la seguridad y apoyar un mejor acceso

- peatones.
- Devolver terrenos públicos para uso local.
- Separar el tráfico local y directo para mejorar la seguridad y apoyar un mejor acceso a los distritos comerciales de Covington y Cincinnati.
- Cumplir con las solicitudes locales para separar la escorrentía de la carretera del sistema de alcantarillado combinado.
- Invertir en servicios de calidad de vida muy necesarios en comunidades urbanas dentro del corredor.
- Proporcionar características estéticas que establezcan un sentido de lugar más dinámico en los vecindarios urbanos.
- Completar e invertir en las conexiones necesarias para ciclistas, peatones y carreteras, tanto en Cincinnati como en Covington, que mejorarán las conexiones entre los lados este y oeste de la autopista e incorporarán las recientes inversiones RAISE e INFRA en la ciudad de Cincinnati.
- Participación pública continua a medida que se refinan los planes del proyecto.
 - Las próximas reuniones individuales con las comunidades directamente afectadas se están programando ahora.
 - Reuniones vecinales adicionales que se llevarán a cabo a fines de este año.
 - Se realizarán dos audiencias públicas en 2023.
 - En BrentSpenceBridgeCorridor.com se puede encontrar información del proyecto y la oportunidad de enviar comentarios.

Recientemente, ODOT y KYTC recibieron algunos comentarios públicos relacionados con la posibilidad de bajar y cubrir la I-75. El equipo del proyecto consideró cuidadosamente estos comentarios. El análisis muestra que este concepto sería inseguro y poco práctico de construir.

Aquí encontrará más información:

[Respuesta a un comentario público. DOCUMENTO DE PUESTO DE TRABAJO: REDISEÑO DEL PROYECTO DEL CORREDOR DEL PUENTE BRENT SPENCE](#)

[Respuesta a un comentario público. PROYECTO DEL PUENTE BRENT SPENCE. RECONECTAR CINCINNATI MEJORAS DE DISEÑO DE WESTWAY](#)

Manténgase conectado

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Visite el sitio web del proyecto

Regístrese para recibir novedades

Envíe un comentario



The Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT) continue working together to advance the project.

Following a successful Industry Forum that was hosted back in June, the project team has continued to discuss various items that were raised during meetings held with members of the design-build community. To offset the uncertainties in today's construction market, including inflation and supply chain pricing and availability, the project team has made the decision to move forward with Progressive Design-Build Procurement, which will allow the Owners (ODOT and KYTC) and the Design-Build Team (Design Firm and Contractors) to price materials closer to when they will actually be used for construction.

The Design-Build Team will be selected based on qualifications while considering their pricing approach rather than qualifications and just a fixed or lowest bidder price. This approach also allows the project team to work collaboratively on the preferred alternative with the Design-Build Team to identify cost-effective solutions that meet the goals of the project and community. It also brings more certainty to our project delivery process and eliminates several variables for those seeking to bid on the project.

The decision to move forward with the Progressive Design-Build Procurement will allow ODOT and KYTC to meet the goal of having a Design-Build Team selected and project groundbreaking in 2023, with construction activities expected to begin in earnest in 2024.

[Read more on our website](#)

THANK YOU!

With your help, the project received over 200 letters of support for the Multimodal Projects Discretionary Grant (MEGA) and Bridge Investment Program grant applications, which were submitted in May and August respectively. These letters show community and business support for this project and will underscore the importance and impact this project will have on the region to decision makers at the United States Department of Transportation.

As the project team waits for grant announcements, project development continues ensuring the project is well positioned to move forward when necessary funding is obtained.

Out in the Community



The Brent Spence Bridge Corridor project team is committed to community outreach and is excited to share details below and over future newsletters. Various committee and neighborhood meetings are being planned so keep an eye out for opportunities to join in. Below is a breakdown of recent and upcoming outreach.

Project Advisory Committee Meeting



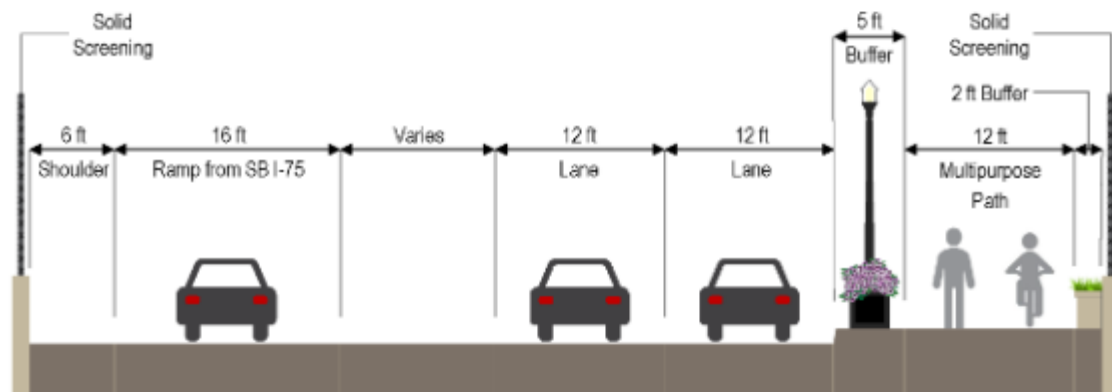
On June 29, 2022, a Project Advisory Committee meeting was held in Cincinnati, Ohio. The Committee members heard from Stacey Hans and Gary Valentine from the Kentucky Transportation Cabinet, Stefan Spinosa from the Ohio Department of Transportation, and Mark Becherer from HNTB.

Project history, design and grant application updates and procurement next steps were discussed. To see a copy of the meeting summary, please [click here](#).

Aesthetics Committee

The Aesthetics Committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge, companion bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor. A list of the Aesthetics Committee members can be found [here](#).

ODOT held a meeting of this committee for the two northern phases under development (north of Linn St.) on June 28, 2022. ODOT's Aesthetic Design Guidelines and aesthetic options for the corridor were presented. Feedback was received from the City of Cincinnati and other committee members. The below image is a proposed rendering for the West 7th Street Bridge over I-75. Other overpasses will have a similar appearance.



**West 7th Street Bridge Over I-75
(Looking East)**

KYTC held Aesthetics Committee meetings on June 28 and August 10, 2022. The meetings presented samples of aesthetic designs from other projects, discussed the project's design and opportunities for streetscape improvements and discussed integration with the Covington Master Plan, as well as gateway opportunities for the Cities of Fort Wright and Fort Mitchell.

Additional meetings and coordination will continue to occur through the final design of the project. Future updates will be provided in upcoming newsletters.

Neighborhood Outreach

KYTC and ODOT will be scheduling meetings in the neighborhoods and communities surrounding the Brent Spence Bridge Corridor. The purpose of the meetings will be to present the current plans for the Brent Spence Bridge Corridor and to obtain feedback on how the project may affect the people who live near the project area. The outreach will involve small-scale meetings in the neighborhoods directly adjacent to the project and larger-scale meetings for neighborhoods that are further removed from the corridor. These meetings are currently scheduled to begin in November.

Diversity and Inclusion Committee

ODOT and KYTC have established a Diversity and Inclusion Committee that will work to collaborate, coordinate, and communicate information related to disadvantaged business enterprise (DBE) goals, initiatives, and requirements encouraging diversity and inclusion. This will include workforce development measures including on the job training. Over 100 invitations have been sent out to business groups, local chambers of commerce, governments, and elected officials to participate on this committee. Additional information regarding DBE and workforce development outreach events and opportunities will be coming soon.

Project Updates



Traffic

- Completed [Traffic Operations Report](#)*
 - Supported design updates in analyzing traffic operation impacts
 - Preparing Interchange Modification Study for coordination with Federal Highway Administration (FHWA)
-



Design

- Completed [Design Summary Report](#)*
 - Continued to refine the design to minimize impacts
 - Updated the project cost estimate to reflect changing market conditions
-



Supplemental Environmental Assessment (EA)

- Performing surveys to assess changes that have occurred in the Project corridor since the project's Environmental Assessment was approved in 2012
 - Updating the impacts assessment to reflect refinements that have been made to the Preferred Alternative
 - Preparing a Supplemental Environmental Assessment to document the Project's effects on the human and natural environment
 - Environmental project information can be found [here](#)
-



Outreach

- Continuing neighborhood outreach meetings
 - Beginning Diversity and Inclusion Committee meetings
 - Preparing renderings
 - Holding Aesthetic Committee meetings in both Ohio and Kentucky
-



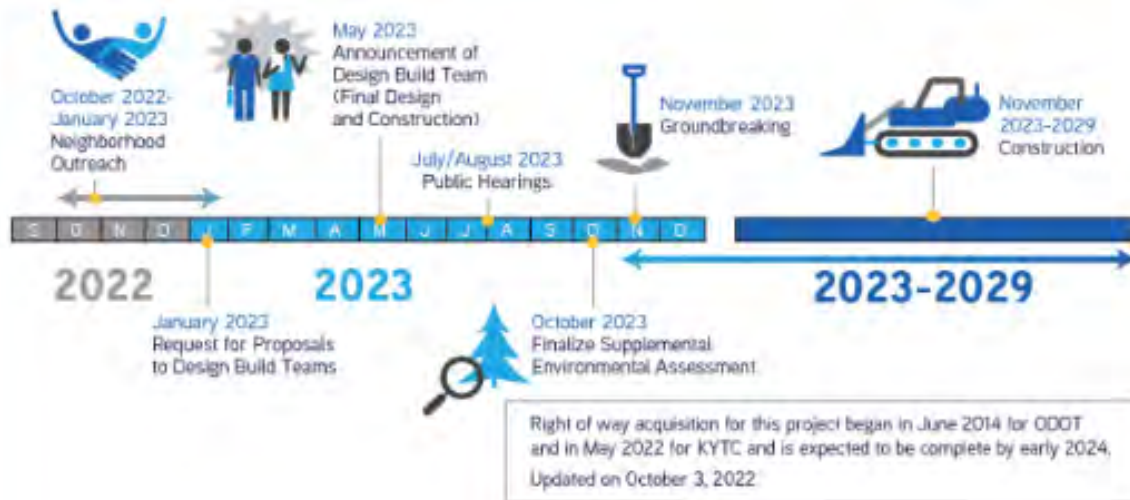
Progressive Design-Build Procurement

- Developing a request for proposal to evaluate and select a Design-Build Team
 - Performing a Cost and Schedule Risk Analysis workshop in conjunction with Federal Highway Administration (FHWA) guidelines
 - Continue to perform industry outreach with contractors
-

*Supporting documentation is available [upon request](#)

Timeline

The below graphic shows the anticipated timeline for the Brent Spence Bridge Corridor and companion bridge activities as of early October. The public hearing has been timed to follow an aggressive public outreach schedule. Another recent change affecting the schedule is the shift from a traditional design-build to progressive design-build.



Stay Connected

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Boletín electrónico del corredor del puente Brent Spence, 21 de octubre de 2022



El Gabinete de Transporte de Kentucky (KYTC) y el Departamento de Transporte de Ohio (ODOT) continúan trabajando juntos para que el proyecto avance.

Tras un foro exitoso de la industria que tuvo lugar el pasado mes de junio, el equipo del proyecto continuó analizando varios asuntos que surgieron durante las reuniones llevadas a cabo con miembros de la comunidad de diseño y construcción. Para compensar las incertidumbres en el mercado actual de la construcción, incluida la inflación y los precios y la disponibilidad de la cadena de suministro, el equipo del proyecto ha tomado la decisión de seguir adelante con la adquisición progresiva de diseño y construcción, lo que permitirá a los propietarios (ODOT y KYTC) y al equipo de

inflación y los precios y la disponibilidad de la cadena de suministro, el equipo del proyecto ha tomado la decisión de seguir adelante con la adquisición progresiva de diseño y construcción, lo que permitirá a los propietarios (ODOT y KYTC) y al equipo de diseño y construcción (empresa de diseño y contratistas) fijar el precio de los materiales más cerca del momento en que estos efectivamente se utilizarán para la construcción.

El equipo de diseño y construcción se seleccionará en función de las cualificaciones y teniendo en cuenta su enfoque de precios, en lugar de las cualificaciones y solo un precio fijo o el más bajo. Este enfoque también permite que el equipo del proyecto trabaje en colaboración en la alternativa preferida con el equipo de diseño y construcción para identificar soluciones rentables que cumplan con los objetivos del proyecto y la comunidad. También aporta más certeza a nuestro proceso de entrega del proyecto y elimina varias variables para aquellos que buscan ofertar por el proyecto.

La decisión de seguir adelante con la adquisición progresiva de diseño y construcción permitirá a ODOT y KYTC cumplir con el objetivo de tener un equipo de diseño y construcción seleccionado y el proyecto innovador en 2023, y se espera que las actividades de construcción comiencen a principios de 2024.

[Lea más en nuestro sitio web](#)

¡GRACIAS!

Con su ayuda, el proyecto recibió más de 200 cartas de apoyo para las solicitudes de subvención de proyectos multimodales discrecionales (MEGA) y del programa de inversión Bridge, que se presentaron en mayo y agosto respectivamente. Estas cartas muestran el apoyo de la comunidad y las empresas a este proyecto y subrayarán la importancia y el impacto que este proyecto tendrá en la región para quienes toman decisiones en el Departamento de Transporte de los Estados Unidos.

Mientras el equipo del proyecto espera los anuncios de subvenciones, el desarrollo del proyecto continúa asegurando que esté bien posicionado para avanzar cuando se obtenga la financiación necesaria.

En la comunidad





El equipo del proyecto del corredor del puente Brent Spence está comprometido con la comunidad y se complace en compartir detalles a continuación y en futuros boletines. Se están planificando varios comités y reuniones de vecinos, así que esté atento para conocer futuras oportunidades en las que sumarse. Más abajo encontrará un desglose de la participación reciente y futura.

Reunión del comité de asesoramiento de proyecto



El 29 de junio de 2022 se realizó una reunión del comité asesor del proyecto en Cincinnati, Ohio. Los miembros del Comité escucharon a Stacey Hans y Gary Valentine, del Gabinete de Transporte de Kentucky, Stefan Spinosa, del Departamento de Transporte de Ohio, y Mark Becherer, de HNTB.

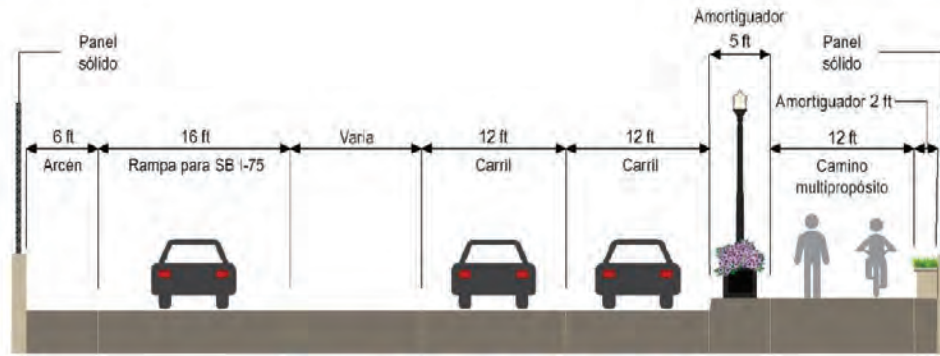
Se abordaron los próximos pasos de adquisición y actualizaciones de la historia del proyecto, el diseño y la solicitud de subsidios. Para ver una copia del resumen de la reunión, haga [clic aquí](#).

Comité de estética

El comité de estética fue creado para dar perspectiva sobre el diseño y la apariencia estética del puente Brent Spence, el puente complementario y el corredor vial. El rol de este grupo es evaluar el tratamiento estético a lo largo del corredor. Aquí puede encontrar una lista de los miembros [del comité](#).

ODOT realizó una reunión de este comité para las dos fases del norte en desarrollo (al norte de Linn St.) el 28 de junio de 2022. Se presentaron las guías de diseño estético de ODOT y las opciones estéticas para el corredor. Se recibieron comentarios de la ciudad de Cincinnati y otros miembros del comité. La siguiente imagen es una representación

ODOT y las opciones estéticas para el corredor. Se recibieron comentarios de la ciudad de Cincinnati y otros miembros del comité. La siguiente imagen es una representación propuesta para el puente de West 7th Street sobre la I-75. Otros pasos elevados tendrán una apariencia similar.



**Puente de West 7th Street sobre I-75
(mirando hacia el este)**

KYTC realizó reuniones con el comité de estética el 28 de junio y el 10 de agosto de 2022. En las reuniones se presentaron muestras de diseños estéticos de otros proyectos, se abordó el diseño del proyecto y las oportunidades para mejorar el paisaje urbano y se analizó la integración con el Plan Maestro de Covington, así como las oportunidades de entrada para las ciudades de Fort Wright y Fort Mitchell.

Se continuarán realizando otras reuniones y coordinación hasta el diseño final del proyecto. Se brindarán más actualizaciones en los próximos boletines.

Participación de los vecinos

KYTC y ODOT estarán coordinando reuniones en los barrios y comunidades aledaños al corredor del puente Brent Spence. El propósito de las reuniones será presentar los planes actuales para el corredor del puente Brent Spence y obtener comentarios sobre cómo el proyecto puede afectar a las personas que viven cerca del área del proyecto. La participación incluirá reuniones a pequeña escala en los vecindarios directamente adyacentes al proyecto y reuniones a mayor escala para los vecindarios que están más alejados del corredor. Estas reuniones actualmente están programadas para comenzar en noviembre.

Comité de diversidad e inclusión

ODOT y KYTC han establecido un Comité de diversidad e inclusión que trabajará para colaborar, coordinar y comunicar información relacionada con los objetivos, iniciativas y requisitos para Empresas Comerciales en Desventaja (DBE), fomentando así la diversidad y la inclusión. Esto incluirá medidas de desarrollo de la fuerza laboral, incluida la capacitación en el trabajo. Se han enviado más de 100 invitaciones a grupos empresariales, cámaras de comercio locales, gobiernos y funcionarios electos para participar en este comité. Próximamente habrá información adicional sobre DBE y eventos y oportunidades de divulgación para el desarrollo de la fuerza laboral.

Actualizaciones del proyecto



Tráfico

Informe de operaciones de tráfico* completado



Tráfico

- [Informe de operaciones de tráfico*](#) completado
- Actualizaciones de diseño admitidas en el análisis de los impactos de las operaciones de tráfico
- Preparación del estudio de modificación de intercambio para la coordinación con la Administración Federal de Carreteras (FHWA)



Diseño

- [Informe de resumen de diseño*](#) completado
- Se continúa refinando el diseño para minimizar los impactos
- Se actualizó la estimación de costos del proyecto para reflejar las condiciones cambiantes del mercado



Evaluación Medioambiental (EA) complementaria

- Realización de encuestas para evaluar los cambios que han realizado en el Proyecto del corredor desde que se aprobó la evaluación ambiental del proyecto en 2012
- Actualizar la evaluación de impactos para reflejar las mejoras que se han realizado a la alternativa preferida
- Preparar una evaluación ambiental adicional para documentar los efectos del proyecto en el medio ambiente natural y humano
- [Aquí](#) se puede encontrar información del proyecto ambiental



Participación

- Continuación de las reuniones de participación para vecinos
- Inicio de las reuniones del Comité de inclusión y diversidad
- Preparación de representaciones
- Realización de reuniones del Comité de estética tanto en Ohio como Kentucky



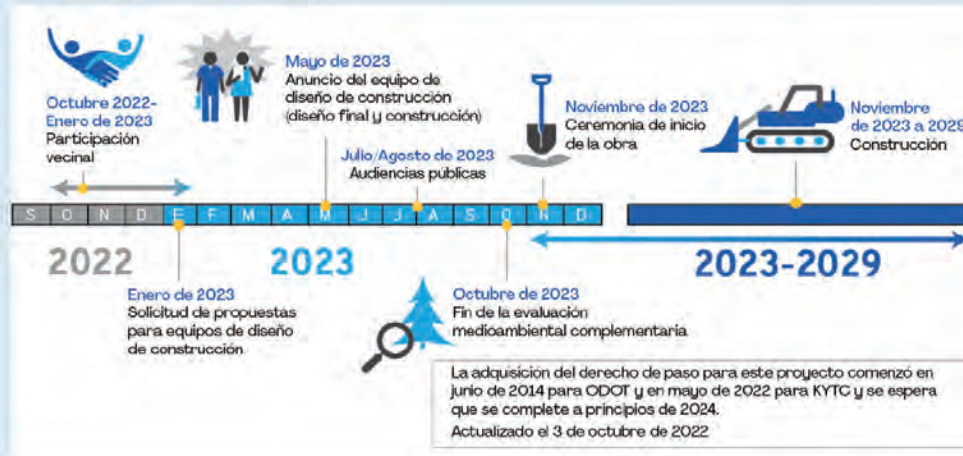
Adquisición progresiva de diseño y construcción

- Desarrollo de una solicitud para evaluar y seleccionar un equipo de diseño y construcción
- Realización de un taller de análisis de riesgo de costo y cronograma en conjunto con las guías de la Administración Federal de Carreteras (FHWA)
- Continuación de las actividades de divulgación en el sector con los contratistas

*Documentación de respaldo disponible [a pedido](#)

Línea de tiempo

El siguiente gráfico muestra el cronograma anticipado para las actividades del corredor del puente Brent Spence y el puente complementario a principios de octubre. La audiencia pública se programó para seguir un cronograma de alcance público intenso. Otro cambio reciente que afecta el cronograma es el cambio de un diseño y construcción tradicional a un diseño y construcción progresivo.



Manténgase conectado

Hay varias maneras de mantenerse conectado con el proyecto del corredor del puente Brent Spence.

Visite la página web del proyecto

Regístrese para recibir actualizaciones

Envíe un comentario

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COMMUNITY ENGAGEMENT WORK DEEPENS FOR BRENT SPENCE BRIDGE CORRIDOR PROJECT

Diversity & Inclusion Committee Holds Inaugural Meeting

Deliberate, intentional, meaningful inclusion. That is the commitment the project team made to the partners who gathered for the inaugural meeting of the Diversity & Inclusion (D&I) Committee last week.



The Brent Spence Bridge Corridor (BSBC) project represents a historic federal investment in our region, and as such brings the opportunity for unprecedented participation by small, minority, and women-owned businesses who are certified as Disadvantaged Business Enterprise (DBE) firms in Ohio and Kentucky, as well as tremendous workforce development opportunities.

ODOT and KYTC created a [Diversity & Inclusion Committee](#) to help deliver strong involvement from partners through planned events and activities, and to support the inclusivity of the team's outreach efforts. Tactics are being developed and implemented to ensure disadvantaged businesses and individuals interested in working on the project are informed about opportunities available and engaged in discussions with potential partners.

The committee held its first meeting on Nov. 7, at the Laborers' International Union. ODOT Director Jack Marchbanks, KYTC Secretary Jim Gray, representatives from the project team, ODOT's [Division of Opportunity, Diversity, and Inclusion](#), and KYTC's [Office for Civil Rights and Small Business Development](#) were present, along with several dozen committee members representing various organizations, including the public and private sectors, labor, and workforce development groups.

November 10, 2022 E-Newsletter



"Our discussion about the benefits of this project often focus on improving travel and easing congestion," said ODOT Project Director Tommy Arnold. "But there's so much more to it than that," he continued. "The benefits of this project will be shared by our entire community – in large part because of the enormous workforce opportunities that exist. This committee will help us get there."

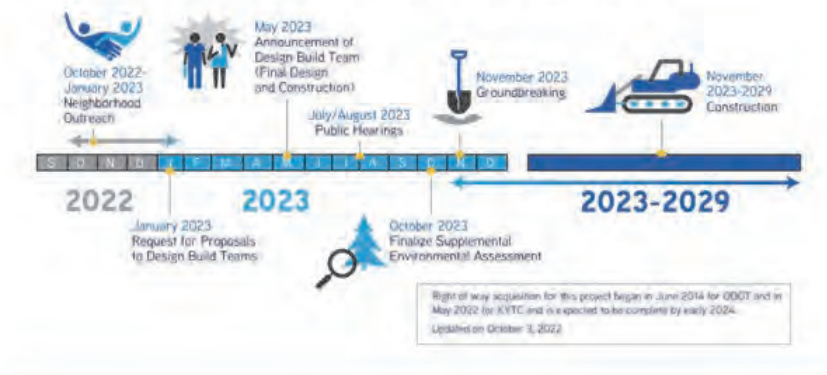
Committee members expressed appreciation to the project team for the opportunity to help shape workforce engagement priorities on the front end of the project, as the scope of work is being identified and developed. The committee will meet quarterly.

Neighborhood Outreach Meetings Kick Off Next Week

Starting next week, the project team will be out in the community attending various meetings in neighborhoods that are directly adjacent to the project corridor.

Project team members will give an update on project status, receive feedback from those in attendance, and share additional opportunities for engagement.

Additional meetings, as well as a formal public hearing, will be held in 2023.

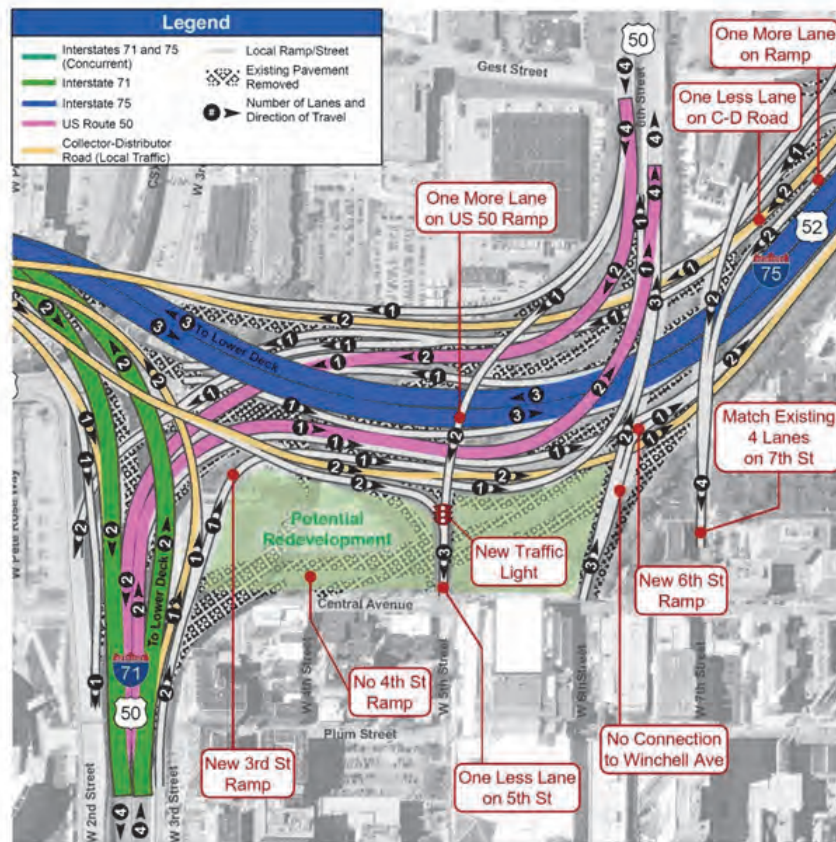


Shared Community Priorities Continue to be Reflected in Project Decisions

In an earlier project update, we detailed some of the changes that have been made to the project based on feedback from several local partners. As conversations with key stakeholders have continued, the opportunity to further refine the project's footprint have been identified.

After careful review, discussion, and technical evaluation of the recommendations received from the City of Cincinnati and the Cincinnati USA Regional Chamber, the project team made several additional changes to project plans, which will open up nearly 10 acres of additional land for local development.

These changes meet project goals and align with shared priorities to reconnect communities through multi-modal and pedestrian-friendly improvements and return public land for local use.



[Read more here.](#)

Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

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Boletín electrónico del corredor del puente Brent Spence, 10 de noviembre de 2022



INVIRTIENDO EN LAS COMUNIDADES LOCALES.
HACIENDO CRECER LA ECONOMÍA ESTADOUNIDENSE.



BrentSpenceBridgeCorridor.com



EL TRABAJO DE PARTICIPACIÓN COMUNITARIA SE PROFUNDIZA PARA EL PROYECTO DEL CORREDOR DEL PUENTE BRENT SPENCE

El Comité de Diversidad e Inclusión celebra la reunión inaugural

Inclusión deliberada, intencional y significativa. Ese es el compromiso que el equipo del proyecto asumió ante los socios que se dieron cita en la reunión inaugural del Comité de Diversidad e Inclusión (D&I) la semana pasada.



El proyecto del Corredor del Puente Brent Spence (BSBC) representa una inversión federal histórica en nuestra región y, como tal, trae consigo la oportunidad de una participación sin precedentes de empresas pequeñas, que pertenecen a minorías y a mujeres, certificadas como Empresas en Desventaja (DBE) en Ohio y Kentucky, y brinda grandes oportunidades de desarrollo de la fuerza laboral.

El Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC) crearon un [Comité de Diversidad e Inclusión](#) para ayudar a generar una importante participación de los socios mediante actividades y eventos planificados, y para apoyar la inclusividad de los esfuerzos de divulgación del equipo. Se están desarrollando e

participación de los socios mediante actividades y eventos planificados, y para apoyar la inclusión de los esfuerzos de divulgación del equipo. Se están desarrollando e implementando tácticas para garantizar que las empresas desfavorecidas y las personas interesadas en trabajar en el proyecto estén informadas sobre las oportunidades disponibles y participen en conversaciones con socios potenciales.

El comité celebró su primera reunión el 7 de noviembre en el Sindicato Internacional de Trabajadores. Estuvieron presentes el director de ODOT, Jack Marchbanks, el secretario de KYTC, Jim Gray, representantes del equipo del proyecto, la [División de Oportunidades, Diversidad e Inclusión](#) de ODOT y la [Oficina de Derechos Civiles y Desarrollo de Pequeñas Empresas](#) de KYTC, junto con varias docenas de miembros del comité que representan a diversas organizaciones; entre ellas, los sectores público y privado, los ámbitos relacionados con el trabajo y los grupos de desarrollo de la fuerza laboral.



“Nuestro diálogo acerca de los beneficios de este proyecto a menudo se enfoca en mejorar los viajes y aliviar la congestión”, dijo Tommy Arnold, el director del proyecto de ODOT. “Pero hay mucho más que eso”, continuó. “Este proyecto beneficiará a toda nuestra comunidad, en gran parte debido a las grandes oportunidades laborales que se presentan. Este comité nos ayudará a conseguirlo”.

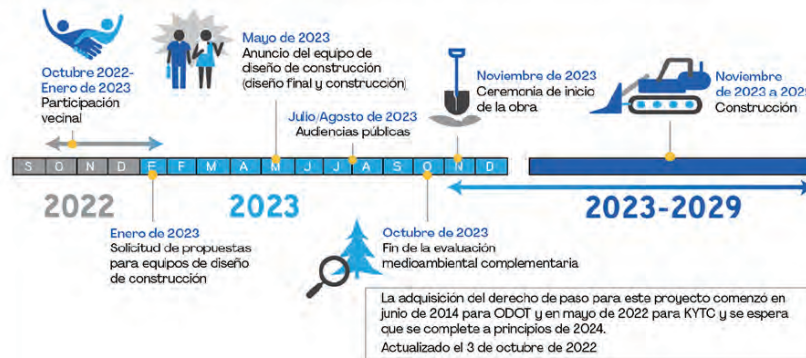
Los miembros del comité expresaron su agradecimiento al equipo del proyecto por la oportunidad de ayudar a dar forma a las prioridades de participación de la fuerza laboral en la primera etapa del proyecto, mientras se identifica y desarrolla el alcance del trabajo. El comité se reunirá trimestralmente.

Las reuniones de alcance vecinal comienzan la próxima semana

A partir de la próxima semana, el equipo del proyecto estará en la comunidad asistiendo a varias reuniones en vecindarios adyacentes al corredor objeto del proyecto.

Los miembros del equipo del proyecto presentarán una actualización sobre el estado del mismo, recibirán comentarios de los asistentes e informarán oportunidades adicionales para la participación.

En 2023 se llevarán a cabo reuniones adicionales y una audiencia pública formal.



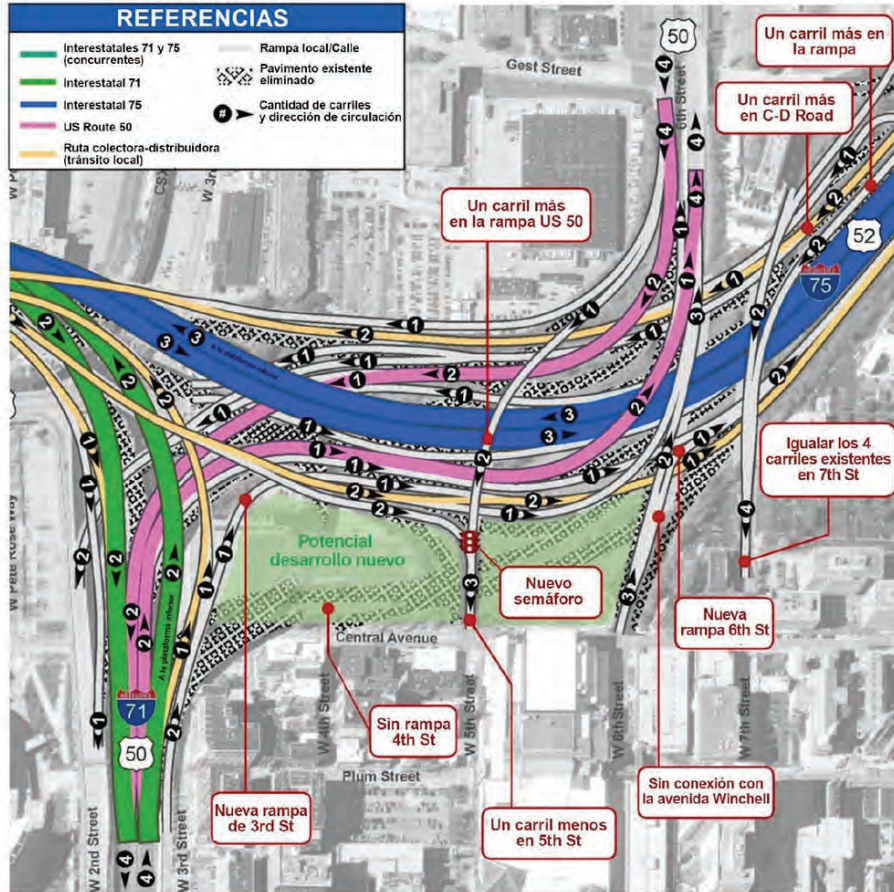
Las prioridades comunitarias compartidas continúan reflejándose en las decisiones del proyecto

En una actualización anterior del proyecto, detallamos algunos de los cambios que se

En una actualización anterior del proyecto, detallamos algunos de los cambios que se realizaron en base a los comentarios de varios socios locales. Durante las conversaciones con las partes interesadas clave, se identificó la oportunidad de mejorar aún más la huella del proyecto.

Después de una cuidadosa revisión, debate y evaluación técnica de las recomendaciones recibidas de la ciudad de Cincinnati y la Cámara Regional de Cincinnati EE. UU, el equipo del proyecto añadió varios cambios a los planes del proyecto; esto abrirá casi 10 acres de tierra adicional para el desarrollo local.

Estos cambios cumplen con los objetivos del proyecto y se alinean con las prioridades compartidas para volver a conectar a las comunidades mediante mejoras multimodales y amigables para los peatones y devolver las tierras públicas para su uso local.



[Aquí encontrará más información.](#)

Manténgase conectado

Hay varias maneras de mantenerse conectado con el proyecto del corredor del puente Brent Spence.

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Envíe un comentario

From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Monday, December 5, 2022 3:58 PM
To:
Subject: Brent Spence Bridge Corridor Project Update - DBE Matchmaker Event

Brent Spence Bridge Corridor December 5, 2022 eNewsletter



DBE MATCHMAKER EVENT TAKING PLACE DEC. 7

*Small, Minority and Women-Owned Businesses Encouraged to Learn
about Potential Work Opportunities*

The Brent Spence Bridge Corridor project will offer unprecedented opportunities for small, minority, and women-owned businesses in our region. To support the inclusion of Disadvantaged Business Enterprise (DBE) consultants and contractors on the Brent Spence Bridge Corridor Project, the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) will host two events Wednesday, Dec. 7 at the Anderson Pavilion, 8 Mehring Way, in Cincinnati.

From 9 a.m. to noon, representatives from ODOT and KYTC will provide an overview of the project for DBE firms. The presentation will include details about prequalification requirements for the project and support services available to DBE consultants and contractors.

From 1:30 to 4:30 p.m., ODOT and KYTC will conduct a “matchmaker” event in which DBE firms can meet one-on-one with prime contractors and firms who work directly with the government and manage subcontractors and consultants.

“This is a great opportunity for the industry,” said ODOT’s Tom Arnold of the afternoon event. “Each DBE gets 15 minutes to present their capabilities and work experience to a number of

prime contractors. Our goal is to help our prime design and construction firms understand how many great small businesses they can work with to meet our project inclusion goals.”

Arnold, a member of the project’s Diversity and Inclusion Outreach Committee, added ODOT and KYTC plan to issue a request for proposals for the project’s prime contractor early next year. Construction is scheduled to begin late next year or early 2024.

“We strongly recommend DBE firms [preregister](#) for both events because we want to help make as many connections as possible,” Arnold said. He also recommended DBE firms follow ODOT’s Outreach [webpage](#) for additional updates and events.

Schedule of DBE Events

Wednesday, Dec. 7 | Anderson Pavilion
8:30 to 9 a.m.: Registration
9 a.m. to Noon: Project Overview
1:30 to 4:30 p.m.: Matchmaker Event with Prime Contractors

ODOT AND KYTC CONTINUE ROBUST PUBLIC OUTREACH THROUGH NEIGHBORHOOD MEETINGS

Public Invited to Attend to Ask Questions and Share Feedback

The joint project team from ODOT and KYTC continues to gather valuable feedback from communities surrounding the Brent Spence Bridge Corridor project area through a public meeting and listening tour. Suggestions gathered during these sessions will enhance the overall project, including opportunities to reconnect neighborhoods, improve bike and pedestrian safety, and reduce the project’s footprint.

Through the end of 2022 and into 2023, the project team will continue meeting one-on-one with directly affected communities to answer questions and gather ideas.

As the project gets under way, ODOT and KYTC will continue to explore ways to improve pedestrian access, improve safety, and support better access to business districts while reclaiming public land for local use.

During the eight meetings conducted to date, residents and local officials have identified opportunities that are now part of the plan, including enhanced streetscape amenities, aesthetic treatments, and bicycle and pedestrian infrastructure. All will improve connectivity to transit, jobs, health care, cultural, recreational and commercial destinations.

In addition to the neighborhood meetings, the public will be provided the opportunity to review project materials and provide formal comments at public hearings currently scheduled for July/August 2023.



KENTUCKY'S UPCOMING NEIGHBORHOOD MEETING SCHEDULE

Neighborhood	Meeting Date	Meeting Time	Meeting Location
Mutter Gottes/CBD	December 5, 2022	7:00 p.m.-8:00 p.m.	Kenton County Public Library 502 Scott Street, Covington
Lewisburg/Botany Hills	December 6, 2022	7:00 p.m.-8:00 p.m.	Holiday Inn Express & Suites 200 Crescent Avenue, Covington
Park Hills	December 12, 2022	7:00 p.m.-8:30 p.m.	Garden of Park Hills 1622 Dixie Highway, Park Hills
Broad Neighborhood Meeting	December 14, 2022	12:00 p.m.-2:00 p.m. 5:00 p.m.-7:00 p.m.	Kenton County Fiscal Court (Riedlin Schott Community Room) 1840 Simon Kenton Way, Covington
Public Hearing	TBD July/August 2023	TBD	TBD

OHIO'S UPCOMING NEIGHBORHOOD MEETING SCHEDULE

Neighborhood	Meeting Date	Meeting Time	Meeting Location
Camp Washington	December 12, 2022	7:00 p.m.-8:30 p.m.	Camp Washington Urban Revitalization Corporation 2951 Sidney Avenue, Cincinnati
Broad Neighborhood Meeting	December 13, 2022	12:00 p.m.-2:00 p.m. 5:00 p.m.-7:00 p.m.	Lincoln Recreation Center 1027 Linn Street, Cincinnati
West End	December 20, 2022	6:00 p.m.-8:00 p.m.	Lincoln Recreation Center 1027 Linn Street, Cincinnati
Public Hearing	TBD July/August 2023	TBD	TBD

REVISED PROJECT DESIGN RETURNS 10 ACRES TO CINCINNATI CENTRAL BUSINESS DISTRICT

In its continuing collaboration with Cincinnati’s elected officials and community leaders, the Brent Spence Bridge Corridor Project team redesigned plans for the area immediately west of the Duke Energy Convention Center, freeing up 10 acres for future development.

Last summer the city and the Cincinnati USA Regional Chamber submitted comments and requested changes to the Ohio side of the corridor to enlarge the central business district, which were approved by the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC).

This creates “some really exciting opportunities for large-scale investments,” said Cincinnati Mayor Aftab Pureval at a news conference last month. “We want to be a city that’s growing, and these 10 acres right in the middle of our downtown” will be like pouring “gasoline on that effort.”

Changes include refinements to the entrance and exit ramps on Third, Fourth, Fifth, and Sixth streets to better integrate them into the city’s street network. The revised plan also removes one exit ramp from Interstate 75 South.

Though the city has no definite plans for the reclaimed area, Pureval said he envisions creating a place where people “can either work or enjoy retail and restaurants.” He added the 10 acres has an estimated value of \$20 million and could generate more than \$500,000 a year in taxes.

Brendon Cull, president of the chamber, said adding 10 acres in downtown Cincinnati represents a significant economic opportunity for the city and region. “Our goal all along has been to make sure that the Brent Spence Bridge gets done and gets done in a way that maximizes opportunity for our region,” he said. “This successful effort will create more economic opportunity for the people who live here and work here, and that’s what matters.”



Image from Spectrum News 1

[Read more here.](#)

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Boletín electrónico del corredor del puente Brent Spence, 5 de diciembre de 2022



INVIRTIENDO EN LAS COMUNIDADES LOCALES.
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BrentSpenceBridgeCorridor.com



EL EVENTO DE BÚSQUEDA DE SOCIOS POTENCIALES PARA DBE SE REALIZA EL 7 DE DICIEMBRE

*Se invita a empresas pequeñas, propiedad de minorías y mujeres a
informarse sobre posibles oportunidades de trabajo*

El proyecto del corredor del puente Brent Spence ofrecerá oportunidades sin precedentes para empresas pequeñas, de propiedad de minorías y mujeres en nuestra región. Para apoyar la inclusión de Empresas en Desventaja (DBE), consultores y contratistas que participan en el proyecto del corredor del puente Brent Spence, el Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC) organizarán dos eventos el miércoles 7 de diciembre en Anderson Pavilion, 8 Mehring Way, en Cincinnati.

Desde las 9 a.m. hasta el mediodía, los representantes de ODOT y KYTC ofrecerán una descripción general del proyecto para las DBE. La presentación incluirá detalles sobre los requisitos de precalificación para el proyecto y los servicios de apoyo disponibles para los consultores y contratistas de DBE.

De 1:30 p.m. a 4:30 p.m., ODOT y KYTC realizarán un evento de búsqueda de socios potenciales, conocido como "matchmaker", en el que las empresas DBE podrán reunirse de forma individual con los principales contratistas y empresas que trabajan directamente con el gobierno y administran subcontratistas y consultores.

"Esta es una gran oportunidad para la industria", dijo Tom Arnold, de ODOT, sobre el evento de la tarde. "Cada DBE tiene 15 minutos para presentar sus capacidades y experiencia laboral a varios contratistas principales. Nuestro objetivo es ayudar a nuestras principales empresas de diseño y construcción a comprender con cuántas excelentes pequeñas empresas pueden trabajar para cumplir con nuestros objetivos de inclusión de proyectos".

Arnold, miembro del Comité de Divulgación de la Inclusión y Diversidad del proyecto, agregó que ODOT y KYTC tienen previsto publicar una solicitud de propuestas para el contratista principal del proyecto a principios del año próximo. La construcción está programada para comenzar a fines del año próximo o a principios de 2024.

"Recomendamos con énfasis a las DBE que se [preinscriban](#) para ambos eventos porque queremos ayudar a establecer la mayor cantidad de conexiones posibles", dijo Arnold. También recomendó a las DBE que sigan la [página web](#) de divulgación de ODOT para conocer actualizaciones y eventos adicionales.

Calendario de eventos DBE

Miércoles, 7 de diciembre | Anderson Pavilion,

8:30 a 9 a.m.: Inscripción

9 a.m. al mediodía: Descripción general del proyecto

1:30 a 4:30 p.m.: Evento de búsqueda de socios con los principales contratistas

ODOT Y KYTC CONTINÚAN SU SÓLIDA TAREA DE DIVULGACIÓN REALIZANDO REUNIONES VECINALES

Se invita al público a asistir para formular preguntas y compartir opiniones

El equipo del proyecto conjunto de ODOT y KYTC continúa recogiendo comentarios valiosos de las comunidades que rodean el área del proyecto del corredor del puente Brent Spence mediante una reunión pública y un recorrido de escucha. Las sugerencias recogidas en esas sesiones mejorarán el proyecto en general; esto incluye las oportunidades para volver a conectar los vecindarios, mejorar la seguridad de los ciclistas y peatones y reducir la huella del proyecto.

Hasta fines de 2022, y principios 2023, el equipo del proyecto continuará reuniéndose de forma individual con las comunidades que se ven directamente afectadas, con el fin de responder preguntas y recabar ideas.

A medida que el proyecto se pone en marcha, ODOT y KYTC seguirán buscando formas de facilitar el acceso peatonal, mejorar la seguridad y apoyar un mejor acceso a los distritos comerciales a la vez que se recuperan terrenos públicos para uso local.

Durante las ocho reuniones realizadas hasta la fecha, los residentes y los funcionarios locales identificaron oportunidades que ahora forman parte del plan; esto incluye mejoras en el paisaje urbano, tratamientos de la estética e infraestructura para ciclistas y peatones. Todo esto mejorará la conectividad con el tránsito, el empleo, la atención de la salud, los destinos culturales, recreativos y comerciales.

Además de asistir a las reuniones vecinales, el público tendrá la oportunidad de revisar los materiales del proyecto y hacer comentarios formales en las audiencias públicas actualmente programadas para julio/agosto de 2023.



PROGRAMA DE LAS PRÓXIMAS REUNIONES VECINALES DE KENTUCKY

Vecindario	Fecha de la reunión	Hora de la reunión	Lugar de la reunión
Mutter Gottes/CBD	5 de diciembre de 2022	7:00 p.m. a 8:00 p.m.:	Biblioteca pública del condado de Kenton 502 Scott Street, Covington
Lewisburg/Botany Hills	6 de diciembre de 2022	7:00 p.m. a 8:00 p.m.:	Holiday Inn Express & Suites 200 Crescent Avenue, Covington
Park Hills	12 de diciembre de 2022	7:00 p.m. a 8:30 p.m.:	Garden of Park Hills 1622 Dixie Highway, Park Hills
Reunión vecinal amplia	14 de diciembre de 2022	12:00 p.m. a 2:00 p.m.: 5:00 p.m. a 7:00 p.m.:	Tribunal Fiscal del Condado de Kenton (Riedlin Schott Community Room) 1840 Simon Kenton Way, Covington
Audiencia pública	A determinar julio/agosto 2023	A determinar	A determinar

PROGRAMA DE LAS PRÓXIMAS REUNIONES VECINALES DE OHIO

Vecindario	Fecha de la reunión	Hora de la reunión	Lugar de la reunión
Camp Washington	12 de diciembre de 2022	7:00 p.m. a 8:30 p.m.:	Camp Washington Urban Revitalization Corporation 2951 Sidney Avenue, Cincinnati
Reunión vecinal amplia	13 de diciembre de 2022	12:00 p.m. a 2:00 p.m.: 5:00 p.m. a 7:00 p.m.:	Lincoln Recreation Center 1027 Linn Street, Cincinnati
West End	20 de diciembre de 2022	6:00 p.m. a 8:00 p.m.:	Lincoln Recreation Center 1027 Linn Street, Cincinnati
Audiencia pública	A determinar julio/agosto	A determinar	A determinar

Audiencia pública	A determinar julio/agosto 2023	A determinar	A determinar
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EL DISEÑO DEL PROYECTO REVISADO DEVUELVE 10 ACRES AL DISTRITO CENTRAL DE NEGOCIOS DE CINCINNATI

En su continua colaboración con los funcionarios electos y los líderes comunitarios de Cincinnati, el equipo del proyecto del corredor del puente Brent Spence rediseñó los planes para el área inmediatamente al oeste de Duke Energy Convention Center, liberando 10 acres para futuros desarrollos.

El verano pasado, la ciudad y la Cámara Regional de Cincinnati EE. UU. presentaron comentarios y solicitaron cambios en el lado de Ohio del corredor para ampliar el distrito central de negocios; estos fueron aprobados por el Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC).

Esto crea "algunas oportunidades realmente interesantes para inversiones a gran escala", dijo el alcalde de Cincinnati, Aftab Pureval, en una conferencia de prensa el mes pasado. "Queremos ser una ciudad en crecimiento, y estos 10 acres justo en el medio del centro de nuestra ciudad" será como echar "gasolina a ese esfuerzo".

Los cambios incluyen mejoras en las rampas de entrada y salida en las calles Third, Fourth, Fifth, y Sixth para integrarlas mejor a la red de calles de la ciudad. El plan revisado también elimina una rampa de salida de la Interestatal 75 Sur.

Aunque la ciudad no tiene planes definitivos para el uso del área recuperada, Pureval dijo que imagina un lugar donde la gente "pueda trabajar o disfrutar de tiendas y restaurantes". Agregó que los 10 acres tienen un valor estimado de \$20 millones y podrían generar más de \$500,000 al año en impuestos.

Brendon Cull, presidente de la cámara, dijo que agregar 10 acres al centro de Cincinnati representa una importante oportunidad económica para la ciudad y la región. "Nuestro objetivo siempre ha sido asegurarnos de que el puente Brent Spence se haga, y de forma que maximice las oportunidades para nuestra región", dijo. "Este esfuerzo exitoso creará más oportunidades económicas para las personas que viven y trabajan aquí, y eso es lo que importa".



Imagen de Spectrum News 1

[Aquí encontrará más información.](#)

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Hay varias maneras de mantenerse conectado con el proyecto del corredor del puente Brent Spence.

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Envíe un comentario

From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Thursday, January 26, 2023 9:19 AM
To:
Subject: Brent Spence Bridge Corridor Project Update - Grant Funding Approved

Brent Spence Bridge Corridor January 26, 2023 eNewsletter



BIG NEWS

GRANT FUNDING COMES THROUGH, PROJECT ADVANCES TOWARD 2023 GROUNDBREAKING

Bipartisanship is emphasized during \$1.635 billion grant announcement

Kentucky Gov. Andy Beshear and Ohio Gov. Mike DeWine were among the officials to announce the award of federal funding grants worth more than \$1.635 billion for the Brent Spence Bridge Corridor Project, giving the landmark bridge and corridor project the green light to move toward construction with no tolls.



Joining the governors to address a crowd of more than 300 at the January 4 announcement in Covington were former Ohio Sen. Rob Portman and Ohio Sen. Sherrod Brown along with U.S. Senate Republican Leader Mitch McConnell and President Joe Biden, who talked about the bipartisan support for the project.

The grant accounts for 98.2 percent of the amount Ohio and Kentucky requested for this vital project that Senator McConnell called "one of the most important infrastructure projects in the country."

"I'm thrilled the time has finally come for us to get the companion bridge built," Gov. Beshear said. "Funding and constructing the Brent Spence Bridge Corridor Project is more than the fulfillment of my administration's promise – it's a dream fulfilled for the thousands of travelers who pass through the bustling region every day waiting eagerly for traffic relief to come on this nationally significant corridor. It also shows what's possible when we prioritize people over politics."

"Throughout the years, good times and bad, when you ask the Cincinnati Chamber of Commerce and you ask the business community 'what's your number one priority?' in Cincinnati, they came back every single time and said this bridge," Gov. DeWine said.

With funding secured, groundbreaking on the project is anticipated for late 2023, with substantial completion slated for 2029.

Project plans call for the construction of a companion bridge to the west of the existing Brent Spence Bridge, as well as improvements to the current bridge and the roadway network that ties into each river crossing. As a result of robust engagement with local partners, there will be enhanced pedestrian access across I-75 in Cincinnati to reconnect downtown with western neighborhoods, and the City of Cincinnati will regain nearly 10 acres of land to develop in the downtown area. In Kentucky, the project will include a new storm sewer system to reduce flooding and improve local roads, including enhanced pedestrian and bicycle facilities, in the area of the existing and new bridge.

PROJECT TEAM HOLDS NEIGHBORHOOD OUTREACH MEETINGS THROUGHOUT NORTHERN KENTUCKY AND SOUTHWEST OHIO NEIGHBORHOODS

More than 400 people attended 16 neighborhood meetings conducted by the Brent Spence Bridge Corridor Project Bi-State team in November and December.

These meetings were designed to share updates with the neighborhoods and to give attendees an opportunity to share their feedback on how the project may affect their neighborhood.

Updates presented at the meetings included:

- Concept I-W, which designated the companion bridge for through (interstate) traffic and the existing bridge for local traffic, thus reducing the overall bridge width and impacts;
- Reconfiguring the ramps in downtown Cincinnati to free up 9.5 acres to return to the city for potential development;
- Reducing the project footprint, which, in turn, reduced residential relocations from more than 40 to 4;
- Introducing the results of noise wall studies and locations where noise walls are being added, as well as the potential for additional noise walls in Kentucky; and
- Providing details on the separation of stormwater from combined sanitary sewers.

Responses to comments raised during the neighborhood outreach activities will be posted on the Brent Spence Bridge Corridor Project website under the [Public Involvement and Comments section](#).

Three hearings on the project's supplemental Environmental Assessment will be conducted this summer. One in-person hearing each will be conducted in Ohio and Kentucky. The third will be a virtual hearing. For more about the supplemental Environmental Assessment, visit the [Environmental Process](#) section of the website.



DBE OPPORTUNITIES SHARED AND CONNECTIONS MADE AT INFORMATION SESSION

The \$3.6 billion bipartisan investment in the Brent Spence Bridge Corridor (BSBC) Project creates historic opportunities for small, minority, and women-owned businesses in the region. The project team is committed to building an inclusive, robustly diverse project workforce representative of the rich diversity of the Greater Cincinnati and Northern Kentucky region. Ohio and Kentucky are working together to support diverse businesses and workforce development organizations and encourage strong representation throughout the six-year project.

At the first networking session held December 7 by the Diversity & Inclusion (D&I) Committee, more than 100 representatives of small, minority, and women-owned businesses attended. The two-part event featured a presentation on the BSBC Project for Disadvantaged Business Enterprise (DBE) consultants and contractors in the morning followed by one-on-one meetings between the DBE consultants and contractors and prime contractors and firms who work directly with the government and manage subcontractors and consultants.

Speakers for the morning session included representatives from DBE and D&I support teams from both the Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT), the BSBC project team, the U.S. Department of Labor, and more. The group shared:

- A project overview;
- DBE reciprocity agreement between ODOT and KYTC;
- ODOT and KYTC pre-qualification requirements;
- Overview of the Progressive Design Build model;
- DBE Supportive Services information; and
- Overview of U.S. Department of Labor Davis Bacon requirements

“We were very happy with the turnout for the event,” said Tom Arnold, interim capital programs administrator for ODOT. “We’re excited about the many opportunities this project offers for DBE firms, and we’re just getting started in helping them make connections to be able to engage in the project.”

The presentation from the event can be found [here](#). DBE firms interested in more information can visit ODOT’s outreach [webpage](#).

DRAFT RFP RELEASED; TEAM ENCOURAGING FEEDBACK AND QUESTIONS

Final RFP scheduled to be posted February 17, 2023

The Brent Spence Bridge Corridor Project team is inviting interested lead contractors to review and provide feedback on the draft instructions to offerors, contract, and technical requirements for the Brent Spence Bridge Project.

The team is providing these drafts to help identify and resolve concerns regarding procurement, terms and conditions, the project’s technical requirements, and evaluation/selection criteria. Please visit the [Comments and Questions](#) section of the website or send an email to bsbprocurement@dot.ohio.gov with feedback or questions.

These documents will be regularly updated by the team until the announcement of the Final RFP, scheduled for February 17, 2023. Each subsequent release will clearly identify any updates or revisions.

Contractors who are interested in meeting with the team before the final RFP is released can contact bsbprocurement@dot.ohio.gov.

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Boletín electrónico del Corredor del Puente Brent Spence, 26 de enero de 2023



EXCELENTES NOTICIAS LLEGA LA FINANCIACIÓN MEDIANTE UNA SUBVENCIÓN, EL PROYECTO AVANZA HACIA SU INICIO EN 2023

El bipartidismo se pone de relieve durante el anuncio de una subvención de \$1.635 billones

El gobernador de Kentucky Andy Beshear y el gobernador de Ohio Mike DeWine fueron algunos de los funcionarios que anunciaron la concesión de subvenciones federales por valor de más de \$1.635 billones para el Proyecto del Corredor del Puente Brent Spence, dando luz verde al emblemático proyecto del puente y el corredor para avanzar hacia la construcción sin peajes.



Junto a los gobernadores, se dirigieron a una multitud de más de 300 personas en el anuncio del 4 de enero en Covington el ex senador de Ohio Rob Portman y el senador de Ohio Sherrod Brown, así como el líder republicano del Senado de EE. UU. Mitch McConnell y el presidente Joe Biden, que hablaron del apoyo bipartidista al proyecto.

La subvención representa el 98,2 % del importe solicitado por Ohio y Kentucky para este proyecto vital que el senador McConnell calificó de "uno de los proyectos de infraestructuras más importantes del país".

"Estoy encantado de que por fin haya llegado el momento de construir el puente complementario", afirmó el gobernador Beshear. "La financiación y construcción del Proyecto del Corredor del Puente Brent Spence es algo más que el cumplimiento de una promesa de mi administración: es un sueño hecho realidad para los miles de viajeros que atraviesan cada día esta dinámica región y esperan con impaciencia que se alivie el tráfico en este corredor de relevancia nacional. También demuestra lo que es posible cuando damos prioridad a las personas sobre la política".

"A lo largo de los años, en los buenos y los malos tiempos, cuando se pregunta a la Cámara de Comercio de Cincinnati y a la comunidad empresarial cuál es su prioridad número uno en Cincinnati, siempre responden que este puente", afirmó el gobernador DeWine.

Una vez garantizada la financiación, se prevé que las obras empiecen a finales de 2023 y se terminen en 2029.

Los planes del proyecto prevén la construcción de un puente complementario al oeste del Puente Brent Spence existente, así como mejoras en el puente actual y en la red viaria que enlaza con cada cruce del río. Como resultado del sólido compromiso con los socios locales, habrá un acceso peatonal mejorado a través de la I-75 en Cincinnati para volver a conectar el centro con los barrios del oeste, y la ciudad de Cincinnati recuperará casi 10 acres de terreno para urbanizar en el centro. En Kentucky, el proyecto incluirá un nuevo sistema de alcantarillado pluvial para reducir las inundaciones y mejorar las carreteras locales, incluidas instalaciones mejoradas para peatones y ciclistas, en la zona del puente existente y del nuevo.

EL EQUIPO DEL PROYECTO CELEBRA REUNIONES DE DIVULGACIÓN EN LOS BARRIOS DEL NORTE DE KENTUCKY Y DEL SUROESTE DE OHIO

Más de 400 personas asistieron en noviembre y diciembre a las 16 reuniones vecinales organizadas por el equipo biestatal del Proyecto del Corredor del Puente Brent Spence.

Estas reuniones tenían por objeto compartir información actualizada con los barrios y brindar a los asistentes la oportunidad de dar su opinión sobre cómo puede afectar el proyecto a su barrio.

Entre las novedades presentadas en las reuniones cabe citar las siguientes:

- Concepto I-W, que designaba el puente complementario para el tráfico de paso (interestatal) y el puente existente para el tráfico local, reduciendo así la anchura total del puente y su impacto;
- reconfiguración de las rampas en el centro de Cincinnati para liberar 9,5 acres y devolverlos a la ciudad para su posible urbanización;
- reducción de la huella del proyecto lo que, a su vez, redujo las reubicaciones residenciales de más de 40 a 4;
- presentación de los resultados de los estudios de muros acústicos y las ubicaciones donde se agregarán, así como el potencial de los muros acústicos adicionales en Kentucky; y
- aportación de detalles sobre la separación de las aguas pluviales del alcantarillado sanitario combinado.

Las respuestas a los comentarios planteados durante las actividades de divulgación en el barrio se publicarán en el sitio web del Proyecto del Corredor del Puente Brent Spence en el apartado de [Participación Pública y la Sección de comentarios](#).

Este verano se llevarán a cabo tres audiencias sobre la Evaluación Ambiental complementaria del proyecto. Se celebrarán una audiencia presencial en Ohio y otra en Kentucky. La tercera será una audiencia virtual. Para obtener más información sobre la Evaluación Ambiental complementaria, visite la sección [Proceso Ambiental](#) del sitio web.



OPORTUNIDADES COMPARTIDAS PARA LAS DBE Y CONEXIONES REALIZADAS EN LA SESIÓN INFORMATIVA

La inversión bipartidista de \$3,6 billones en el Proyecto del Corredor del Puente Brent Spence (BSBC) crea oportunidades históricas para empresas pequeñas, pertenecientes a minorías y mujeres de la región. El equipo del proyecto se compromete a reunir una plantilla inclusiva y diversa, representativa de la rica diversidad del área metropolitana de Cincinnati y de la región del norte de Kentucky. Ohio y Kentucky colaboran para apoyar a diversas empresas y organizaciones de desarrollo de plantillas de trabajadores y fomentar una sólida representación a lo largo del proyecto de seis años de duración.

La primera sesión de *networking* del Comité de Diversidad e Inclusión (D&I), celebrada el 7 de diciembre, contó con la asistencia de más de 100 representantes de pequeñas empresas propiedad de minorías y mujeres. El evento, que constó de dos partes, incluyó una presentación del Proyecto BSBC para consultores y contratistas de empresas desfavorecidas (DBE) por la mañana, seguida de reuniones individuales entre consultores y contratistas DBE y contratistas principales y empresas que trabajan directamente con la Administración y gestionan subcontratistas y consultores.

En la sesión matinal intervinieron representantes de los equipos de apoyo DBE y D&I tanto del Gabinete de Transporte de Kentucky (KYTC) como del Departamento de Transporte de Ohio (ODOT), el equipo del proyecto BSBC y el Departamento de Trabajo de EE.UU., entre otros. El grupo compartió:

- Una descripción general del proyecto;
- acuerdo de reciprocidad DBE entre ODOT y KYTC;
- requisitos de precalificación de ODOT y KYTC;
- descripción general del modelo de construcción de diseño progresivo;
- información de servicios de apoyo para DBE; y
- descripción general de los requisitos Davis Bacon del Departamento de Trabajo.

"Estamos muy contentos con la participación en el evento", comentó Tom Arnold, administrador interino de programas de capital de ODOT. "Estamos entusiasmados con las numerosas oportunidades que este proyecto ofrece a las empresas DBE, y no hemos hecho más que empezar a ayudarles a establecer contactos para que puedan participar en el proyecto".

La presentación del evento se puede encontrar [aquí](#). Las empresas DBE interesadas en obtener más información pueden visitar la [página web](#) de divulgación de ODOT.

SE PUBLICA EL BORRADOR DEL PLIEGO DE CONDICIONES; EL EQUIPO ANIMA A QUE SE ENVÍEN COMENTARIOS Y PREGUNTAS

Publicación del pliego de condiciones definitivo prevista para el 17 de febrero de 2023

El equipo del Proyecto del Corredor del Puente Brent Spence invita a los contratistas principales interesados a revisar y proporcionar comentarios sobre el borrador de las instrucciones a los oferentes, del contrato y de los requisitos técnicos para el Proyecto del Puente Brent Spence.

El equipo proporciona estos borradores para ayudar a identificar y resolver las dudas relativas a la contratación, los términos y condiciones, los requisitos técnicos del proyecto y los criterios de evaluación/selección. Visite la sección [Comentarios y preguntas](#) del sitio web o envíe un correo electrónico a bsbprocurement@dot.ohio.gov con sus comentarios o preguntas.

El equipo actualizará periódicamente estos documentos hasta el anuncio del pliego de condiciones definitivo, previsto para el 17 de febrero de 2023. Cada versión posterior identificará claramente cualquier actualización o revisión.

Los contratistas que estén interesados en reunirse con el equipo antes de que se publique el pliego de condiciones definitivo pueden comunicarse a través de bsbprocurement@dot.ohio.gov.

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From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Tuesday, February 28, 2023 4:29 PM
To:
Subject: Brent Spence Bridge Corridor Project Update - RFP Released

Brent Spence Bridge Corridor February 28, 2023 eNewsletter



Project Team Releases Request for Proposals (RFP) Feb. 17 as Project Marches Forward

Just one month after announcing the \$1.635 billion in federal funding secured for the Brent Spence Bridge Corridor (BSBC) Project, the team released the Request for Proposals (RFP) to select the design-build team to complete the project. As one of the most significant infrastructure projects in the nation, the BSBC Project brings with it tremendous opportunity and economic impact for the region.

[This contract](#) addresses six of the corridor's eight miles: five miles of the I-71/75 corridor in Kentucky and one mile of I-75 in Ohio. It includes improvements to the Brent Spence Bridge and a new companion bridge being built to the west. Work on the two northernmost sections of the corridor in Ohio will be done under separate contracts.

"Just a month ago, we celebrated a historic \$1.635 billion in federal grant funding to build the new bridge crossing over the Ohio River and improve the entire Brent Spence Bridge Corridor with no tolls," said Kentucky Gov. Andy Beshear. "This is a huge milestone that gets us one step closer to fulfilling the dreams of thousands of travelers by providing traffic relief, increased safety, and a boost to our nation's commerce."

"When I've asked people in southwest Ohio what issues need to be addressed, for many, many years the Brent Spence Bridge has consistently been at the top of the list," said Ohio Gov. Mike DeWine. "Today, we're closer than ever to beginning work on this transformational project."

Responses to the RFP are due March 31, 2023. The schedule calls for the design-build team to be selected in May, allowing for planning to take place immediately and initial construction work to begin before the year's end.

Diversity & Inclusion Committee Details Goals and Initiatives at Quarterly Meeting

The Brent Spence Bridge Corridor Project's Diversity & Inclusion (D&I) Committee works together to collaborate, coordinate, and communicate information related to disadvantaged business enterprise (DBE) goals, initiatives, and requirements encouraging diversity and inclusion and workforce development. Made up of more than [25 representatives](#) from business groups, local chambers of commerce, governments, and elected officials, the D&I Committee convened virtually for its quarterly meeting on January 25.

Members heard from project team representatives on topics including updated project goals, the recent federal grant announcement, ongoing public outreach efforts, and the Progressive Design-Build concept and schedule. Lauren Purdy, Deputy Director of the Division of Opportunity, Diversity and Inclusion for ODOT, outlined the technical requirements for the project's Diversity, Inclusion, and Outreach Plan, explaining the role of the D&I manager, the project's plans for DBE performance, workforce development, and compliance.

The project's DBE and on-the-job training (OJT) goals were shared by KYTC's Melvin Bynes, Executive Director for the Office of Civil Rights at KYTC, who also reviewed the methodology used to develop those goals. Independent consultant BBC Research and Consulting, a 50-year-old economic and diversity research firm based in Denver, conducted an in-depth analysis using proven and statistically valid census methodology to determine DBE availability within the geographic market, which was a central part of determining the project goals.

The meeting concluded with an open discussion centered on members' ideas and recommendations for how the project team can support workforce development and help get individuals job ready in the near future. The project team also launched a new [website page](#) to assist those seeking opportunities to work on the project.

The full presentation from the quarterly meeting can be found [here](#).

A follow-up meeting was conducted Feb. 6, enabling BBC to answer additional questions and provide additional details about the DBE goal methodology.

Register Now for the March 7 Networking Meet and Greet Session

The D&I Committee will host its second DBE Networking Event from noon-4 p.m., March 7 at the Radisson Hotel Cincinnati Riverfront in Covington, KY. Register [here](#), and click on the image for more information.



BRENT SPENCE
BRIDGE CORRIDOR



**BRENT SPENCE BRIDGE CORRIDOR PROJECT
NETWORKING MEET & GREET**

Tuesday, March 7, 2023
12 p.m. to 4 p.m.
Radisson Hotel Cincinnati Riverfront
668 W. 5th Street, Covington, KY 41011

BSBC Aesthetics Committee Quarterly Meeting Held

The [Aesthetics Committee](#) was created as a sub-group of the Brent Spence Bridge Corridor (BSBC) Project Advisory Committee to provide input on the design and aesthetic appearance of the new companion bridge and related roadway corridor. Comprised of representatives from a dozen regional organizations, the committee helps evaluate and provides guidance on design goals and aesthetic treatments such as structure type, corridor theme, lighting, landscaping, and more. The broader committee also is divided into subcommittees to support the different aesthetic treatments in specific areas in the corridor.

The BSBC project team met with the full Aesthetics Committee on January 31 to provide an update on project initiatives and an overview of the progressive design-build process. Updates included the design-build RFP schedule and process for selecting a design-build team, as well as expected construction timelines.

The committee also reviewed and discussed its overall role in the design process, including a briefing on the potential design of the arch and cable-stayed options for the new companion bridge. Once selected, the progressive design-build team will recommend an option. That recommendation will be presented to the committee for feedback on design elements such as color, pattern, and lighting.

An additional meeting was held for the Ohio subcommittee. The group reviewed the aesthetics design checklist that the project team developed with the City of Cincinnati in 2022 to ensure project aesthetics blend seamlessly with the City's design standards, including landscaping, lighting, signals, concrete elements, signage, and sidewalks.

The next round of Aesthetics Committee meetings will be conducted before this summer's supplemental environmental assessment hearings.



Brent Spence Bridge Corridor
(Looking Northeast from Kentucky – Cable Stayed Option)



Brent Spence Bridge Corridor
(Looking Northeast from Kentucky – Arch Bridge Option)

Share Your Thoughts and Ideas

The Brent Spence Bridge Corridor Project team welcomes comments and feedback from the public. To submit a question or comment, visit the project [website](#) and click on the “Contact Us” button in the upper right-hand corner.

Responses to all public comments can be viewed on the [Public Involvement and Comments](#) section of the website. This section features project exhibits, summaries of outreach activities, and summaries of responses to questions posed to the project team during meetings or via email. The document with the summary of responses from all sources is updated monthly.

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INVIRTIENDO EN LAS COMUNIDADES LOCALES.
HACIENDO CRECER LA ECONOMÍA ESTADOUNIDENSE.



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El equipo del proyecto publica la Solicitud de Propuestas (RFP) el 17 de febrero mientras el proyecto avanza

Apenas un mes después de haber anunciado los \$1.635 mil millones en fondos federales asegurados para el Proyecto del Corredor del Puente Brent Spence (BSBC), el equipo publicó la Solicitud de Propuestas (RFP) para seleccionar el equipo de diseño y construcción para completar el proyecto. Como uno de los proyectos de infraestructura más importantes de la nación, el Proyecto BSBC trae consigo una gran oportunidad e impacto económico para la región.

[Este contrato](#) abarca seis de las ocho millas del corredor: cinco millas del corredor I-71/75 en Kentucky y una milla de la I-75 en Ohio. Incluye mejoras al Puente Brent Spence y un nuevo puente complementario que se está construyendo hacia el oeste. El trabajo en las dos secciones más al norte del corredor en Ohio se realizará en virtud de contratos separados.

“Hace solo un mes, celebramos una histórica subvención federal de \$1.635 mil millones para construir el nuevo puente que cruza el río Ohio y para mejorar todo el corredor del Puente Brent Spence sin el cobro de peaje”, dijo el gobernador de Kentucky, Andy Beshear. “Se trata de un gran hito que nos acerca un paso más a cumplir los sueños de miles de viajeros, proporcionando alivio del tráfico, mayor seguridad y un impulso al comercio de nuestra nación”.

“Cuando le preguntaba a la gente en el suroeste de Ohio qué problemas deberían abordarse, durante muchos años el Puente Brent Spence ha estado constantemente en el primer lugar de la lista”, dijo el gobernador de Ohio, Mike DeWine. “Hoy estamos más cerca que nunca de comenzar a trabajar en este proyecto transformador”.

Las respuestas a la RFP deben presentarse hasta el 31 de marzo de 2023. El cronograma exige que el equipo de diseño y construcción sea seleccionado en mayo; esto permite que la planificación se lleve a cabo de inmediato y que el trabajo de construcción inicial comience antes de fin de año.

El Comité de Diversidad e Inclusión detalla los objetivos e iniciativas en la reunión trimestral

El Comité de Diversidad e Inclusión (D&I) del proyecto del Corredor del Puente Brent Spence en conjunto para colaborar, coordinar y comunicar información relacionada con los DBE, iniciativas y requisitos para las empresas comerciales en desventaja (DBE) que fomentan la diversidad, así como la inclusión y el desarrollo de la fuerza laboral. Compuesto por más de [25 representantes](#) de grupos empresariales, cámaras de comercio locales, gobiernos y funcionarios electos, el Comité D&I se reunió de forma virtual en su encuentro trimestral el 25 de enero.

Los miembros escucharon a los representantes del equipo del proyecto referirse a temas que incluyen los objetivos actualizados del proyecto, el reciente anuncio de la subvención federal, los esfuerzos continuos de divulgación pública y el concepto y cronograma de Diseño y Construcción Progresivos. Lauren Purdy, subdirectora de la División de Oportunidades, Diversidad e Inclusión del Departamento de Transporte de Ohio (ODOT), describió los requisitos técnicos para el Plan de Diversidad, Inclusión y Alcance del proyecto, explicando el rol del gerente de D&I, los planes del proyecto para el desempeño, el desarrollo de la fuerza laboral y el cumplimiento de las DBE.

Melvin Bynes, director ejecutivo de la Oficina de Derechos Civiles de KYTC, compartió los objetivos de las DBE y de la Capacitación en el Trabajo (OJT) del proyecto; también revisó la metodología utilizada para desarrollar esos objetivos. La consultora independiente BBC Research and Consulting, una firma de investigación en asuntos económicos y de diversidad con 50 años, con sede en Denver, realizó un análisis en profundidad utilizando una metodología de censo comprobada y estadísticamente válida para determinar la disponibilidad de las DBE en el mercado geográfico; este era un aspecto central para determinar los objetivos del proyecto.

La reunión concluyó con un debate abierto enfocado en las ideas y recomendaciones de los miembros sobre cómo el equipo del proyecto puede apoyar el desarrollo de la fuerza laboral y ayudar a que las personas estén listas para hacer el trabajo en un futuro cercano. El equipo del proyecto también lanzó una nueva [página web](#) para ayudar a quienes buscan oportunidades para trabajar en el proyecto.

La presentación completa de la reunión trimestral se puede encontrar [aquí](#).

Se llevó a cabo una reunión de seguimiento el 6 de febrero. Esto permitió a BBC responder preguntas adicionales y dar detalles adicionales acerca de la metodología de objetivos para las DBE.

Regístrese ahora para participar en la sesión *Networking Meet and Greet* del 7 de marzo

El Comité D&I organizará su segundo evento de trabajo en redes de DBE desde el mediodía hasta las 4 p.m., el 7 de marzo, en el Radisson Hotel Cincinnati Riverfront en Covington, KY. Inscribese [aquí](#) y haga clic en la imagen para obtener más información.



NETWORKING MEET & GREET PARA EL PROYECTO DEL CORREDOR DEL PUENTE BRENT SPENCE

Martes 7 de marzo de 2023

12 p.m. a 4 p.m.

Radisson Hotel Cincinnati Riverfront
668W. 5th Street, Covington, KY 41011

Celebración de la reunión trimestral del Comité de Estética de BSBC

El [Comité de Estética](#) se creó como un subgrupo del Comité Asesor del Proyecto del Corredor del Puente Brent Spence (BSBC) para brindar información acerca del diseño y la apariencia estética del nuevo puente complementario y el corredor vial asociado a este. El comité está compuesto por representantes de una docena de organizaciones regionales y ayuda a evaluar y brinda orientación sobre los objetivos de diseño y los tratamientos estéticos, como el tipo de estructura, el tema del corredor, la iluminación, el paisajismo, entre otros. El comité también se divide en subcomités para apoyar los diferentes tratamientos estéticos en áreas específicas del corredor.

El equipo del proyecto BSBC se reunió el 31 de enero con el Comité de Estética completo para dar una actualización sobre las iniciativas del proyecto y ofrecer una descripción general del proceso de diseño y construcción progresivos. Las actualizaciones incluyeron el cronograma de RFP de diseño y construcción, el proceso para seleccionar un equipo de diseño y construcción y los plazos de construcción esperados.

El comité también revisó y abordó su papel general en el proceso de diseño, incluida una sesión informativa sobre el diseño potencial del arco y las opciones con tirantes para el nuevo puente complementario. Una vez seleccionado, el equipo de diseño y construcción progresivos recomendará una opción. Dicha recomendación se presentará al comité para obtener comentarios respecto de los elementos de diseño, como el color, el patrón y la iluminación.

Se llevó a cabo una reunión adicional para el subcomité de Ohio. El grupo revisó la lista de verificación del diseño estético que el equipo del proyecto desarrolló con la ciudad de Cincinnati en 2022, para garantizar que la estética del proyecto se integre sin fisuras con los estándares de diseño de la ciudad. Esto incluye el paisajismo, la iluminación, los carteles, los elementos de concreto, la señalización y las aceras.

La próxima ronda de reuniones del Comité de Estética se llevará a cabo antes de las audiencias de evaluación ambiental complementarias de este verano.



Comparta sus pensamientos e ideas

El equipo del proyecto del Corredor del Puente Brent Spence agradece los comentarios y opiniones del público. Para enviar una pregunta o un comentario, visite el [sitio web](#) del proyecto y haga clic en el botón “Contact Us” (Contáctenos) que está en la esquina superior derecha.

Las respuestas a todos los comentarios públicos se pueden ver en la sección [Public Involvement and Comments](#) (Participación pública y comentarios) del sitio web. Esta sección presenta exhibiciones de proyectos, resúmenes de actividades de divulgación y resúmenes de respuestas a preguntas presentadas al equipo del proyecto en las reuniones o por medio del correo electrónico. El documento con el resumen de las respuestas de todas las fuentes se actualiza mensualmente.

Manténgase informado

Hay varias maneras de mantenerse informado acerca del Proyecto del Corredor del Puente Brent Spence.

Visite nuestro sitio web

Regístrese para recibir actualizaciones

Envíe un comentario

From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Friday, March 31, 2023 1:54 PM
To:
Subject: Brent Spence Bridge Corridor Project Update - DBE Meet and Greet Held

Brent Spence Bridge Corridor March 31, 2023 eNewsletter



DBE Outreach Event Brings More than 260 Together in Covington to Discuss Opportunities

With federal funding secured and a request for proposals issued on behalf of the Brent Spence Bridge Corridor (BSBC) Project, the project team wasted no time putting boots on the ground in an effort to secure diverse and inclusive participation in this historic infrastructure investment. Brent Spence Bridge Corridor Project team members held an all-day Networking Meet & Greet for Disadvantaged Business Enterprise (DBE) firms on March 7 at the Radisson Hotel Cincinnati Riverfront in Covington to connect DBE contractors with potential bidders on the project to discuss opportunities.



More than 260 individuals representing prime and DBE firms from Ohio, Kentucky, Indiana, and the rest of the nation joined representatives from ODOT and KYTC for a presentation on the project funding, goals, and timeline. Participants were provided the opportunity to schedule 15-minute one-on-one appointments with prime consultants and contractors interested in pursuing the Design Build contract, the Quality Assurance Manager/Owner's Representative contract, or the Construction Engineering and Inspection (CEI) contract.

Response from participants on both sides was enthusiastic, with nearly all DBE and prime attendees ranking the event an 8 or higher on a scale of 1-10 via a post-event survey.

"The turnout was absolutely tremendous," said Tom Arnold, acting capital programs administrator and BSBC deputy project manager for ODOT. "We're elated at the interest and enthusiasm for this project. This \$3.6 billion project creates a once-in-a-lifetime opportunity for building and investing in a diverse workforce for this region, and we're excited to make that opportunity a reality. This event is just one step in a decade-long initiative with many more to come."

For resource links for consultants, contractors and DBE firms, and to keep up with future events, visit the [Work With Us](#) page on the BSBC website.

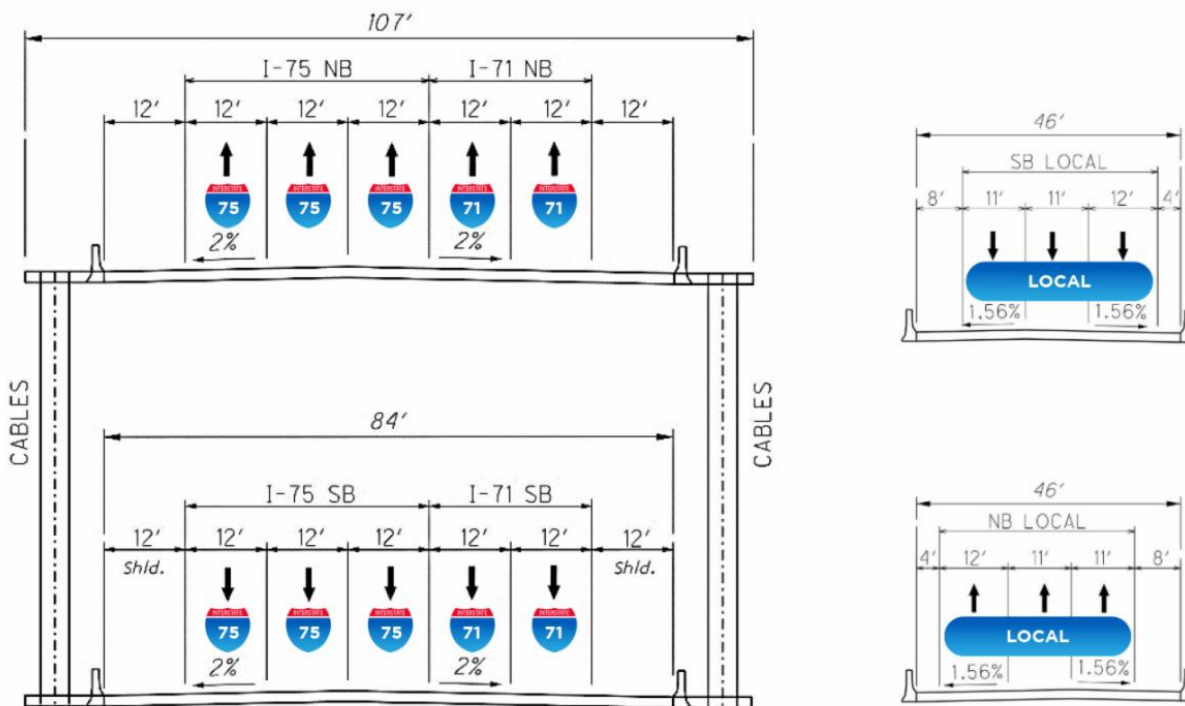
ODOT AND KYTC SHARE THE GOAL OF REDUCING IMPACTS TO LOCAL COMMUNITIES; PROJECT CHANGES BENEFIT RESIDENTS THROUGHOUT THE CORRIDOR

Since the design process began in 2005, the Brent Spence Bridge Corridor (BSBC) Project has undergone a series of changes to minimize its impact and address public comments. As a result, project design improvements have been made throughout the corridor to benefit residents and regional travelers for decades to come. A summary of those improvements is outlined below.

Reduced number of residential relocations by over 90%, and business relocations by 50%

The new companion bridge just west of the existing Brent Spence Bridge was originally planned to cover nearly 25 acres and span nearly 150 feet in width. Revised plans show the new bridge at almost half the size, covering approximately 14 acres and 84 feet in width.

As a result, the project’s footprint has been significantly reduced. The original plan called for the relocation of 40 residential units, including apartment buildings with multiple units, and 14 businesses in the project area. The current plan, however, indicates just four total residential units, five commercial lots and portions of two others will require relocation.



COMPANION BRIDGE - 2022 REVISION

EXISTING BRENT SPENCE BRIDGE

Reduced impacts to number of Lewisburg Historic District properties by 68%

Similarly, the original plan indicated 21 properties and portions of seven others would need to be acquired in the Lewisburg Historic District. Today, only three properties and portions of six others will be acquired.

Reconfigured ramps downtown, returning 10 acres to Cincinnati for development

Meanwhile, in its continuing collaboration with Cincinnati’s elected officials and community leaders, the project team last year announced redesigned plans for the area immediately west of the Duke Energy Convention Center, freeing up 10 acres for future development.

Reconnecting western Cincinnati neighborhoods with new and improved pedestrian and bike facilities

At the request of residents and city officials, pedestrian and bicycle facilities will be constructed across and parallel to I-71 and I-75 at most cross-street intersections and highway crossings, helping to reconnect western neighborhoods with downtown Cincinnati. You can see these [here](#).

Adding noise walls, addressing storm water runoff, and other measures to support local parks and communities

Other changes prompted by public comment include the addition of noise walls and noise/visual screening walls that exceed Kentucky and Ohio requirements. In addition, the project team has agreed to fund measures to:

- Replace land, reconstruct a walking trail, replace a basketball court, relocate an outdoor pool and develop a master plan for Goebel Park, Kenney Shields Park, and the SFC Jason Bishop Memorial Dog Park;
- Protect and restore the Firefighters Memorial and Ezzard Charles Park during construction; and
- Reduce flooding and combined sewer overflows.

Interstate Noise Barriers: Reducing Noise, Supporting Neighborhoods

Initially developed in the 1960s to support noise reduction as the federal highway system took shape, the first noise walls were designed to reduce traffic sounds along I-680 in Milpitas, CA. Officially known as highway noise barriers, the 2,748 miles of noise walls across the U.S. have continued to evolve, improving in both effectiveness and appearance.

Noise walls will be installed in various locations throughout the Brent Spence Bridge Corridor (BSBC) Project based on both technical analysis and public input. In fact, the 8-mile project will include approximately 8.75 miles of noise barriers to support sound reduction in neighborhoods throughout Ohio and Kentucky.

The project's original Environmental Assessment (EA) was developed in 2011 and updated in 2022 to account for changes in project impacts and regulatory updates. Currently, noise walls are recommended for parts of I-71/75 along Ft. Mitchell, Ft. Wright, and Park Hills, as well as the Covington neighborhoods of Peaselburg, Botany Hills Lewisburg, and Mainstrasse. In Ohio, noise walls are recommended along the interstate near Cincinnati's West End neighborhood and the Queensgate Playground and Ball Field. You can find more information on those studies [here](#).

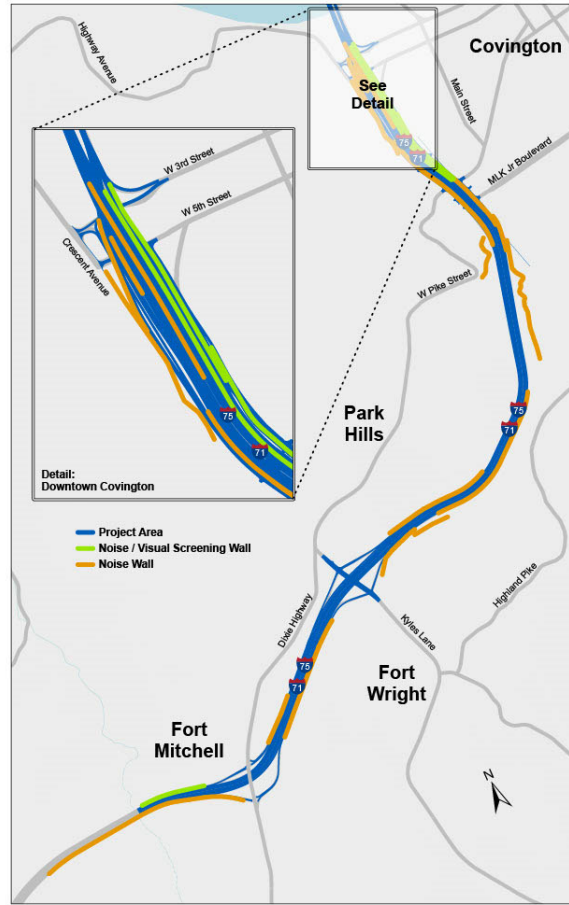
How Do Noise Walls Work?

It's simple sound science. While walls won't completely block all noise, those with the best design and placement can knock down the sounds of passing cars and trucks by five to 10 decibels, reducing the sound level of a typical tractor trailer to that of an automobile. The walls work by either absorbing the sound, transmitting it, reflecting it back across the highway, or forcing it to take a longer path over and around the barrier.

Attractive Buffers to Reduce Intrusive Sounds

It's important to note that noise walls don't just protect neighborhoods from noise pollution. They must also be visually pleasing enough to blend into the everyday scenery – and in some cases, even showcase a neighborhood's personality, history, or creativity. For the BSBC project, some of these aesthetic decisions are up to the project's [Aesthetics Committee](#), and

many are open to input from the neighborhoods themselves, gleaned during neighborhood meetings with project team members. Case in point – Northern Kentucky residents in Goebel Park and along Crescent Avenue raised concerns that noise walls might block the view they enjoy now. As a result, KYTC is evaluating the potential for transparent noise walls in certain locations and will continue to coordinate with the City of Covington during the project’s final design and construction to address those concerns. Other neighborhoods are considering showcasing local art, turning noise walls into murals. The project team will continue to work with neighborhoods adjacent to the project area to address concerns and get feedback on noise wall plans.



Share Your Thoughts and Ideas

The Brent Spence Bridge Corridor Project team welcomes comments and feedback from the public. To submit a question or comment, visit the project [website](#) and click on the “Contact Us” button in the upper right-hand corner.

Responses to all public comments can be viewed on the [Public Involvement and Comments](#) section of the website. This section features project exhibits, summaries of outreach activities, and summaries of responses to questions posed to the project team during meetings or via email. The document with the summary of responses from all sources is updated monthly.



Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

[Visit our website](#)

[Sign up for updates](#)

[Submit a comment](#)

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Brent Spence Bridge Corridor | 505 South State Route 741, Lebanon, OH 45036

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Sent by info@brentspencebridgecorridor.com powered by



Boletín electrónico del Corredor del Puento Brent Spence, 31 de marzo de 2023



El evento de extensión para empresas comerciales en desventaja (DBE) reúne a más de 260 personas en Covington para analizar oportunidades

Una vez asegurado el financiamiento federal y emitida una solicitud de propuestas en nombre del Proyecto del Corredor del Puento Brent Spence (BSBC), el equipo del proyecto no tardó en ponerse manos a la obra en un esfuerzo por asegurar una participación diversa e inclusiva en esta histórica inversión en infraestructura. El 7 de marzo, los miembros del equipo del proyecto del Corredor del Puento Brent Spence llevaron a cabo un encuentro para crear contactos para empresas comerciales en desventaja (DBE) en el Radisson Hotel Cincinnati Riverfront en Covington, con el objetivo de poner en contacto a los contratistas de DBE con posibles licitadores en el proyecto para analizar oportunidades.





Más de 260 personas representantes de firmas principales y DBE de Ohio, Kentucky, Indiana y del resto del país se reunieron con representantes del Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC) para la presentación del financiamiento, los objetivos y el cronograma del proyecto. Los participantes tuvieron la oportunidad de programar citas individuales de 15 minutos con los principales consultores y contratistas interesados en conseguir el contrato de diseño y construcción, al contrato de gerente de garantía de calidad/representante del propietario o el contrato de Ingeniería e Inspección de la Construcción (CEI).

La respuesta de los participantes fue entusiasta y casi todas las DBE y los principales asistentes calificaron al evento con un 8 o más en una escala del 1 al 10 en una encuesta que se realizó después del evento.

“La participación fue impresionante”, indicó Tom Arnold, administrador interino de programas de capital y subdirector de proyectos del BSBC para el ODOT. “Nos sentimos muy emocionados por el interés y el entusiasmo que hay por este proyecto. Este proyecto de \$3,600 millones crea una oportunidad única para construir e invertir en una fuerza laboral diversa para esta región, y estamos entusiasmados de hacer realidad esa oportunidad. Este evento es solo un paso de una iniciativa con duración de una década con muchos más por venir”.

Para obtener enlaces de recursos para consultores, contratistas y firmas de DBE, y para estar actualizado sobre eventos futuros, visite la página [Work With Us](#) en el sitio web del BSBC.

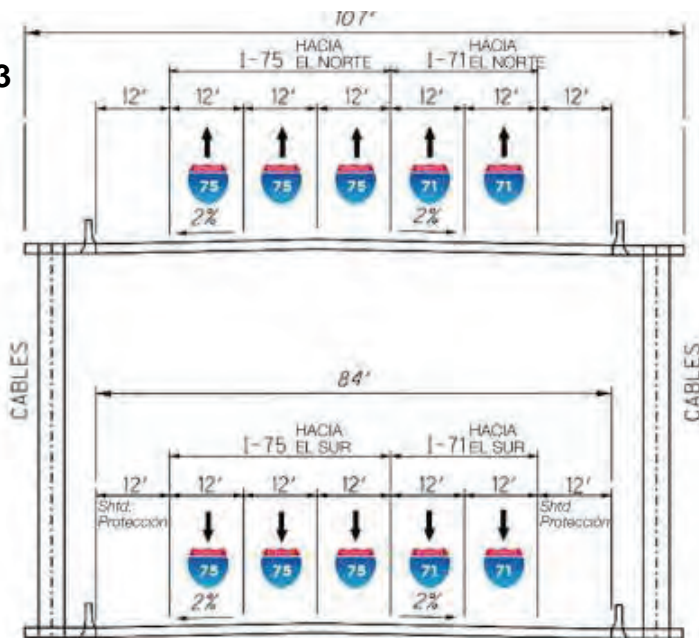
ODOT Y KYTC COMPARTEN EL OBJETIVO DE REDUCIR EL IMPACTO EN LAS COMUNIDADES LOCALES; LOS CAMBIOS DEL PROYECTO BENEFICIAN A LOS RESIDENTES EN TODO EL CORREDOR

Desde que comenzó el proceso de diseño en 2005, el Proyecto del Corredor del Puente Brent Spence (BSBC) ha tenido una serie de cambios para minimizar su impacto y abordar los comentarios del público. Como resultado, se han realizado mejoras en el diseño del proyecto en todo el corredor para beneficiar a los residentes y viajeros regionales en las próximas décadas. A continuación se describe un resumen de esas mejoras.

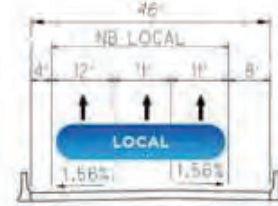
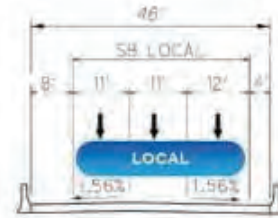
Se redujo el número de reubicaciones residenciales en más del 90% y el número de reubicaciones comerciales en un 50%

El nuevo puente complementario justo al oeste del actual puente Brent Spence se planeó originalmente para cubrir casi 25 acres y abarcar casi 150 pies de ancho. Los planos revisados muestran que el nuevo puente con casi la mitad del tamaño, cubrirá aproximadamente 14 acres y 84 pies de ancho.

Como resultado, la huella del proyecto se ha reducido significativamente. El plan original requería la reubicación de 40 unidades residenciales, incluidos edificios de departamentos con varias unidades, y 14 empresas en el área del proyecto. El plan actual, sin embargo, indica que solo cuatro unidades residenciales en total, cinco lotes comerciales y partes de otros dos requerirán reubicación.



PUENTE COMPLEMENTARIO; REVISIÓN 2022



PUENTE BRENT
SPENCE ACTUAL

Se redujeron los impactos en el número de propiedades del distrito histórico de Lewisburg en un 68%

De manera similar, el plan original indicaba que sería necesario adquirir 21 propiedades y partes de otras siete en el distrito histórico de Lewisburg. Actualmente solo se adquirirán tres propiedades y partes de otras seis.

Gracias a la reconfiguración de rampas en el centro, se devolvieron 10 acres a Cincinnati para su desarrollo

En su colaboración continua con los funcionarios electos y líderes comunitarios de Cincinnati, el año pasado el equipo del proyecto dio a conocer los planes rediseñados para el área inmediatamente al oeste de Duke Energy Convention Center, con los que se liberaron 10 acres para desarrollo futuro.

Reconexión de los vecindarios del oeste de Cincinnati con nuevas y mejoradas instalaciones para peatones y ciclistas

A pedido de los residentes y funcionarios de la ciudad, se construirán instalaciones para peatones y bicicletas a lo largo y en paralelo a la I-71 y la I-75 en la mayoría de las intersecciones de calles transversales y cruces de autopistas, lo que ayudará a reconectar los vecindarios del oeste con el centro de Cincinnati. Puede verlo [aquí](#).

Incorporación de muros acústicos, drenaje de la escorrentía de aguas pluviales y la toma de otras medidas para apoyar a los parques y comunidades locales

Otros cambios que se implementaron por comentarios del público incluyen la adición de muros acústicos y muros protectores visuales/de ruido que superan los requisitos de Kentucky y Ohio. Además, el equipo del proyecto ha acordado financiar medidas para:

- reemplazar terrenos, reconstruir un sendero para caminar, reemplazar una cancha de baloncesto, reubicar una piscina al aire libre y desarrollar un plan maestro para Goebel Park, Kenney Shields Park y el SFC Jason Bishop Memorial Dog Park;
- proteger y restaurar el Monumento a los Bomberos y el Parque Ezzard Charles durante la construcción; y
- reducir las inundaciones y los desbordamientos de alcantarillado combinado.

Barreras interestatales contra el ruido: reducción del ruido, apoyo a los vecindarios

Desarrollados inicialmente en la década de 1960 para contribuir a la reducción del ruido a medida que tomaba forma el sistema federal de carreteras, los primeros muros acústicos se diseñaron para reducir los sonidos del tráfico a lo largo de la I-680 en Milpitas, CA.

Oficialmente conocidas como barreras acústicas para carreteras, las 2,748 millas de muros acústicos en los EE. UU. han seguido evolucionando, y han mejorado tanto en eficacia como en apariencia.

Se instalarán muros acústicos en varios lugares a lo largo del Proyecto del Corredor del Puente Brent Spence (BSBC) en función del análisis técnico y la opinión del público. De hecho, el proyecto de 8 millas incluirá aproximadamente 8.75 millas de barreras acústicas para apoyar la reducción del sonido en los vecindarios de Ohio y Kentucky.

La Evaluación Ambiental (EA) original del proyecto se desarrolló en 2011 y se actualizó en 2022 para tener en cuenta los cambios en los impactos del proyecto y las actualizaciones normativas. Actualmente, se recomienda instalar muros acústicos en partes de la I-71/75 a lo largo de Ft. Mitchell, Ft. Wright y Park Hills, así como en los barrios de Peaselburg, Botany Hills Lewisburg y Mainstrasse, en Covington. En Ohio, se recomienda instalar muros acústicos a lo largo de la interestatal cerca del vecindario West End de Cincinnati y el Queensgate Playground y Ball Field. Puede encontrar más información sobre esos estudios [aquí](#).

¿Cómo funcionan los muros acústicos?

Es simple ciencia sonora. Si bien las paredes no bloquearán por completo el ruido, si cuentan con el mejor diseño y ubicación, pueden reducir los sonidos de los automóviles y camiones que pasan entre cinco y 10 decibeles, reduciendo el nivel de sonido de un camión con remolque típico al de un automóvil. Las paredes funcionan absorbiendo el sonido, transmitiéndolo, reflejándolo a través de la carretera u obligándolo a tomar un camino más largo sobre y alrededor de la barrera.

Amortiguadores atractivos para reducir los sonidos intrusivos

Es importante tener en cuenta que los muros acústicos no solo protegen a los vecindarios de la contaminación acústica. También deben ser lo suficientemente agradables a la vista para hacer juego con el paisaje cotidiano y, en algunos casos, incluso mostrar la personalidad, la historia o la creatividad de un vecindario. Para el proyecto BSBC, algunas de estas decisiones estéticas dependen del [Comité de Estética](#) del proyecto, y muchas están abiertas a los aportes de los propios vecindarios que se recogen durante las reuniones vecinales con los miembros del equipo del proyecto. Caso en cuestión: los residentes del norte de Kentucky en Goebel Park y a lo largo de Crescent Avenue expresaron su preocupación de que los muros acústicos podrían bloquear la vista de la que disfrutaban ahora. Como resultado, KYTC está evaluando la posibilidad de usar muros acústicos transparentes en ciertos lugares y continuará coordinándose con la ciudad de Covington durante el diseño final y la construcción del proyecto para abordar esas preocupaciones. Otros vecindarios están considerando exhibir el arte local, convirtiendo los muros acústicos en murales. El equipo del proyecto continuará trabajando con los vecindarios adyacentes al área del proyecto para abordar las inquietudes y obtener comentarios sobre los planes de muros acústicos.





Comparta sus pensamientos e ideas

El equipo del proyecto del Corredor del Puente Brent Spence agradece los comentarios y opiniones del público. Para enviar una pregunta o comentario, visite el [sitio web](#) del proyecto y haga clic en el botón “Contact Us” (Contáctenos) en la esquina superior derecha.

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Manténgase informado

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Visite nuestro sitio web

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Envíe un comentario

From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Friday, April 28, 2023 2:24 PM
To:
Subject: Brent Spence Bridge Corridor Project Update - Creating Vibrant Spaces

Brent Spence Bridge Corridor April 28, 2023 eNewsletter



CONNECTING COMMUNITIES AND CREATING VIBRANT SPACES

New bike and pedestrian connections and aesthetic features can have transformative impact in urban neighborhoods.

The Brent Spence Bridge Project team is taking a holistic approach to this historic infrastructure investment with a specific focus on enhancing connections between neighborhoods and creating multi-modal spaces that work for everyone. This means investing in pedestrian walkways, bike lanes, and other shared-use spaces that make it easier and safer for people to get around. It's about enhancing safety, improving accessibility, and creating a more walkable and vibrant urban environment.



But it's not just about function - the project is also incorporating aesthetic features that add to the community's sense of place. Decorative treatments on bridges and walls, planters, lighting, and other design features will all be part of the project's vision for innovative improvements that represent communities throughout the corridor.

The best part? Projects like these have been proven to make a real difference in communities. Studies have shown that multi-modal improvements not only enhance safety and accessibility, but also create economic benefits for local businesses and can boost property values in the surrounding area.

Visit the [multi-modal exhibit](#) posted on the project website to see examples, like how the shared use path in Goebel Park will be extended to connect to the existing path along the Ohio River. The legend on each slide shows where new sidewalks, new bike lanes, and new shared-use paths will go, including improvements on nine east-west roadways that intersect I-75 in Ohio. It also details new pedestrian bridges and where existing pavement will be converted into green space.

So, whether you're a driver, pedestrian, or bicyclist, the Brent Spence Bridge Corridor Project will benefit neighborhoods on both sides of the river.

DESIGN-BUILD TEAM TO BE ANNOUNCED IN JUNE

The Brent Spence Bridge Corridor Project Team is scheduled to announce the design-build team in early June.

Proposals to become the prime contractor for the \$3.6 billion project were due April 14. Interviews were scheduled for the last week in April. The announcement will mark the culmination of a nearly five-month procurement process that began with issuing a draft request for proposals in mid-January.

The first assignment for the winning bidder will be to confirm the feasibility of the project plan as outlined in the request for proposal. This is commonly referred to as a “proof of concept” phase. The next step will be to complete the design.

The project includes the construction of a companion bridge and improvements to the current bridge and the roadway network leading to both. Work is scheduled to be completed in 2030.

RIGHT-OF-WAY ACQUISITION UPDATE

In road construction, "right-of-way" (ROW) often refers to property needed to advance a project. The ROW area must be enough to accommodate all elements of the roadway cross-section and any public utility facilities that might be installed along the roadway.

For a project as large as the Brent Spence Bridge Corridor Project (BSBC), the right-of-way acquisition process requires utmost planning and attention. Because the BSBC project area is urban, impacting both residential and commercial properties, the project team has taken great care to keep the community top-of-mind, working hard to minimize the project footprint and reduce property impacts while still delivering a solid solution to traffic congestion in our region.

In July 2022, based on significant community engagement and a thorough technical analysis, ODOT and KYTC introduced updated project maps with new lane configurations that significantly reduced the project footprint from its original 2012 design. The revised plan reduced the new bridge footprint to almost half the originally planned size – in turn, significantly reducing the number of impacted properties.



Where Are We Now?

Each state is separately conducting their ROW process according to their individual regulations. Ohio began the acquisition of Right-of-Way in 2014 and projects completion by the end of 2023. Of the 79 parcels to be acquired, 70 are complete. The remaining nine are all commercial and railroad properties located between the Ohio River and West Third Street in Cincinnati.

In Kentucky, the process began in early 2022. Of the 72 parcels to be acquired, 40 parcels are complete and 12 are now actively in the ROW process. The remaining 20 parcels are contingent upon the update of the project's environmental document. You can learn more about that process [here](#).

Share Your Thoughts and Ideas

The Brent Spence Bridge Corridor Project team welcomes comments and feedback from the public. To submit a question or comment, visit the project [website](#) and click on the “Contact Us” button in the upper right-hand corner.

Responses to all public comments can be viewed on the [Public Involvement and Comments](#) section of the website. This section features project exhibits, summaries of outreach activities, and summaries of responses to questions posed to the project team during meetings or via email. The document with the summary of responses from all sources is updated monthly.



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Boletín electrónico del Corredor del Puente Brent Spence, 28 de abril de 2023

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INVIRTIENDO EN LAS COMUNIDADES LOCALES.
HACIENDO CRECER LA ECONOMÍA ESTADOUNIDENSE.



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CONECTAR COMUNIDADES Y CREAR ESPACIOS DINÁMICOS

Las nuevas conexiones para bicicletas y peatones y las características estéticas pueden tener un impacto transformador en los vecindarios urbanos.

El equipo del Proyecto del Puente Brent Spence está adoptando un enfoque holístico para esta inversión histórica en infraestructura con un enfoque específico en mejorar las conexiones entre los vecindarios y crear espacios multimodales que sean funcionales para todos. Esto significa invertir en senderos para peatones, carriles para bicicletas y otros espacios de uso compartido que hacen que las personas puedan moverse de un modo más fácil y seguro. Se trata de reforzar la seguridad, mejorar la accesibilidad y crear un entorno urbano más peatonal y dinámico.





Pero no se trata solo de la funcionalidad; el proyecto también incorpora elementos estéticos que se suman al sentido de pertenencia de la comunidad. Los tratamientos decorativos en puentes y paredes, jardineras, iluminación y otros elementos de diseño serán parte de la visión del proyecto para hacer mejoras innovadoras que representen a las comunidades a lo largo de todo el corredor.

¿Cuál es la mejor parte? Se ha demostrado que proyectos como estos marcan una diferencia real en las comunidades. Los estudios indican que las mejoras multimodales no solo mejoran la seguridad y la accesibilidad, sino que crean beneficios económicos para las empresas locales y pueden aumentar el valor de las propiedades en el área circundante.

Visite la [exhibición multimodal](#) publicada en el sitio web del proyecto para ver ejemplos; como de qué manera se extenderá el camino de uso compartido en Goebel Park para conectarse con el camino existente junto al río Ohio. La leyenda en cada diapositiva muestra dónde irán las nuevas aceras, los nuevos carriles para bicicletas y los nuevos caminos de uso compartido, incluidas las mejoras en nueve carreteras de este a oeste que se cruzan con la I-75 en Ohio. También detalla nuevos puentes peatonales y dónde el pavimento existente se convertirá en espacio verde.

Entonces, sea usted un conductor, un peatón o un ciclista, el proyecto del Corredor del Puente Brent Spence beneficiará a los vecindarios a ambos lados del río.

EL EQUIPO DE DISEÑO Y CONSTRUCCIÓN SERÁ ANUNCIADO EN JUNIO

Está previsto que el equipo del proyecto del Corredor del Puente Brent Spence anuncie el equipo de diseño y construcción a principios de junio.

Las propuestas para convertirse en el contratista principal del proyecto de \$3,600 millones debían presentarse el 14 de abril. Las entrevistas estaban programadas para la última semana de abril. El anuncio marcará la culminación de un proceso de adquisición de casi cinco meses que comenzó con la emisión de un borrador de solicitud de propuestas a mediados de enero.

La primera tarea para el adjudicatario será confirmar la viabilidad del plan del proyecto como se describe en la solicitud de propuesta. Esto se conoce comúnmente como una fase de "prueba de concepto". El siguiente paso será completar el diseño.

El proyecto incluye la construcción de un puente complementario y mejoras al puente actual y la red vial que conduce a ambos. El trabajo está programado para completarse en 2030.

ACTUALIZACIÓN DE ADQUISICIÓN DE DERECHO DE PASO

En la construcción de carreteras, el "Derecho de Paso" (ROW) a menudo se refiere a la

En la construcción de carreteras, el Derecho de Paso (ROW) a menudo se refiere a la propiedad que se necesita para avanzar en un proyecto. El derecho de paso debe ser suficiente para incorporar todos los elementos de la sección transversal de la calzada y cualquier infraestructura de servicios públicos que pueda instalarse a lo largo de la calzada.

Para un proyecto tan grande como el proyecto del Corredor del Puente Brent Spence (BSBC), el proceso de adquisición del derecho de paso requiere la máxima planificación y atención. Debido a que el área del proyecto BSBC es urbana, lo que afecta tanto a las propiedades residenciales como comerciales, el equipo del proyecto se ha esforzado por mantener a la comunidad en primer plano, trabajando arduamente para minimizar la huella del proyecto y reducir el impacto en la propiedad sin dejar de ofrecer una solución sólida para la congestión vehicular en nuestra región.

En julio de 2022, gracias a la participación significativa de la comunidad y un análisis técnico exhaustivo, ODOT y KYTC introdujeron mapas de proyectos actualizados con nuevas configuraciones de carriles que redujeron significativamente la huella del proyecto con respecto a su diseño original de 2012. El plan revisado redujo la huella del nuevo puente a casi la mitad del tamaño originalmente planificado, lo que a su vez disminuyó significativamente la cantidad de propiedades afectadas.



¿Dónde estamos ahora?

Cada estado lleva a cabo por separado su proceso de ROW de acuerdo con sus regulaciones individuales. Ohio comenzó la adquisición de derechos de paso en 2014 y se completarán los proyectos a fines de 2023. De las 79 parcelas a adquirir, 70 están concluidas. Las nueve restantes son todas propiedades comerciales y ferroviarias ubicadas entre el río Ohio y West Third Street en Cincinnati.

En Kentucky, el proceso comenzó a principios de 2022. De las 72 parcelas que se adquirirán, 40 están finalizadas y 12 ahora están activamente en el proceso de ROW. Las 20 parcelas restantes están sujetas a la actualización del documento ambiental del proyecto. Puede obtener más información sobre ese proceso [aquí](#).

El equipo del proyecto del Corredor del Puente Brent Spence agradece los comentarios y opiniones del público. Para enviar una pregunta o comentario, visite el [sitio web](#) del proyecto y haga clic en el botón "Contact Us" (Contáctenos) en la esquina superior derecha.

Las respuestas a todos los comentarios públicos se pueden ver en la sección [Public Involvement and Comments](#) (Participación pública y comentarios) del sitio web. Esta sección presenta exhibiciones de proyectos, resúmenes de actividades de divulgación y resúmenes de respuestas a preguntas que se han presentado al equipo del proyecto en las reuniones o por medio del correo electrónico. El documento con el resumen de las respuestas de todas las fuentes se actualiza mensualmente.



Manténgase informado

Hay varias maneras de mantenerse informado acerca del proyecto del Corredor del Puente Brent Spence.

[Visite nuestro sitio web](#)

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From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Wednesday, May 31, 2023 1:42 PM
To:
Subject: Brent Spence Bridge Corridor Project Update - Enhancing Urban Living

Brent Spence Bridge Corridor May 31, 2023 eNewsletter



A CLOSER LOOK: CONNECTING COMMUNITIES AND ENHANCING URBAN LIVING

ODOT Director and Cincinnati Mayor Reflect on Enhancements Following West End Walking Tour

Ohio Department of Transportation Director Jack Marchbanks and Cincinnati Mayor Aftab Pureval recently conducted a walking tour in neighborhoods along the I-75 corridor for a closer look at how the Brent Spence Bridge Corridor Project (BSBC) project will improve pedestrian and bicyclist experiences. While the primary focus of the project is to alleviate traffic congestion and improve connectivity across the Ohio River, it also aims to foster connections and revitalize the relationship between downtown Cincinnati and the West End neighborhoods.



During the tour, Director Marchbanks and Mayor Pureval visited key locations, including Linn Street, West Court Street, Ezzard Charles Drive, Winchell Avenue, and West Liberty Street. These areas are poised for transformation into vibrant spaces, with many featuring green space and park areas, dedicated walking paths, bicycle lanes, and more.

ODOT actively partners with the city on the Rebuilding American Infrastructure with Sustainability and Equity (RAISE) Grant improvements at Linn Street and 8th Street, which align with improvements coming as part of the Brent Spence Bridge Corridor (BSBC) Project. Furthermore, plans are underway for the development and replacement of a pedestrian bridge over Winchell Avenue.

Following the tour, Mayor Pureval expressed his enthusiasm for the project, acknowledging its potential to enhance safety and stimulate growth in the region. In a tweet, he emphasized the importance of reconnecting communities that have been harmed by past infrastructure decisions. The City of Cincinnati has played an active role in the planning process, with a vision to create an inviting, accessible, and pedestrian-friendly urban environment.

Director Marchbanks shared a personal connection, saying, "I understand the impact of building an interstate highway through a neighborhood home to thousands of minority families. My childhood home in Dayton was taken by the construction of I-75 in the 1960s. This project will help reconnect the West End to downtown by creating new connections and strengthening existing ones across the interstate."

Marchbanks added, "As an avid cyclist myself, I am thrilled to see that we're focused not just on moving vehicles but moving people with all modes of transportation."

The project team has successfully reduced the project's footprint, freeing up nearly 10 acres of contiguous developable land and presenting opportunities for future city development. As

the design-build team takes shape, one of their objectives will be to explore feasible ways to expand on this already significant land allocation.

BRENT SPENCE BRIDGE CORRIDOR PROJECT LEADERSHIP MEETS WITH THE GREATER CINCINNATI AND NORTHERN KENTUCKY AFRICAN-AMERICAN CHAMBER OF COMMERCE

As part of an effort to attract a diverse workforce and share opportunities, Ohio Department of Transportation (ODOT) Director Jack Marchbanks, BSBC Project Manager Tom Arnold, and other members of the BSBC leadership team recently met with Eric Kearney, president and CEO of the [African-American Chamber of Cincinnati/Northern Kentucky](#) and his staff, as well as several chamber members.

"The Greater Cincinnati & Northern Kentucky African American Chamber of Commerce was honored to host ODOT Director Dr. Jack Marchbanks to discuss the inclusion of DBEs in the Brent Spence Bridge project," said Mr. Kearney. "We greatly appreciate Dr. Marchbanks's pragmatic and direct approach and his willingness to listen to small business owners. The Brent Spence Bridge project is a transformative opportunity for Ohio businesses to grow and prosper. We certainly look forward to working with ODOT towards those goals."

The group gathered at the Chamber's Cincinnati office on May 2 to review the BSBC project plan and discuss ways the Chamber and its members can get involved in the transformative \$3.6 billion construction process. Marchbanks and Arnold outlined the project's goals for Disadvantaged Business Enterprise (DBE) involvement and resources available for certification and asked the group's input and advice on engaging minority-owned businesses and ensuring the project's goals are not just met but surpassed.

"I'm very grateful for Mr. Kearney and his team's time and candor," said Arnold. "We had an excellent, honest discussion. We listened carefully and are already putting their good advice to work to make our efforts even better."

Find out more about the BSBC Project's work to foster DBE involvement, as well as certification resources, [here](#).



ODOT PROJECT TEAM MEMBERS MEET WITH BRIDGE FORWARD FOR FOURTH TIME THIS YEAR

Public input has always been a key pillar in delivering a project that will address current and future transportation needs in the region and beyond. The project team has met with the community advocacy group Bridge Forward on several occasions this year as part of its collaboration with the City of Cincinnati to discuss their ideas and provide feedback. Prior to their most recent meeting, ODOT reached out to Bridge Forward to request a meeting to gain insights on their current revised proposal.

In general, the Bridge Forward proposal advocates for “stacking” the highway on the Ohio side to potentially free up more land for development in the city center. While ODOT is aligned with the idea of keeping the project footprint to a minimum, freeing up land for development, and reducing impacts wherever possible.

“We appreciate Bridge Forward’s passion for making sure we do this project right and don’t miss any opportunities to bring value back to the city. We agree,” said Project Manager Tom Arnold. “In our role as transportation engineers, we just have to be sure any idea can be done safely without jeopardizing the goals of the project or creating significant additional cost.”

The project team committed to the group that it will continue to examine ways to provide more land to the city and share this information with the selected design-build team. Until then, ODOT will continue meeting and working with the City of Cincinnati engineering team and community stakeholders to ensure the best ideas are pushed forward for evaluation.



BOOTS ON THE GROUND: HELPING DEVELOP THE WORKFORCE OF TOMORROW

The Brent Spence Bridge Corridor Diversity & Inclusion Outreach team has swung into action, working to ensure Disadvantaged Business Enterprises (DBE), small businesses, and individual workers are aware of and can access the once-in-a-generation opportunities this \$3.6 billion project offers the region's workforce.

"This project needs all hands on deck to get to completion," said Brent Spence Bridge Corridor Project Manager Tom Arnold. "It's critical that the project workforce accurately reflects our region's diversity, and that the opportunities this massive investment creates are shared with folks who are interested in learning how they can be a part of this project. We've been busy getting the word out so people know how to prepare for upcoming opportunities once we have a contractor on board."

Over the past four weeks, the team has deployed to three major events targeted to each of these audiences, starting with the Tri-State DBE Meeting on April 26. Presented by [CEI DBE Support Services](#), this virtual event brought together representatives from the Ohio Department of Transportation, the Kentucky Transportation Cabinet and the Indiana Department of Transportation in an effort to connect DBEs with prime contractors.

Just a week later, the team staffed their booth at the [Allied Construction Career Days](#) event at the Butler County Fairgrounds. The largest construction-based career exploration and hiring event in the Tri-State, this in-person event brings together more than 3,000 middle and high school students from 60 school districts, with 50-plus exhibitors from every sector of the industry providing hands-on activities, information on careers and employment opportunities.

At the [Hamilton County Small Business Day](#) on May 22, BSBC Project Manager Tom Arnold led a breakout session titled “How to Get Involved with the Brent Spence Bridge Project.” The event officially launched the county’s [Office of Small Business](#) and its new Small Business Relief Grants, hosting more than 900 entrepreneurs for networking and educational opportunities. Other organizations involved included local chambers of commerce, the U.S. Small Business Association, and the Minority Business Assistance Center.

You can find out where the Diversity & Inclusion team will appear next and find resources to help you get involved in project construction [here](#).



PUBLIC SHARES ENTHUSIASM FOR POTENTIAL PROJECT WORK

The Brent Spence Bridge Corridor Project team welcomes feedback from the public. Recently, many comments and questions have come from local business owners and individuals about potential work and/or career opportunities. This \$3.6 billion investment in the region will bring with it a steady stream of opportunity over the next seven years. More information about how to become trained and/or certified to work on the project is available on our website [here](#). Once the design-build team is selected, there will be more information about how to reach those hiring businesses or individuals for project work.

To submit a question or comment, please visit the project [website](#) and click on the “Contact Us” button in the upper right-hand corner.

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Boletín electrónico del corredor del puente Brent Spence, 31 de mayo de 2023



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TRANSPORTATION CABINET

UNA MIRADA EN DETALLE: CONECTAR COMUNIDADES Y MEJORAR LA VIDA URBANA

Luego de recorrer a pie el West End, el director de ODOT y el alcalde de Cincinnati reflexionan sobre las mejoras

El director del Departamento de Transporte de Ohio, Jack Marchbanks, y el alcalde de Cincinnati, Aftab Pureval, realizaron recientemente un recorrido a pie por las colonias a lo largo del corredor I-75 para ver más de cerca cómo el proyecto del corredor del puente Brent Spence (BSBC) mejorará las experiencias de peatones y ciclistas. Si bien el enfoque principal del proyecto es aliviar la congestión del tráfico y mejorar la conectividad a lo largo del río Ohio, también tiene como objetivo fomentar las conexiones y revitalizar la relación entre el centro de Cincinnati y las colonias del West End.





Durante el recorrido, el director Marchbanks y el alcalde Pureval visitaron lugares clave, como Linn Street, West Court Street, Ezzard Charles Drive, Winchell Avenue y West Liberty Street. Estas áreas están preparadas para transformarse en espacios dinámicos; muchas cuentan con espacios verdes y áreas de parques, senderos exclusivos para caminar, carriles para bicicletas, entre otros.

El ODOT se asocia activamente con la ciudad en las mejoras por la subvención Rebuilding American Infrastructure with Sustainability and Equity (Reconstruir la infraestructura estadounidense con sostenibilidad y equidad) (RAISE) en Linn Street y 8th Street, que se alinean con las mejoras que son parte del proyecto del corredor del puente Brent Spence (BSBC). Además, hay planes en marcha para el desarrollo y reemplazo de un puente peatonal sobre Winchell Avenue.

Luego del recorrido, el alcalde Pureval expresó su entusiasmo por el proyecto al reconocer su potencial para mejorar la seguridad y estimular el crecimiento en la región. En un tuit, enfatizó la importancia de volver a conectar las comunidades que se han visto perjudicadas por decisiones de infraestructura anteriores. La ciudad de Cincinnati ha desempeñado un papel activo en el proceso de planificación, con la visión de crear un entorno urbano acogedor, accesible y amigable para los peatones.

El director Marchbanks compartió una conexión personal y dijo: "Entiendo el impacto de construir una carretera interestatal a través de una colonia que alberga a miles de familias provenientes de minorías. La casa de mi niñez en Dayton fue tomada por la construcción de la I-75 en la década de 1960. Este proyecto ayudará a reconectar el West End con el centro de la ciudad mediante la creación de nuevas conexiones y fortaleciendo las existentes a través de la autopista".

Marchbanks agregó: "Como un ávido ciclista, estoy encantado de ver que nos enfocamos no solo en mover vehículos, sino también en mover personas con todos los modos de transporte".

El equipo del proyecto redujo con éxito la huella del proyecto al liberar casi 10 acres de tierra urbanizable contigua y presentar oportunidades para el futuro desarrollo de la ciudad. A medida que el equipo de diseño y construcción se constituyen, uno de sus objetivos será explorar formas viables de ampliar esta asignación de terrenos que ya es significativa.

**EL LIDERAZGO DEL PROYECTO DEL CORREDOR DEL PUENTE
BRENT SPENCE SE REÚNE CON LA AFRICAN-AMERICAN
CHAMBER OF COMMERCE (CÁMARA DE COMERCIO**

AFROAMERICANA) DEL AREA METROPOLITANA DE CINCINNATI Y EL NORTE DE KENTUCKY

May 2023
Spanish
E-Newsletter

Como parte de un esfuerzo por atraer una fuerza laboral diversa y compartir oportunidades, el director del Departamento de Transporte de Ohio (ODOT), Jack Marchbanks, el gerente de proyectos de BSBC, Tom Arnold, y otros miembros del equipo de liderazgo de BSBC se reunieron recientemente con Eric Kearney, presidente y director ejecutivo de la [Cámara Afroamericana de Cincinnati/Norte de Kentucky](#) y su personal, así como con varios miembros de la cámara.

"La Cámara de Comercio Afroamericana del Área Metropolitana de Cincinnati y el Norte de Kentucky tuvo el honor de recibir al director de ODOT, Dr. Jack Marchbanks, para abordar el tema de la inclusión de empresas comerciales en desventaja (DBE) en el proyecto del puente Brent Spence", dijo el Sr. Kearney. "Apreciamos mucho el enfoque pragmático y directo del Dr. Marchbanks y su disposición a escuchar a los propietarios de pequeñas empresas. El proyecto del puente Brent Spence es una oportunidad transformadora para que las empresas de Ohio crezcan y prosperen. Ciertamente esperamos trabajar con el ODOT para lograr esos objetivos".

El grupo se reunió en la oficina de la Cámara en Cincinnati el 2 de mayo para revisar el plan del proyecto BSBC y analizar las formas en que la Cámara y sus miembros pueden participar en el transformador proceso de construcción de \$3.6 mil millones. Marchbanks y Arnold describieron los objetivos del proyecto para la participación de empresas comerciales en desventaja (DBE) y los recursos disponibles para la certificación, y solicitaron la opinión y el consejo del grupo para involucrar a las empresas propiedad de minorías y garantizar que los objetivos del proyecto no solo se cumplan, sino que se superen.

"Estoy muy agradecido por el tiempo y la franqueza del Sr. Kearney y su equipo", dijo Arnold. "Tuvimos una conversación excelente y sincera. Escuchamos con atención y ya estamos poniendo en práctica sus buenos consejos para hacer que nuestros esfuerzos sean aún mejores".

Obtenga más información sobre el trabajo del proyecto BSBC para fomentar la participación de DBE, así como los recursos de certificación, [aquí](#).



LOS MIEMBROS DEL EQUIPO DEL PROYECTO ODOT SE REÚNEN CON BRIDGE FORWARD POR CUARTA VEZ ESTE AÑO

La opinión pública siempre ha sido un pilar clave en la entrega de un proyecto que abordará las necesidades de transporte actuales y futuras en la región y más allá de esta. El equipo del proyecto se ha reunido con el grupo de defensores de la comunidad Bridge Forward en varias

proyecto se ha reunido con el grupo de aerensa de la comunidad Bridge Forward en varias ocasiones este año como parte de su colaboración con la ciudad de Cincinnati para hablar sobre sus ideas y brindar comentarios. Antes de su reunión más reciente, el ODOT contactó a Bridge Forward para solicitar una reunión y obtener información sobre su propuesta revisada actual.

En general, la propuesta de Bridge Forward aboga por "apilar" la carretera en el lado de Ohio para potencialmente liberar más terreno para el desarrollo en el centro de la ciudad. El ODOT se alinea con la idea de mantener la huella del proyecto al mínimo al liberar terrenos para el desarrollo y reducir los impactos siempre que sea posible.

"Apreciamos la pasión de Bridge Forward por asegurarse de hacer bien este proyecto y no perder ninguna oportunidad de devolver valor a la ciudad. Estamos de acuerdo", dijo Tom Arnold, el gerente de proyecto. "En nuestra función como ingenieros de transporte, solo debemos asegurarnos de que cualquier idea se pueda realizar de manera segura sin poner en peligro los objetivos del proyecto o generar un costo adicional significativo".

El equipo del proyecto se comprometió con el grupo a continuar examinando formas de proporcionar más terreno a la ciudad y compartir esta información con el equipo de diseño y construcción seleccionado. Hasta entonces, el ODOT continuará reuniéndose y trabajando con el equipo de ingeniería de la ciudad de Cincinnati y las partes interesadas de la comunidad para garantizar que se presenten las mejores ideas para su evaluación.



MANOS A LA OBRA: AYUDAR A DESARROLLAR LA FUERZA LABORAL DEL MAÑANA

El equipo de Divulgación de diversidad e inclusión del corredor del puente Brent Spence ha entrado en acción, trabajando para garantizar que las empresas comerciales en desventaja (DBE), las pequeñas empresas y los trabajadores individuales conozcan y puedan acceder a las oportunidades únicas que este proyecto de \$3.6 mil millones ofrece la mano de obra de la región.

"Este proyecto necesita todas las manos a la obra para completarse", dijo Tom Arnold, gerente de proyecto del corredor del puente Brent Spence. "Es fundamental que la fuerza laboral del proyecto refleje con precisión la diversidad de nuestra región, y que las oportunidades que crea esta inversión masiva se compartan con personas interesadas en saber cómo pueden ser parte de este proyecto. Hemos estado ocupados haciendo correr la voz para que la gente sepa cómo prepararse para las próximas oportunidades una vez que tengamos un contratista a bordo".

Durante las últimas cuatro semanas, el equipo se ha desplegado en tres eventos importantes dirigidos a cada una de estas audiencias, comenzando con la reunión para DBE de los tres estados el 26 de abril. Este evento virtual, presentado por [CEI DBE Support Services](#), reunió a representantes del Departamento de Transporte de Ohio, el Gabinete de Transporte de Kentucky y el Departamento de Transporte de Indiana en un esfuerzo por conectar las DBE con las principales contratistas.

Apenas una semana después, el equipo estuvo presente en su caseta en el evento [Allied Construction Career Days](#) en Butler County Fairgrounds. Se trata del evento más grande de exploración y contratación de carreras basadas en la construcción en los tres estados; este evento presencial reúne a más de 3,000 estudiantes de secundaria y preparatoria de 60 distritos escolares, con más de 50 expositores de todos los sectores de la industria que ofrecen actividades prácticas, información sobre carreras y oportunidades de empleo.

El [Día de la Pequeña Empresa del condado de Hamilton](#), el 22 de mayo, el gerente del proyecto BSBC, Tom Arnold, dirigió una sesión de trabajo titulada "Cómo participar en el proyecto del puente Brent Spence". El evento inauguró oficialmente la [Oficina de Pequeñas Empresas](#) del condado y sus nuevas subvenciones de ayuda para pequeñas empresas, albergando a más de 900 empresarios para establecer contactos y oportunidades educativas. Otras organizaciones involucradas incluyeron cámaras de comercio locales, la U.S. Small Business Association (Administración de Pequeñas Empresas de EE. UU.) y Minority Business Assistance Center (Centro de Asistencia para Empresas Minoritarias).

Puede averiguar dónde se presentará el equipo de Diversidad e inclusión próximamente y encontrar recursos para participar en la construcción del proyecto [aquí](#).



EL PÚBLICO COMPARTE EL ENTUSIASMO POR EL POTENCIAL DE TRABAJO DEL PROYECTO

El equipo del proyecto del corredor del puente Brent Spence agradece los comentarios del público. Recientemente, han llegado muchos comentarios y preguntas de personas y dueños de negocios locales sobre posibles oportunidades laborales o profesionales. Esta inversión de \$3.6 mil millones en la región traerá consigo un flujo constante de oportunidades durante los próximos siete años. Encuentre más información sobre cómo capacitarse o certificarse para trabajar en el proyecto en nuestro sitio web, [aquí](#).

Una vez que se seleccione el equipo de diseño y construcción, habrá más información sobre cómo llegar a las empresas o personas que contratan trabajadores para el proyecto.

May 2023
Spanish
E-Newsletter

Para enviar una pregunta o comentario, visite el [sitio web](#) del proyecto y haga clic en el botón "Contact Us" (Contáctenos) en la esquina superior derecha.

Las respuestas a todos los comentarios públicos se pueden ver en la sección [Public Involvement and Comments](#) (Participación pública y comentarios) del sitio web. Esta sección presenta exhibiciones de proyectos, resúmenes de actividades de divulgación y resúmenes de respuestas a las preguntas que presentadas al equipo del proyecto en las reuniones o por medio del correo electrónico. El documento con el resumen de las respuestas de todas las fuentes se actualiza mensualmente.



Manténgase informado

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From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Friday, June 30, 2023 4:26 PM
To:
Subject: Brent Spence Bridge Corridor Project Update - Progressive Design Build

Brent Spence Bridge Corridor June 30, 2023 eNewsletter



PROGRESSIVE DESIGN-BUILD: A PROCESS THAT IMPROVES COLLABORATION

Members of the Brent Spence Bridge Corridor (BSBC) Project team have selected a design and construction process that will create a partnership between the designer, contractor, the Kentucky Transportation Cabinet (KYTC), and the Ohio Department of Transportation (ODOT) to fulfill the \$3.6 billion project's mission.

The process is called progressive design-build. Under this approach, a single entity is selected to design and construct the project based on qualifications rather than price. The entity and the owner collaborate on innovation to finalize the design. They then work together to decide which party can best handle certain aspects of financial risk before a final price is determined. The process is intended to better address project goals while also keeping the project on time and within budget.

As the design progresses, the design-build team will consult with and obtain approvals from the project team at predetermined points throughout the process. The design-build team will also coordinate with KYTC and ODOT on financial risk responsibility and produce forecasts for the total cost of the project at regular intervals during this phase.

Under this process, the design-build team can price materials closer to when they actually will be used for construction. If cost estimates exceed budget expectations, the project team can work with the design team to revise the design accordingly. Once the design has reached a

level of detail where all the financial risks have been identified and prices can be determined, the design-build and project team will then negotiate a guaranteed maximum price.

By working collaboratively and closely with the design-build team early and often throughout the progressive design-build process, the Brent Spence Bridge Corridor Project team can exert control over the design, cost, and timeline.



BRENT SPENCE BRIDGE CORRIDOR PROJECT DISCUSSED AT CINCINNATI DBE EVENT; EXPO PLANNED FOR SEPTEMBER

With a project of this scale, there is tremendous opportunity for economic impact throughout the region and for businesses of all sizes. The Brent Spence Bridge Corridor Project team is taking proactive measures to engage with small, minority, women-owned, and Disadvantaged Business Enterprises (DBE). In this photo, ODOT Southwest Ohio Regional Outreach Manager Larry Brown is seen speaking at the 2023 We Mean Business Conference on June 8, presented by the Minority Business Assistance Center and the African American Chamber of Commerce. The team also sponsored a breakout session at the event about the BSBC project.

For more information on upcoming events, DBE opportunities, and the work of the project team's Diversity & Inclusion Outreach advisory committee, visit <https://brentspencebridgecorridor.com/work-with-us/>

An upcoming event that highlights this commitment is the 2023 Business Enterprise Expo, scheduled for September 29, 2023. City-certified Minority Business Enterprise, Small

Business Enterprise, and Women's Business Enterprise (MBE/SBE/WBE) companies are invited to showcase their offerings at the expo, which is sponsored by the City of Cincinnati's Department of Economic Inclusion and will be held at the Duke Energy Convention Center. The expo promises to be a platform for minority-owned and diverse businesses to connect with potential customers, partners, and investors. Attendees can look forward to engaging speakers, informative workshops, and ample networking opportunities. Free for MBE/WBE/SBEs certified with the City of Cincinnati's Department of Economic Inclusion. For more information and to secure your spot at the expo, please visit: <https://www.eventbrite.com/e/2023-business-enterprise-expo-tickets-600667912897>.

SHARE YOUR THOUGHTS AND IDEAS

The Brent Spence Bridge Corridor Project team welcomes comments and feedback from the public. To submit a question or comment, visit the project [website](#) and click on the "Contact Us" button in the upper right-hand corner. Responses to all public comments can be viewed on the [Public Involvement and Comments](#) section of the website. This section features project exhibits, summaries of outreach activities, and responses to questions posed to the project team during meetings or via email. The document with the responses from all sources is updated monthly.

Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

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Boletín electrónico del corredor del puente Brent Spence, 30 de junio de 2023



DISEÑO Y CONSTRUCCIÓN PROGRESIVOS: UN PROCESO QUE MEJORA LA COLABORACIÓN

Los miembros del equipo del proyecto del corredor del puente Brent Spence (BSBC) seleccionaron un proceso de diseño y construcción que creará una asociación entre el diseñador, el contratista, el Gabinete de Transporte de Kentucky (KYTC) y el Departamento de Transporte de Ohio (ODOT) para cumplir con la misión del proyecto de \$3.6 mil millones.

El proceso se llama diseño y construcción progresivos. Según este enfoque, se selecciona una sola entidad para diseñar y construir el proyecto basándose en las calificaciones más que en el precio. La entidad y el propietario colaboran en innovación para finalizar el diseño. Luego trabajan juntos para decidir qué parte puede manejar mejor ciertos aspectos del riesgo financiero antes de que se determine el precio final. El propósito del proceso es abordar mejor los objetivos del proyecto y, a la vez, mantenerlo a tiempo y dentro del presupuesto.

A medida que avanza el diseño, el equipo de diseño y construcción consultará y obtendrá aprobaciones del equipo del proyecto en puntos predeterminados durante todo el proceso. El equipo de diseño y construcción también coordinará con el KYTC y el ODOT la responsabilidad del riesgo financiero y producirá pronósticos para el costo total del proyecto a intervalos regulares durante esta fase.

Siguiendo este proceso, el equipo de diseño y construcción puede fijar el precio de los materiales más cerca del momento en que realmente se usarán para la construcción. Si las estimaciones de costos exceden las expectativas presupuestarias, el equipo del proyecto puede trabajar con el equipo de diseño para revisar el diseño. Una vez que el diseño haya alcanzado un nivel de detalle en el que se identificaron todos los riesgos financieros y se puedan determinar los precios, el equipo de diseño, construcción y proyecto negociará un precio máximo garantizado.

Al trabajar juntos y en estrecha colaboración con el equipo de diseño y construcción desde el principio y, a menudo, durante todo el proceso progresivo de diseño y construcción, el equipo

del proyecto del corredor del puente Brent Spence puede mantener el control sobre el diseño, el costo y el cronograma.



EL PROYECTO DEL CORREDOR DEL PUENTE BRENT SPENCE SE DEBATE EN EL EVENTO DBE EN CINCINNATI; EXPO PLANIFICADA PARA SEPTIEMBRE

Con un proyecto de esta magnitud, existe una enorme oportunidad de impacto económico en toda la región y para empresas de todos los tamaños. El equipo del proyecto del corredor del puente Brent Spence está tomando medidas proactivas para interactuar con empresas comerciales pequeñas, propiedad de minorías, de mujeres y en desventaja (DBE). En esta foto, se ve al gerente de extensión regional del ODOT del suroeste de Ohio, Larry Brown, hablando en la conferencia We Mean Business 2023, el 8 de junio, presentada por Centro de Asistencia de Empresas Propiedad de Minorías y la Cámara de Comercio Afroamericana. El equipo también patrocinó una sesión de trabajo en el evento sobre el proyecto BSBC.

Para obtener más información sobre los próximos eventos, oportunidades de DBE y el trabajo del comité asesor de Diversidad e Inclusión del equipo del proyecto, visite <https://brentspencebridgecorridor.com/work-with-us/>

Un próximo evento que destaca este compromiso es la Business Enterprise Expo de 2023, programada para el 29 de septiembre de 2023. Las empresas propiedad de minorías,

pequeñas empresas y empresas de mujeres (MBE/SBE/WBE) certificadas por la ciudad están invitadas a mostrar sus ofertas en la exposición, que está patrocinada por el Departamento de Inclusión Económica de la ciudad de Cincinnati y se llevará a cabo en Duke Energy Convention Center. La exposición promete ser una plataforma para que empresas diversas y de propiedad de minorías se conecten con clientes, socios e inversores potenciales. Los asistentes pueden esperar escuchar a oradores interesantes, participar en talleres informativos y tener amplias oportunidades para establecer contactos. Es gratis para MBE/WBE/SBE certificadas por el Departamento de Inclusión Económica de la ciudad de Cincinnati.

Para obtener más información y asegurar su lugar en la exposición, visite: <https://www.eventbrite.com/e/2023-business-enterprise-expo-tickets-600667912897>.

COMPARTA SUS PENSAMIENTOS E IDEAS

El equipo del proyecto del corredor del puente Brent Spence agradece los comentarios y opiniones del público. Para enviar una pregunta o comentario, visite el [sitio web](#) del proyecto y haga clic en el botón "Contact Us" (Contáctenos) en la esquina superior derecha. Las respuestas a todos los comentarios públicos se pueden ver en la sección [Public Involvement and Comments](#) (Participación pública y comentarios) del sitio web. Esta sección presenta exhibiciones de proyectos, resúmenes de actividades de divulgación y respuestas a las preguntas que presentadas al equipo del proyecto en las reuniones o por medio del correo electrónico. El documento con las respuestas de todas las fuentes se actualiza mensualmente.

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From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Friday, July 28, 2023 4:38 PM
To:
Subject: Brent Spence Bridge Corridor Project Update - Design-Build Team Named

Brent Spence Bridge Corridor July 28, 2023 eNewsletter



Governors DeWine and Beshear Announce Brent Spence Bridge Design-Build Team

(COVINGTON, Kentucky) – Ohio Governor Mike DeWine and Kentucky Governor Andy Beshear announced that Walsh Kokosing has been awarded the design-build contract for the Brent Spence Bridge Corridor Project.

The governors made the announcement during a news conference in Covington, Kentucky on July 27. They were joined by officials from the Ohio Department of Transportation (ODOT), Kentucky Transportation Cabinet (KYTC), and Federal Highway Administration (FHWA).



"It's great that Kokosing, an Ohio-based company, will play a pivotal role in the Brent Spence Bridge Corridor Project," said Governor DeWine. "Both Kokosing and Walsh have a long history of working with the Ohio Department of Transportation, and I'm confident that we have the best people on both sides of the river to see this project to completion."

As prime contractor, the Walsh Kokosing team will continue to recruit participant companies and workers for the \$3.6 billion project, which is one of the most significant infrastructure megaprojects in the nation. Approximately \$3.1 billion will be spent on the design-build portion of the project. The remaining \$500 million is allocated for improvements on a two-mile stretch of the I-75 corridor north of Linn Street in Cincinnati, including a new interchange for the Western Hills Viaduct.

"Today shows what's possible when folks from different political parties, different states, and different levels of government all work together to do what's right for our people," said Governor Beshear. "With the help of this new design-build team, we are one step closer to building the companion bridge without tolls and providing the many families, communities and businesses that rely on this vital commercial link with new and exciting growth and development opportunities."

Walsh Kokosing will drive the progressive design-build process, along with AECOM, Parsons, and Jacobs, consisting of some of the most experienced bridge designers, transportation engineers, and highway engineers in the United States.

In its submission, the joint venture also identified WEB Ventures as the diversity, inclusion, and outreach consultant.

The contract addresses six of the corridor's eight total miles, including five miles of I-71/75 in Kentucky and one mile of I-75 in Ohio. It also includes improvements to the Brent Spence Bridge and the construction of a new companion bridge to its immediate west. Work on the two northernmost miles of the corridor in Ohio will be done under separate contracts.

The primary goals of the project include improving safety and traffic flow and maintaining connections to key regional and national transportation corridors.

"Working in concert with our local partners, we've heard loud and clear that they want to ensure this project is not only a benefit to traffic traversing the tri-state, but that it also has a positive impact on those who live here," said ODOT Director Jack Marchbanks. "We've freed up nearly 10 acres of developable land and remain committed to minimizing the footprint of the project while maximizing the benefits to Cincinnati and the surrounding neighborhoods."

"This is one of the largest highway construction projects ever undertaken in the United States," said KYTC Secretary Jim Gray. "We are confident that this experienced team, with their solid track record of completing projects approaching this size, possesses the talent, skills, and know-how to deliver this complex bridge and highway system."

"President Biden's Investing in America agenda is moving critically needed projects forward in Kentucky, Ohio, and all across the country that will make travel safer, move goods to market more quickly, and improve the daily lives of countless Americans," said Federal Highway Administrator Shailen Bhatt. "We are proud to invest in the success of this project and partner with Governors Beshear and DeWine as they reach a key milestone on the road to delivering the Brent Spence Bridge Corridor Project on time, on task, and on budget."

Stormwater Drainage Improvements Included as Part of Highway Projects in Both Ohio and Kentucky

ODOT and KYTC have committed to improving stormwater runoff issues as part of the highway improvements for the Brent Spence Bridge Corridor Project. During heavy rain events, stormwater flows into combined sewer and stormwater systems throughout the region, causing untreated wastewater to flow into creeks and streams. As local water utilities work to address these issues systemically, ODOT and KYTC are working to ensure any highway construction projects address rainwater runoff and prevent it from contributing to the problem.

ODOT has been working with the Metropolitan Sewer District of Greater Cincinnati to address highway water runoff issues since 2011.

"Fixing stormwater drainage issues to reduce overflows into creeks and streams has been a commitment of ours on all I-75 projects for over a decade, and the Brent Spence project is no different," said Stefan Spinosa, Capital Programs Administrator for ODOT, District 8. "Our plans call for the separation of stormwater from the combined sewer overflow system as part of this project along I-75 to help prevent overflows into Mill Creek."

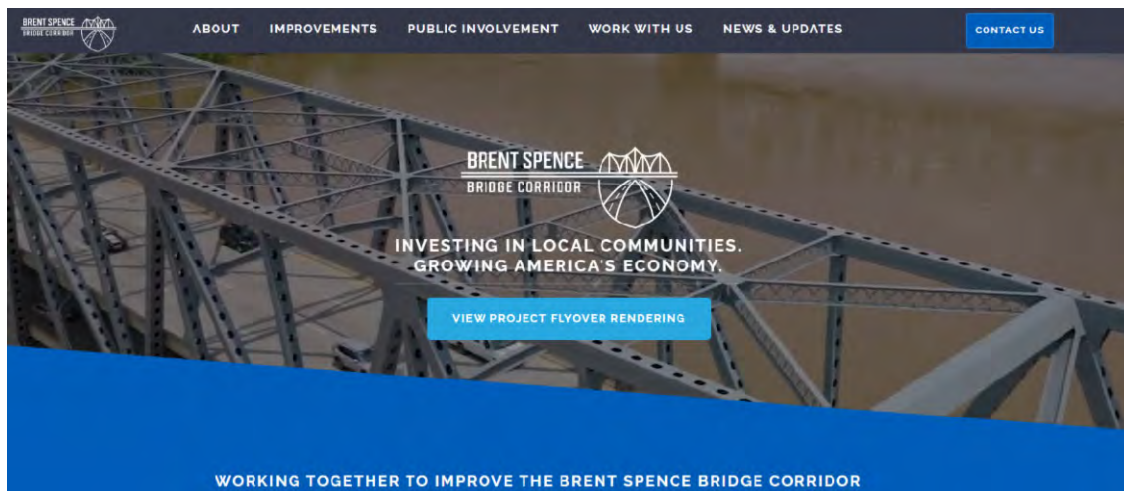
In Kentucky, KYTC is working with the City of Covington and Sanitation District No. 1 to construct a new interstate storm sewer system to remove stormwater runoff from the Willow Run combined sewer system. In addition, KYTC will upsize a portion of the combined sewer system that is impacted by the project. These commitments should reduce flooding in the Euclid Avenue area, as well as Goebel Park.

"Overall, we'll be removing 467 acres of drainage area from the Willow Run sewer in the Euclid Avenue and West 9th Street basins," said Stacey Hans, KYTC project manager. "Based on modeling we've done, this should greatly reduce overflow and flooding events for this entire region."

Brent Spence Bridge Corridor Website Design Updated to Prepare for Next Phase of Project

If you've visited the Brent Spence Bridge Corridor website recently, you may have noticed a new look at the top of the site. The project team recently updated its navigation system to make information and resources easier to find. At the same time, the team created several new pages to help move the website into the next phase as the project moves toward construction. For example, pages like Traffic Updates under the News & Updates header will be where the public can find real-time updates on roadwork, detours, and more, while the pages under the Work With Us section will help people find out how to become part of the project workforce.

As always, the project team is interested in your feedback. Let us know what you think of the changes by visiting the project [website](#) and click on the "Contact Us" button in the upper right-hand corner.



SHARE YOUR THOUGHTS AND IDEAS

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Boletín electrónico del corredor del puente Brent Spence, 28 de julio de 2023



Los gobernadores DeWine y Beshear anuncian el equipo de diseño y construcción del puente Brent Spence

(COVINGTON, Kentucky) – El gobernador de Ohio, Mike DeWine, y el gobernador de Kentucky, Andy Beshear, anunciaron que se le adjudicó a Walsh Kokosing el contrato de diseño y construcción para el proyecto del corredor del puente Brent Spence.

Los gobernadores hicieron el anuncio durante una conferencia de prensa en Covington, Kentucky, el 27 de julio. A ellos se unieron funcionarios del Departamento de Transporte de Ohio (ODOT), el Gabinete de Transporte de Kentucky (KYTC) y la Administración Federal de Carreteras (FHWA).



"Es fantástico que Kokosing, una empresa con sede en Ohio, desempeñe un papel fundamental en el proyecto del corredor del puente Brent Spence," dijo el gobernador DeWine. "Tanto Kokosing como Walsh tienen una larga trayectoria de trabajo con el Departamento de Transporte de Ohio y estoy seguro de que contamos con las mejores personas en ambos lados del río para completar este proyecto."

Como contratista principal, el equipo de Walsh Kokosing seguirá reclutando empresas participantes y trabajadores para el proyecto de \$3.6 mil millones, que es uno de los megaproyectos de infraestructura más importantes del país. Se gastarán aproximadamente \$3.1 mil millones en la parte de diseño y construcción del proyecto. Los \$500 millones

restantes se asignan para mejoras en un tramo de dos millas del corredor I-75 al norte de Linn Street en Cincinnati, incluido un nuevo intercambio para el viaducto Western Hills.

"Esto muestra lo que es posible cuando personas de diferentes partidos políticos, diferentes estados y diferentes niveles de gobierno trabajan juntos para hacer lo correcto para nuestra gente," dijo el gobernador Beshear. "Con la ayuda de este nuevo equipo de diseño y construcción, estamos un paso más cerca de construir el puente complementario sin peajes y brindar a las muchas familias, comunidades y empresas que dependen de este vínculo comercial vital nuevas y emocionantes oportunidades de crecimiento y desarrollo."

Walsh Kokosing impulsará el proceso progresivo de diseño y construcción, junto con AECOM, Parsons y Jacobs, que está formado por algunos de los diseñadores de puentes, ingenieros de transporte e ingenieros de carreteras más experimentados de los Estados Unidos.

En su presentación, la empresa conjunta también identificó a WEB Ventures como el consultor de diversidad, inclusión y extensión.

El contrato abarca seis de las ocho millas totales del corredor, incluidas cinco millas de la I-71/75 en Kentucky y una milla de la I-75 en Ohio. También incluye mejoras al puente Brent Spence y la construcción de un nuevo puente complementario hacia el oeste inmediato. El trabajo en las dos millas más al norte del corredor en Ohio se realizará en virtud de contratos separados.

Los objetivos principales del proyecto incluyen mejorar la seguridad y el flujo de tráfico y mantener las conexiones con corredores de transporte regionales y nacionales clave.

"Al trabajar en conjunto con nuestros socios locales, sabemos que lo que quieren es garantizar que este proyecto no solo sea un beneficio para el tráfico que atraviesa los tres estados, sino que también tenga un impacto positivo en quienes viven aquí," dijo el director de ODOT, Jack Marchbanks. "Liberamos casi 10 acres de terreno urbanizable y seguimos comprometidos a minimizar la huella del proyecto; al mismo tiempo, esperamos maximizar los beneficios para Cincinnati y las colonias circundantes."

"Este es uno de los proyectos de construcción de carreteras más grandes jamás emprendidos en los Estados Unidos," dijo el secretario de KYTC, Jim Gray. "Estamos seguros de que este equipo experimentado, con su sólida trayectoria en la realización de proyectos de este tamaño, posee el talento, las habilidades y los conocimientos para ejecutar este complejo sistema de puentes y carreteras."

"La agenda del presidente Biden en la Inversión en Estados Unidos impulsa proyectos muy necesarios en Kentucky, Ohio y en todo el país; estos harán que los viajes sean más seguros, trasladarán los productos al mercado más rápidamente y mejorarán la vida diaria de muchos estadounidenses," dijo el administrador federal de Carreteras, Shailen Bhatt. "Estamos orgullosos de invertir en el éxito de este proyecto y asociarnos con los gobernadores Beshear y DeWine mientras alcanzan un hito clave en el camino hacia la entrega del proyecto del corredor del puente Brent Spence a tiempo, cumpliendo con la tarea y dentro del presupuesto."

Mejoras en el drenaje de aguas pluviales se incluyen como parte de proyectos de carreteras, tanto en Ohio como en Kentucky

El ODOT y el KYTC se comprometieron a mejorar los problemas de escorrentía de aguas pluviales como parte de las mejoras en las carreteras para el proyecto del corredor del puente Brent Spence. Durante los eventos de lluvias intensas, las aguas pluviales fluyen hacia los sistemas combinados de alcantarillado y aguas pluviales en toda la región, lo que hace que las aguas residuales no tratadas fluyan hacia los arroyos. Mientras las empresas de agua locales trabajan para abordar estos problemas de manera sistémica, ODOT y KYTC trabajan para garantizar que cualquier proyecto de construcción de carreteras aborde la escorrentía de agua de lluvia y evite que contribuya al problema.

El ODOT trabaja con el Distrito Metropolitano de Alcantarillado del Área Metropolitana de Cincinnati para abordar los problemas de escorrentía de agua en las carreteras desde 2011.

"Resolver los problemas de drenaje de aguas pluviales para reducir los desbordamientos de los arroyos ha sido nuestro compromiso en todos los proyectos de la I-75 durante más de una década; lo mismo ocurre con el proyecto Brent Spence," dijo Stefan Spinosa, administrador de Programas Captales del ODOT, Distrito 8. "Nuestros planes exigen la separación de las aguas pluviales del sistema combinado de desbordamiento del alcantarillado como parte de este proyecto a lo largo de la I-75 para ayudar a prevenir desbordamientos hacia Mill Creek."

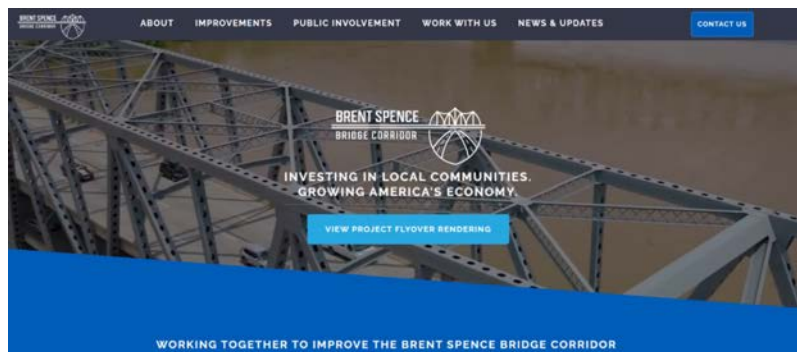
En Kentucky, el KYTC trabaja con la ciudad de Covington y el Distrito Sanitario nro. 1 para construir un nuevo sistema de alcantarillado pluvial interestatal para eliminar la escorrentía de aguas pluviales del sistema de alcantarillado combinado Willow Run. Además, el KYTC ampliará una parte del sistema de alcantarillado combinado afectado por el proyecto. Estos compromisos deberían reducir las inundaciones en el área de Euclid Avenue, así como en Goebel Park.

"En general, eliminaremos 467 acres de área de drenaje de la alcantarilla Willow Run en las cuencas de Euclid Avenue y West 9th Street," dijo Stacey Hans, gerente de proyecto de KYTC. "Según los modelos que realizamos, esto debería reducir en gran medida los eventos de desbordamiento e inundaciones en toda esta región."

Diseño del sitio web del corredor del puente Brent Spence actualizado para prepararse para la próxima fase del proyecto

Si visitó el sitio web del corredor del puente Brent Spence recientemente, es posible que haya notado una nueva apariencia en la parte superior del sitio. El equipo del proyecto actualizó recientemente su sistema de navegación para facilitar la búsqueda de información y recursos. Al mismo tiempo, el equipo creó varias páginas nuevas para ayudar a que el sitio web pase a la siguiente fase a medida que el proyecto avanza hacia la construcción. Por ejemplo, páginas como Traffic Updates (Actualizaciones de tráfico) bajo el encabezado News & Updates (Noticias y actualizaciones) serán donde el público podrá encontrar actualizaciones en tiempo real sobre obras viales, desvíos y más, mientras que las páginas bajo la sección Work With Us (Trabaja con nosotros) ayudarán a las personas a descubrir cómo sumarse a la fuerza laboral del proyecto.

Como siempre, el equipo del proyecto está interesado en sus comentarios. Háganos saber lo que piensa de los cambios en el [sitio web](#) del proyecto y al hacer clic en el botón "Contact Us" (Contáctenos) en la esquina superior derecha.



COMPARTA SUS PENSAMIENTOS E IDEAS

El equipo del proyecto del corredor del puente Brent Spence agradece los comentarios y opiniones del público. Para enviar una pregunta o comentario, visite el [sitio web](#) del proyecto y haga clic en el botón "Contact Us" (Contáctenos) en la esquina superior derecha.

Las respuestas a todos los comentarios públicos se pueden ver en la sección [Public Involvement and Comments](#) (Participación pública y comentarios) del sitio web. Esta sección presenta exhibiciones de proyectos, resúmenes de actividades de divulgación y respuestas a las preguntas que presentadas al equipo del proyecto en las reuniones o por medio del correo electrónico. El documento con las respuestas de todas las fuentes se actualiza mensualmente.

Manténgase informado

Hay varias maneras de mantenerse informado acerca del proyecto del corredor del puente Brent Spence.

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From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Thursday, August 31, 2023 10:12 AM
To:
Subject: Brent Spence Bridge Corridor Project Update - Open Houses Held

Brent Spence Bridge Corridor August 31, 2023 eNewsletter



Open House Meetings Offer Update on Project Status

More than 300 people attended two open-house meetings conducted by the Brent Spence Bridge Corridor Project team this month to hear an update on the status of the \$3.6 billion project.



Conducted at the Radisson Hotel in Covington and Longworth Hall in Cincinnati on August 23 and August 24, the open houses offered visitors an opportunity to browse exhibits, review project information, and talk one-on-one with project team members.

Maps, posters, renderings, and a video provided information on a variety of topics, including a brief history of the project, which dates to 2004, when the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) formally began studying ways to improve 7.8 miles of I-71 and I-75.

Open house visitors also learned about the project's innovation process. During this 60-day period, the team designing and building the project will identify and evaluate ways to refine or change the project's basic configuration to meet established goals and objectives, or to reduce costs, impacts, and schedule. This process includes an evaluation of ideas submitted by individuals, organizations, and local agencies. *(For more information, please see article below.)*

The open house meetings also provided visitors with an update on the project's environmental process. Environmental approval for the project was issued in 2012 and re-evaluated in 2015 and 2018. Since 2018, regulations have changed and refinements to the project's design have been made. As a result, in 2022 KYTC and ODOT agreed with the Federal Highway Administration to prepare an enhanced re-evaluation known as a Supplemental Environmental Assessment.

Publication of this supplemental environmental assessment is expected to be available for public review later this year, and public hearings will be conducted in Kentucky and Ohio. The

Federal Highway Administration's final decision on the supplemental environmental assessment is expected to be announced early next year.

The open houses were a part of ODOT's and KYTC's continuing efforts to work with community members to assure their voices are heard and their needs are addressed in the project plan.

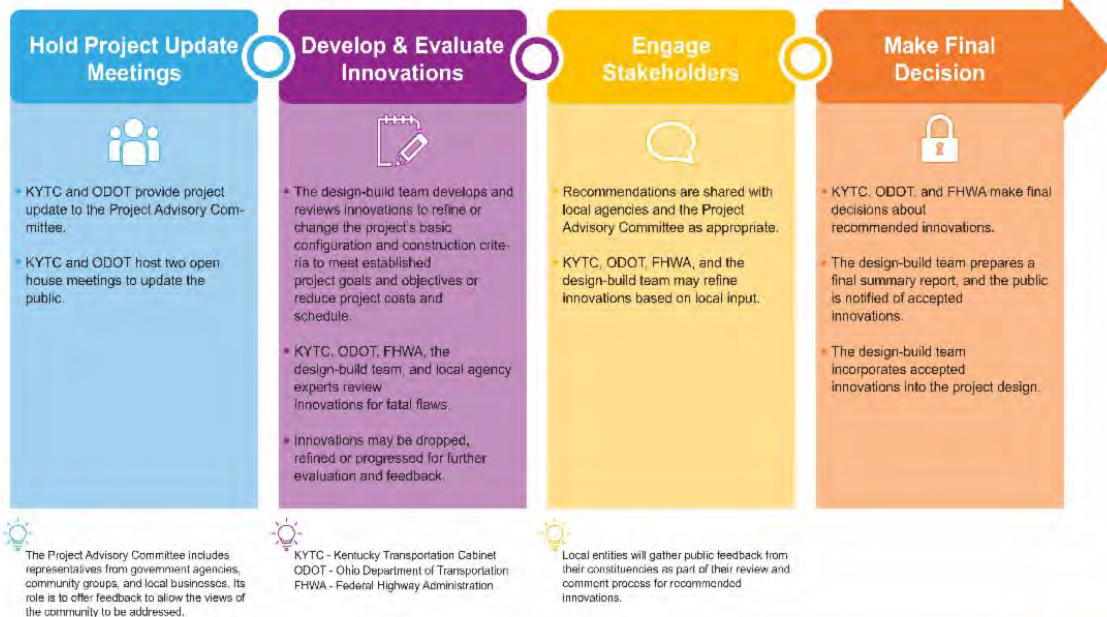


Innovation Period for Brent Spence Bridge Corridor Project to Begin September 2023

Refinements to the proposed design of the Brent Spence Bridge Corridor (BSBC) Project will be evaluated during a 60-day innovation period, which is expected to begin next month.

This innovation period will give the [Walsh Kokosing](#) design-build team and the bi-state management team — comprised of representatives from the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) — the opportunity to explore refinements that could reduce project costs, shorten the project schedule, provide some other sort of benefit or value, or help with project delivery. Alternative technical concepts that are deemed feasible (i.e., able to be built within the project's footprint, timeline, and budget) and add value to the project will be incorporated into the project's design, while those that do not meet those criteria will be rejected.

Progressive Design-Build Innovation Process



Brent Spence Bridge Corridor Project



“We’re not looking at whole new alternatives with this process,” ODOT Project Manager Tommy Arnold said. “We’re not looking to move I-75 and buy more right-of-way. We’re generally looking at tweaks.”

Engineers from municipalities within the project area, such as the City of Cincinnati and the City of Covington, will also be included in the evaluation process to give feedback on concepts affecting their locales.

The innovation period will commence shortly after the Walsh Kokosing design-build team signs the contract for the project, which is likely to occur early next month, Arnold said.

In addition to brainstorming and evaluating ideas of their own, Walsh Kokosing and the bi-state management team will be examining comments and proposed design concepts submitted by other parties, such as private citizens and local advocacy groups. Comments from the project advisory committee and those received during the two public open houses held earlier this month may also be evaluated.

“We’ll need to move through that innovation process in the time allotted in order to keep the project moving forward and avoid delays,” Arnold said.

Reports requested via resolution by the City of Cincinnati and the Hamilton County Commissioners on the possibility of returning more developable land are expected to be published shortly after the conclusion of the innovation period.

After the innovation period ends, Walsh Kokosing will “switch into production mode” and work to get all accepted changes from the innovation period incorporated into the project design and generate cost estimates, Arnold said.

Diversity & Inclusion Committee Meets Brent Spence Bridge Corridor Project's DBT Diversity & Inclusion Manager

More than 50 community representatives convened for the recent meeting of the Brent Spence Bridge Corridor (BSBC) Diversity & Inclusion Committee, held August 22 at the Ohio Kentucky Indiana (OKI) Regional Council office in downtown Cincinnati.



BSBC Project Manager Tommy Arnold welcomed the group, provided an update on project progress and next steps. He then introduced the project's recently-selected design-build partner, [Walsh Kokosing](#), before sharing an overview of the goals behind the project's outreach efforts for Disadvantaged Business Enterprises (DBE) and workforce development.

"The estimated total cost for the Brent Spence Bridge Corridor Project is larger than ODOT's annual capital budget," said Arnold. "The DBE goal for Phase 1 of this project is nine percent of approximately \$250 million. We can't reach that goal alone. We need this committee's help to make sure businesses and individuals here in our hometown know how to take advantage of this unprecedented opportunity."

Icy Williams, Walsh Kokosing DBT Diversity & Inclusion manager for the project, took the podium next for a presentation on the project's Diversity, Inclusion and Outreach plan (DIOP). Slated to be complete in September, the plan includes components for goal attainment and tracking, outreach, on-the-job training, workforce development, and community engagement. Williams asked the committee for ideas and assistance to reach eligible members of the

Greater Cincinnati community and opened the floor, creating an opportunity for what became a lively discussion around outreach ideas.

In addition to the D&I committee, the Project Advisory Committee (PAC) convened in August.

To learn more about the BSBC project's DBE efforts, opportunities, and requirements, visit [here](#) and [here](#). For more about the PAC, click [here](#).



SHARE YOUR THOUGHTS AND IDEAS

The Brent Spence Bridge Corridor Project team welcomes comments and feedback from the public. To submit a question or comment, visit the project [website](#) and click on the “Contact Us” button in the upper right-hand corner.

Responses to all public comments can be viewed on the [Public Involvement and Comments](#) section of the website. This section features project exhibits, summaries of outreach activities, and responses to questions posed to the project team during meetings or via email. The document with the responses from all sources is updated monthly.

Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

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Boletín informativo electrónico del 31 de agosto de 2023 sobre el corredor de Brent Spence Bridge



**INVIRTIENDO EN COMUNIDADES LOCALES.
HACIENDO CRECER LA ECONOMÍA DE ESTADOS UNIDOS.**

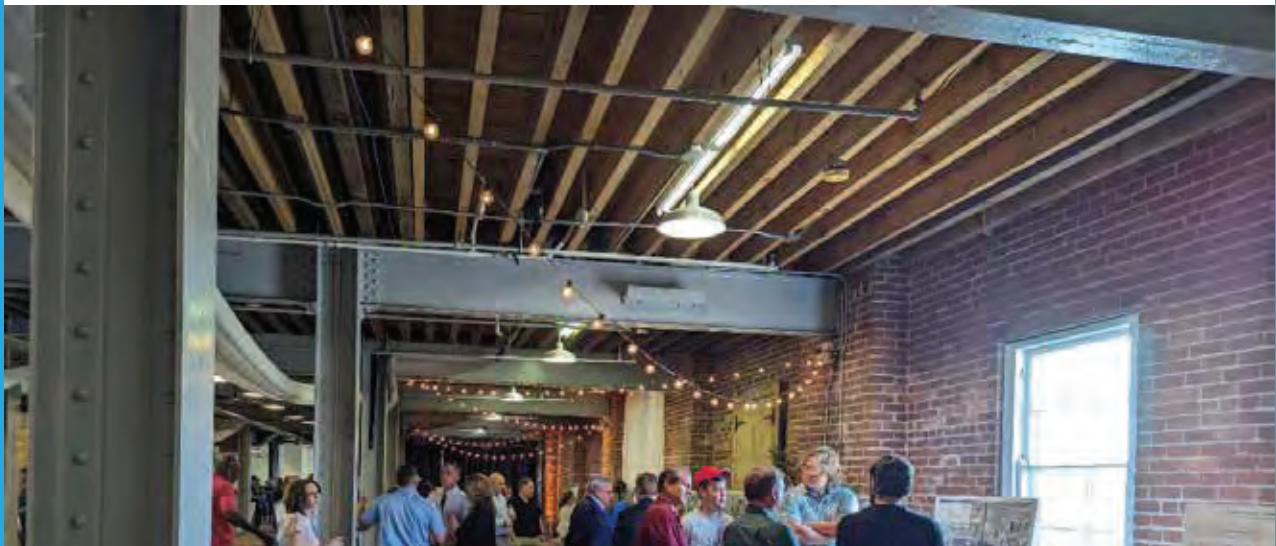


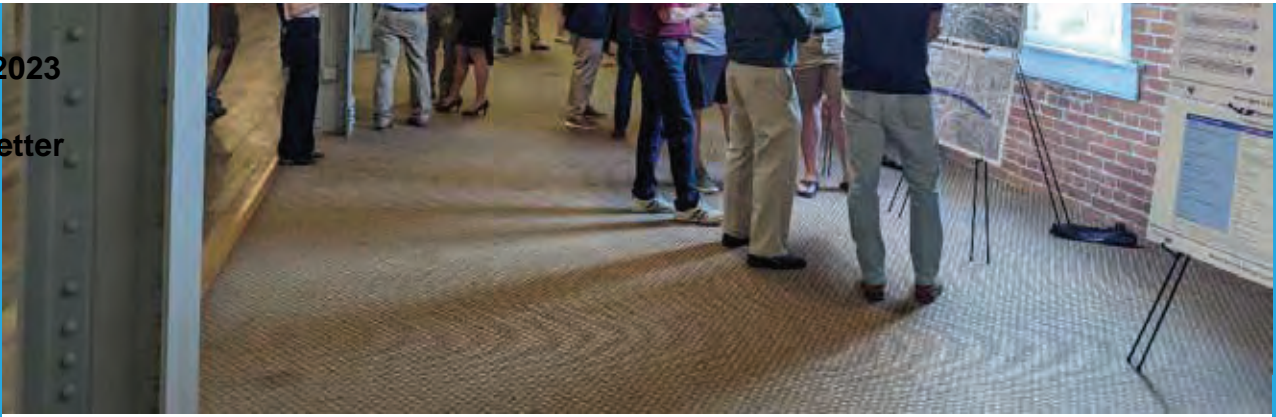
BrentSpenceBridgeCorridor.com



Reuniones de puertas abiertas ofrecen actualización sobre el estado del proyecto

Más de 300 personas asistieron a dos reuniones de puertas abiertas organizadas por el equipo del Proyecto del corredor de Brent Spence Bridge este mes para recibir una actualización del estado del proyecto de \$3.6 mil millones.





Las reuniones de puertas abiertas, realizadas en el Radisson Hotel en Covington y el Ayuntamiento de Longworth en Cincinnati el 23 y el 24 de agosto, ofrecieron a los visitantes la oportunidad de navegar por las exhibiciones, revisar información del proyecto y hablar de manera individual con los miembros del equipo del proyecto.

Con los mapas, los afiches, las representaciones y un video, se brindó información sobre una variedad de temas, incluida una historia breve del proyecto, que se remonta al año 2004, cuando el Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC) empezaron formalmente a investigar maneras de mejorar 7.8 millas de la I-71 y la I-75.

Los asistentes a la reunión de puertas abiertas también aprendieron sobre el proceso de innovación del proyecto. Durante este período de 60 días, el equipo que diseñó y creó el proyecto identificará y evaluará formas de perfeccionar o cambiar la configuración básica del proyecto para cumplir con las metas y los objetivos establecidos, o para reducir costos, impactos y horarios. En este proceso, se incluye una evaluación de las ideas presentadas por las personas, las organizaciones y las agencias locales. *(Para obtener más información, consulte el siguiente artículo.)*

Durante las reuniones de puertas abiertas, los visitantes también recibieron una actualización sobre el proceso ambiental del proyecto. La aprobación ambiental del proyecto se emitió en 2012 y se volvió a evaluar en 2015 y 2018. Desde 2018, las regulaciones han cambiado, y se han realizado perfeccionamientos en el diseño del proyecto. Como resultado, en 2022, el KYTC y el ODOT aceptaron, junto con la Administración Federal de Autopistas, preparar una reevaluación mejorada conocida como Evaluación ambiental complementaria.

Se prevé que la publicación de esta evaluación ambiental complementaria estará disponible para revisión pública a finales de este año, y se realizarán audiencias públicas en Kentucky y Ohio. Se espera que la decisión final de la Administración Federal de Autopistas sobre la evaluación ambiental complementaria se anuncie a principios del próximo año.

Las reuniones de puertas abiertas formaron parte de los esfuerzos continuos del ODOT y del KYTC por trabajar con los miembros de la comunidad para asegurarse de que sus voces sean escuchadas y sus necesidades sean abordadas en el plan del proyecto.

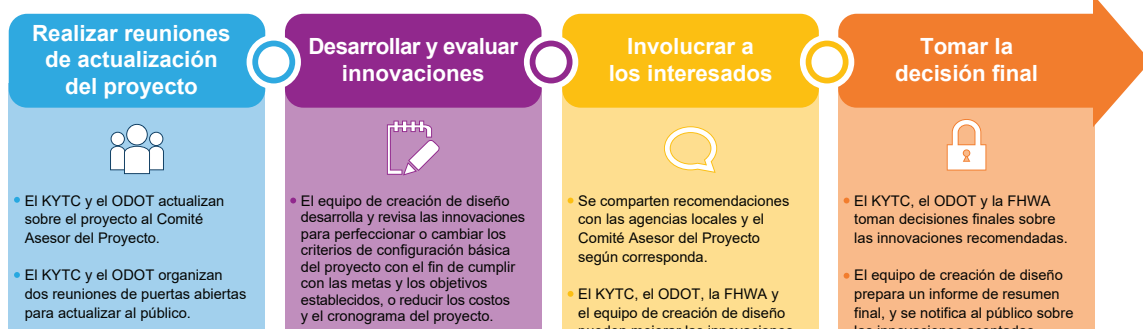


El período de innovación para el proyecto del corredor de Brent Spence Bridge empezará en septiembre de 2023

Se evaluarán las mejoras del diseño propuesto para el Proyecto del corredor de Brent Spence Bridge (BSBC) durante un período de innovación de 60 días, cuyo inicio se anticipa para el próximo mes.

Este período de innovación dará al equipo de creación de diseño de [Walsh Kokosing](#) y al equipo de gestión biestatal, conformado por representantes del Departamento de Transporte de Ohio (ODOT) y del Gabinete de Transporte de Kentucky (KYTC), la oportunidad de explorar mejoras que puedan reducir los costos del proyecto, acortar el cronograma del proyecto, brindar algún otro tipo de beneficio o valor, o ayudar con la entrega del proyecto. Los conceptos técnicos alternativos que se consideran factibles (por ejemplo, que pueden construirse dentro de la huella, el plazo y el presupuesto del proyecto) y agregan valor al proyecto se incorporarán al diseño del proyecto, mientras que aquellos que no cumplen con esos criterios serán rechazados.

Proceso progresivo de innovación en la creación del proyecto



• El KYTC, el ODOT, la FHWA, el equipo de creación de diseño y los expertos de las agencias locales revisan las innovaciones en busca de fallas fatales.

• Las innovaciones pueden ser descartadas o mejoradas o pueden continuarse con más evaluaciones y comentarios.

pueden mejorar las innovaciones según los aportes locales.

las innovaciones aceptadas.

• El equipo de creación de diseño incorpora las innovaciones aceptadas en el diseño del proyecto.



En el Comité Asesor del Proyecto, se incluyen representantes de agencias gubernamentales, grupos comunitarios y negocios locales. Su función es ofrecer retroalimentación para permitir abordar las opiniones de la comunidad.



KYTC: Gabinete de Transporte de Kentucky
ODOT: Departamento de Transporte de Ohio
FHWA: Administración Federal de Autopistas



Las entidades locales recopilarán los comentarios públicos de sus circunscripciones como parte de su revisión y comentarán sobre el proceso de las innovaciones recomendadas.



Proyecto del corredor de Brent Spence Bridge



KYTC ítem n.º 6-17 | ODOT PID 89068

“No estamos observando alternativas completamente nuevas con este proceso”, comenta el Director de Proyecto del ODOT, Tommy Arnold. “No buscamos mover la I-75 y comprar más derecho de paso. En general, buscamos modificaciones.”

Los ingenieros de las municipalidades dentro del área del proyecto, como la Ciudad de Cincinnati y la Ciudad de Covington, también participarán en el proceso de evaluación para brindar comentarios sobre conceptos que afectan a sus zonas.

El período de innovación empezará poco después de que el equipo de creación de diseño de Walsh Kokosing firme el contrato del proyecto, lo que probablemente ocurra a principios del próximo mes, afirma Arnold.

Además de hacer una lluvia de ideas y evaluar ideas por su cuenta, Walsh Kokosing y el equipo de gestión biestatal examinarán los comentarios y los conceptos de diseño propuestos que otras partes presentaron, como ciudadanos privados y grupos de defensa locales. Asimismo, se evaluarán los comentarios del comité asesor del proyecto y aquellos recibidos durante las dos reuniones públicas de puertas abiertas llevadas a cabo a principios de este mes.

“Necesitaremos atravesar ese proceso de innovación en el tiempo asignado para poder mantener el proyecto a flote y evitar retrasos”, sostiene Arnold.

Se espera que los informes solicitados mediante resolución de la Ciudad de Cincinnati y los Comisionados del Condado de Hamilton sobre la posibilidad de devolver más suelo urbanizable se publiquen poco después de la conclusión del período de innovación.

Tras el fin del período de innovación, Walsh Kokosing "cambiará al modo de producción" y trabajará para lograr que todos los cambios aceptados del período de innovación se incorporen al diseño del proyecto y generen cálculos de costos, menciona Arnold.

El Comité de Diversidad e Inclusión conoce al director de Diversidad e Inclusión del DBT del Proyecto del corredor de Brent Spence Bridge

Más de 50 representantes de la comunidad fueron convocados para la reunión reciente del Comité de Diversidad e Inclusión del corredor de Brent Spence Bridge (BSBC), realizada el 22 de agosto en la oficina del Consejo Regional de Ohio-Kentucky-Indiana (OKI) en el centro de la ciudad de Cincinnati.



El director de proyecto del BSBC, Tommy Arnold, dio la bienvenida al grupo y brindó una actualización sobre el progreso del proyecto y los próximos pasos. Luego, presentó al socio de creación de diseño del proyecto, que fue seleccionado recientemente, [Walsh Kokosing](#), antes de compartir una descripción general de los objetivos detrás de los esfuerzos del proyecto por llegar a las empresas comerciales en desventaja (DBE) y el desarrollo de la fuerza laboral.

“El costo total estimado del Proyecto del corredor de Brent Spence Bridge es mayor que el presupuesto de capital anual del ODOT”, comenta Arnold. “El objetivo de las DBE para la fase 1 de este proyecto es nueve por ciento de aproximadamente \$250 millones. No podemos alcanzar ese objetivo solos. Necesitamos la ayuda de este comité para asegurarnos de que los negocios y las personas de nuestra ciudad sepan cómo aprovechar esta oportunidad sin precedentes.”

Luego, Icy Williams, directora de Diversidad e Inclusión del DBT de Walsh Kokosing para el proyecto, subió al podio para presentar el Plan de diversidad, inclusión y alcance del proyecto (DIOP). El plan, cuya finalización está programada para septiembre, incluye componentes de logro y seguimiento de objetivos, alcance, capacitación en el trabajo, desarrollo de la fuerza laboral y participación comunitaria. Williams pidió ideas y asistencia al comité para llegar a los miembros elegibles de la comunidad general de Cincinnati y dio paso a un debate, creando la oportunidad de conversar de forma animada sobre las ideas de alcance.

Además del comité de D&I, el Comité Asesor del Proyecto (PAC) fue convocado en agosto.

Para conocer más acerca de los esfuerzos, las oportunidades y los requisitos de las DBE del proyecto BSBC, haga clic [aquí](#) y [aquí](#). Para conocer más acerca del PAC, haga clic [aquí](#).



COMPARTA SUS PENSAMIENTOS E IDEAS

El equipo del Proyecto del corredor de Brent Spence Bridge acoge los comentarios y la retroalimentación del público. Para enviar una pregunta o comentario, visite el [sitio web](#) del proyecto y haga clic en el botón "Contact Us" (Contacto) ubicado en la esquina superior derecha.

Puede visualizar las respuestas a todos los comentarios públicos en la sección [Participación y comentarios del público](#) del sitio web. En esta sección, se presentan exhibiciones del proyecto, resúmenes de las actividades de alcance y respuestas a las preguntas planteadas al equipo del proyecto durante las reuniones o por correo electrónico. El documento con las respuestas de todas las fuentes se actualiza cada mes.

Mantenerse conectado

Existen muchas formas de mantenerse conectado con el Proyecto del corredor de Brent Spence Bridge.

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Envíe un comentario

From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Tuesday, October 3, 2023 10:08 AM
To:
Subject: Brent Spence Bridge Corridor Project Update - Construction Careers Available

Brent Spence Bridge Corridor October 3, 2023 eNewsletter



\$3.6 BILLION INVESTMENT EQUALS UNPRECEDENTED OPPORTUNITY

The Brent Spence Bridge Corridor Project is poised to transform more than just the landscape of Greater Cincinnati and Northern Kentucky. This \$3.6 billion infrastructure investment creates significant opportunities for individuals considering careers in the engineering and construction industries.

Take it from Justin Phillips, business manager and secretary treasurer for Local 265, Laborers' International Union of North America. "We're on the verge of a once-in-a-lifetime opportunity to recruit a whole new generation of construction workers and sustain a workforce that is vital to our region's continued growth," Phillips says. "That's why we at Local 265 have ramped up our efforts to attract members in preparation of the Brent Spence Bridge Corridor Project."

Now is the perfect time to consider a career in these fields. Construction professions offer competitive wages, comprehensive benefits such as health insurance, and a wide range of paths to suit individuals with diverse educational backgrounds and experience levels. From those just entering the workforce to those seeking a fresh start or a midcareer transition, there is a construction option tailored to your aspirations.

And at the Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT), you'll find [scholarship opportunities](#) in engineering and construction

management, as well as information on apprenticeships. In fact, KYTC just [celebrated](#) its 75th year of its Civil Engineering Scholarship Program.

To learn more about how you can start a new path in the construction field and become a driving force in the future of our region, visit the project website's Explore [Construction Careers](#) page.

PROJECT WILL ENHANCE MULTI-MODAL CONNECTIONS THROUGHOUT THE CORRIDOR

Maps below show various improvements to bike and pedestrian connections throughout the corridor which will improve safety and enhance connectivity across the I-71/75 highway. More detailed maps of the multi-modal connections are available [here](#) on the project website.



In Ohio, the Brent Spence Bridge Corridor Project will incorporate new and improved shared use paths, sidewalks, and bike paths as shown above, including bicycle and pedestrian infrastructure on connections across I-75, to better connect communities and improve access to bus services and community amenities.



In Kentucky, the Brent Spence Bridge Corridor Project will incorporate new and improved shared use paths, sidewalks, and bike paths from the Covington business district to the western communities across I-75, as well as through Goebel Park and Kenney Shields Park to better connect neighborhoods and improve access to bus services and community amenities.

VIEW OPEN HOUSE SUMMARY ONLINE NOW

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) held open house-style project update meetings for the Brent Spence Bridge Corridor (BSBC) Project in August 2023. More than 275 residents attended the two gatherings, held at the Radisson Hotel in Covington and Longworth Hall in Cincinnati.

[View the open house meetings summary.](#)

For more information about the open house meetings or to view public comments received and answered by the project team, visit the [Public Involvement and Comments page](#) of the website.



WALSH KOKOSING PROGRESSIVE DESIGN-BUILD TEAM RECEIVES NOTICE TO PROCEED

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) informed the Walsh Kokosing team that the contracting process has been completed and work may officially kick off for the progressive design-build process. This method offers the most opportunity for innovation around the project approach to reduce impacts and improve overall community benefits while meeting the original transportation needs. The next steps include an innovation period, which will allow the team to evaluate potential refinements to the project that could further improve it with respect to reduced costs, a shorter schedule, or other value to the community.

GET TO KNOW THE BSBC PROJECT TEAM

Someone on the project team has spent 20 years with ODOT, touching projects across the state throughout his career.

Another person has always been drawn to engineering, and now is excited to work on one of the largest infrastructure projects in the country.

Someone else is a self-proclaimed "big bridge guy," who has been lobbying to work on this project for years.

Still another brings an environmental background to this transformative project and is committed to delivering a project that's environmentally sensitive.

Get to know the people behind the project in an upcoming Meet-the-Team series. Full profiles will be on the project website, social media, and in future editions of this newsletter in the coming weeks.

SHARE YOUR THOUGHTS AND IDEAS

The Brent Spence Bridge Corridor Project team welcomes comments and feedback from the public. To submit a question or comment, visit the project [website](#) and click on the “Contact Us” button in the upper right-hand corner. You can also contact the Design-Build team directly about job opportunities at WalshKokosingBrentSpence@walshgroup.com, or visit their [website](#).

Responses to all public comments can be viewed on the [Public Involvement and Comments](#) section of the website. This section features project exhibits, summaries of outreach activities, and responses to comments submitted to the project team.

Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

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Boletín informativo electrónico del 3 de octubre de 2023 sobre el corredor de Brent Spence Bridge



**INVIRTIENDO EN COMUNIDADES LOCALES.
HACIENDO CRECER LA ECONOMÍA DE ESTADOS UNIDOS.**



BrentSpenceBridgeCorridor.com



UNA INVERSIÓN DE \$3.6 MIL MILLONES EQUIVALE A UNA OPORTUNIDAD SIN PRECEDENTES

El Proyecto del corredor de Brent Spence Bridge está listo para transformar mucho más que tan sólo el panorama de Gran Cincinnati y del norte de Kentucky. Esta inversión de \$3.6 mil millones en infraestructura genera oportunidades significativas para las personas que consideran seguir carreras en las industrias de la ingeniería y la construcción.

Creemos en las palabras de Justin Phillips, gerente de negocios y secretario-tesorero de Local 265, Sindicato Internacional de Obreros de Norteamérica. "Estamos a punto de alcanzar una oportunidad única en la vida para reclutar a toda una nueva generación de obreros de la construcción y de mantener a una fuerza laboral que es vital para el crecimiento continuo de nuestra región", comenta Phillips. "Es por ello que en Local 265 hemos redoblado esfuerzos para atraer a miembros durante nuestra preparación para el Proyecto del corredor de Brent Spence Bridge".

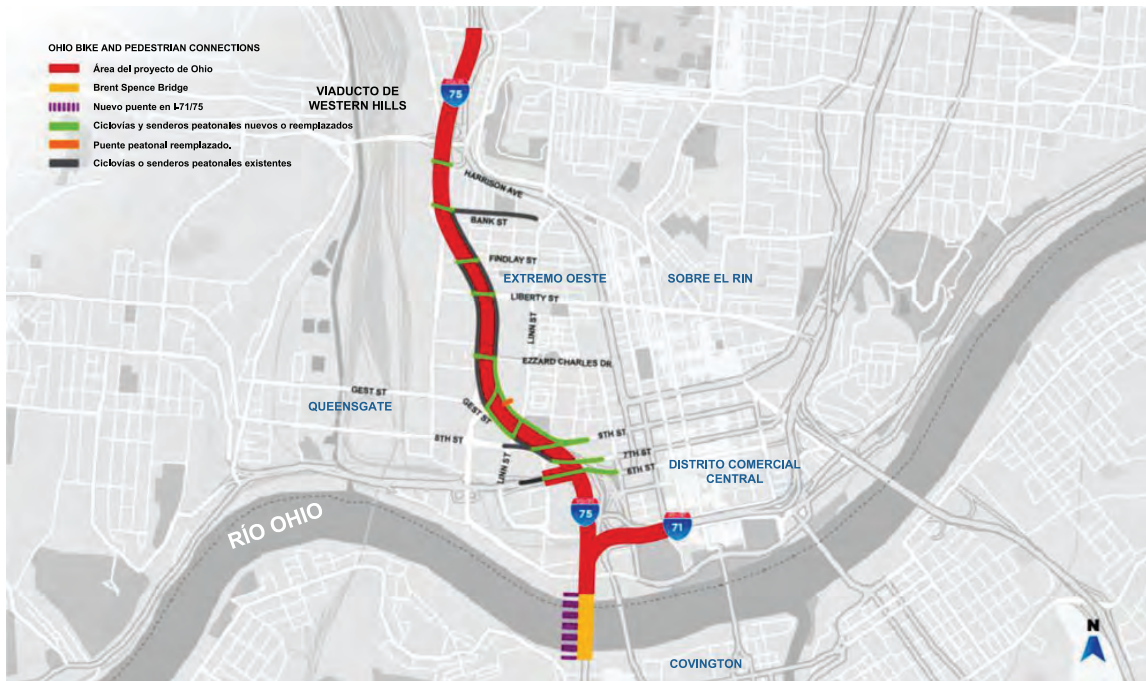
Ahora es el momento perfecto para considerar una carrera en estos campos. Las profesiones relacionadas con la construcción ofrecen salarios competitivos, beneficios integrales como seguro de salud, y una amplia gama de caminos que se ajustan a personas con antecedentes educativos y niveles de experiencia diversos. Desde aquellos que acaban de incorporarse a la fuerza laboral hasta aquellos que buscan un nuevo comienzo o una transición a mitad de carrera, hay una opción de construcción que se ajusta a sus aspiraciones.

Y en el Gabinete de Transporte de Kentucky (KYTC) y el Departamento de Transporte de Ohio (ODOT), encontrará [oportunidades de beca](#) en ingeniería y administración de construcciones, así como información sobre formación. De hecho, el KYTC acaba de [celebrar el año](#) 75 de su Programa de becas de ingeniería civil.

Para conocer más acerca de cómo puede empezar un nuevo camino en el campo de la construcción y convertirse en una fuerza motriz en el futuro de nuestra región, visite la página [Carreras de construcción](#) en el sitio web del proyecto.

EL PROYECTO PERMITIRÁ MEJORAR LAS CONEXIONES MULTIMODALES EN TODO EL CORREDOR

Los siguientes mapas muestran varias mejoras en las conexiones para ciclistas y peatones a lo largo del corredor, lo que mejorará la seguridad y fortalecerá la conectividad en toda la autopista I-71/75. Puede encontrar mapas más detallados de las conexiones multimodales [aquí](#) en el sitio web del proyecto.



En Ohio, el Proyecto del corredor de Brent Spence Bridge incorporará senderos, aceras y ciclovías nuevos y mejorados de uso compartido tal como se mostró anteriormente, incluida la infraestructura para bicicletas y peatones en las conexiones a lo largo de la I-75, para conectar mejor a las comunidades y mejorar el acceso a los servicios de autobús y a las comodidades de la comunidad.





En Kentucky, el Proyecto del corredor de Brent Spence Bridge incorporará senderos, aceras y ciclovías nuevos y mejorados de uso compartido desde el distrito comercial de Covington hasta las comunidades occidentales a lo largo de la I-75, así como en todo Goebel Park y Kenney Shields Park para conectar mejor a los vecindarios y mejorar el acceso a los servicios de autobús y a las comodidades de la comunidad.

VER EL RESUMEN DE LA JORNADA DE PUERTAS ABIERTAS EN LÍNEA AHORA

El Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC) realizaron reuniones estilo puertas abiertas para actualizar sobre el Proyecto del corredor de Brent Spence Bridge (BSBC) en agosto de 2023. Más de 275 residentes asistieron a las dos reuniones, organizadas en el Radisson Hotel en Covington y en Longworth Hall en Cincinnati.

[Ver el resumen de las reuniones de puertas abiertas.](#)

Para obtener más información sobre las reuniones de puertas abiertas o para ver los comentarios públicos recibidos y respondidos por el equipo del proyecto, visite la [Página de participación pública y comentarios](#) del sitio web.



EL EQUIPO DE CREACIÓN DE DISEÑO PROGRESIVO DE WALSH KOKOSING RECIBE UNA NOTIFICACIÓN PARA PROCEDER

El Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC) informaron al equipo de Walsh Kokosing que el proceso de contratación se completó y que las obras pueden empezar oficialmente para el proceso de creación de diseño progresivo. Mediante este método, se ofrece la mayor oportunidad de innovación en torno al enfoque del proyecto de

reducir el impacto y mejorar los beneficios generales de la comunidad, mientras se satisfacen las necesidades de transporte originales. Entre los próximos pasos, se incluye un período de innovación, que permitirá al equipo evaluar posibles perfeccionamientos en el proyecto que podrían mejorarlo aun más en relación con costos bajos, un horario reducido u otro valor para la comunidad.

CONOZCA AL EQUIPO DEL PROYECTO BSBC

Un miembro del equipo del proyecto ha trabajado 20 años para el ODOT, abordando proyectos en todo el estado a lo largo de su carrera. Otra persona siempre se ha sentido atraída hacia la ingeniería, y ahora está emocionada por trabajar en uno de los proyectos de infraestructura más grandes del país. Alguien más es un autoproclamado "chico de puentes grandes", quien ha estado ejerciendo presión para trabajar en este proyecto durante años. Otra persona aporta un trasfondo ambiental a este proyecto transformador y se compromete a entregar un proyecto que sea ambientalmente sensible.

Conozca a las personas detrás del proyecto en una próxima serie "Meet-the-Team" (Conocer al equipo). Los perfiles completos estarán en el sitio web del proyecto, en las redes sociales y en futuras ediciones de este boletín informativo en las próximas semanas.

COMPARTA SUS PENSAMIENTOS E IDEAS

El equipo del Proyecto del corredor de Brent Spence Bridge acoge los comentarios y la retroalimentación del público. Para enviar una pregunta o comentario, visite el [sitio web](#) del proyecto y haga clic en el botón "Contact Us" (Contacto) ubicado en la esquina superior derecha. Además, puede comunicarse con el equipo de creación de diseño directamente para conocer las oportunidades laborales escribiendo a WalshKokosingBrentSpence@walshgroup.com, o visitando su sitio web.

Puede visualizar las respuestas a todos los comentarios públicos en la sección [Participación y comentarios del público](#) del sitio web. En esta sección, se presentan exhibiciones del proyecto, resúmenes de las actividades de alcance y respuestas a los comentarios enviados al equipo del proyecto.

Mantenerse conectado

Existen muchas formas de mantenerse conectado con el Proyecto del corredor de Brent Spence Bridge.

Visite nuestro sitio web

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Envíe un comentario

From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Monday, October 30, 2023 4:19 PM
To:
Subject: Brent Spence Bridge Corridor Project Update - Innovation Period Open

Brent Spence Bridge Corridor October 30, 2023 eNewsletter



INNOVATION PERIOD KICKS OFF PROOF OF CONCEPT PHASE OF DESIGN

With the design-build contract officially signed, the Brent Spence Bridge Corridor Project team commenced the project's Proof of Concept Phase on October 2. The Innovation Period is part of the project's Proof of Concept phase, which will conclude in the spring of 2024.

"The Innovation Period is critical for efficiency's sake," said Ohio Department of Transportation (ODOT) Project Manager Tommy Arnold. "We continue to evaluate ways to save money and time, and to meet the contract objectives, such as achieving effective project delivery, minimizing traffic disruption during construction, and maximizing the public investment in the project. It's a period of refinement of the base plan."

While the 60-day innovation period will end in December, the resulting ideas will still need further evaluation for constructability and cost. Final decisions are expected by mid-2024.

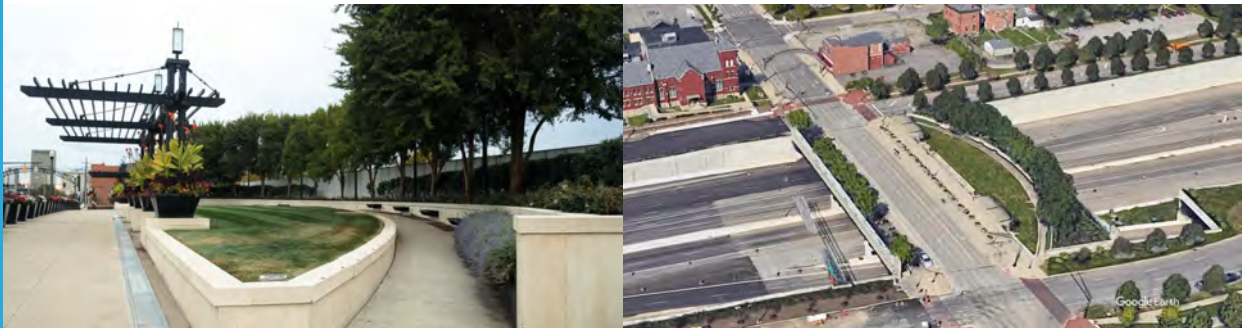
CONTRACT OBJECTIVES

- Maximize project scope within programmed funding amounts through innovation, design optimization and effective risk mitigation
- Achieve effective project delivery
- Open new companion bridge to traffic by July 15, 2029
- Minimize traffic disruption during construction with minimal detours or traffic diversions to local streets
- Minimize physical intrusion and impact
- Maximize public investment in the project by minimizing the footprint
- Minimize the footprint of the interstate system to maximize developable space
- Improve neighborhood connectivity across the interstate
- Build the project with a context-sensitive design that fits within the community
- Provide strong aesthetic value along the project corridor
- Improve local road aesthetics when crossing the interstate
- Provide opportunities for workforce development and DBE utilization
- Create best environmental outcomes
- Design for sustained quality of life



CHANGES PROPOSED TO THE EZZARD CHARLES BRIDGE

The Brent Spence Bridge Corridor project will include changes proposed by the Cincinnati Department of Transportation & Engineering – a wider design that will create additional connectivity and the potential to support future development.



Above: Ground-level and Google Earth images of the green space added to Long Street bridge in Columbus. Ezzard Charles Drive could look similar.

The proposed Ezzard Charles Drive bridge would widen the overpass without changing the crossing distance, while potentially allowing for enhanced landscaping, social gathering areas and small retail development. The wider bridge would improve connectivity and create a gateway for the Cincinnati Museum Center and the West End neighborhood.

The need to widen the Ezzard Charles bridge emerged following recent public outreach meetings and is a testament to the strong coordination between ODOT and the City of Cincinnati.

PARKING GARAGE WILL BENEFIT OFFICE WORKERS, COVINGTON'S

ECONOMIC OPPORTUNITY ... AND SAVE MONEY

In a continuing effort to invest in and minimize the impact of the Brent Spence Bridge Corridor (BSBC) Project on communities, KYTC will contribute \$15 million toward the construction of an underground parking garage in Covington. This is a great example of providing solutions that invest in the community and minimize project costs and impacts.

The new parking facility will replace the approximately 160 spaces that are currently located under Interstates 71 and 75 and directly across the street from the Kenton County Government Center.



Above: Existing parking spaces available for the Kenton County Government Center.

“This is truly a win-win,” Gov. Andy Beshear said in announcing the agreement. “Kenton County can keep the parking for its government center. It also will have a valuable space for future development on top of the new garage. At the same time, the Brent Spence Bridge Corridor Project will incur less cost because it will require far less highway bridge area.”

Building the parking garage and removing the parking from under the interstate reduces the length of highway bridge required in this area, thus reducing both overall construction costs and future maintenance costs.

“We appreciate the Commonwealth working with Kenton County to mitigate the loss of parking caused by the Brent Spence Bridge Corridor Project,” said Kenton County Judge/Executive Kris Knochelmann, who added that officials are considering a site immediately east of the government center for the parking garage. “Convenient and abundant parking was key to our decision to relocate the Kenton County Government Center to the 12th Street interchange, and this solution makes us whole.”

“Secretary Jim Gray and his team have been great partners in recognizing and addressing the local issues generated by such a large and nationally significant project. This new garage will also contribute to Kenton County’s already significant work to make the 12th Street interchange a premier gateway into the city of Covington.”

KYTC TO PILOT TRANSPARENT NOISE WALLS IN COVINGTON

A KYTC pilot project will provide residents and businesses in Covington the opportunity to experience how well transparent noise walls work to dampen highway noise without blocking views to the local business district and beyond.

KYTC will soon begin working on the construction of transparent noise barriers along Crescent Avenue in Covington, located just to the west of Interstates 71 and 75 (I-71/75). See-through material will be used in the noise walls along a 1,000-foot stretch to cut down noise pollution from the highway without sacrificing sight lines to Downtown Cincinnati and the Ohio River.

"Of course, everyone is concerned with noise, but they're also concerned with the loss of views in the area, particularly from Goebel Park and from the west side of the interstate looking toward the Cincinnati skyline or the Covington-Mainstrasse area," said Gary Valentine, KYTC's BSBC major project coordinator. "We listened and started exploring how to make transparent noise barriers in this area a reality."



Crescent Avenue With Noise Wall - Kentucky
(Looking South)



Clear barriers have long been seen in coastal states and in Europe, and more recently have been adopted in Ohio and Tennessee. This project will be Kentucky's first utilization of the concept.

With completion expected in 2024, the pilot project is intended to provide the Covington community the opportunity to see transparent barriers in place before they provide feedback on final noise wall decisions for the Brent Spence Bridge Corridor project.

INVESTING IN THE FUTURE WORKFORCE

With project construction on the horizon, ODOT and KYTC are investing in educating youth about careers in the transportation industry. In conjunction with Big Brothers Big Sisters,

ODOT recently participated in a 2023 summer program, [Camp Oty'Okwa](#), where 395 children from across Ohio learned about the relationship between the transportation industry and our environment.

Campers participated in hands-on activities such as the Building Paper Bridges activity. In this activity, students learned about the Brent Spence Bridge Corridor project and worked in teams of five to design, sketch, and build a paper bridge. Students were assigned the roles of the Governor of Ohio, Governor of Kentucky, Project Designer, Structural Engineer, and Bridge Safety Inspector to design and build a paper bridge. Activities like this created a fun interactive environment for campers to learn and develop interests in transportation careers.



SHARE YOUR THOUGHTS AND IDEAS

The Brent Spence Bridge Corridor Project team welcomes comments and feedback from the public. To submit a question or comment, visit the project [website](#) and click on the “Contact Us” button in the upper right-hand corner. You can also contact the Design-Build team directly about job opportunities at WalshKokosingBrentSpence@walshgroup.com, or visit their [website](#).

Responses to all public comments can be viewed on the [Public Involvement and Comments](#) section of the website. This section features project exhibits, summaries of outreach activities, and responses to comments submitted to the project team.

Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

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October 30, 2023 E-Newsletter

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Boletín electrónico del 30 de octubre de 2023 sobre el corredor del puente Brent Spence

BRENT SPENCE
BRIDGE CORRIDOR



**INVIRTIENDO EN LAS COMUNIDADES LOCALES.
HACIENDO CRECER LA ECONOMÍA DE ESTADOS UNIDOS.**



BrentSpenceBridgeCorridor.com

**TEAM
KENTUCKY**
GABINETE DE TRANSPORTE

EL PERÍODO DE INNOVACIÓN INICIA LA FASE DE PRUEBA DE CONCEPTO DEL DISEÑO

Con la firma oficial del contrato de diseño y construcción, el equipo del Proyecto del Corredor del Puente Brent Spence inició el 2 de octubre la fase de prueba de concepto del proyecto. El período de innovación forma parte de la fase de prueba de concepto del proyecto, que concluirá en la primavera de 2024.

"El período de innovación es fundamental para lograr una mayor eficiencia", afirmó el director del proyecto del Departamento de Transporte de Ohio (ODOT), Tommy Arnold. "Seguimos evaluando formas de ahorrar dinero y tiempo y de cumplir los objetivos del contrato, como lograr una ejecución eficaz del proyecto, minimizar las interrupciones del tráfico durante la construcción y maximizar la inversión pública en el proyecto. Es un período de perfeccionamiento del plan base".

Si bien el período de innovación de 60 días finalizará en diciembre, las ideas resultantes aún deberán someterse a una evaluación adicional en cuanto a constructibilidad y costo. Se espera que las decisiones finales se tomen a mediados de 2024.

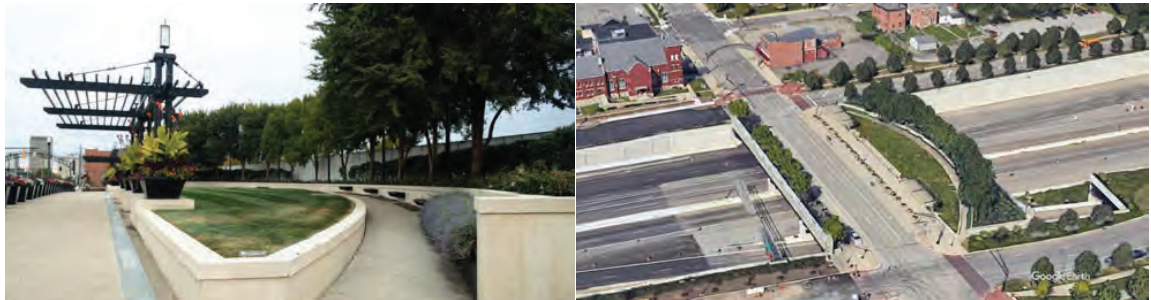
OBJETIVOS DEL CONTRATO

- Maximizar el alcance del proyecto respetando las cantidades de financiación programadas mediante la innovación, la optimización del diseño y la mitigación eficaz de los riesgos.
- Lograr una ejecución eficaz del proyecto.
- Abrir el nuevo puente complementario al tráfico antes del 15 de julio de 2029.
- Minimizar las interrupciones del tráfico durante la construcción, con mínimos rodeos o desvíos del tráfico a las calles locales.
- Minimizar los inconvenientes y el impacto físicos.
- Maximizar la inversión pública en el proyecto, reduciendo al mínimo el espacio ocupado.
- Minimizar el espacio ocupado por el sistema interestatal para maximizar el espacio urbanizable.
- Mejorar la conectividad de los vecindarios a través de la interestatal.
- Construir el proyecto con un diseño que tenga en cuenta el contexto y se adapte a la comunidad.
- Proporcionar un gran valor estético a lo largo del corredor del proyecto.
- Mejorar la estética de las carreteras locales al cruzar la interestatal.
- Proporcionar oportunidades para el desarrollo laboral y la utilización de las DBE.
- Conseguir los mejores resultados a nivel medioambiental.
- Diseñar un proyecto que garantice una calidad de vida sostenible.



CAMBIOS PROPUESTOS PARA EL PUENTE EZZARD CHARLES

El Proyecto del Corredor del Puente Brent Spence incluirá cambios propuestos por el Departamento de Transporte e Ingeniería de Cincinnati: un diseño más ancho que creará una conectividad adicional y el potencial para apoyar futuros proyectos de desarrollo.



Arriba: imágenes a nivel del suelo y de Google Earth del espacio verde añadido al puente Long Street en Columbus. Ezzard Charles Drive tendría un aspecto similar.

El puente propuesto para Ezzard Charles Drive ensancharía el paso elevado sin modificar la distancia de cruce, lo que permitiría mejorar la parquización, las zonas de socialización y el desarrollo de las pequeñas tiendas. El ensanchamiento del puente mejoraría la conectividad y crearía una puerta de entrada al Cincinnati Museum Center y al vecindario de West End.

La necesidad de ensanchar el puente Ezzard Charles surgió tras las recientes reuniones de divulgación pública y es un ejemplo de la buena coordinación entre el ODOT y la Ciudad de Cincinnati.

EL ESTACIONAMIENTO BENEFICIARÁ A LOS TRABAJADORES DE OFICINAS, LAS POSIBILIDADES ECONÓMICAS DE COVINGTON... Y AHORRARÁ DINERO

En un esfuerzo continuo por invertir en el Proyecto del Corredor del Puente Brent Spence (BSBC) y para minimizar su impacto en las comunidades el KYTC contribuirá con 15 millones

(BSBC) y para minimizar su impacto en las comunidades, el KYTC contribuirá con 15 millones de dólares a la construcción de un estacionamiento subterráneo en Covington. Este es un gran ejemplo de cómo aportar soluciones que inviertan en la comunidad y minimicen los costos e impactos del proyecto.

El nuevo estacionamiento sustituirá a los cerca de 160 espacios situados actualmente bajo las carreteras interestatales 71 y 75, y justo enfrente del Centro de Gobierno del condado de Kenton.



Arriba: estacionamientos existentes disponibles para el Centro de Gobierno del condado de Kenton.

"Esto es favorable para todos", dijo el gobernador Andy Beshear al anunciar el acuerdo. "El condado de Kenton puede conservar el estacionamiento para su Centro de Gobierno. También dispondrá de un valioso espacio para futuros proyectos de desarrollo encima del nuevo estacionamiento. Al mismo tiempo, el Proyecto del Corredor del Puente Brent Spence tendrá un costo menor porque requerirá mucha menos superficie del puente de la autopista".

La construcción del estacionamiento y la eliminación del estacionamiento de debajo de la interestatal reduce la extensión del puente de la autopista necesaria en esta zona, con lo que se reducen los costos generales de construcción y los futuros costos de mantenimiento.

"Agradecemos que la mancomunidad trabaje con el condado de Kenton para mitigar la pérdida de estacionamientos causada por el Proyecto del Corredor del Puente Brent Spence", dijo el juez y ejecutivo del condado de Kenton, Kris Knochelmann, quien añadió que los funcionarios están considerando un sitio justo al este del Centro de Gobierno para construir el estacionamiento. "La disponibilidad de un estacionamiento conveniente y amplio fue clave en nuestra decisión de reubicar el Centro de Gobierno del condado de Kenton en la intersección de 12th Street, y esta solución nos satisface".

"El secretario Jim Gray y su equipo han sido unos excelentes socios a la hora de reconocer y abordar los problemas locales generados por un proyecto tan grande y de tanta importancia a nivel nacional. Este nuevo estacionamiento también contribuirá al ya significativo trabajo realizado por el condado de Kenton para hacer de la intersección de 12th Street una puerta de entrada principal a la ciudad de Covington".

EL KYTC PROBARÁ BARRERAS ACÚSTICAS TRANSPARENTES EN COVINGTON

Un proyecto piloto del KYTC proporcionará a los residentes y negocios de Covington la oportunidad de experimentar lo bien que funcionan las barreras acústicas transparentes, que mitigan el ruido de la autopista sin bloquear las vistas al distrito comercial local y más allá.

El KYTC empezará pronto a trabajar en la construcción de barreras acústicas transparentes a lo largo de Crescent Avenue en Covington, situada justo al oeste de las interestatales 71 y 75 (I 71/75). Se utilizará material transparente en las barreras acústicas a lo largo de un

y 75 (I-71/75). Se utilizará material transparente en las barreras acústicas a lo largo de un tramo de 1,000 pies para reducir la contaminación acústica de la autopista sin sacrificar el campo visual hacia el centro de Cincinnati y el río Ohio.

"Por supuesto, todo el mundo está preocupado por el ruido, pero también por la pérdida de vistas en la zona, sobre todo desde el parque Goebel y desde el lado oeste de la interestatal en dirección al paisaje urbano de Cincinnati o a la zona de Covington-Mainstrasse", dijo Gary Valentine, coordinador principal del proyecto del BSBC del KYTC. "Escuchamos y empezamos a investigar cómo hacer realidad las barreras acústicas transparentes en esta zona".



Crescent Avenue con barrera acústica, Kentucky (mirando al sur)



KYTC Item n.º 6-17 | ODOT PID 89068

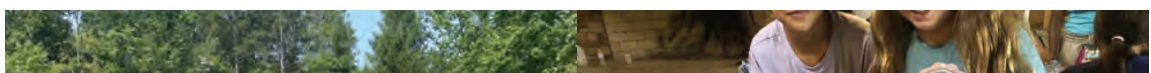
Las barreras transparentes se utilizan desde hace tiempo en los estados costeros y en Europa, y más recientemente se han adoptado en Ohio y Tennessee. Con este proyecto, Kentucky utilizará por primera vez este concepto.

El proyecto piloto, cuya finalización está prevista para 2024, pretende ofrecer a la comunidad de Covington la posibilidad de ver las barreras transparentes en funcionamiento antes de que den su opinión sobre las decisiones finales relativas a las barreras acústicas para el Proyecto del Corredor del Puente Brent Spence.

INVIRTIENDO EN LA MANO DE OBRA DEL FUTURO

Con la construcción del proyecto aproximándose, el ODOT y el KYTC están invirtiendo en educar a los jóvenes sobre las carreras en la industria del transporte. Junto con Big Brothers Big Sisters, el ODOT participó hace poco en un programa de verano 2023, [Camp OtyOkwa](#), donde 395 niños de todo Ohio aprendieron sobre la relación entre la industria del transporte y nuestro medioambiente.

Los campistas participaron en actividades prácticas como la construcción de puentes de papel. En esta actividad, los estudiantes aprendieron sobre el Proyecto del Corredor del Puente Brent Spence y trabajaron en equipos de cinco para diseñar, bocetar y construir un puente de papel. A los estudiantes se les asignaron los cargos de gobernador de Ohio, gobernador de Kentucky, diseñador del proyecto, ingeniero estructural e inspector de seguridad de puentes para diseñar y construir un puente de papel. Actividades como esta crearon un entorno interactivo y divertido para que los campistas aprendieran y desarrollaran su interés por las carreras relacionadas con el transporte.





COMPARTA SUS OPINIONES E IDEAS

El equipo del Proyecto del Corredor del Puente Brent Spence agradece los comentarios y la opinión del público. Para enviar una pregunta o comentario, visite el [sitio web](#) del proyecto y haga clic en el botón "Contact Us" (Contacto), ubicado en la esquina superior derecha. Además, puede comunicarse directamente con el equipo de diseño y construcción para conocer las oportunidades laborales escribiendo a WalshKokosingBrentSpence@walshgroup.com o visitando su [sitio web](#).

Puede ver las respuestas a todos los comentarios del público en la sección [Public Involvement and Comments](#) (Participación y comentarios del público) del sitio web. En esta sección, se presentan exhibiciones del proyecto, resúmenes de las actividades de divulgación y respuestas a los comentarios enviados al equipo del proyecto.

Manténgase conectado

Existen muchas formas de mantenerse conectado con el Proyecto del Corredor del Puente Brent Spence.

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Envíe un comentario

From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Thursday, November 30, 2023 3:07 PM
To:
Subject: Brent Spence Bridge Corridor Project Update and Q&A

Brent Spence Bridge Corridor November 30, 2023 eNewsletter



LONGWORTH HALL TO REMAIN INTEGRAL PART OF THE COMMUNITY

The State of Ohio has reached an agreement to purchase Longworth Hall and plans to use a portion of the building for BSBC project team office space.

As part of the project, approximately 200 feet will be carefully removed from the far eastern end of the building to make way for construction of approaches to the new companion bridge.



Importantly, the bulk of the 1,160-foot building will remain completely untouched. Construction workers will close the opened wall using bricks salvaged from the partial removal, preserving Longworth Hall's historic character.

Areas of Longworth Hall not impacted by the construction necessary to accommodate the approaches to the new companion bridge will continue to function and operate as they do currently. No impacts to the long-term operations of the building are expected.

Originally built as a freight terminal for the Baltimore and Ohio Railroad in 1904, Longworth Hall is known as the sister building to Camden Yards in Baltimore. Nicholas Longworth, patriarch of Cincinnati's business, political and philanthropic family, originally owned the land.

MEET THE PROJECT TEAM

Learn more about the professionals who make up the BSBC Project's bi-state management team. [Visit the website](#) to read profiles on each of the eight people who lead the management team. To get you started, here are short video intros for Stacey Hans and Tommy Arnold, project managers for KYTC and ODOT, respectively.



[Stacey Hans](#) is the KYTC project manager for the BSBC Project, where she brings the leadership and team-building skills she refined on the basketball court in college to the world of transportation and project management via an environmental career.

"It's almost surreal to be part of what I consider to be one of the most transformative projects for not only Northern Kentucky and Cincinnati but our entire region. It's amazing and sometimes there are not even words to describe it."

[Tommy Arnold](#) is the ODOT project manager for the BSBC project. His background includes serving as project manager for the Interstate 71-Martin Luther King Jr. Drive interchange, the first two-step, low-bid design-build project ever constructed in Ohio.

"This is a huge opportunity for our region, and what we're looking to do is spread the word about the construction and transportation industry. It is an excellent career to get into and there are more opportunities every day."

BRENT SPENCE BRIDGE CORRIDOR PROJECT Q&A

Have a question for the Brent Spence Bridge Corridor Project team? Feel free to submit a question or comment on our website here. All questions are answered and posted monthly on the Public Comments and Responses page. Here are a few recently received questions and answers.

Q: What are the 'new shared use paths'?

A: A shared-use path is a 10-foot paved path that can be shared by pedestrians, cyclists and other non-motorized users. Renderings of what the shared-use paths might look like on 6th Street in Ohio are shown in slides 23-26 of [this link](#).

Q: I'm a journeyman carpenter with an extensive resume. I live in Cincinnati and I would like to be a part of this project.

A: Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding opportunities to work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website [here](#). Additional links to resources for firms that want to work on the project are available on the ["Work With Us"](#) page on the project website.

Q: We are interested in providing pricing for the project. We are a minority, woman-owned disadvantaged business enterprise heavy-highway contractor.

A: Please visit the ["Work With Us"](#) page on the project website for links to resources for firms that want to work on the project and/or reach out directly to the Walsh Kokosing Design-Build Team [here](#). In addition, please visit the ["DBE/D&I Resources"](#) page on the project website for a list of upcoming networking events.



View Project Rendering Flyover

SHARE YOUR THOUGHTS AND IDEAS

The Brent Spence Bridge Corridor Project team welcomes comments and feedback from the public. To submit a question or comment, visit the project [website](#) and click on the "Contact Us" button in the upper right-hand corner. You can also contact the Design-Build team directly about job opportunities at WalshKokosingBrentSpence@walshgroup.com, or visit their [website](#).

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Follow us on [Facebook](#) and [X, formerly Twitter](#), and [Threads](#) for timely updates and information.

Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

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Boletín electrónico del 30 de noviembre de 2023 sobre el corredor del puente Brent Spence

BRENT SPENCE
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HACIENDO CRECER LA ECONOMÍA DE ESTADOS UNIDOS.**



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TEAM
KENTUCKY
GABINETE DE TRANSPORTE

LONGWORTH HALL PERMANECERÁ COMO PARTE INTEGRAL DE LA COMUNIDAD

El estado de Ohio ha llegado a un acuerdo para adquirir Longworth Hall y planea utilizar una parte del edificio como espacio de oficina para el equipo del proyecto BSBC.

Como parte del proyecto, se removerán con cuidado unos 200 pies del extremo este del edificio para dar paso a la construcción de accesos al nuevo puente complementario.





Es importante destacar que la mayor parte del edificio de 1,160 pies permanecerá completamente intacta. Los trabajadores de la construcción cerrarán el muro abierto con ladrillos recuperados de la eliminación parcial, preservando así el carácter histórico de Longworth Hall.

Las áreas de Longworth Hall que no se verán afectadas por la construcción necesaria para dar cabida a los accesos al nuevo puente complementario seguirán funcionando y operando como lo hacen actualmente. No se esperan impactos en las operaciones a largo plazo del edificio.

Longworth Hall, construido originalmente como terminal de carga para el ferrocarril de Baltimore y Ohio en 1904, es conocido como el edificio hermano de Camden Yards en Baltimore. Nicholas Longworth, patriarca de la familia empresarial, política y filantrópica de Cincinnati, fue el propietario original de la tierra.

CONOZCA AL EQUIPO DEL PROYECTO

Conozca más sobre los profesionales que conforman el equipo de gestión biestatal del proyecto BSBC. [Visite el sitio](#) web para leer los perfiles de cada una de las ocho personas que lideran el equipo de gestión. Para comenzar, aquí hay breves presentaciones en video de Stacey Hans y Tommy Arnold, gerentes de proyectos de KYTC y ODOT, respectivamente.



[Stacey Hans](#) es la gerente de proyectos de KYTC para el proyecto BSBC, donde aporta las habilidades de liderazgo y formación de equipos que perfeccionó en la cancha de



[Tommy Arnold](#) es el gerente de proyecto de ODOT para el proyecto BSBC. Su experiencia incluye haber sido gerente de proyecto para el intercambio entre la Interestatal 71 y

baloncesto en la universidad hacia el mundo del transporte y la gestión de proyectos a través de una carrera ambiental.

s casi irreal ser parte de lo que considero uno de los proyectos más transformadores no solo para el norte de Kentucky y Cincinnati, sino también para toda nuestra región. Es asombroso y a veces ni siquiera hay palabras para describirlo".

Martin Luther King Jr. Drive, el primer proyecto de diseño y construcción de bajo costo jamás construido en Ohio.

"Esta es una gran oportunidad para nuestra región, y lo que estamos buscando es dar a conocer la industria de la construcción y el transporte. Es una excelente carrera para emprender y cada día hay más oportunidades".

PREGUNTAS FRECUENTES SOBRE EL PROYECTO DEL CORREDOR DEL PUENTE BRENT SPENCE

¿Tiene alguna pregunta para el equipo del proyecto del corredor del puente Brent Spence? No dude en enviar una pregunta o comentario en nuestro sitio web aquí. Todas las preguntas se responden y publican mensualmente en la página de Comentarios y respuestas del público. Estas son algunas de las preguntas frecuentes recibidas recientemente.

P: ¿Cuáles son los "nuevos caminos de uso compartido"?

R: Un camino de uso compartido es un camino pavimentado de 10 pies que pueden compartir peatones, ciclistas y otros usuarios no motorizados. Se muestran representaciones de cómo podrían ser los caminos de uso compartido en 6th Street en Ohio en las diapositivas 23 a 26 de [este enlace](#).

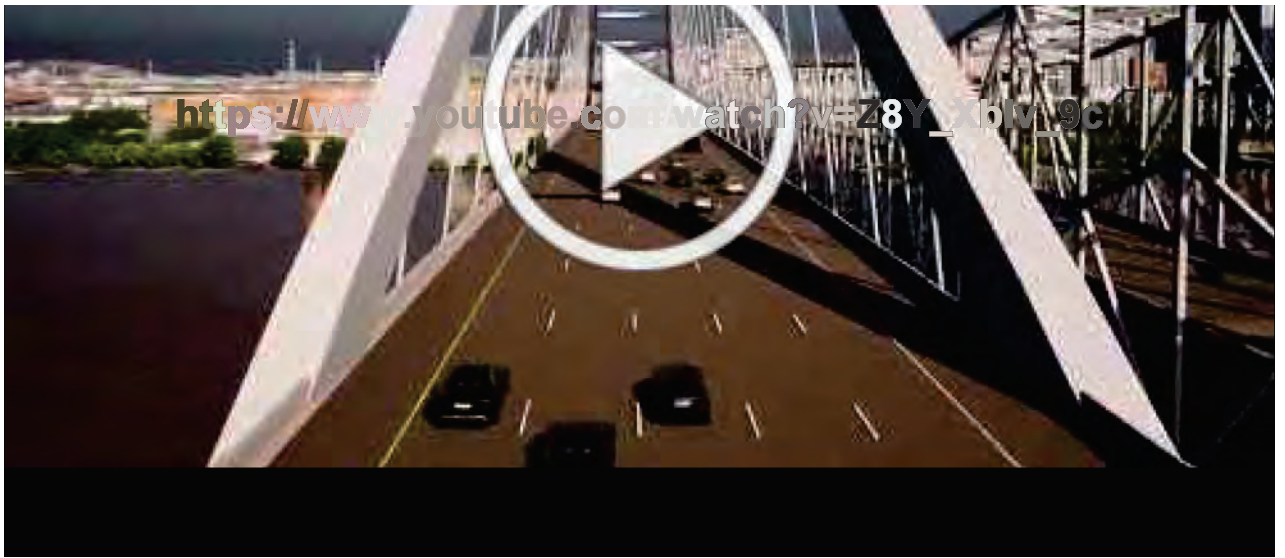
P: Soy un carpintero experto con un extenso currículum. Vivo en Cincinnati y me gustaría formar parte de este proyecto.

R: Gracias por su interés en el proyecto del corredor del puente Brent Spence. Puede comunicarse directamente con el equipo de diseño y construcción sobre oportunidades para trabajar en el proyecto utilizando la siguiente dirección de correo electrónico: WalshKokosingBrentSpence@walshgroup.com. También puede visitar el sitio web del equipo de diseño y construcción de Walsh Kokosing [aquí](#). Encontrará enlaces adicionales a recursos para empresas que quieran trabajar en el proyecto en la página "[Work With Us](#)" (Trabaje con nosotros) en el sitio web del proyecto.

P: Nos interesa proporcionar un presupuesto para el proyecto. Somos una empresa comercial desfavorecida, propiedad de mujeres minoritarias y contratista de carreteras pesadas.

R: Visite la página "[Work With Us](#)" (Trabaje con nosotros) en el sitio web del proyecto para obtener enlaces a recursos para empresas que quieran trabajar en el proyecto o comuníquese directamente con el equipo de diseño y construcción de Walsh Kokosing [aquí](#). Además, lo invitamos a visitar la página "[DBE/D&I Resources](#)" (Recursos DBE/D&I) en el sitio web del proyecto para obtener una lista de próximos eventos de networking.





Ver renderización aérea del proyecto

COMPARTA SUS OPINIONES E IDEAS

El equipo del proyecto del corredor del puente Brent Spence agradece los comentarios y opiniones del público. Para enviar una pregunta o comentario, visite el [sitio web](#) del proyecto y haga clic en el botón "Contact Us" (Contacto), ubicado en la esquina superior derecha. Además, puede comunicarse directamente con el equipo de diseño y construcción para conocer las oportunidades laborales escribiendo a WalshKokosingBrentSpence@walshgroup.com o visitando su [sitio web](#).

Puede ver las respuestas a todos los comentarios del público en la sección [Public Involvement and Comments](#) (Participación y comentarios del público) del sitio web. En esta sección, se presentan exhibiciones del proyecto, resúmenes de las actividades de divulgación y respuestas a los comentarios enviados al equipo del proyecto.

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Manténgase conectado

Existen muchas formas de mantenerse conectado con el Proyecto del Corredor del Puente Brent Spence.

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[Envíe un comentario](#)

From: Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>
Sent: Thursday, December 28, 2023 1:06 PM
To:
Subject: Brent Spence Bridge Corridor Project 2023 Recap

Brent Spence Bridge Corridor December 28, 2023 eNewsletter



Please enjoy this version of our year-end recap edition. We corrected a couple of link errors below. Happy holidays!

2023: A YEAR TO REMEMBER

What a year this has been for the Brent Spence Bridge Corridor Project! Starting with President Joe Biden's visit in January to present the much-anticipated \$1.635 billion grant that gave the project the green light, 2023 was the year that this transformative project truly became a reality.



To capture the highlights of this future-changing year, the project team produced a video recap that highlights milestones along the path to construction. You'll see footage and images from President Biden's visit, the neighborhood tour with Ohio Department of Transportation Director Jack Marchbanks and Cincinnati Mayor Aftab Pureval, the announcement of Walsh-Kokosing as the project's Design-Build Team, public open houses in Kentucky and Ohio, nearby work in Kentucky like the pilot noise wall project and Kenton County parking garage, and more as we walk you through all that's been done in 2023.

We invite you to celebrate with us as we look to even more progress in 2024. You can find the year-end recap video here or by clicking the image above.

MEET THE PROJECT TEAM

Each month, we're introducing you to the professionals who are working to bring the Brent Spence Bridge Corridor Project to life. In December, we want you to meet Nikki Boden and Joe Smithson, design leads for KYTC and ODOT. Just click on the photos to see short videos and find out more about each of them. You can also [visit the website](#) to read profiles on each of the eight people who lead the bistate management team.



Northern Kentucky native [Nikki Boden](#) is KYTC’s design lead for the BSBC. With nearly 20 years in engineering under her belt, Boden says she’s excited for the once-in-a-career challenge of helping build one of the largest infrastructure projects in the country.

“I love the collaborative environment,” she explains. “Every member of the design-build team brings individual expertise to the table, so we can work together toward the best possible outcome. I’ve always had a love of learning, and this project brings the opportunity to learn from the very best in our field.”

A multi-decade career in highway engineering prepared Blanchester, Ohio, native [Joe Smithson](#) for his role as ODOT’s design lead on the \$3.6 billion project. After nearly eight years as a geotechnical engineer with a Cincinnati consulting firm, Smithson joined the Ohio Department of Transportation (ODOT) in 2005 as the district geotechnical engineer in the District 8 office in Lebanon. In 2017, he was named district design engineer and became a member of the Bistate Management Team for the BSBC Project in early 2023.

“No two highways are alike,” Smithson says. “Even the simplest of projects requires outside-the-box thinking.”

SHARE YOUR THOUGHTS AND IDEAS

The Brent Spence Bridge Corridor Project team welcomes comments and feedback from the public. To submit a question or comment, visit the project [website](#) and click on the “Contact Us” button in the upper right-hand corner. You can also contact the Design-Build team directly about job opportunities at WalshKokosingBrentSpence@walshgroup.com, or visit their [website](#).

Responses to all public comments can be viewed on the [Public Involvement and Comments](#) section of the website. This section features project exhibits, summaries of outreach activities, and responses to comments submitted to the project team.

Follow us on [Facebook](#) and [X, formerly Twitter](#), and [Threads](#) for timely updates and information.

Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

[Visit our website](#)

[Sign up for updates](#)

[Submit a comment](#)

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Brent Spence Bridge Corridor | 505 South State Route 741, Lebanon, OH 45036

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Boletín electrónico del 28 de diciembre de 2023 sobre el corredor del puente Brent Spence

BRENT SPENCE
BRIDGE CORRIDOR



**INVIRTIENDO EN LAS COMUNIDADES LOCALES.
HACIENDO CRECER LA ECONOMÍA DE ESTADOS UNIDOS.**



BrentSpenceBridgeCorridor.com

TEAM
KENTUCKY
GABINETE DE TRANSPORTE

Disfrute de esta versión de nuestra edición de recapitulación de fin de año. Corregimos un par de errores de enlace a continuación. ¡Felices fiestas!

2023: UN AÑO PARA RECORDAR

¡Qué año ha sido este para el Proyecto del Corredor del Puente Brent Spence! Empezando por la visita del presidente Joe Biden en enero para presentar la tan anticipada subvención de \$1635 mil millones que dio luz verde al proyecto, 2023 fue el año en el que este proyecto transformador se convirtió en realidad.





Para capturar los eventos destacados de este año determinante para el futuro, el equipo del proyecto realizó una recapitulación en video donde se destacan los hitos a lo largo del camino hasta la construcción. Usted verá material e imágenes de la visita del presidente Biden, el recorrido por el vecindario con el director del Departamento de Transporte de Ohio, Jack Marchbanks, y el alcalde de Cincinnati, Aftab Pureval, el anuncio de Walsh-Kokosing como equipo de creación de diseños del proyecto, las jornadas de puertas abiertas en Kentucky y Ohio, las obras cercanas en Kentucky, como el proyecto de barreras acústicas y el estacionamiento del condado de Kenton, y mucho más, mientras lo guiamos por todo lo realizado en 2023.

Lo invitamos a celebrar con nosotros mientras anticipamos aun más progreso en 2024. Puede encontrar el video de recapitulación de fin de año aquí o haciendo clic en la imagen anterior.

CONOZCA AL EQUIPO DEL PROYECTO

Cada mes, le presentamos a los profesionales que trabajan para dar vida al Proyecto del Corredor del Puente Brent Spence. En diciembre, queremos que conozca a Nikki Boden y Joe Smithson, líderes de diseño para KYTC y ODOT. Solo debe hacer clic en las fotos para ver videos cortos y descubrir más acerca de cada uno de ellos. También puede [visitar el sitio web](#) para leer los perfiles de cada una de las ocho personas que lideran el equipo de gestión biestatal.



[Nikki Boden](#), nativa del norte de Kentucky, es la líder de diseño de KYTC para el BSBC. Con casi 20 años de experiencia en ingeniería, Boden comenta que está emocionada por el



Una carrera de varias décadas en ingeniería de autopistas preparó a [Joe Smithson](#), nativo de Blanchester, Ohio, para su rol como líder de diseño de ODOT en el proyecto de

Bodeni comenta que esta emocionada por el desafío único en su carrera de ayudar a construir uno de los proyectos de infraestructura más grandes del país.

“Me encanta el ambiente colaborativo”, explica. “Cada miembro del equipo de creación de diseño trae a la mesa su experiencia individual, para que podamos trabajar juntos para lograr el mejor resultado posible. Siempre he sentido amor por el aprendizaje, y este proyecto me da la oportunidad de aprender de los mejores en nuestro campo”.

de diseño de ODOT en el proyecto de \$3.6 mil millones. Después de casi ocho años como ingeniero geotécnico en una firma consultora de Cincinnati, Smithson se unió al Departamento de Transporte de Ohio (ODOT) en 2005 como ingeniero geotécnico distrital en la oficina del Distrito 8 en el Líbano. En 2017, fue nombrado ingeniero de diseño distrital y se convirtió en miembro del Equipo de Gestión Biestatal para el proyecto BSBC a principios de 2023.

“No hay dos autopistas iguales”, afirma Smithson. “Hasta el proyecto más simple requiere pensar de manera innovadora”.

COMPARTA SUS OPINIONES E IDEAS

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Appendix F: Press Releases





OHIO AND KENTUCKY TAKE NEXT STEPS TO IMPROVE BRENT SPENCE BRIDGE CORRIDOR



Improvements to I-71/I-75 corridor and addition of a new bridge begin to take shape through pre-procurement contracting, application for funding, and industry outreach.

FRANKFORT (May 16, 2022) – This week marks another milestone in the Brent Spence Bridge Corridor (BSBC) improvement project as the Ohio Department of Transportation (ODOT) requested proposals from consultants to support contract administration, public outreach, project management, and control tasks necessary to manage the eventual design-build project. The selected consultant will be co-managed by a joint team within ODOT and the Kentucky Transportation Cabinet (KYTC).

“Our team has been busy working with our counterparts in Ohio to ensure we are ready to move forward once funds are awarded,” said KYTC Secretary Jim Gray. “We have already taken many of the important steps necessary for a project of this size and scope that start long before the first shovel is placed.”

“These next key steps move us ever closer to unlocking the real potential of the I-71/75 corridor for this region, our two states, and our country,” said ODOT Director Jack Marchbanks. “The request for a pre-procurement consultant and these next steps are very important as we ramp up to getting this much-needed project built.”

The Brent Spence Bridge Corridor project includes improvements to approximately 8 miles of highway and the addition of a companion bridge on the west side of the existing Brent Spence Bridge. This additional capacity will alleviate congestion, improve safety, and open up this nationally significant freight corridor, which carries an estimated 3% of the nation’s GDP annually. Current plans also call for the separation of local and interstate traffic, which will improve access to local businesses and communities in Covington and Cincinnati.

ODOT and KYTC Continue Preparations and Apply for Federal Grants

The joint project team within ODOT and KYTC has already been working on securing property, preparing an initial financial plan, coordinating with utility companies, meeting with stakeholders, and a host of other activities, including applying for funding.

After Ohio Gov. Mike DeWine and Kentucky Gov. Andy Beshear [signed](#) a memorandum of understanding on February 28, the bi-state project team began preparing an application to support this \$2.8 billion project through the federal [Multimodal Projects Discretionary Grant](#), which includes funding set aside for projects that are so large that traditional funding and grant mechanisms are not an option.

The Kentucky General Assembly recently passed and [Gov. Beshear signed](#), a budget bill that included funding required to fulfill state match requirements for large projects. The State of Ohio is actively

developing its financial strategy, which will be included in the grant application to be submitted by May 23.

“We’re working closely with ODOT on this application to position the project to secure every available federal dollar,” said Stacey Hans, KYTC BSBC Project Manager. “With the full backing of both state’s leadership, our collaborative team in place, and the work we’ve done to prepare for construction, we feel we will have a compelling application.”

Value Engineering Effort to Be Completed by Mid-June

The joint ODOT and KYTC project team also has been working with engineering firm HNTB to evaluate potential opportunities to reduce overall costs of the construction project through a value engineering process. This work seeks to identify ways to deliver the project at the lowest possible cost without sacrificing safety, quality, or environmental attributes. The project team completing this work also takes into account the federally-required process completed in 2012 that considers potential environmental impacts.

“It’s very important in our current efforts that we confirm the previous decisions that were made through careful and thorough public engagement and environmental analysis in order to keep the project moving forward,” said Stefan Spinosa, P.E., ODOT BSBC Project Manager. “We will continue to pursue cost-saving measures and ways to improve the project that will be consistent with our targeted timeline to begin construction by the end of 2023.”

Contractor Outreach to Take Place in Coming Weeks

The joint project team will provide opportunities in the coming weeks for all firms interested in this historic project to ask questions, provide input to the process, and meet with ODOT and KYTC personnel to learn more about the details. Interested parties may contact the agencies at BSBProcurement@dot.ohio.gov.

###

Next steps taken to improve Brent Spence Bridge Corridor

May 16, 2022 | [ODOT](#)



COLUMBUS — The Brent Spence Bridge Corridor (BSBC) improvement project is hitting another milestone as the Ohio Department of Transportation (ODOT) requested proposals from consultants to support contract administration, public outreach, project management, and control tasks necessary to manage the eventual design-build project. The selected consultant will be co-managed by a joint team within ODOT and the Kentucky Transportation Cabinet (KYTC).

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Additional Downloads

[Project Fact Sheet](#)

BRENT SPENCE BRIDGE CORRIDOR FACT SHEET

OVERVIEW

The passage of the bipartisan federal infrastructure package has created a once-in-a-lifetime opportunity to improve the quality of life for the millions of Americans who use the federal highway system to travel between Ohio and Kentucky and beyond. It also presents the opportunity to invest in local businesses and a growing workforce by improving safety and travel along one of the most important national corridors for commerce and freight.

The Brent Spence Bridge is the critical link in this important eight-mile corridor from the Western Hills Viaduct in Ohio to Dixie Highway in Kentucky. The bridge is structurally sound and will remain in service for decades to come.

New funding will support construction of a new companion bridge – not a replacement – as well as updates to the existing bridge and the interstate network throughout the corridor. Ohio and Kentucky are working together to ensure the Brent Spence Bridge Corridor Project is shovel-ready and meets the needs of our growing region.

Both states are finalizing applications for federal infrastructure funding from the United States Department of Transportation.

A Preferred Alternative for A New Companion Bridge Has Been Approved

- A preferred alternative for a new companion bridge was approved in 2012. That decision was based on a federally-prescribed evaluation process that included detailed technical and environmental analysis, as well as comprehensive public engagement.
- The current anticipated project cost is \$2.8 billion, which will be shared by each state. The cost of the companion bridge will be split 50/50 by Ohio and Kentucky, and each state will pay for the approach work on their respective end of the bridge. The current estimates for each state are \$1.48 billion for Ohio and \$1.31 billion for Kentucky.
- Opportunities to reduce the cost of the preferred alternative are being evaluated. The 2012 decision is not being reopened because the goal of building a new companion structure to the west of the existing bridge remains unchanged – to improve safety and ease congestion by providing additional capacity that separates local and through traffic. The preferred alternative – with potential adjustments to design – meets that objective.



The Brent Spence Bridge is safe and structurally sound and will serve a critical transportation need for decades to come.

- The bridge was designed to carry 80-100,000 vehicles per day. It currently carries twice that volume. During peak travel times, the number of vehicles crossing the bridge significantly exceeds what each lane can efficiently handle without travel delays.
- The bridge was reconfigured in 1985 to provide an additional travel lane on each deck.
 - Before: three 12-foot lanes + approximately five-foot shoulders
 - Now: four 11-foot lanes + one-foot shoulders
- Current planning and traffic analyses indicate that additional capacity is needed to support safer travel for drivers who use I-71/75 to travel between Ohio and Kentucky. Long-term use of the Brent Spence Bridge, as well as a new companion structure, would allow for the separation of local and through traffic. This separation would improve safety and support better access to the Covington and Cincinnati business districts.
- The bridge was built in 1963 and is maintained by KYTC. Multiple routine maintenance projects have taken place throughout the history of the bridge to ensure it remains viable and safe for long-term use, including the recent project to clean and paint the bridge, as well as a project in 2017 to replace the concrete deck on both the north and southbound travel lanes.

Ohio and Kentucky Will Share Responsibility in Next Steps and Construction

An Interstate Cooperative Agreement will allow Ohio and Kentucky to begin the process of preparing for construction. The agreement defines the roles and responsibilities for procurement, funding, construction, and maintenance of the Brent Spence Bridge Corridor Project.

Anticipated Next Steps:

- Advertise to hire consultant to support pre-procurement activities
- Apply for federal grants
- Sign Interstate Cooperative Agreement
- Complete cost reduction evaluation
- Acquire property needed for construction
- Prepare documents and plans to contract with companies who will manage the design and construction
- Construction



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TRANSPORTATION CABINET
transportation.ky.gov

Andy Beshear
GOVERNOR

Jim Gray
SECRETARY

FOR IMMEDIATE RELEASE

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Contact: Chuck Wolfe
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chuck.wolfe@ky.gov

Governors Beshear and DeWine Reveal Revamped Plans that Shrink Brent Spence Corridor

New plans reduce footprint for new bridge and propose new lane configurations to improve safety and travel between Ohio and Kentucky

FRANKFORT, Ky. (July 15, 2022) – Kentucky Governor Andy Beshear and Ohio Governor Mike DeWine today revealed updated bridge maps detailing new lane configurations and revamped plans to deliver the regionally-transformative Brent Spence Bridge Corridor Project with fewer property impacts.

“While this is a nationally significant project, it is still very much a community-minded project,” said Gov. Beshear. “Our teams have gone to great lengths to shrink property impacts while still delivering a solid solution to the traffic congestion issues in the region.”

“It has been important from the beginning to make sure we’re meeting the needs of today and tomorrow while also being mindful about the impacts this project has on surrounding properties,” said Gov. DeWine. “I will continue to challenge our teams to look for additional ways to further these goals.”

The passage of the Bipartisan Infrastructure Law earlier this year provided a once-in-a-lifetime funding opportunity to forge ahead on the Brent Spence Bridge Corridor project. The Ohio Department of Transportation (ODOT) and Kentucky Transportation Cabinet (KYTC) began a thorough review of former plans for a second bridge, as well as improvements to the entire eight-mile corridor between the Western Hills Viaduct in Ohio and Dixie Highway in Kentucky.

Revised plans for new companion bridge show dramatically reduced footprint

In 2012, the Federal Highway Administration (FHWA) approved a plan for a new companion bridge to the west of the existing Brent Spence Bridge that will address capacity and mobility issues that have plagued interstate traffic using I-71/75 to cross the Ohio River between Ohio and Kentucky.

Based on significant community engagement, as well as a thorough technical analysis, the footprint of the new bridge has been significantly reduced from the alternative approved in 2012. Initial plans included two 14-foot shoulders on each deck of the new bridge and expanded shoulder widths on the existing bridge. The new bridge was planned to cover nearly 25 acres and span nearly 150 feet in width. Revised plans show the new bridge at almost half the size of the 2012 footprint – covering approximately 14 acres and 84 feet in width. [Updated maps](#) show widened emergency shoulders on the existing Brent Spence Bridge to safely stow stalled vehicles, and 12-foot shoulders are provided on the new companion bridge. Additionally, interstate and local traffic are separated – the new companion structure will carry I-71/75 traffic; local traffic will use the existing Brent Spence Bridge.

“We felt good about where we were a decade ago because that solution provided additional capacity that reduces congestion and improves travel throughout the corridor,” said ODOT Director Jack Marchbanks. “We feel even better about this revision because it dramatically reduces the footprint of the new bridge and completely separates interstate and local traffic.”

With the exception of one commercial property, some railroad-owned land, and the negotiation of two utility owned properties, all parcels needed for the project have been acquired on the Ohio side of the project.

“For decades, the Kentucky Transportation Cabinet has completed ongoing maintenance work to ensure the safety and long-term viability of the Brent Spence Bridge,” KYTC Secretary Jim Gray said. “The Brent Spence plays a critical role in the solution being put forward and we are excited that our partners in Covington and other local municipalities in Kentucky have voiced their support for our current plan.”

In Kentucky, right of way activities have been divided into two categories – impacts north of 12th Street and impacts south of 12th Street. In both sections, the number of impacted parcels was reduced significantly following the 2022 plan revisions. In the southern portion, there are 38 impacted properties- one of which is a residential relocation; all property owners in the area have been contacted. Plans in the northern section are still being reviewed.

BrentSpenceBridgeCorridor.com source for project information

While bridge construction tends to be the public focus of the project, the bridge project only accounts for approximately one-third of the corridor project. The bi-state project team has launched a new website to provide information about the full corridor and to keep the public up to date on current planning and progress. BrentSpenceBridgeCorridor.com provides detailed information on interstate plans in Ohio, Kentucky, and both river crossings. Those interested in receiving regular project updates are encouraged to sign up for regular email updates to receive information on project status.

The project team is currently working on a second [federal grant application](#) that will be submitted by Aug. 9, and will continue to work with project partners on refining current plans, which call for breaking ground by the fall of 2023.

About the Brent Spence Bridge Corridor Project

Fueled by bipartisan cooperation and community engagement, the Brent Spence Bridge Corridor Project will invest in local communities and help grow America's economy. Spanning eight miles between the Western Hills Viaduct in Ohio and Dixie Highway in Kentucky, the project will address the second-worst truck bottleneck in the nation by improving safety and travel on the interstate connection that carries more than \$700 billion worth of freight every year.

In addition, the project will improve access to the central business districts of Cincinnati, Ohio, and Covington, Kentucky, and will also support local businesses and underserved communities in historic neighborhoods on both sides of the river. Ohio and Kentucky are working together to deliver this transformative project that will improve the quality of life for the millions of Americans who use the federal highway system to travel between the two states and beyond.

###



Source:

<https://www.transportation.ohio.gov/about-us/news/statewide/revamped-plans-shrink-brent-spence-corridor>

Governors DeWine and Beshear reveal revamped plans that shrink Brent Spence Corridor

July 15, 2022 | [ODOT](#)

BRENT SPENCE
BRIDGE CORRIDOR



COLUMBUS - Ohio Governor Mike DeWine and Kentucky Governor Andy Beshear today revealed updated bridge maps detailing new lane configurations and revamped plans to deliver the regionally-transformative Brent Spence Bridge Corridor Project with fewer property impacts.

”It has been important from the beginning to make sure we’re meeting the needs of today and tomorrow while also being mindful about the impacts this project has on surrounding properties,” said Gov. DeWine. “I will continue to challenge our teams to look for additional ways to further these goals.”

“While this is a nationally significant project, it is still very much a community-minded project,” said Gov. Beshear. “Our teams have gone to great lengths to shrink property impacts while still delivering a solid solution to the traffic congestion issues in the region.”

The passage of the Bipartisan Infrastructure Law earlier this year provided a once-in-a-lifetime funding opportunity to forge ahead on the Brent Spence Bridge Corridor project. The Ohio Department of Transportation (ODOT) and Kentucky Transportation Cabinet (KYTC) began a thorough review of former plans for a second bridge, as well as improvements to the entire eight-mile corridor between the Western Hills Viaduct in Ohio and Dixie Highway in Kentucky.

Revised plans for new companion bridge show dramatically reduced footprint

In 2012, the Federal Highway Administration (FHWA) approved a plan for a new companion bridge to the west of the existing Brent Spence Bridge that will address capacity and mobility issues that have plagued interstate traffic using I-71/75 to cross the Ohio River between Ohio and Kentucky.

Based on significant community engagement, as well as a thorough technical analysis, the footprint of the new bridge has been significantly reduced from the alternative approved in 2012. Initial plans included two 14-foot shoulders on each deck of the new bridge and expanded shoulder widths on the existing bridge. The new bridge was planned to cover nearly 25 acres and span nearly 150 feet in width. Revised plans show the new bridge at almost half the size of the 2012 footprint – covering approximately 14 acres and 84 feet in width. [Updated maps](#) show widened emergency shoulders on the existing Brent Spence Bridge to safely stow stalled vehicles, and 12-foot shoulders are provided on the new companion bridge. Additionally, interstate and local traffic are separated – the new companion structure will carry I-71/75 traffic; local traffic will use the existing Brent Spence Bridge.

“We felt good about where we were a decade ago because that solution provided additional capacity that reduces congestion and improves travel throughout the corridor,” said ODOT Director Jack Marchbanks. “We feel even better about this revision because it dramatically reduces the footprint of the new bridge and completely separates interstate and local traffic.”

With the exception of one commercial property, some railroad-owned land, and the negotiation of two utility owned properties, all parcels needed for the project have been acquired on the Ohio side of the project.

“For decades, the Kentucky Transportation Cabinet has completed ongoing maintenance work to ensure the safety and long-term viability of the Brent Spence Bridge,” KYTC Secretary Jim Gray said. “The Brent Spence plays a critical role in the solution being put forward and we are excited that our partners in Covington and other local municipalities in Kentucky have voiced their support for our current plan.”

In Kentucky, right of way activities have been divided into two categories – impacts north of 12th Street and impacts south of 12th Street. In both sections, the number of impacted parcels was reduced significantly following the 2022 plan revisions. In the southern portion, there are 38 impacted properties- one of which is a residential relocation; all property owners in the area have been contacted. Plans in the northern section are still being reviewed.

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External Resources

[Brent Spence Bridge Corridor Project](#)



OFFICE OF GOVERNOR ANDY BESHEAR

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Governors Beshear and DeWine and Senator McConnell Announce Brent Spence Bridge Corridor Project Awarded More Than \$1.6 Billion in Federal Grants

*Transformative project to build second bridge and improve interstates
forges ahead for late 2023 groundbreaking*

FRANKFORT, Ky. (Dec. 29, 2022) – Kentucky Gov. Andy Beshear, Ohio Gov. Mike DeWine and U.S. Senate Republican Leader Mitch McConnell announced today that the Brent Spence Bridge Corridor Project was awarded federal funding grants worth more than \$1.6 billion, giving the landmark bridge and corridor project the green light to move toward construction.

“I’m thrilled the time has finally come for us to get the companion bridge built,” **Gov. Beshear** said. “Funding and constructing the Brent Spence Bridge Corridor Project is more than the fulfillment of my administration’s promise – it’s a dream fulfilled for the thousands of travelers who pass through the bustling region every day waiting eagerly for traffic relief to come on this nationally significant corridor. It also shows what’s possible when we prioritize people over politics. Once complete, drivers will have a more enjoyable and efficient drive and we’ll have the

infrastructure in place to support the booming economy in this part of the state. Hats off to the Kentucky Transportation Cabinet and Ohio Department of Transportation, and the countless partners and advocates who played a role in today's monumental achievement, including the Kentucky General Assembly for the last budget allocation and Sen. McConnell for supporting the federal legislation. We appreciate President Biden and Secretary Buttigieg for this historic investment."

Sen. McConnell helped secure the funding for today's grant announcement by supporting last year's bipartisan Infrastructure Investment and Jobs Act (IIJA) and shepherding the bill through the Senate. The legislation authorized a historic investment in our nation's infrastructure and provides robust resources to states to fund major bridge projects specifically. Over five years, the IIJA will deliver billions of dollars to Kentucky to improve the commonwealth's roads, bridges, airports, railroads, waterways, broadband and more. Sen. McConnell contacted U.S. Secretary of Transportation Pete Buttigieg to advocate directly for Brent Spence Bridge Corridor Project funding.

"For decades, inadequate capacity on the aging Brent Spence Bridge has created headaches for drivers traveling between Kentucky and Ohio. Today, we're taking a major step toward fixing the problem," said **Sen. McConnell**. "Using my role as Senate Republican Leader, I stood with Sen. Rob Portman to break through gridlock and pass last year's bipartisan infrastructure deal, delivering record funding for landmark infrastructure projects including today's grant. Building a new companion bridge on the Brent Spence Bridge corridor will be one of the bill's crowning accomplishments, bringing long-awaited safety improvements, traffic relief and rejuvenated commerce to Northern Kentucky and Southwestern Ohio."

With funding secured, groundbreaking on the project is anticipated for late 2023, with substantial completion slated for 2029.

"Ohio and Kentucky have been discussing the Brent Spence Bridge Corridor Project for almost two decades, and now, we can finally move beyond the talk and get to work," said **Gov. DeWine**. "This project will not only ease the traffic

nightmare that drivers have suffered through for years, but it will also help ensure that the movement of the supply chain doesn't stall on this nationally significant corridor. My administration vowed to press the federal government to fund this project, and we're glad that they have recognized its significance. I'm grateful to the teams in both states who have worked so hard to make this project a reality."

The Brent Spence Bridge was constructed in the 1960s to carry around 80,000 vehicles a day, but the daily traffic load on Interstate Highways 75 and 71 has reached 160,000 vehicles in recent years. Because I-75 is a key freight corridor stretching from Canada to Florida, the congestion impacts commerce and commuters who travel the corridor in the eastern United States.

Project plans call for the construction of a companion bridge to the west of the existing Brent Spence Bridge, as well as improvements to the current bridge and the roadway network that ties into each river crossing. As a result of robust engagement with local partners, there will be enhanced pedestrian access across I-75 in Cincinnati to reconnect downtown with western neighborhoods and the City of Cincinnati will regain nearly 10 acres to develop in the downtown area. In Kentucky, the project will include a new storm sewer system to reduce flooding and improve local roads, including enhanced pedestrian and bicycle facilities, in the area of the existing and new bridge.

The project team – consisting of representatives from the Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT) – applied jointly for funding and have outlined terms to move the project to construction.

The once-in-a-lifetime competitive funding was made available through the passage of the federal Bipartisan Infrastructure Law that invested billions of dollars to revitalize infrastructure and spur the economy.

In July, Govs. DeWine and Beshear [announced](#) revised plans based on community engagement and technical analysis to shrink the project footprint while still delivering a transformative project that meets the needs of the region.

“Nothing great is achieved alone, and I’m proud of all the people who’ve come to the table over the years to ensure that we’ll deliver a project with huge benefits and minimal impacts to the communities that live in and around the project area,” KYTC Secretary Jim Gray said.

“This project doesn’t just move vehicles, it moves people. We’re able to reconnect neighborhoods to downtown Cincinnati, improve safety and reduce congestion on this route, all while only adding a single lane to the interstate with this project,” said ODOT Director Jack Marchbanks.

With funding secured, groundbreaking on the project is anticipated in late 2023 with larger construction activities in 2024. Substantial completion on the project is slated for 2029. The immediate next step will be the selection process to determine the Progressive Design Build contractor team to complete the project. That effort will begin in January when the Request for Proposals (RFP) is released to the contractor community.

The Federal Highway Administration recently required the project team to complete an analysis to better identify project cost estimates. Through this process, the project team determined the project is likely to cost about \$3.6 billion. The increase from previous estimates was not unexpected, due to the recent impact of inflation in construction prices. As a result, the project team is updating the project’s financial plan to account for the change.

About the Brent Spence Bridge Corridor Project

[Brent Spence Bridge Fact Sheet](#)

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###

Editors: Click [here](#) to view project renderings.

Brent Spence Bridge Corridor Project gets federal support

December 29, 2022 / [ODOT](#)



COLUMBUS — Ohio Governor Mike DeWine and Kentucky Governor Andy Beshear announce the awarding of federal funding grants worth \$1.635 billion for the Brent Spence Bridge Corridor Project – giving the landmark bridge and corridor project the green light to move toward construction.

“Ohio and Kentucky have been discussing the Brent Spence Bridge Corridor Project for almost two decades, and now, we can finally move beyond the talk and get to work,” said Governor DeWine. “This project will not only ease the traffic nightmare that drivers have suffered through for years, but it will also help ensure that the movement of the supply chain doesn’t stall on this nationally significant corridor. My administration vowed to press the federal government to fund this project, and we’re glad that they have recognized its significance. I’m grateful to the teams in both states who have worked so hard to make this project a reality.”

With funding secured, groundbreaking on the project is anticipated for late 2023, with substantial completion slated for 2029.

“I’m thrilled the time has finally come for us to get the companion bridge built,” Gov. Beshear said. “Funding and constructing the Brent Spence Bridge Corridor Project is more than the fulfillment of my administration’s promise – it’s a dream-fulfilled for the thousands of

Source: <https://www.transportation.ohio.gov/about-us/news/statewide/bsbc-grant-announcement>

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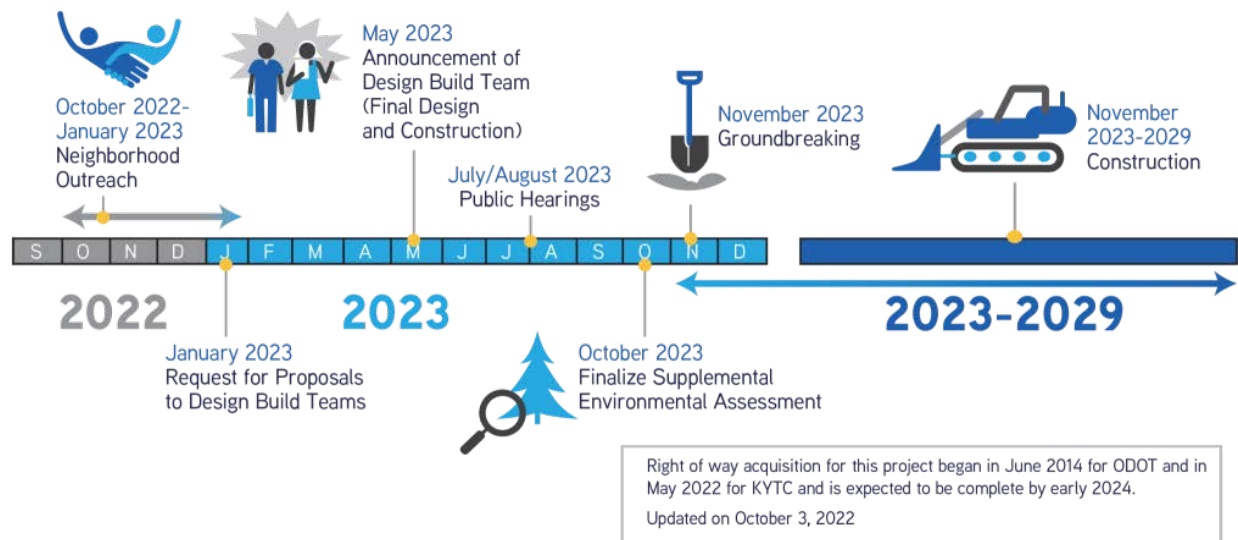
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Source: <https://www.transportation.ohio.gov/about-us/news/statewide/bsbc-grant-announcement>

Additional Downloads

[Renderings of the project](#)

Share this

For more information

[Matt Bruning](#)

ODOT Press Secretary

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External Resources

[Brent Spence Bridge Corridor project website](#)

Source: <https://brentspencebridgecorridor.com/firms-sought-to-kickstart-construction-on-brent-spence-bridge-corridor-project/>

FIRMS SOUGHT TO KICKSTART CONSTRUCTION ON BRENT SPENCE BRIDGE CORRIDOR PROJECT

TUESDAY FEBRUARY 21, 2023

Request for proposals issued just 1 month after states celebrate \$1.6 billion in federal grants

FRANKFORT, Ky. (Feb. 21, 2023) – Kentucky and Ohio have put the call out to firms interested in building one of the most significant infrastructure projects in the nation – the Brent Spence Bridge Corridor – just one month after Govs. Andy Beshear of Kentucky and Mike DeWine of Ohio joined bipartisan leaders to celebrate a historic \$1.635 billion federal investment to move the project forward.

A request for proposals (RFP) to provide construction and design services on the long-awaited project was released by the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC).

This contract addresses six of the eight miles of the total corridor; five miles of the I-71/75 corridor in Kentucky and one mile of I-75 in Ohio. It includes improvements to the Brent Spence Bridge and a companion bridge to the west. Work on the two northernmost sections of the corridor in Ohio will be done under separate contracts.

“Just a month ago, we celebrated a historic \$1.635 billion in federal grant funding to build the new bridge crossing over the Ohio River and improve the entire Brent Spence Bridge Corridor with no tolls,” said Gov. Beshear. “This is a huge step that gets us one step closer to fulfilling the dreams of thousands of travelers by providing traffic relief, increased safety and a boost to our nation’s commerce.”

“When I’ve asked people in southwest Ohio what issues need to be addressed, for many, many years the Brent Spence Bridge has consistently been at the top of the list,” said Gov. DeWine. “Today, we’re closer than ever to beginning work on this transformational project.”

Responses to the RFP are due March 31, 2023. The schedule calls for the design-build team to be selected in May, allowing for planning to take place immediately and initial construction work to begin before the year’s end.

This project will be delivered using a delivery method known as “progressive design-build.” Unlike construction projects that typically use a lowest-bidder approach, the progressive design-build contract is awarded based on qualifications, the best overall approach and value. This allows the design-build team the opportunity to collaborate with the bi-state project team and local stakeholders early in the process, so potential risks can be assessed and mitigated while there is still an opportunity to influence them. It also helps the bidder and the project team avoid excessive cost overruns, because the design-build team will have more thorough knowledge of the project’s design and risks before negotiating a price.

“The progressive design-build process is the right delivery approach based on the complexity of this project,” said KYTC Secretary Jim Gray. “Working in collaboration with the contractor during the design process will bring more innovative design ideas to the table and improve the project overall.”

“Our bi-state project team has spent a lot of time preparing for this work and reaching out to the public to ensure those within the project area have had a voice in the process,” said ODOT Director Jack Marchbanks. “This project connects neighborhoods, states, and the nation. We can’t wait to get started.”

The states earned two federal grants including the recent U.S. Department of Transportation award of \$250 million for the project from the new National Infrastructure Project Assistance (MEGA) discretionary grant program to go along with the \$1.38 billion from the Bridge Investment Program.

Kentucky and Ohio also signed an Interstate Cooperative Agreement that allows both states to begin preparing for construction. The agreement defines the roles and responsibilities for procurement, funding, construction and maintenance of the Brent Spence Bridge Corridor Project.

Groundbreaking for the Brent Spence Bridge Corridor Project is expected to take place in late 2023, with construction beginning in earnest in 2024 and a substantial completion goal of 2029.

Click [here](#) to view the request for proposals.

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OFFICE OF GOVERNOR ANDY BESHEAR
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**Firms Sought to Kickstart Construction on Brent Spence Bridge Corridor
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About the Brent Spence Bridge Corridor Project

The Brent Spence Bridge Corridor project is designed to improve approximately eight miles of I-71/I-75 through Kentucky and Ohio, including the addition of a new companion bridge to the existing Brent Spence Bridge to reduce congestion, improve traffic flow and safety, and maintain key regional and national transportation corridors. Learn more about the Brent Spence Bridge Project at <https://brentspencebridgecorridor.com/>.

###

TEAM
KENTUCKY™



For official news and updates from the Beshear/Coleman administration, visit governor.ky.gov/news.



ODOT Director joins Cincinnati Mayor Pureval for walking tour of West End

May 22, 2023 | [ODOT](#)



CINCINNATI - The Brent Spence Bridge Corridor project is more than just about crossing the Ohio River; it's about improving connections between downtown Cincinnati and West End neighborhoods.

Today, Ohio Department of Transportation Director Jack Marchbanks walked with Cincinnati Mayor Aftab Pureval to tour neighborhoods around the I-75 corridor. The tour included locations along Linn Street, West Court Street, Ezzard Charles Drive, Winchell Avenue, Wade Walk, and West Liberty Street.

ODOT is working with the city on Rebuilding American Infrastructure with Sustainability and Equity (RAISE) Grant improvements at Linn Street and 8th Street that overlap with the Brent Spence Bridge Project. The agency also plans new development and replacement of a pedestrian bridge over Winchell Avenue and

provided \$200,000 for improvements to the Queensgate Playfield.

"I understand the impact of building an interstate highway through a neighborhood home to thousands of minority families. My childhood home in Dayton was taken by the construction of I-75 in the 1960s. This project will help reconnect the West End to downtown by creating new connections and strengthening existing ones across the interstate," said ODOT Director Jack Marchbanks. "As an avid cyclist myself, I am thrilled to see that we're focused not just on moving vehicles but moving people with all modes of transportation."

Cincinnati has been an engaged partner throughout the planning process. The city expressed a desire to create an urban feel with the connections across I-75 to make it more accessible and inviting for pedestrians and cyclists, and the project team is working toward this goal.

"I appreciate the opportunity to continue working with Director Marchbanks and ODOT to discuss opportunities to improve safety, mobility, and connections for those living in and traveling through Cincinnati," said Mayor Pureval.

In addition to reconnecting downtown to western neighborhoods, the project team has significantly reduced the footprint of the project, freeing up nearly 10 acres of contiguous developable land for the city. Once the design-build team is named, one of their first tasks will be to look for feasible ways to add to that total.

"While this project has been refined over the years, we still think more can be done to make a great project even better. We remain committed to working with our local partners like the city, Hamilton County, and other key stakeholders to stay on budget and on schedule," Marchbanks said.

This phase of the project addresses six of the eight miles of the total corridor; five miles of the I-71/75 corridor in Kentucky and one mile of I-75 in Ohio. It includes improvements to the Brent Spence Bridge and a companion bridge to the west. Work on the two northernmost sections of the corridor in Ohio will be done under separate contracts.

Groundbreaking for the Brent Spence Bridge Corridor Project is expected to take place later this year, with construction beginning in earnest in 2024 and completion by 2029.



Governor DeWine, Beshear announce Brent Spence Bridge Design-Build Team

July 27, 2023 | [ODOT](#)



(COLUMBUS) – Ohio Governor Mike DeWine and Kentucky Governor Andy Beshear today announced that Walsh Kokosing has been awarded the design-build contract for the Brent Spence Bridge Corridor Project.

The governors made the announcement during a news conference in Covington, Kentucky, this afternoon. They were joined by officials from the Ohio Department of Transportation (ODOT), Kentucky Transportation Cabinet (KYTC), and Federal Highway Administration (FHWA).

“It’s great that Kokosing, an Ohio-based company, will play a pivotal role in the Brent Spence Bridge Corridor Project,” said Governor DeWine. “Both Kokosing and Walsh have a long history of working with the Ohio Department of Transportation, and I’m confident that we have the best people on both sides of the river to see this project to completion.”

As prime contractor, the Walsh Kokosing team will continue to recruit participant companies and workers for the \$3.6 billion project, which is one of the most significant infrastructure megaprojects in the nation. Approximately \$3.1 billion will be spent on the design-build portion of the project. The remaining \$500 million is allocated for improvements on a two-mile stretch of the I-75 corridor north of Linn Street in Cincinnati, including a new interchange for the Western Hills viaduct.

“Today shows what’s possible when folks from different political parties, different states, and different levels of government all work together to do what’s right for our people,” said Governor Beshear. “With the help of this new design-build team, we are one step closer to building the companion bridge without tolls and providing the many families, communities and businesses that rely on this vital commercial link with new and exciting growth and development opportunities.”

The contract addresses six of the corridor's eight total miles, including five miles of I-71/75 in Kentucky and one mile of I-75 in Ohio. It also includes improvements to the Brent Spence Bridge and the construction of a new companion bridge to its immediate west. Work on the two northernmost miles of the corridor in Ohio will be done under separate contracts.

The primary goals of the project include improving safety and traffic flow and maintaining connections to key regional and national transportation corridors.

"Working in concert with our local partners, we've heard loud and clear that they want to ensure this project is not only a benefit to traffic traversing the tri-state, but that it also has a positive impact on those who live here," said ODOT Director Jack Marchbanks. "We've freed up nearly 10 acres of developable land and remain committed to minimizing the footprint of the project while maximizing the benefits to Cincinnati and the surrounding neighborhoods."

"This is one of the largest highway construction projects ever undertaken in the U.S.," said KYTC Secretary Jim Gray. "We are confident that this experienced team, with their solid track record of completing projects approaching this size, possesses the talent, skills, and know-how to deliver this complex bridge and highway system."

"President Biden's Investing in America agenda is moving critically needed projects forward in Kentucky, Ohio, and all across the country that will make travel safer, move goods to market more quickly, and improve the daily lives of countless Americans," said Federal Highway Administrator Shailen Bhatt. "We are proud to invest in the success of this project and partner with Governors Beshear and DeWine as they reach a key milestone on the road to delivering the Brent Spence Bridge Corridor Project on time, on task, and on budget."

About the Brent Spence Bridge Corridor Project

The Brent Spence Bridge Corridor Project is designed to improve approximately eight miles of Interstates 71 and 75 through Kentucky and Ohio, including the addition of a new companion bridge to the west of the existing Brent Spence Bridge to reduce congestion, improve traffic flow and safety, and maintain key regional and national transportation corridors. More information about the Brent Spence Bridge Project is available at [BrentSpenceBridgeCorridor.com](https://www.BrentSpenceBridgeCorridor.com).

GOVS. DEWINE, BESHEAR ANNOUNCE BRENT SPENCE BRIDGE DESIGN-BUILD TEAM

MEDIA CONTACTS

ODOT: [Matt Bruning](#) – (614) 512-5121

KYTC: [Chuck Wolfe](#) – (502) 892-9038

THURSDAY JULY 27, 2023

(COVINGTON, Kentucky) – Ohio Governor Mike DeWine and Kentucky Governor Andy Beshear today announced that Walsh Kokosing has been awarded the design-build contract for the Brent Spence Bridge Corridor Project.

The governors made the announcement during a news conference in Covington, Kentucky, this afternoon. They were joined by officials from the Ohio Department of Transportation (ODOT), Kentucky Transportation Cabinet (KYTC), and Federal Highway Administration (FHWA).

“It’s great that Kokosing, an Ohio-based company, will play a pivotal role in the Brent Spence Bridge Corridor Project,” said Governor DeWine. “Both Kokosing and Walsh have a long history of working with the Ohio Department of Transportation, and I’m confident that we have the best people on both sides of the river to see this project to completion.”

As prime contractor, the Walsh Kokosing team will continue to recruit participant companies and workers for the \$3.6 billion project, which is one of the most significant infrastructure megaprojects in the nation. Approximately \$3.1 billion will be spent on the design-build portion of the project. The remaining \$500 million is allocated for improvements on a two-mile stretch of the I-75 corridor north of Linn Street in Cincinnati, including a new interchange for the Western Hills viaduct.

“Today shows what’s possible when folks from different political parties, different states, and different levels of government all work together to do what’s right for our people,” said Governor Beshear. “With the help of this new design-build team, we are one step closer to building the companion bridge without tolls and providing the many families, communities and businesses that rely on this vital commercial link with new and exciting growth and development opportunities.”

The contract addresses six of the corridor's eight total miles, including five miles of I-71/75 in Kentucky and one mile of I-75 in Ohio. It also includes improvements to the Brent Spence Bridge and the construction of a new companion bridge to its immediate west. Work on the two northernmost miles of the corridor in Ohio will be done under separate contracts.

The primary goals of the project include improving safety and traffic flow and maintaining connections to key regional and national transportation corridors.

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PUBLIC INVOLVEMENT SUMMARY

This report consists of 4 parts. [Click below to access each part.](#)

[Part 1](#)

Report Sections 1-9

Appendix A: Draft Travel Survey Analysis

Appendix B: Project Advisory Committee

Appendix C: Aesthetic Committee

Appendix D: Stakeholder and Public Outreach

Activities Appendix E: E-Newsletters

Appendix F: Press Releases

[Part 2](#)

Appendix G: Targeted Environmental Justice/Neighborhood Outreach Meeting Summaries

[Part 3](#)

Appendix H: 2012-2021 Comments and Responses

Appendix I: 2022-Present Comments and Responses

Appendix J: Alternative Concepts Letters and Responses

Appendix K: Bridge Forward Coalition

Appendix L: Westway Comments and Responses

Appendix M: Response to Cincinnati & Chamber Comments

[Part 4](#)

Appendix N: Cincinnati Process Improvements Emails

Appendix O: Neighborhood Outreach Comments and Responses

Appendix P: Open-House Project Update Meetings

Appendix Q: Public Engagement Plan

