

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



BRENT SPENCE BRIDGE CORRIDOR PROJECT

# **PUBLIC INVOLVEMENT SUMMARY**

## **Part 2**

KYTC PROJECT ITEM NO. 6-17 | ODOT PID 89068  
JANUARY 11, 2024

This report consists of 4 parts. Click to access [Part 1](#), [Part 3](#), and [Part 4](#).



---

# PUBLIC INVOLVEMENT SUMMARY

This report consists of 4 parts. Click below to access each part.

## Part 1

Report Sections 1-9

Appendix A: Draft Travel Survey Analysis

Appendix B: Project Advisory Committee

Appendix C: Aesthetic Committee

Appendix D: Stakeholder and Public Outreach

Activities Appendix E: E-Newsletters

Appendix F: Press Releases

## Part 2

Appendix G: Targeted Environmental Justice/Neighborhood Outreach Meeting Summaries

## Part 3

Appendix H: 2012-2021 Comments and Responses

Appendix I: 2022-Present Comments and Responses

Appendix J: Alternative Concepts Letters and Responses

Appendix K: Bridge Forward Coalition

Appendix L: Westway Comments and Responses

Appendix M: Response to Cincinnati & Chamber Comments

## Part 4

Appendix N: Cincinnati Process Improvements Emails

Appendix O: Neighborhood Outreach Comments and Responses

Appendix P: Open-House Project Update Meetings

Appendix Q: Public Engagement Plan



---

**Appendix G:  
Targeted Environmental Justice/  
Neighborhood Outreach Meeting Summaries**





---

# Brent Spence Bridge Corridor Project

## CUF Meeting Summary

### November 15, 2022

---

#### Introduction

The CUF meeting was held on November 15, 2022 from 7:00 pm to 9:00 pm. The meeting was held at Hughes STEM High School located at 2515 Clifton Avenue in Cincinnati, Ohio. It was publicized through various CUF neighborhood resources including: webpage, Facebook, the November newsletter, and via an announcement at a prior neighborhood meeting (see Attachment 1). Attendees at the meeting included the City of Cincinnati, the project team, members of the CUF Neighborhood Association, and members of the public. In total, 40 people attended the meeting, including ten individuals from the City and the project team (see Attachment 2). Photographs are included in Attachment 3.

The meeting was a regularly scheduled meeting of the CUF Community Council with members attending both in-person and remotely. Members of the Community Council and other groups gave various presentations and updates. During that time, ODOT gave a formal presentation about the Brent Spence Bridge (BSB) Corridor Project (see Attachment 4). After the meeting, those present viewed project exhibits, and the project team responded to questions and comments on a one-on-one basis.

Exhibits showing the renderings of Findlay Street, Ezzard Charles Drive, an aerial near Ezzard Charles Drive, and a view of the bridges from Kentucky were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](https://www.PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. No comment forms or demographic surveys were returned at the meeting.

#### Comments

Summary of comments/questions received during presentation:

- Will there be bike lanes and/or pedestrian facilities over the river?
  - No, the scope of the project does not include these facilities on either bridge. The Clay-Wade Bailey Bridge is the designated bicycle crossing over the Ohio River.

Questions asked one-on-one after the meeting included:

- Various questions about local street and ramp alignments and connections to the interstate.
- How the project will be constructed?
- How the project will be funded?
- Various questions about where right-of-way will be required and if those property owners have been resistant.
- Various questions about the aesthetics of the streets and ramps once the project is constructed.
- How debris on the interstate will be handled.

All questions asked during the presentation and after the formal meeting were answered by the project team, and no opposition to the project was expressed.

No comment sheets or demographic surveys were returned at the meeting; however, comments may be submitted via [www.PublicInput.com/bsbc](https://www.PublicInput.com/bsbc) through January 5, 2023. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.



---

Attachment 1: Advertising Materials

[Back](#)

[Add to my calendar](#) 

## CUFNA General Membership Meeting

When **11/15/2022**  
7:00 PM - 9:00 PM

Location Hughes High School



Hybrid Virtual / In Person Meeting - A link to the virtual meeting will be provided to those on our mailing list. If you would like to be added, please send us a request by going to our contact page.

If attending in person, the meeting will be at Hughes High School on the first floor toward the back of the building. Enter at the main entrance. Signs on the door and a CUFNA member will direct you when you walk through the main entrance.


Note: Representatives from the Brent Spence Bridge (BSB) Corridor Project will be presenting at the November General Meeting to share updates and get feedback.

CUF Neighborhood Association is a 501(c) 3 non-profit organization, 2364 McMicken Avenue, Cincinnati, Ohio 45214.

# CUF Neighborhood Association Facebook Page







Facebook.com/CUFNA

Email or phone Pas

 CUF Neighborhood Association


...


## Intro


-  Page · Nonprofit organization
-  Cincinnati, OH, United States · Ohio
-  [cufna@cufna.org](mailto:cufna@cufna.org)
-  [cufna.org](http://cufna.org)
-  Rating · 5.0 (5 Reviews) 


## Photos



[See all photos](#)




[Privacy](#) · [Terms](#) · [Advertising](#) · [Ad Choices](#)  · [Cookies](#) · [More](#) · Meta © 2022

 Like

 Comment

 CUF Neighborhood Association  
October 27 at 11:55 AM · 

Members of the Brent Spence Bridge Project will be at the next CUFNA general meeting on November 15th.



## You're Invited

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of [neighborhood outreach meetings](#) for the Brent Spence Bridge (BSB) Corridor Project.

**Purpose:** To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team.


**When:** November 15, 2022 | 7:00pm  
(During the regular meeting for the CUF Neighborhood Association)


**Where:** Hughes STEM High School  
2515 Clifton Avenue, Cincinnati, OH 45219


## Hope to see you there!

If you need assistance attending this meeting, please call Keith Smith at [513.932.6500](tel:513.932.6500).

View the most recent project newsletter at the following link:  
<https://mitchell.mpls.com/2022/11/04/brms-spmc-a-bridge-a-community>

 2


 Like


 Comment

# CUF Neighborhood Association Facebook Page

Facebook.com/CUFNA

Accessed November 20, 2022





## CUF Neighborhood Association

525 likes • 632 followers


Posts About Photos Videos

### Intro

- Page · Nonprofit organization
- Cincinnati, OH, United States, Ohio
- cufta@cufta.org
- cufta.org
- Rating · 5.0 (5 Reviews)

### Photos

See all photos





### CUF Neighborhood Association

Posting an ODOT survey regarding Brent-Spence / I-75 project.

**WE WANT TO HEAR FROM YOU!**


Please scan the code to leave your feedback or visit: [PublicInput.com/bsbc](https://PublicInput.com/bsbc)





Fueled by bipartisan cooperation and community engagement, the Brent Spence Bridge Corridor Project will invest in local communities and help grow America's economy. Spanning eight miles between the Western Hills Viaduct in Ohio and Dixie Highway in Kentucky, the project will improve safety and travel on an interstate connection that carries more than \$700 billion worth of freight every year.

In addition, the project will improve access to the central business districts of Cincinnati.



Like Comment

### CUF Neighborhood Association

Privacy · Terms · Advertising · Ad Choices · Cookies · More



## Seth Turner

---

**From:** Jack Martin [REDACTED]  
**Sent:** Tuesday, October 25, 2022 3:51 PM  
**To:** Sherry Kish  
**Cc:** Seth Turner; Jodi Heflin  
**Subject:** Re: Brent Spence Bridge Presentation on November 15th

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hi Sherry, Seth -

We can get some notice out before the 1st, if you can provide us with the basic info, and what you are looking for.

Thanks! - Jack

Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous neighborhood meeting?	X	We can put a notice on our website.
Email list	Can you email a neighborhoodwide or citywide email list with information about BSB presentation at the upcoming meeting?	X	We would like to have a brief description and before and after drawings.
Facebook (private to neighborhood)	Can you share a Facebook message about BSB presentation?	X	"
Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?	Probably wouldn't be effective.	
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?	Probably wouldn't be effective.	
Other	Is there another outlet you have available to get the word out? -	-	-
Do you need assistance from us with advertising?	Do you need the meeting information as an email or flyer?	Yes	PDF description and before and after drawings.

On Tue, Oct 25, 2022 at 3:37 PM Sherry Kish [REDACTED] wrote:

Jack,

Thank you so much for adding us to your agenda for your November CUF Neighborhood meeting for the Brent Spence Bridge (BSB) project team to share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

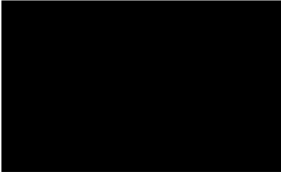
Our goal is to be sure all notifications about meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

<b>Media Outlet</b>	<b>Description</b>	<b>Can do this (mark with an X)</b>	<b>Completed? (Please provide date completed for Federal tracking.)</b>
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous neighborhood meeting?		
Email list	Can you email a neighborhoodwide or citywide email list with information about BSB presentation at the upcoming meeting?		
Facebook (private to neighborhood)	Can you share a Facebook message about BSB presentation?		
Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?		
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?		
Other	Is there another outlet you have available to get the word out?		
Do you need assistance from us with advertising?	Do you need the meeting information as an email or flyer?		

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!  
Sherry

**Sherry R. Kish**



**HNTB CORPORATION**

88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

 **100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

--

Jack Martin





NEIGHBORHOOD  
ASSOCIATION

# Newsletter

REPRESENTING CLIFTON HEIGHTS • UNIVERSITY HEIGHTS • FAIRVIEW

November, 2022

**Next monthly meeting:** November 15, 2022, hybrid format, 7 p.m.  
Google Meet and In-Person at Hughes High School, 1st floor

## CUF Neighborhood Association General Meeting

Meeting Commenced on Tuesday, October 18,  
2022 at 7:01pm

Safety Update:

CPD District 5:

Officer Shawn Dent shared the crime statistics.  
He also reported that:

- There is a nationwide increase in auto thefts of Hyundais and Kias, and Cincinnati is affected. The District 3 sergeant received a grant for a limited number of steering wheel locks.
- CPD is still following up on the large parties, which occurred on Flora St. and Rohs St.
- To increase pedestrian safety in the area, police cars have been parked at key intersections during peak hours to encourage vehicles to be vigilant. They are also working to educate students on pedestrian safety.
- There is a residential street calming program, which residents and neighborhoods can apply for. Eligible streets are assessed using a comprehensive point system. If at least 50% of residents on an eligible street sign a petition, then the city will conduct additional assessments and determine next steps.
- There will be a city-wide traffic blitz: police will be doing a speed assessment in areas of concern; if it's determined that the area has a speeding problem, then police can look into targeting specific areas.
- A number of residents expressed concerns about parking violations in the area. If there are parking issues that require enforcement, call and report it.

UC Police:

Jim Vestring reported that:

- Per the MOU, UCPD is not permitted to make traffic stops, however, UCPD has implemented high visibility patrols at designated intersections to assist with pedestrian safety during peak

hours.

- There are also event signs placed near key intersections that alert motorists that they are entering a pedestrian area. They will be installing three additional signs.
- DOTE will place paddles at MLK and Jefferson.
- DOTE has also been given a grant to help increase pedestrian safety in the area. The funds will be available in 2023 for installation in 2024.
- On 10/28/2022 there will be a Kane Brown charity concert at Fifth Third Arena.
- There are three upcoming football home games: 11/05/2022 vs. Navy (Game Time TBA); 11/11/2022 vs. East Carolina at 8:00 pm, and a senior night on 11/25/2022 at either 12:00pm or 3:30pm vs., Tulane
- Basketball Season starts on 11/07/2022. There are also games on 11/10/2022 vs. Cleveland State and 11/13/2022 vs. EKU.

Guests:

Emma Daniel, UC Student and Co-President of Clean Up Cincy: Emma is in Rob Hyland's environmental studies class and is working on a complete streets design survey for an undergraduate research project. The survey is about the Clifton Ave. corridor from MLK to McMillan St. Anyone who uses the street is eligible to complete the survey. Emma provided flyers with a QR code, and sent the QR code and a link to Jack Martin to distribute.

District at Clifton Heights Update:

Mark Becher reported that they are making progress on block A1 at Stratford Ave. and Straight St. The majority of the streetscaping is complete for the time being and they are working to move the utilities below ground. They are also working on pouring the concrete columns and slabs for the foundation.

Tim Westley reported that the permit for the foundation of the hotel at the northwest corner of Clifton Ave and Straight St. has been approved and they have equipment onsite. It is on a 22-month schedule with the completion

date set for June of 2024. They will bump out the fencing to the parking line, and all construction activities will occur inside of the fence.

Mary Kate Genis, League of Women Voters: Mary Kate provided copies of the LWV non-partisan voter guide and reviewed the key issues on the ballot for November's election. Additional copies of the voter guide are available at the public library, or the information can be accessed at <https://www.vote411.org/>

Graham Beck, Assistant Location Manager of Stoplight Pictures, Inc.: Graham reported that they will be filming a movie called The Bikeriders in and around the Cincinnati Area. On Monday, 10/24/2022 they will be filming on W. McMicken Ave. between Brighton and Ravine Street from 7:00am to 5:00 pm. The scene involves multiple motorcycles. Graham provided flyers with contact information for anyone who wanted to reach out with any additional comments or concerns.

Old Business / Reports/Announcements:

- Treasurer's Report: Linda Ziegler reported that CUFNA has received funds from the concession sales at the Bellevue Park events.
- Street Party Meeting Between City, UC, and CUFNA: Jack Martin reported that they are working on setting up a meeting, but it has been delayed due to the pedestrian safety issues in the area.
- Pedestrian Safety Meeting Between UC, UCPD, City, and Neighborhoods: Jack Martin reported that this meeting is pending.
- CRC-Clifton Recreation Center: Not Present
- LSDMC / CPS:
- CANS:

Tracy Schwetschenau reported that:

- CANS had its first LSDMC in-person meeting last month, and it was a joint meeting with the PTO.
- On 11/15/2022 from 11:00 am—2:00 pm there will be an up-for-grabs-day at the community pantry. They are looking for donations including gently used clothing, hats, gloves, toiletries and household cleaning items.
- Julia Bonfield reported that the state-issued report cards are complete, and CANS is at or above state recommendations in every category.

- Hughes HS: Lyn Martin reported that:
- Hughes is collecting old sneakers for a fundraiser. If anyone has sneakers that they wish to donate, they can either bring them to the next general meeting or drop them off at the school.
- The Blink display was a collaboration between Hughes and the University of Cincinnati.

- CHCURC (Clifton Heights Community Urban Redevelopment Corporation):
- Chip Kussmaul reported that Block 1 is active again and they will be interviewing three potential architects for the site as part of an RFQ.
- Brenden Regan provided an update on the housing initiative.
- NOU (Neighborhoods of Uptown): No updates at this time.
- Gateway Lofts: Jack Martin reported that they have obtained the building permits.
- Hallmar / Fountain Development (Central Parkway and Marshall): Jack Martin reported that there will be a public staff conference on 10/24/2022.
- CUFNA IT: Mary Singler reported that the new website and membership system are both up and running. There is now a members only page with resources from general meetings, and membership dues can be paid online. There is also an email notification system, which is set to be activated the week of the 23rd. If there are any members that have trouble logging in, please reach out to Mary.
- CBRs (Community Budget Requests): Jack Martin reported that the requests have been submitted but that he doesn't expect to hear anything back until after the 1st of the year.
- KCB (Keep Cincinnati Beautiful): Maureen France reported that they are in the process of spending the grant money on the new community garden and the solar power lighted stop signs. There has also recently been a huge clean-up event from KCB with the help of volunteers from Greek Life at the University of Cincinnati, and Clean-Up Cincy.
- METRO BRT (Bus Rapid Transit) Plan: Jack Martin reported that there is a last chance meeting on 10/19/2022 from 4:30pm—6:30pm at the Clifton United Methodist Church located at 3416 Clifton Avenue.
- Hazardous Household Products Drop Off Event: Jack Martin reported that there will be a Hazardous Households Drop-Off event on 10/21/2022 and 10/22/2022. Registration is required. Anyone interested can visit <https://www.hamiltoncountyr3source.org/> for more information.
- 2023 Green Cincinnati Plan: Jack Martin reported that there is an ongoing focus group for Green Cincinnati. Anyone who is interested can Google it to obtain additional information.
- GoVibrant Million Steps Challenge: Lyn Martin reported that CUF came in second place.

Meeting Adjourned at: 8:27 pm.

Minutes taken by: Andrea Schwallie

## CUF Neighborhood Association Financial Report: October., 2022

Balance on: 9-30-2022                      \$6,996.33

Income:	Amount:
Dues	10.00
Dues	10.00
Dues	10.00
Dues	10.00
Dues	10.00
Dues	20.00
Square Bellevue party	125.90
Square Bellevue party	561.83
Square Bellevue party	335.00
Square Bellevue party	45.88
Square Bellevue party	51.30
Tower fund loan, Garden	26,500.00
Tower fund loan, solar sign	12,500.00

Total Income:                                      + \$40,189.91

Expenses:	
Wild Apricot fee	1.71
Detmer survey, KCB	875.00
Hater, Bellevue party	66.83
Ohio Valley Wine	177.54
Esoteric Brewing	201.42
Genesis Events	350.00
GCWW	10.41
Northside Bank VISA	591.40
Esoteric Brewing	204.62
Ohio Valley Wine	118.36

Total Expenses:                                      - \$2,597.29

Balance on: 10-31-2022                      \$44,588.95



## Uptown Consortium, Inc.

Uptown Consortium, Inc. (UCI) received \$45 million in New Markets Tax Credits, which it will use to spur development in transformational projects in Uptown Cincinnati.

UCI was one of five Ohio entities to receive Federal New Markets Tax Credits (NMTCs), among 107 across the U.S., and the only community development organization in Cincinnati to be awarded.

NMTCs are designed to incentivize investment in low-income communities by attracting the private investment necessary to reinvigorate these areas.

The NMTC tool has been of immense value to UCI in advancing its mission to revitalize Uptown Cincinnati. Before this recent (and third) NMTC award, UCI has been awarded \$130 million in NMTCs since 2009 and, in partnership with the Cincinnati Development Fund, managed an additional \$52 million allocation as part of the Uptown Cincinnati Development Fund (UCDF). Some previous UCI NMTC projects include Digital Futures, UC Gardner Neuroscience Institute, Avondale Town Center, USquare @ the Loop and the Corryville Kroger.



## Burnett Woods service

Preserve Burnett Woods  
Honeysuckle removal.



Date: Saturday, Nov 12, 2022  
Time: 9:00 AM - 12:00 PM

Join Preserve Burnett Woods HABITAT HEROES for Saturday work days for in the coming fall! The work days will focus on honeysuckle eradication.

Additional future dates:  
November 19, December 3 and December 10.

To volunteer, [click here](#).

## Bridge presentation

Shelly Kish, with NTHB (engineering consultant), will be presenting the Brent Spence Bridge project at the November 15 CUF Neighborhood Association general meeting (7pm). The presentation is in accordance with the Federal Environmental Justice requirements.



concept designs

## Old CUF

A photograph taken by the Cincinnati Highway Dept. in 1922 of Clemmer Avenue and Valley View Court shows the area before a number of houses had been built. McMillan Street had not yet been extended down to Central Parkway. Clemmer now meets McMillan where a Model T Ford is parked. A recent photo below shows the same three houses.



photo source: University of Cincinnati Libraries, Digital Resource Commons

# **DISCUSS THE BENEFITS AND CHALLENGES OF OFF-CAMPUS HOUSING AT UC**

**\$10 AMAZON GIFT CARD INCLUDED FOR ALL PARTICIPANTS**



**Whether you are a student or non-student resident, for just 60-90 minutes, participate in a focus group discussion regarding the current state of housing near the University of Cincinnati. If you live in the CUF, Mt. Auburn, and Corryville neighborhoods, come discuss the social, cultural, physical, and economic benefits and challenges related to housing, as well as a two-part map exercise in which such discussions are visually brought to life.**

**All participants are provided with a \$10 Amazon Gift Card at the completion of the focus group discussion.**

**If you are interested in participating, contact me at:  
becker13@miamioh.edu  
513-601-4923**





**CLIFTON HEIGHTS  
UNIVERSITY HEIGHTS  
FAIRVIEW  
NEIGHBORHOOD ASSOCIATION**

2364 West McMicken Avenue, Cincinnati, Ohio 45214

Non-Profit Organization  
U.S. Postage Paid  
Cincinnati, Ohio  
Permit No. 4834

*Funded by the City of Cincinnati*



## CUF membership

Any person subscribing to the purpose of the CUF Neighborhood Association, Inc. and paying dues set by the Association may become a member. Election of Trustees is held at the annual meeting in July. Eligible voters are residents of the community who are at least 18 years of age and who are fully paid members of the

Association and who have attended three general meetings after payment of dues during the year prior to the annual meeting. Non-resident members have voice but no vote in Association meetings and may not hold elective office.

Annual CUF dues are \$10.00.

- CUF general meetings: 3<sup>rd</sup> Tuesday of each month, except August and December, 7:00 p.m, Hughes High School, 1st floor, 2515 Clifton Ave.
- CUF Trustee meetings: 1<sup>st</sup> Tuesday of each month, 7:00 p.m, CHCURC office meeting room.
- CUF annual meeting and election: 3<sup>rd</sup> Tuesday in July, 7:00 pm, Hughes High School, 1st floor, 2515 Clifton Avenue.



## CUF Neighborhood Association

2364 West McMicken Avenue  
Cincinnati, Ohio 45214

Name: \_\_\_\_\_

Address: \_\_\_\_\_

Phone: (     ) \_\_\_\_\_

Email: \_\_\_\_\_

Membership Type:

CUF resident \_\_\_\_\_

Non-voting Membership: \_\_\_\_\_

Membership fee: \$10.00 per year.  
Make checks payable to CUFNA.

W W W . C U F N A . O R G



---

Attachment 2: Sign-In Sheets



**NEIGHBORHOOD ASSOCIATION**  
representing Clifton Heights | University Heights | Fairview

DATE \_\_\_\_\_

Please print

NAME	ADDRESS	PHONE or EMAIL	RESIDENT or ORGANIZATION
------	---------	----------------	--------------------------

1. Phillip P. Lane      BBE      [REDACTED]

2. John Stok      [REDACTED]

3. Stefan Spinos      [REDACTED]      [REDACTED]      ~~BBE~~ org.

4. Kathleen Tucker      [REDACTED]      [REDACTED]

5. Tommy Arnold      "      [REDACTED]      oDOT

6. MARK JAMBALH      [REDACTED]      [REDACTED]      RES

7. \_\_\_\_\_

8. \_\_\_\_\_



NEIGHBORHOOD ASSOCIATION

representing Clifton Heights | University Heights | Fairview

DATE \_\_\_\_\_

Please print

NAME	ADDRESS	PHONE or EMAIL	RESIDENT or ORGANIZATION
------	---------	----------------	--------------------------

1. Kathie Smith      ODOT D9      [REDACTED]      ODOT
2. Nolan Morigold      [REDACTED]      [REDACTED]      Morigold Studios
3. Frank Cassidy      [REDACTED]      [REDACTED]      Morigold Studios
4. Marcus Brown      [REDACTED]      [REDACTED]      Resident
5. Stacey Hans      KYTC District 6      [REDACTED]      KYTC
6. \_\_\_\_\_
7. \_\_\_\_\_
8. \_\_\_\_\_



NEIGHBORHOOD ASSOCIATION

representing Clifton Heights | University Heights | Fairview

DATE \_\_\_\_\_

Please print

NAME	ADDRESS	PHONE or EMAIL	RESIDENT or ORGANIZATION
------	---------	----------------	--------------------------

1. RACHEL WELLS

2. NATHAN HESS

3. Andrea Schwabke

4. Phillip Lane

5. Russell Heuber Crawford Hyging

6. Mik Lindsey Crawford Hyging

7. Chris Donahue Resident

8. Bill Shefalk City of Cincinnati

Resident

Resident

Care Spans

CITY OF CINTI



**NEIGHBORHOOD ASSOCIATION**

representing Clifton Heights | University Heights | Fairview

DATE \_\_\_\_\_

Please print

NAME	ADDRESS	PHONE or EMAIL	RESIDENT or ORGANIZATION
------	---------	----------------	--------------------------

1. Mark Bickel Trinitas [Redacted]

2. Aaron Bantels Trinitas [Redacted]

3. Justin Pappachan [Redacted] UC Student Govt

4. Linda Ziegler [Redacted]

5. Chip Kussmeul [Redacted] Pres. City Council

6. Wisula Miller [Redacted]

7. \_\_\_\_\_

8. \_\_\_\_\_



NEIGHBORHOOD ASSOCIATION

representing Clifton Heights | University Heights | Fairview

Online

DATE Nov 11, 2022

Please print

NAME	ADDRESS	PHONE or EMAIL	RESIDENT or ORGANIZATION
------	---------	----------------	--------------------------

1. Shawn Dent
2. Kelly G
3. Caythia Duval
4. Jennifer Martin
5. Tracy Schwetschenau
6. Michael Brunnemann (Megan)
7. Craig Lloyd
8. Julie Murray
9. Nick DenGinger
10. Jason Kravine
11. Robert Neel
12. Suzanne Summer
13. Keith De Graen
14. Dina Craeger
15. Jodi Heffin
- 16.
- 17.
- 18.
- 19.
- 20.



---

[Attachment 3: Photographs](#)



## CUF Neighborhood Meeting

November 15, 2022



Photograph 1. CUF Neighborhood Meeting held on November 15, 2022.



Photograph 2. CUF Neighborhood Meeting held on November 15, 2022.

**CUF Neighborhood Meeting**

**November 15, 2022**



Photograph 3. CUF Neighborhood Meeting held on November 15, 2022.



Photograph 4. CUF Neighborhood Meeting held on November 15, 2022.

**CUF Neighborhood Meeting**

**November 15, 2022**



Photograph 5. CUF Neighborhood Meeting held on November 15, 2022.



---

Attachment 4: Presentation

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



# CUF Neighborhood Meeting (OH)

November 15, 2022



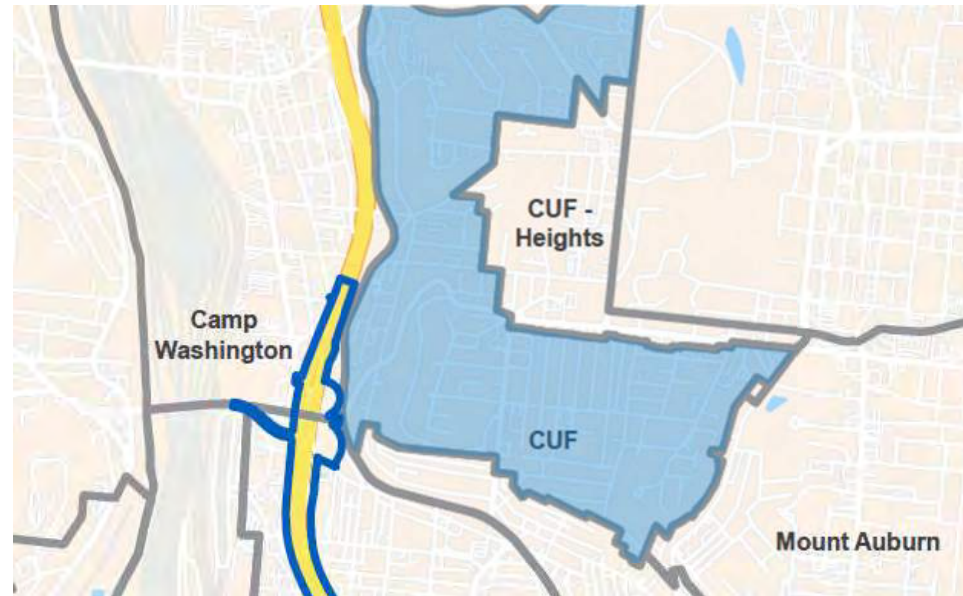
INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with remarks by the City of Cincinnati (5 min) and introductions of the Project Team in attendance (2 min).

# Welcome

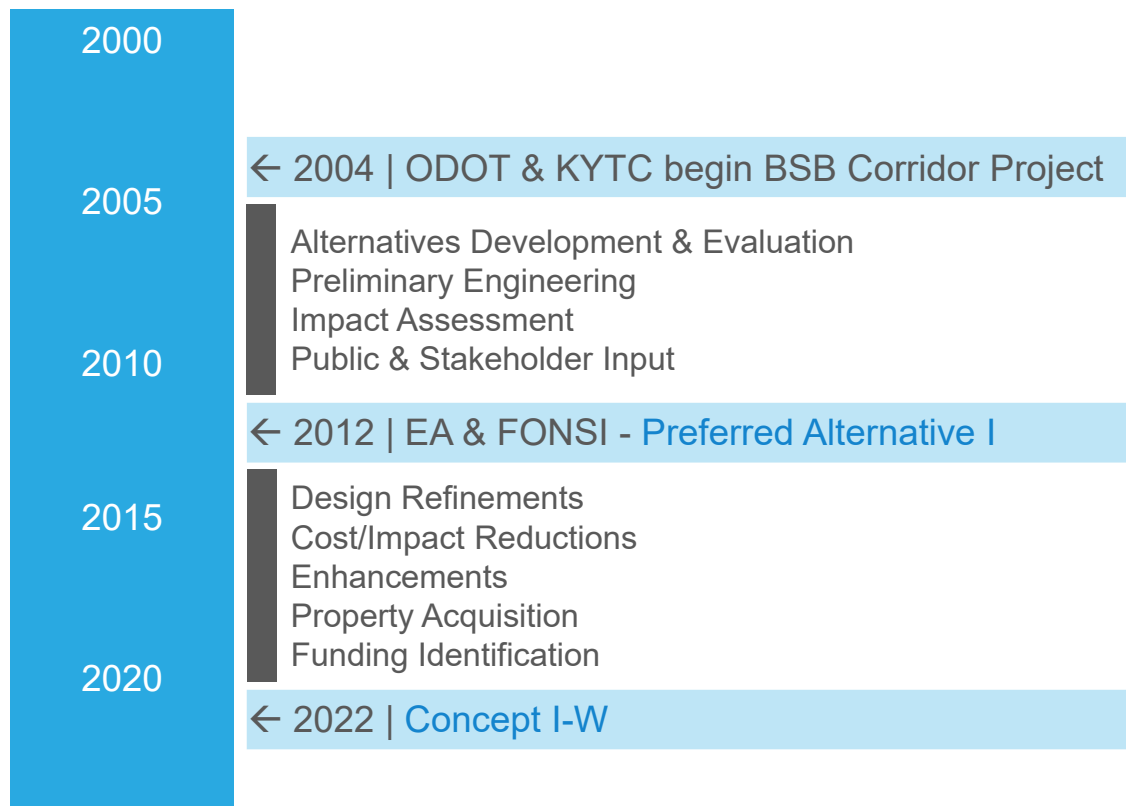
- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the CUF neighborhood the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the CUF area
  - Discussion/feedback from CUF residents



2

The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the CUF neighborhood. Most importantly, tonight we're focusing on hearing from the residents of CUF. We're here to answer your questions and to listen to your feedback about the project.

# Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Ohio, that includes I-75 from the Brent Spence Bridge to just north of the Western Hills Viaduct. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

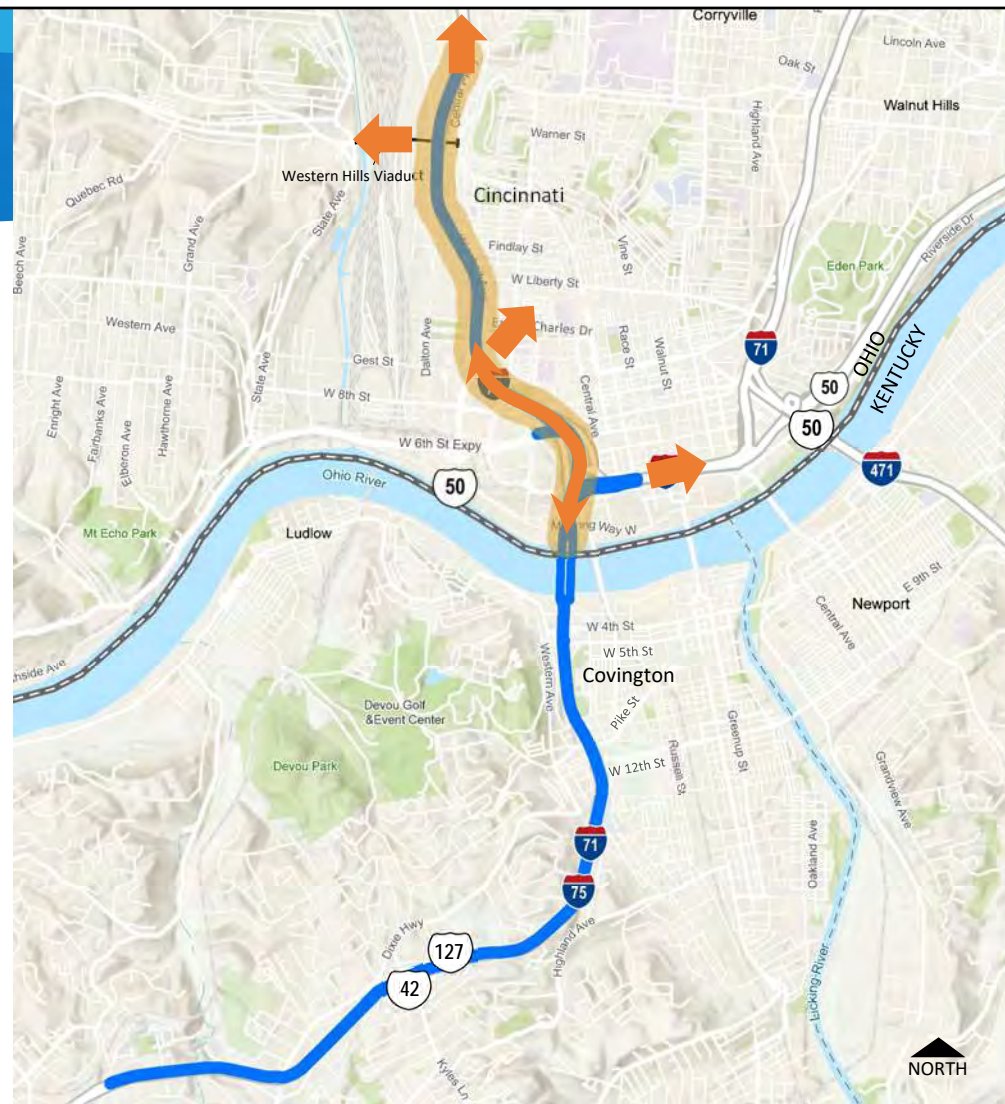
Since the approval of the FONSI in 2012, ODOT has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, ODOT has purchased most of the land needed to build the project. In addition, we've begun detailed design from Linn Street to the northern project limits.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



Note: Individual project elements will come in with click (marked by #).

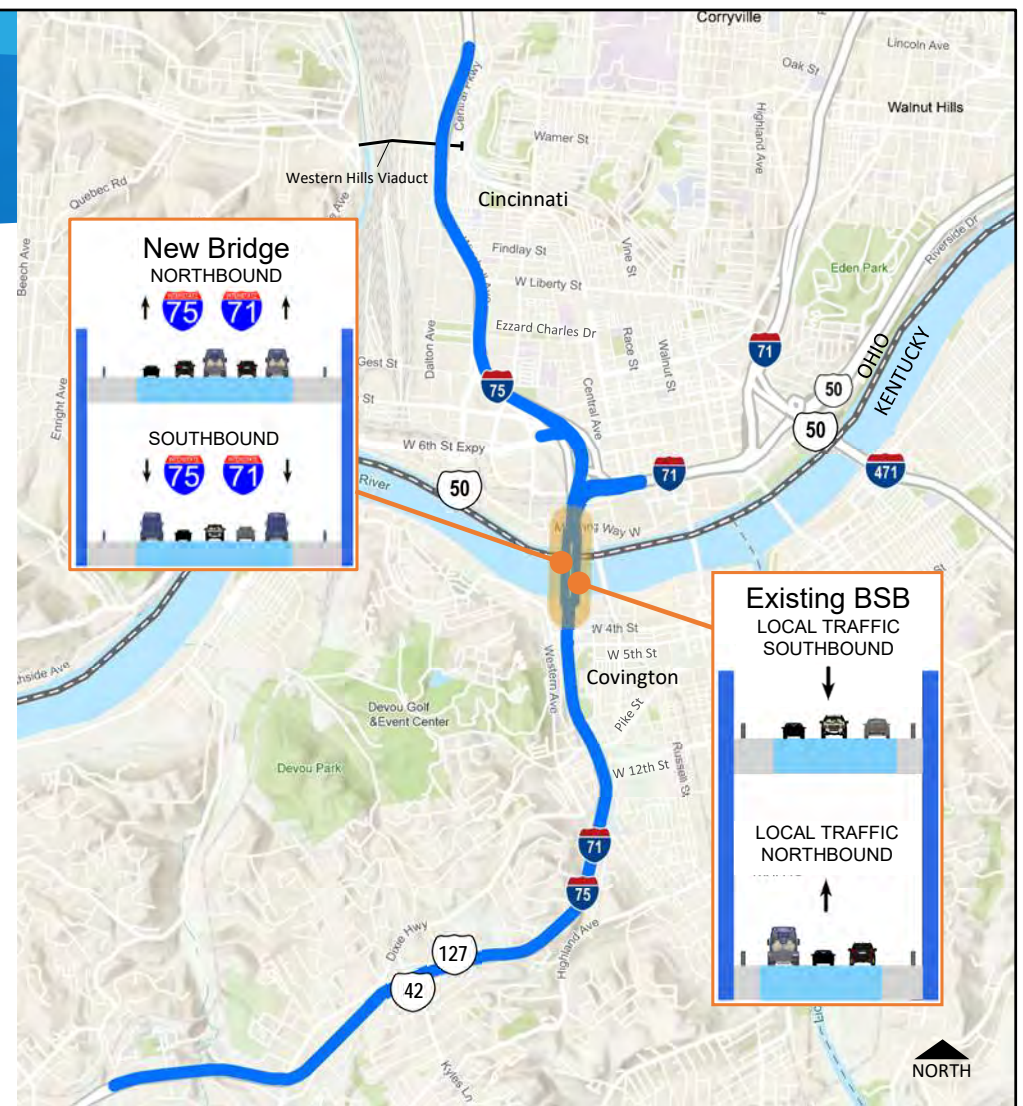
In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. (#) Beginning near Ezzard Charles Drive, a collector-distributor system will be added to connect I-75 traffic to and from the local street network and US-50 West. (#) In the north, it will tie into the recently completed Mill Creek Expressway-Hopple Street Interchange project. (#) The project will rebuild the I-75 interchange at the Western Hills Viaduct and tie into the new bridge replacement project being developed by the City of Cincinnati and Hamilton County. (#) A new northbound exit will be built at Ezzard Charles Drive to improve access to Union Terminal, TQL Stadium, and Over-the-Rhine. Lastly, it will connect to I-71 and US-50 East.



# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

# Project Description

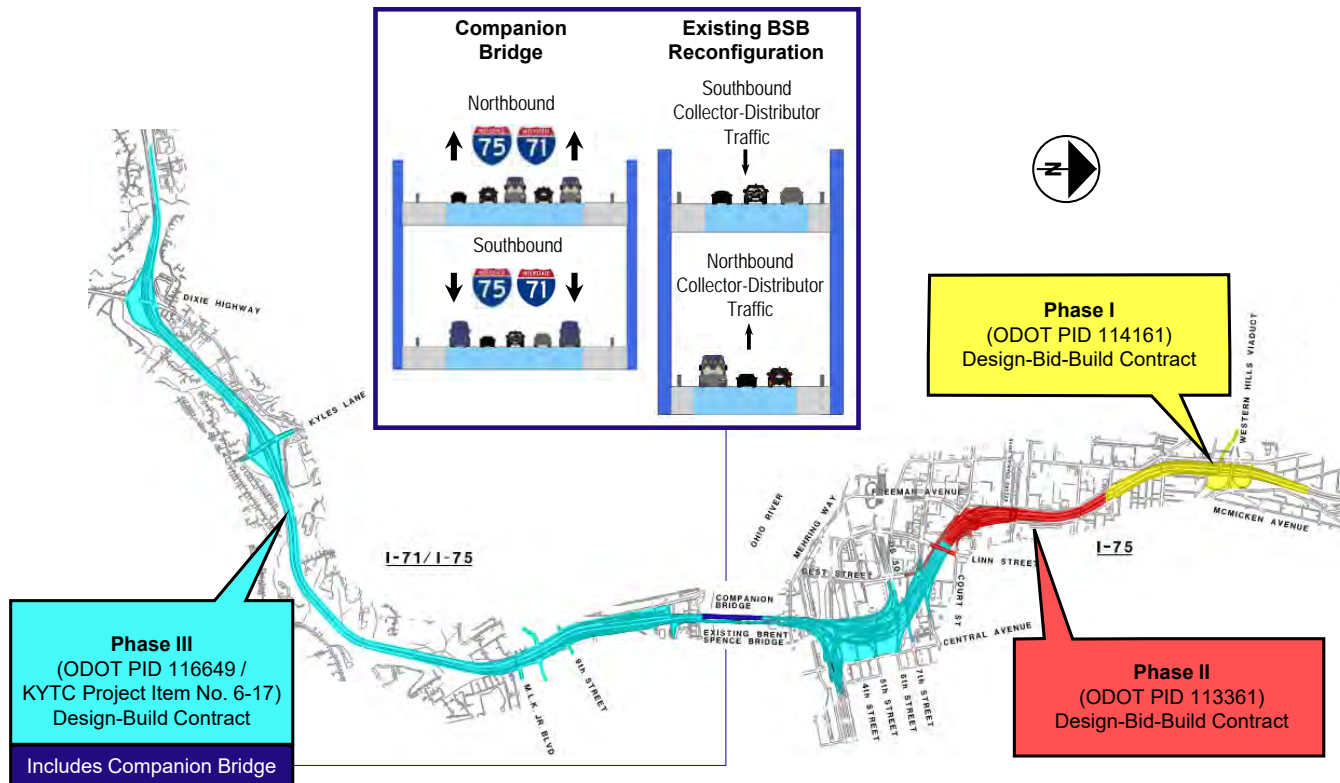
## Kentucky

- Widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct collector-distributor systems in Covington and Ft. Mitchell/Ft. Wright



In Kentucky, the project will widen I-71 and I-75 and rebuild all overpass bridges and interchanges. The project will also extend existing frontage roads to improve connectivity in Covington. A collector-distributor system will also be built beginning in Covington to connect interstate traffic to and from the local street network. Lastly, collector-distributor ramps will be built in Fort Mitchell and Fort Wright to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

# Project Description

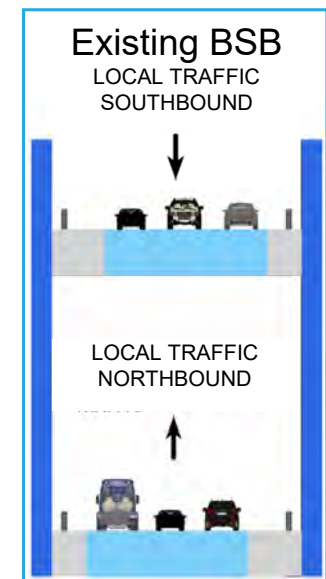
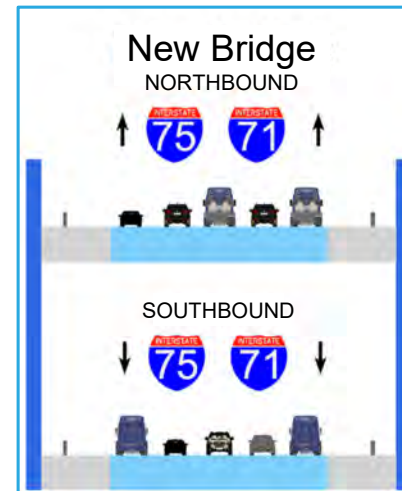


7

The project is going to be built in three phases. Phase I (shown in yellow) will stretch from Findlay Street to the north. Phase II (shown in red) will stretch from Linn Street to Findlay Street. Phase III (shown in blue) will build everything else, including the new companion bridge. Phase I, which is closest to CUF is currently under design with construction expected to begin in 2028. The construction time frame may adjust based on on-going coordination with the City's Western Hills Viaduct Project.

# What Has Changed?

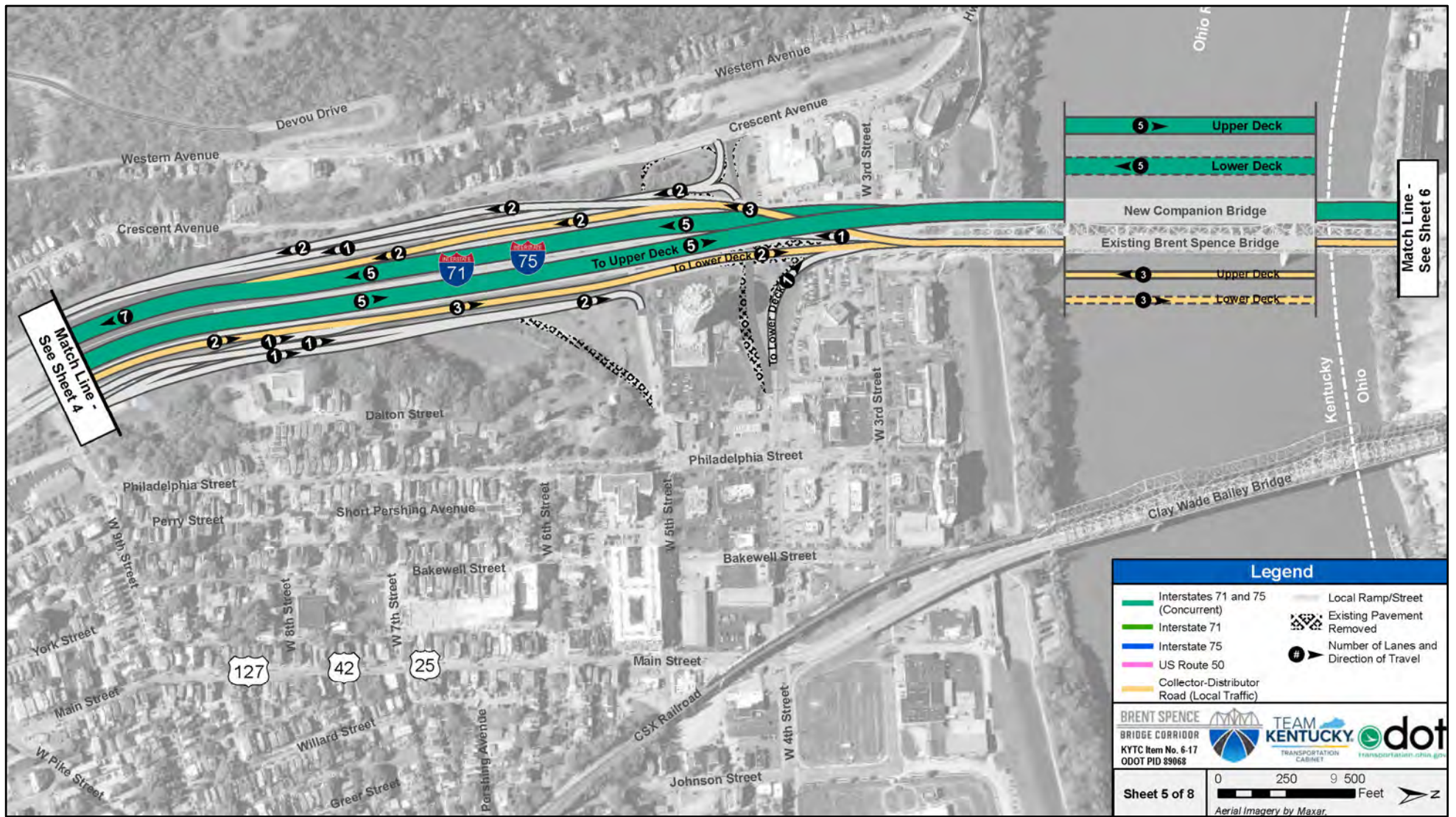
- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



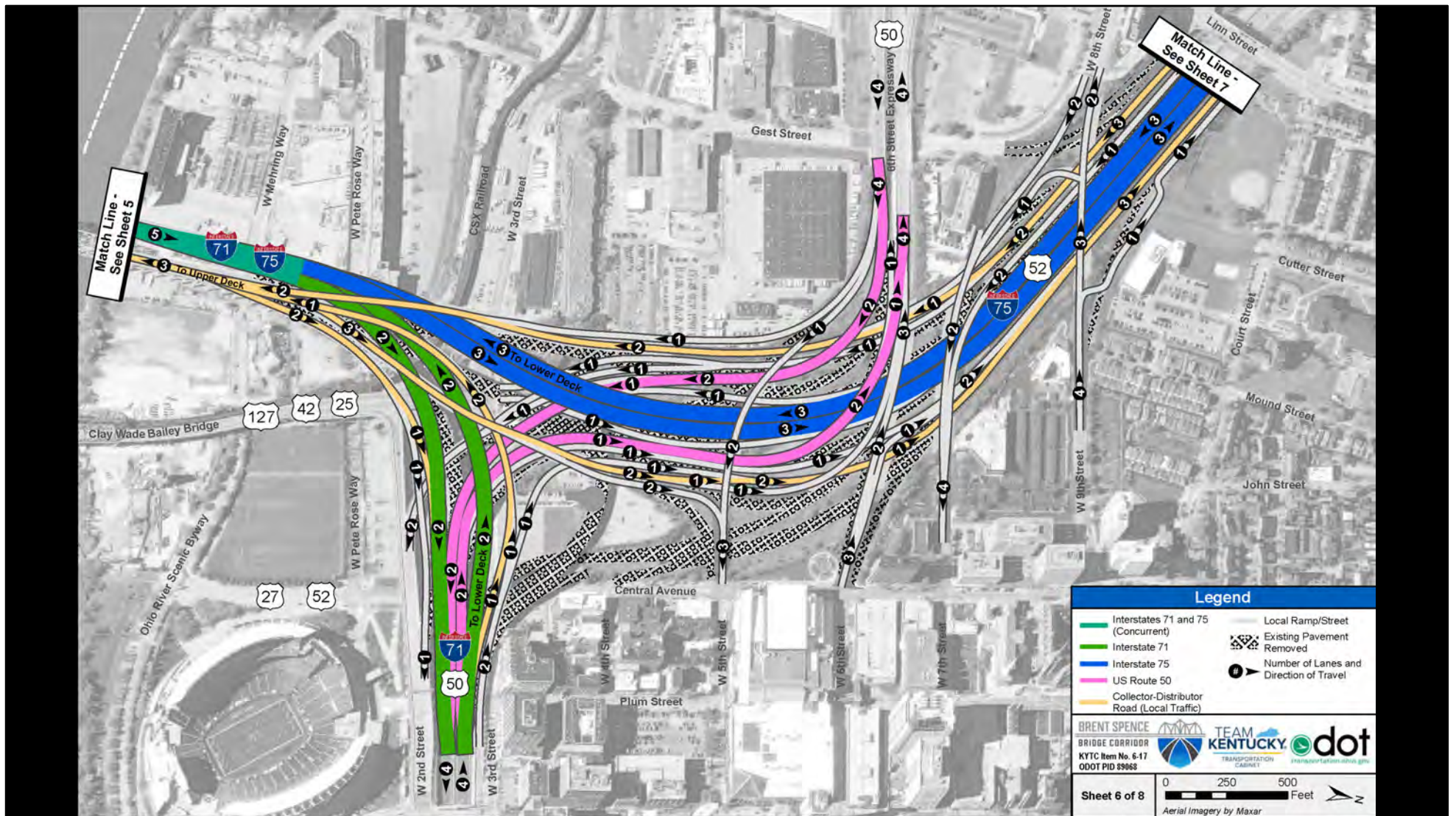
8

As mentioned earlier, since the 2012 EA and FONSI, ODOT has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will through Cincinnati. The blue lines show how I-75 traffic will travel to and from the companion bridge. The green lines show how traffic will on I-71 will be routed to and from the companion bridge. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets. Finally, the pink lines show how traffic will flow on US 50.

Other changes since 2012 include reducing shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet. In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design and allows us to reduce the overall area needed to build the roadways. We also reduced the number of lanes on some of the frontage roads in Kentucky.



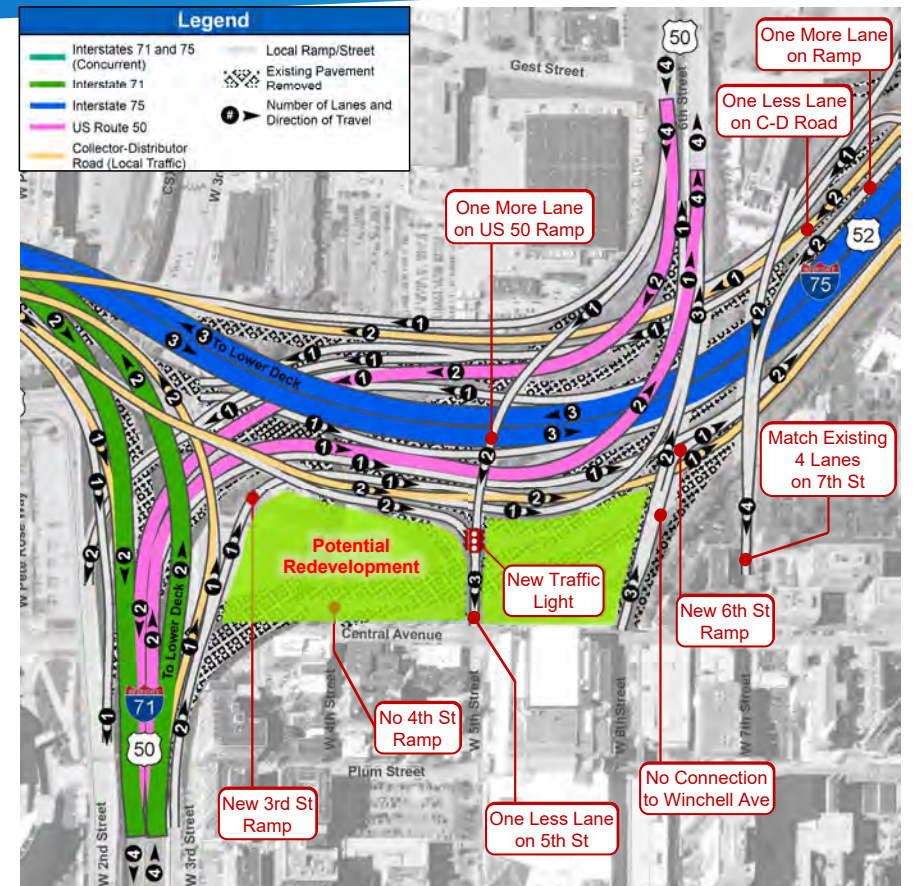
This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

# What Has Changed?

- Reconfigured Downtown Cincinnati Ramps

## Additional changes evaluated

- Depressing I-75 and extending downtown streets to form an urban street grid
  - Cannot meet current design criteria
  - Safety concerns due to steep grades
  - Increased project footprint
  - Continuity along US 50 not maintained
  - Increase traffic in Covington



**Note: Click to walk through ramp changes (marked by #)** | Another change is the layout of the ramps in Downtown Cincinnati. First, ODOT optimized the interchange layout to use land formerly occupied by the Dunnhumby building. More recently, the City asked ODOT to evaluate ramp changes to open up additional land for redevelopment. (#) Based on the City's request, the 4th street ramp to NB I-75 was removed. (#) To provide access for traffic that would have used 4th Street, a new entrance ramp to NB I-75 was added to 3rd Street. (#) The SB exit to 5th Street was removed, which also resulted in fewer lanes on the SB collector-distributor road and at the Central Avenue intersection. The 7th Street exit was also widened to provide additional lanes for traffic that would have used the 5th Street exit. (#) The NB exit to 5th Street was moved closer to the highway to intersect the US 50 ramp at a traffic light, and one more lane was added to the US 50 ramp to make sure all traffic could move smoothly through the light. (#) The 6th Street connection to Winchell Avenue was removed and replaced with a new connection between 6th Street and the NB collector-distributor road. (#) All these changes will open up about 9.5 acres of land for redevelopment, which has been a consistent comment we've been hearing from the public in the last several months.

ODOT has also received several comments about depressing I-75 through downtown Cincinnati and extending downtown streets to form an urban street grid similar to Fort Washington Way. Because I-75 first passes over the railroad, it would have to descend at very steep grades (around 8%) in order to be depressed through downtown. This doesn't meet current design standards which state the maximum grade must be 5%. In addition, such steep grades would introduce safety concerns, particularly given the large number of trucks that travel on I-75. Since I-75 cannot be lowered, local streets would need to be raised to form an urban street grid across I-75, which would increase the project footprint. Finally, building an urban street grid would require starting and ending US 50 on either side of I-75. Moving all local traffic to an urban street grid as opposed to the collector-distributor system currently included in Concept I-W would also substantially increase traffic on the local streets in Covington.





This drawing shows what the Downtown interchange area might look like once the project is built.

# What Has Changed?

- Northbound I-75 entrance ramp moved from Freeman to Winchell
- One Ezzard Charles Drive bridge
- Minimize work along Winchell



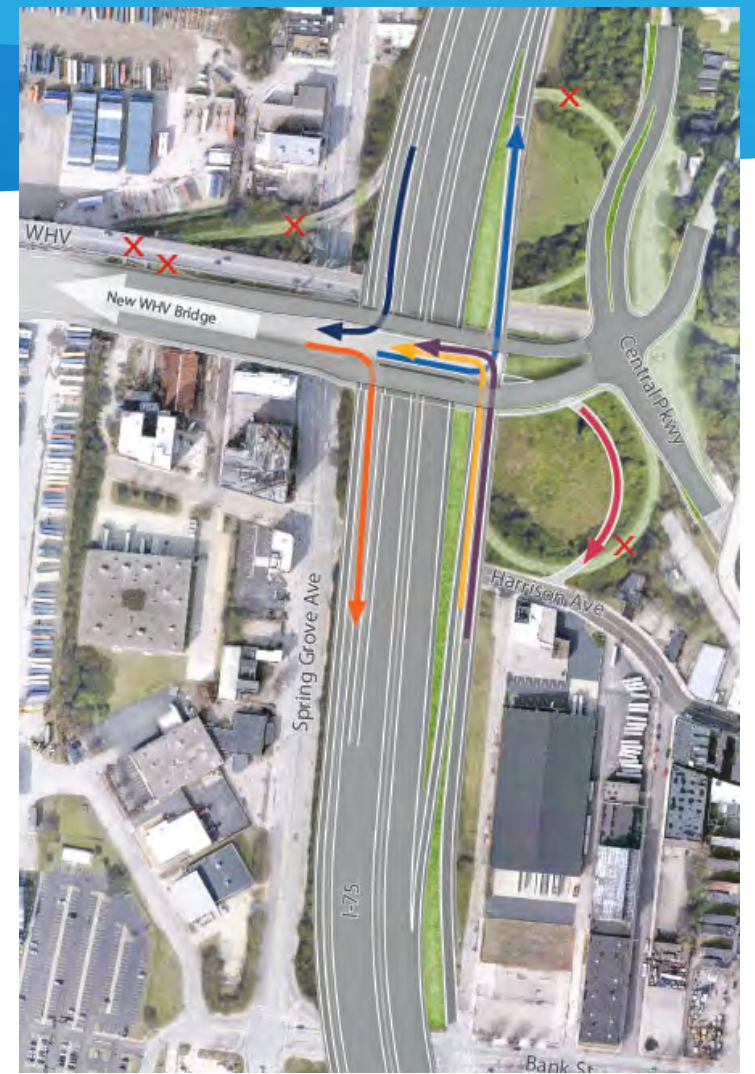
Note: Changes will come in with click (marked by #).

Another change ODOT's made since 2012 involves moving the northbound entrance ramp to I-75 from its existing location at (#) Freeman Avenue (south of Ezzard Charles Drive) (#) to Winchell Avenue (north of Ezzard Charles Drive). Also, (#) the two existing one-way bridges on Ezzard Charles Drive will be replaced with one, two-way bridge. These changes were also made in coordination with the City of Cincinnati to reduce project impacts and costs and improve local access to the interstate.

We've also refined the roadway layouts so that work will not occur along Winchell Avenue beyond what is needed to build the northbound entrance ramp shown in this drawing.

# What Has Changed?

- Interchange at the Western Hills Viaduct



Turning our focus to the areas closest to CUF, the layout of the project has been refined to tie into the new Western Hills Viaduct, which is a separate, stand-alone project being developed by the City and County. The existing ramps at the I-75 interchange will be removed and replaced with new ramps that will provide direct access to and from the new Western Hills Viaduct bridge. The ramps will also connect I-75 to the local street system at Findlay Street for southbound traffic and Bank Street for northbound traffic.

# Aesthetics

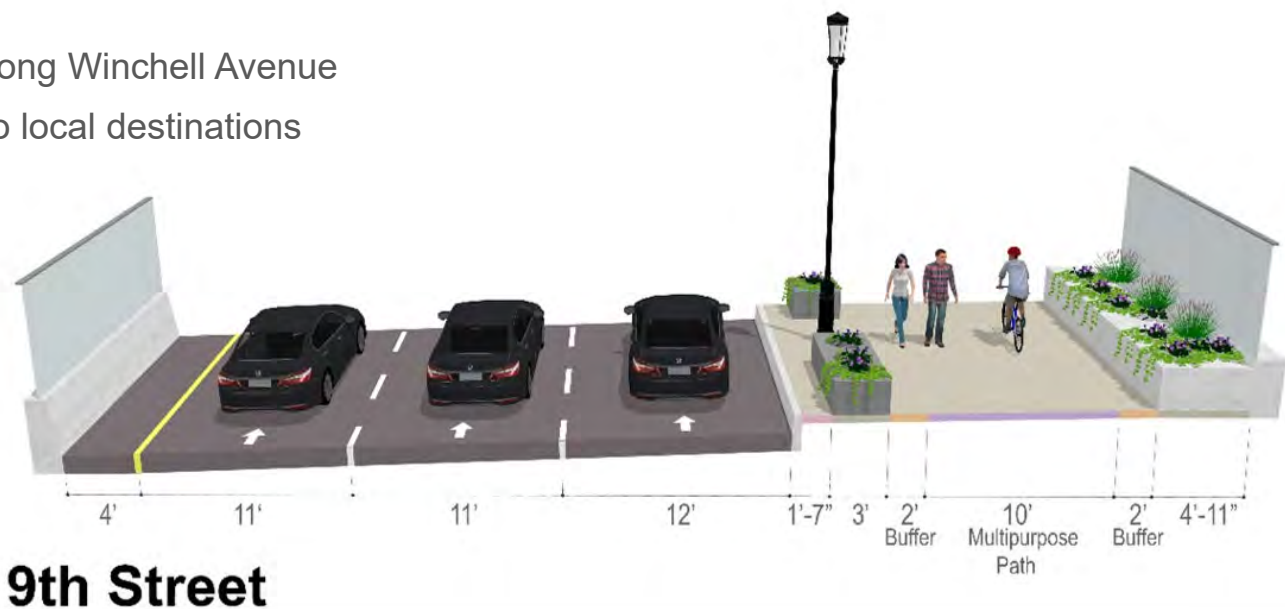
- Corridor-wide aesthetic treatments
  - Ashlar stone treatments for abutments, piers, and walls
  - Decorative bridge parapets
  - Translucent screen walls and planters on bridges
  - Other features (lighting, sidewalks, etc.) to match City standards



ODOT has worked with the City and the project Aesthetic Committee to develop an Aesthetic Design Checklist that will guide what the Brent Spence Bridge Corridor will look like. For example, ODOT will add aesthetic treatments in a pattern called “Ashlar Stone” to all bridge abutments, parapets, and piers. Retaining walls and the extended traffic safety barriers we just discussed will also have ashlar stone treatments. Overhead bridge parapets will have end treatments with the bridge identification name, construction completion data, and rustification design features. Examples of similar bridge parapets are shown in the images on this slide. In addition, overpass bridges will have wide sidewalks or shared-use paths, planters, and translucent screen walls. Other features throughout the corridor will match City aesthetic standards, including lighting, sidewalks, tree lawns, and others. We will show some examples of what specific locations near CUF will look like in just a few slides.

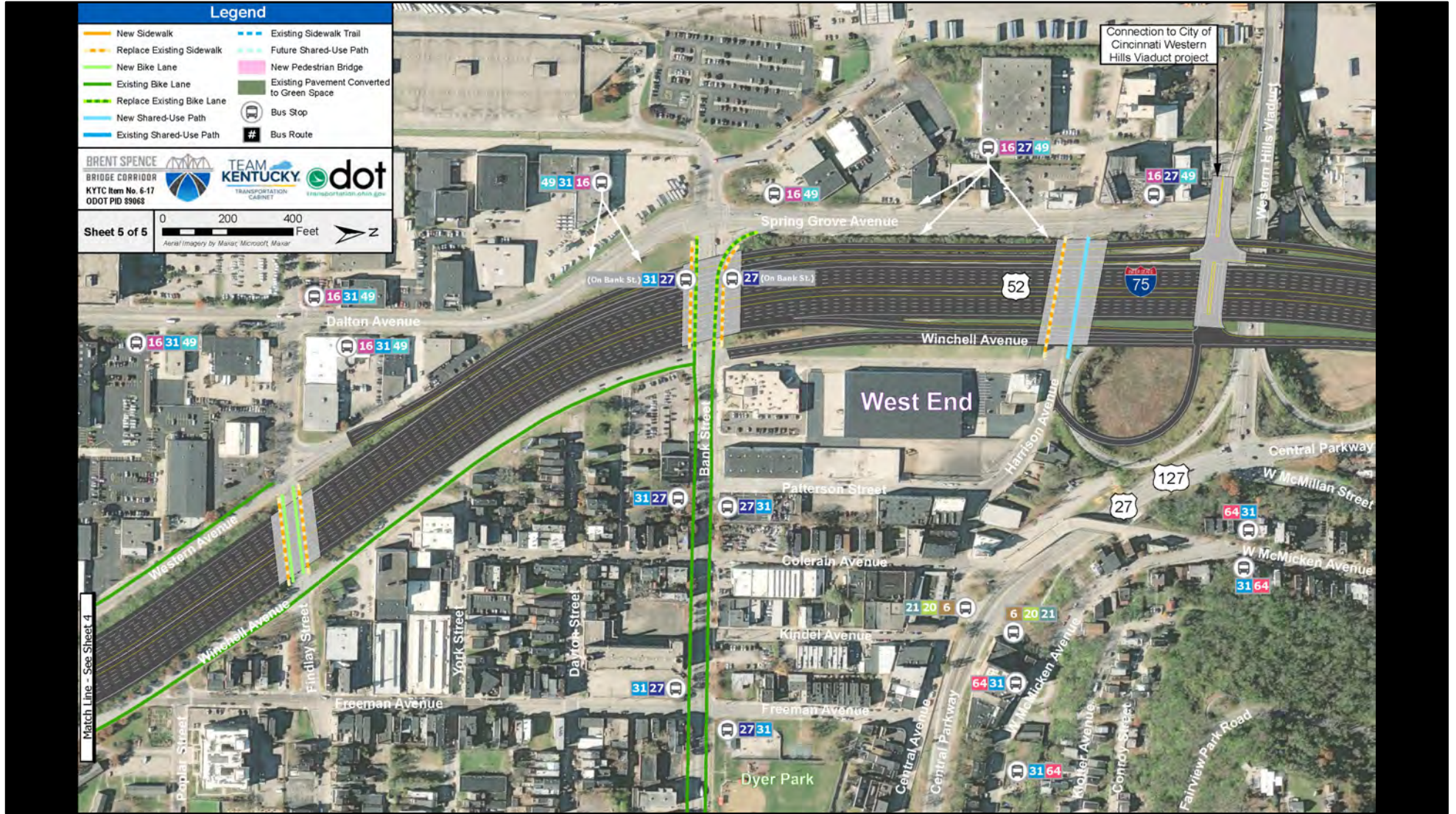
# Pedestrians and Bicycles

- Connections across I-75
  - Sidewalks
  - Shared use paths
  - Bike lanes
- New shared use path along Winchell Avenue
- Improved connections to local destinations



17

The project will install bicycle and pedestrian infrastructure on connections across I-75 like what is shown here for 9th Street. Pedestrian and bicycle connections will also be provided on 6th Street, 7th Street, Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue. To promote safety for bicycles and pedestrians, the ramp connections with local streets are being designed as lower-speed urban intersections in accordance with City of Cincinnati design standards. The pedestrian and bicycle infrastructure included in the project will improve connectivity to transit, employment, healthcare, cultural, recreational, and commercial destinations.



This exhibit shows some of the planned pedestrian and bicycle facilities near CUF. Note the new shared use path on Harrison Avenue, the new bike lanes on Findlay Street, and the replaced sidewalks on Harrison, Bank, and Findlay. These will tie into the existing bike lanes on Western, Winchell, and Bank and connect to existing transit stops.

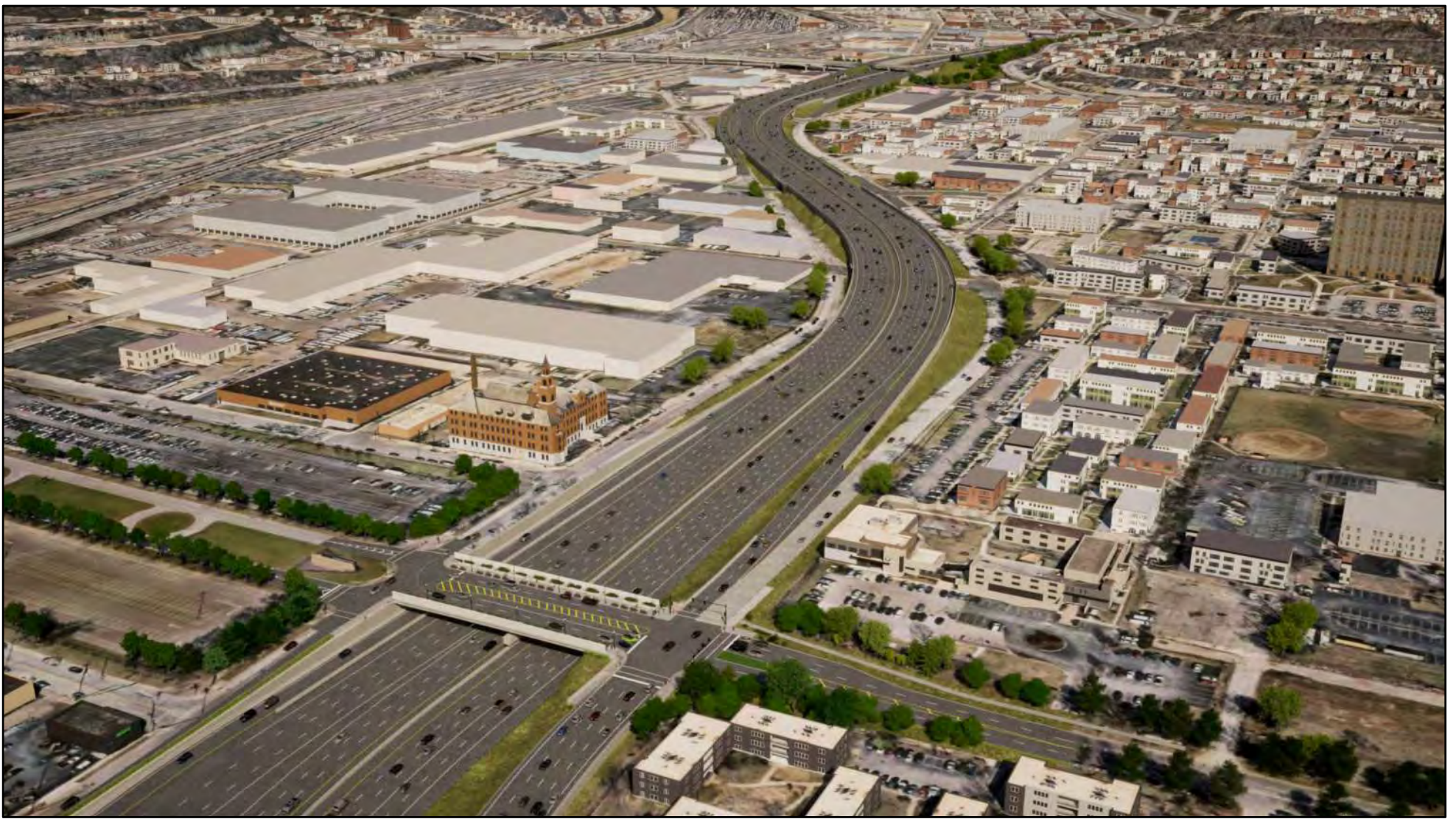


This is a view of what Findlay Street will look like when the project is built. Notice the 57-inch barrier on the I-75 bridge and the 8-foot extended traffic safety barriers on both sides of the bridge to reduce traffic noise. This also shows what the barriers and bridge walls will look like with the ashlar stone treatment. Also notice the rebuilt sidewalks, added bike lanes, and underpass lighting.



This view shows what the new, two-way bridge on Ezzard Charles Drive will look like. Notice the wide shared use path on the right and the sidewalk on the left. Also notice the planters, screenwall, and decorative lighting on the bridge. The bridge lights and traffic signal supports will be black and meet City aesthetic standards.





This view shows what the finished project will look like from Ezzard Charles Drive looking north toward CUF. Notice the 8-foot-tall extended traffic safety barriers for noise reduction. Also notice the ashlar stone treatments on the retaining walls.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.



---

[Attachment 5: Exhibits](#)



Findlay Street (Looking West) - Ohio



KYTC Item No. 9-17 | CDOT PG 0608



Ezzard Charles Drive (Looking West) - Ohio



KYTC Item No. 9-17 | CDOT PG 0608



Brent Spence Bridge Corridor - Ohio  
(Looking North Near Ezzard Charles Drive)



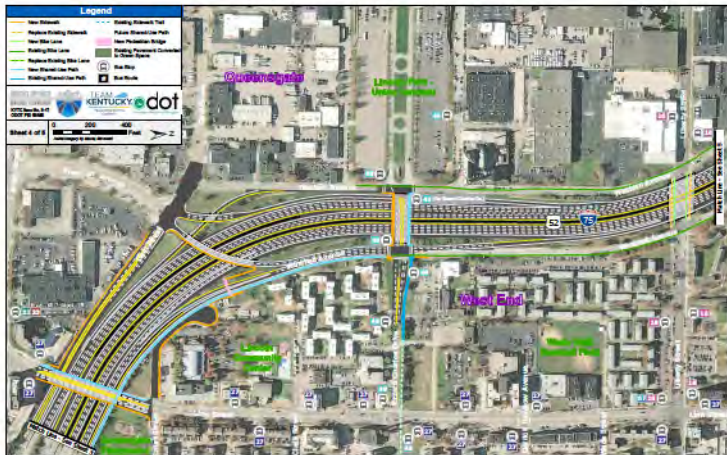
KYTC Item No. 9-17 | CDOT PG 0608



Brent Spence Bridge Corridor  
(Looking North from Kentucky)



KYTC Item No. 9-17 | CDOT PG 0608



# Brent Spence Bridge Corridor Project Mainstrasse (ROMA) Neighborhood Meeting Summary November 21, 2022

## Introduction

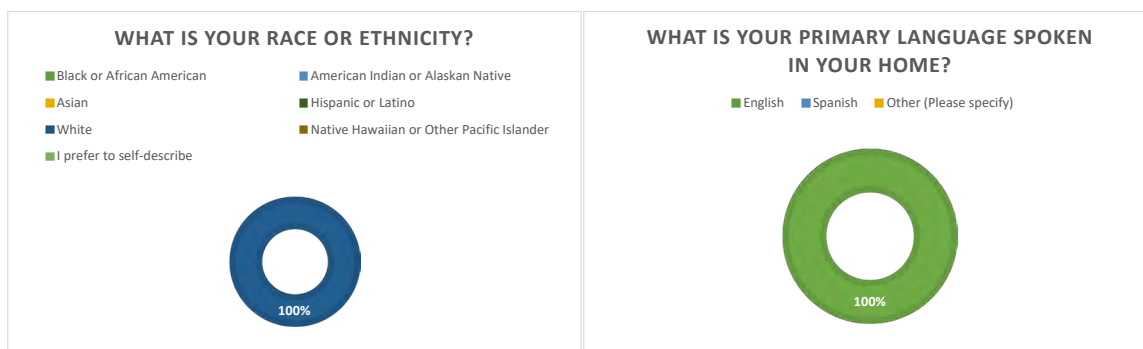
The Mainstrasse (ROMA) Neighborhood Meeting was held on November 21, 2022 from 6:00 pm to 8:00 pm. The meeting was held at the Center for Great Neighborhoods located at 321 West 12<sup>th</sup> Street in Covington, Kentucky. It was publicized through the ROMA Facebook page and the ROMA email list (see Attachment 1). Attendees at the meeting included the City of Covington, the project team, and members of the public. In total, 19 people attended the meeting, including 7 individuals from the city and the project team (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). During the presentation, the project team responded to questions and comments offered by those present.

Exhibits showing noise wall renderings, lane configurations, and a rendering of Goebel Park were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](http://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Four (4) comment forms and seven (7) demographic surveys were returned at the meeting.

## Demographics

The following is a breakdown of the responses received from the demographic survey.





A write in space was available to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. Comments included:

- I appreciate they reached out to our neighborhood. Perhaps Facebook page or Instagram?
- Thank you Stacie (sic - Stacee) – great job!

## Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Clarification / understanding of local versus through traffic and ramp connections.
- Concern over traffic congestion / backup in their neighborhood during bridge closures and / or incidents.
- Clarification / understanding of the process for noise evaluation and discussion of impacts with / without noise wall.
- Drainage for low lying areas, particularly in Goebel Park.
- Clarification of timeline and staging of construction.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.

Comments noted on the survey forms returned at the meeting include:

- Include the noise walls (two comments).
- No extension of public road from Pike to 5<sup>th</sup> Street.
- Traffic mitigation and enforcement during construction.

**Table 1: Q & A from Session**

Question	Project Team Response
Is the object of this meeting to tell us what you are going to do or is it still in design stage?	Here to solicit feedback from the community. Some things are already decided based on engineering and traffic. But some are open for refinement based on feedback from the neighborhood.
Collector-Distributor – like a local / express?	Yes – like Chicago
Will the current bridge be the only one to get on I-71? Trying to figure out how to go west to east.	Discussed points of origin and which bridge to use to travel to different destinations..
Is Buttermilk Pike to Dixie going to be continuous?	The project does not go all the way to Buttermilk Pike and will match the existing number of lanes south of Dixie Highway.



Question	Project Team Response
Time frame?	KYTC applied for federal grants and expects to hear about funding by the end of the year. The progressive design / build contract will be awarded around June 2023. Construction will begin in late 2023 / early 2024.
Are you going to be able to give back to the park? Note the park has a lower elevation.	Enhancing Goebel Park is an area for opportunity in the project. KYTC is coordinating with the City of Covington regarding this.
What do we think the air quality will be in that area?	KYTC is studying air quality as part of the environmental process. The air quality will likely be similar to what it is today.
Can you still get on to Pike – how can you do that? Currently, travelers get on the highway at Pike and 4 <sup>th</sup> , cross the old bridge and then decide which way to go.	All local movements to and from the highway will occur through the collector-distributor system. Local traffic will function similar to how it does today.
Will the existing entrance for 4 <sup>th</sup> be for emergency access only?	The ramp project is scheduled to open by Christmas. West 5 <sup>th</sup> Street will be open, but West 4 <sup>th</sup> Street will be for emergency access only. Once the Brent Spence Bridge Corridor Project is built, the West 4 <sup>th</sup> Street ramp will be opened up for full access.
Differences that will affect ROMA will be all the collector-distributor lanes and the effect of extra traffic during construction.	The new companion bridge introduces resiliency in the system to provide an alternative route if there is an incident to reduce traffic rerouting to the local street network.
Will the route be adequately signed for through and local traffic?	First decision point – interstate or local destinations – will be signed on the highway. Then, additional signage will be provided on the collector-distributor system. Decisions on the collector-distributor system will occur at lower speeds.
Trucks? Main to 5 <sup>th</sup> is an issue. If there is an issue on interstate – does not take many trucks to back up on local roads.	Both the companion bridge and potentially the local West 9 <sup>th</sup> Street to West 5 <sup>th</sup> Street connector will give more options to traffic during incidents, which should reduce traffic rerouting to Main Street.
Design looks great but how are (ROMA) going to be impacted during construction?	KYTC will develop a Traffic Management Plan & Incident Management Plan during construction and will work with the city, first responders, and enforcement when developing the plans. These plans will apply lessons learned from the recent closure for emergency repairs, such as protecting the Roebling Bridge.





Question	Project Team Response
How to address pedestrians (legally blind) and need to walk everywhere? There is zero enforcement when traffic is backed up into local system.	The addition of the second bridge should help with incident management and reduce the effects of traffic backups to the local network.
What is the construction timeline, and will it coincide with work on the Clay Wade Bailey Bridge?	KYTC and the city will coordinate during construction to avoid all river crossings being under construction at the same time.
How does access work locally when rehab work is done on the existing bridge. Concern that residents of Covington will be impacted rather than those in suburbs.	Although construction details will be finalized by the design / build team, KYTC anticipates that the companion bridge will be built first. Once traffic is transferred to the new bridge, then the existing bridge will be rehabilitated. KYTC does not anticipate a full closure of the river crossing during construction.
Will the new T-intersection at the West 5 <sup>th</sup> Street ramp have a light?	The decision about whether or not the intersection will have a traffic light will be made during the design / build process.
How did KYTC determine noise levels at receptors?	The noise study did not take measurements at all receptors. They took field measurements in select locations to verify their model. Once they were satisfied with the model, they used it to develop the noise levels for all scenarios.
What are relative differences between the wall height and the street? Will there be breaks in the walls?	Reviewed cross sections from presentation. At the clock tower, the highway will be approximately 31 feet higher than the existing interstate, and the noise walls will be 16 feet above that. At Goebel Pool, the highway will be about 27 feet higher, and the noise wall will be 16 feet above that. There will be breaks in the walls for the collector-distributor system.
Are noise walls reasonable or feasible?	In Mainstrasse, a noise wall system was found to be feasible but not reasonable because it was not cost effective and did not meet design goals. KYTC is going a step further to ask residents if they want noise walls and are willing to consider building them as an enhancement that goes above and beyond their noise policy.



Question	Project Team Response
Concern about water collection at the low point at Goebel Park.	Runoff from the interstate currently goes into the combined system for Sanitation District No. 1. When the project is built, all interstate drainage will go into a separate storm sewer system, which should reduce flooding in Goebel Park, although some of the flooding occurs due to backwater from the Ohio River.
What about people that don't want the walls because of their visual impact? Will they benefit? Personally, I would rather see a tall wall and benefit from the reduced sound.	KYTC is specifically seeking feedback from the neighborhoods that will benefit from the walls in their decision-making process.
Going back to when data was taken for noise? Summer or winter as the leaves make a huge difference.	The noise data was gathered in late spring / early summer. Official policy does note that foliage is not a recognized buffer.
What about the view of St. John's Church with a noise wall?	The church will not be visible from the east side of the highway with a noise wall.
With walking path being near Jillian – is it coming in a little east – but goes through a gully that floods. What are you doing? Are you looking to raise elevation? Concerned about safety in park – will all the roads going through the park be elevated and able to walk through? Want to minimize walking path interacting with underpass.	Team is looking at opportunities for reconnecting paths in Goebel Park and providing lighting under bridges to provide safe passage.
Question on speed of local streets? 25?	The city would have jurisdiction over speeds on local streets.
9 <sup>th</sup> to 5 <sup>th</sup> Street connector – would still have noise wall? Personal opinion would be nice but does not impact his property.	Yes, a noise wall could still be built if the West 9 <sup>th</sup> Street to West 5 <sup>th</sup> Street connector is built as the noise wall is actually on the Collector-Distributor and 5 <sup>th</sup> Street Exit Ramp in this area.
Will team reach out to neighborhoods about the larger neighborhood meetings?	The project team will forward a flyer about the larger neighborhood meetings to Mainstrasse.



---

## Attachment 1: Advertising Materials

## Seth Turner

---

**From:**  
**Sent:** Wednesday, October 26, 2022 3:56 PM  
**To:**  
**Cc:**  
**Subject:** Re: Brent Spence Bridge Presentation on November 21st

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

Thanks for this information. I have the following updates for our neighborhood group Residents of MainStrasse Association (ROMA).

I have reserved a large conference room for Monday, 11/21/22 at 6pm with the Center for Great Neighborhoods at 321 W 12th St in Covington. That room has plenty of room for easels. There is a projector and laptop hookup for presentations. We have not used virtual capabilities to a great extent but might be open to suggestions on how to best use that.

As for publicizing, we can post to our ROMA Facebook page and can send out to our email list. Given we have many no - residents on our page (folks who work in the village or that live nearby and frequent the village) I would want to emphasize this meeting is intended for residents primarily. We also have an events page we can add this to. I appreciate any wording you can provide to help us communicate this meeting so I welcome your email text and could also print/post fliers if that's also something you can provide.

Andi

Sent from my iPhone

On Oct 25, 2022, at 3:41 PM, > wrote:

Andi and Brian,

Thank you so much for allowing the Brent Spence Bridge project team to attend the Mainstrasse/ROMA meeting you are setting up on November 21<sup>st</sup>. As you know, the project team wants to share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

Our goal is to be sure all notifications about meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous neighborhood meeting?		
Email list	Can you email a neighborhoodwide or citywide email list with information about BSB presentation at the upcoming meeting?		
Facebook (private to neighborhood)	Can you share a Facebook message about BSB presentation?		
Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?		
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?		
Other	Is there another outlet you have available to get the word out?		
Do you need assistance from us with advertising?	Do you need the meeting information as an email or flyer?		

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!  
Sherry

**Sherry R. Kish**

**HNTB CORPORATION**  
88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

**100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

## Seth Turner

---

**From:** Andi Fehr  
**Sent:** Monday, November 21, 2022 4:46 PM  
**To:** Seth Turner  
**Subject:** Fwd: Brent Spence Bridge Meeting Update/Feedback November 21st 6:00 PM

This was the first email we sent to our distribution list

Sent from my iPhone

Begin forwarded message:

**From:** ROMA  
**Date:** November 9, 2022 at 1:37:50 PM EST  
**To:** ANDIFEHR  
**Subject:** Brent Spence Bridge Meeting Update/Feedback November 21st 6:00 PM  
**Reply-To:** ROMA



**The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of neighborhood outreach**

**meetings for the Brent Spence Bridge (BSB) Corridor Project. Details for the meeting with residents of MainStrasse are listed below.**

**Purpose: To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team.**

**When: Monday, November 21, 2022 at 6:00pm.**

**Where: Large community conference room at The Center for Great Neighborhoods of Covington, 321 West 12th Street, Covington, KY 41011.**

**Hope to see you there!**

**If you need assistance attending this meeting, please call Stacey Hans at (859) 341-2700.**

**View the most recent project newsletter at the following link:**



Copyright © 2021 Residents of MainStrasse Association ([Website](#)), All rights reserved.

Want to change how you receive these emails?  
You can [update your preferences](#) or [unsubscribe from this list](#).

---

This email was sent to [andifehr@yahoo.com](mailto:andifehr@yahoo.com)

[why did I get this?](#) [unsubscribe from this list](#) [update subscription preferences](#)

Residents of MainStrasse Association · 321 W. 12th Street · Covington, KY 41011-1369 · USA





## Seth Turner

---

**From:** Andi Fehr  
**Sent:** Monday, November 21, 2022 4:30 PM  
**To:** Seth Turner  
**Subject:** Fwd: Reminder Brent Spence Bridge Meeting tomorrow November 21st 6:00 PM

Here's the email reminder that went out this weekend.

Sent from my iPhone

Begin forwarded message:

**From:** ROMA  
**Date:** November 20, 2022 at 11:12:23 AM EST  
**To:** ANDIFEHR  
**Subject:** Reminder Brent Spence Bridge Meeting tomorrow November 21st 6:00 PM  
**Reply-To:** ROMA



**A quick reminder that we've arranged a meeting tomorrow Monday, November 21, at 6pm at The Center for Great Neighborhoods of Covington to**

**discuss the Brent Spence Bridge project with representatives from the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Department.**

**We will be receiving an update on the project, and what potential impact it could have on our neighborhood. Do you have questions or concerns about the project? This will be an opportunity for you to express those concerns. If you are unable to attend, but would have a question or concern, please let us know and we will try to make sure your voice is heard.**

**Location: Large community conference room at The Center for Great Neighborhoods of Covington, 321 West 12th Street, Covington, KY 41011**

**Hope to see you there!**



Copyright © 2021 Residents of MainStrasse Association ([Website](#)), All rights reserved.

Want to change how you receive these emails?  
You can [update your preferences](#) or [unsubscribe from this list](#).

---

This email was sent to [andifehr@yahoo.com](mailto:andifehr@yahoo.com)


[why did I get this?](#) [unsubscribe from this list](#) [update subscription preferences](#)

Residents of MainStrasse Association · 321 W. 12th Street · Covington, KY 41011-1369 · USA




# ROMA Neighborhood Association Facebook Page

Facebook.com

 ROMA - Residents of MainStrasse Association

---

 **Andi Fehr**  
Admin Top contributor · +1 · November 15 at 7:33 AM · 🌐

A quick reminder that we've arranged a meeting on Monday, November 21, at 6pm at 📍 The Center for Great Neighborhoods of Covington to discuss the Brent Spence Bridge project with representatives from the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Department.


Location: Large community conference room at The Center for Great Neighborhoods of Covington, 321 West 12th Street, Covington, KY 41011


Hope to see you there!


👍❤️ 20 2 Comments

👍 Like    💬 Comment    📧 Send

Top comments ▾

Write a comment... 

 **Kelly Bellisle**  
Hi neighbors, commenting again to boost this visibility. I am hoping to be at the meeting, but planning our life is easier said than done this days. Very interested to hear more about the impacts on our neighborhood.  
Like Reply 3d 👍 2

 **John Saxton**  
Just want to add to **Andi Fehr's** post. This was a meeting scheduled specifically for Mainstrasse residents and not one of the city planned town hall meetings about the bridge. Even if you don't live next to the highway the project is going to have a mas... [See more](#)  
Like Reply 5d 👍 7

### About

Residents of Mainstrasse Association  
A group of committed residents dedicated to establishing & maintaining a safe, thriving, beautiful neighbor... [See more](#)

🔒 **Private**  
Only members can see who's in the group and what they post.

👁️ **Visible**  
Anyone can find this group.

📍 **Covington, Kentucky**

[Learn more](#)


### Topics in this group

**#ittakesavillage** 📌  
Pinned by admin · 13 posts

**#lovethcov** 📌  
Pinned by admin · 11 posts

**#readreadycovingtonupdate** 📌  
Pinned by admin · 1 post

### Upcoming events

 **WED, DEC 21 AT 6:30 PM AND 1 MORE**  
**Holly Jolly Days: S'mores and More**  
Behringer-Crawford Museum  
Crafts · Shared to ROMA - Residents of MainStrasse Association

[See all](#)



Attachment 2: Sign-In Sheet

SIGN-IN SHEET	
BRENT SPENCE BRIDGE CORRIDOR PROJECT Neighborhood Outreach Meeting – November 21, 2022	
Name	Address
Representing	Phone
Email	
1. ANDI FEHR	RESIDENTS OF MAINSTRASSE
2. Mour Diop	ETHNA - KY
3. Brian Gorky	RESIDENTS OF MAINSTRASSE
4. Steve Hu	
5. Hanna Juliana Mwangi	RESIDENTS OF MAINSTRASSE
6. John Saxton	FOMA
7. Michael Schilling	ROMA
8. Jeff & Crystal Nussstein	ROMA
9. Kevin Lammeman	ROMA
10. Emily Herrera	ROMA
11. Alberto Herrera	ROMA
12. PJ Conneund	ROMA
13. John Warner	ROMA
14. Jodi Kellin	ASIB
15. Mark Beckwith	11-15
16. Andrea Wilkoff	Livingston
17. Livosay Wance	NYB
18. Stacey Haas	KYTC
19. Gary Valetta	KYTC
20.	

Please print clearly • Contact information is optional but must be included if you wish to receive updates.

page \_\_\_\_\_ of \_\_\_\_\_  
KYTC Item No. 6-17 | ODOT PID 89068



### Attachment 3: Photographs





---

[Attachment 4: Presentation](#)

**BRENT SPENCE**

**BRIDGE CORRIDOR**



# Mainstrasse Neighborhood Meeting (KY)

November 21, 2022



INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)

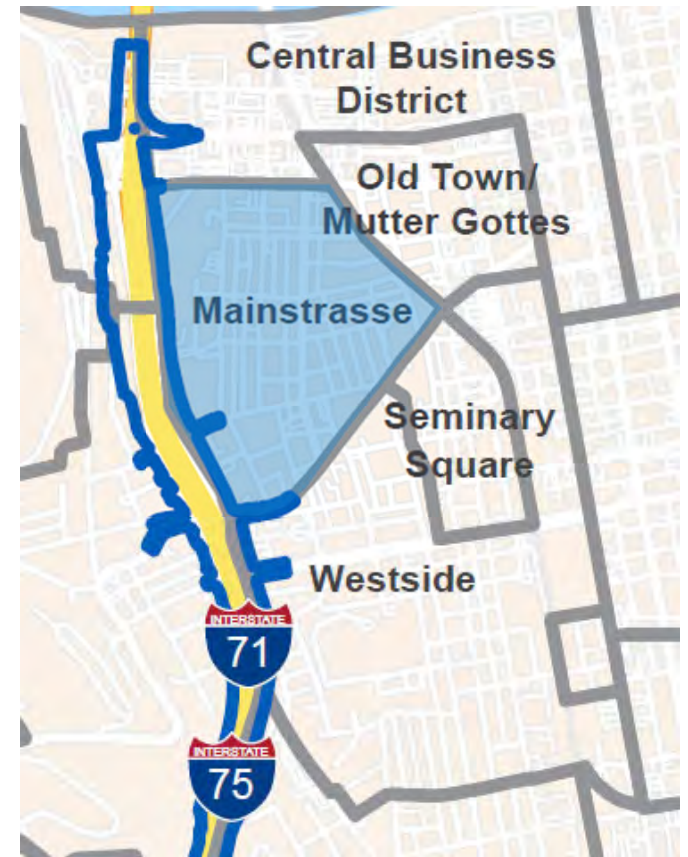


The meeting will open introductions of the Project Team in attendance (2 min).



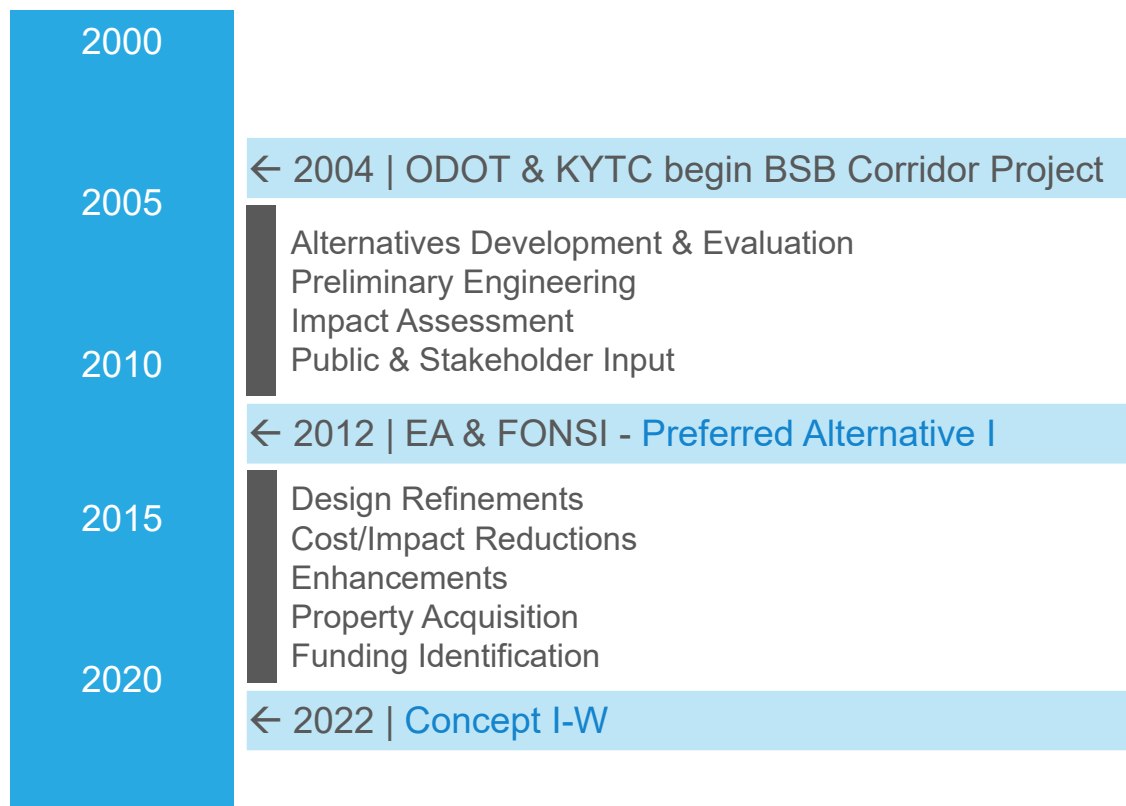
# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the Mainstrasse neighborhood the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Mainstrasse area
  - Discussion/feedback from Mainstrasse residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Mainstrasse neighborhood. Most importantly, tonight we're focusing on hearing from the residents of Mainstrasse. We're here to answer your questions and to listen to your feedback about the project.

# Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

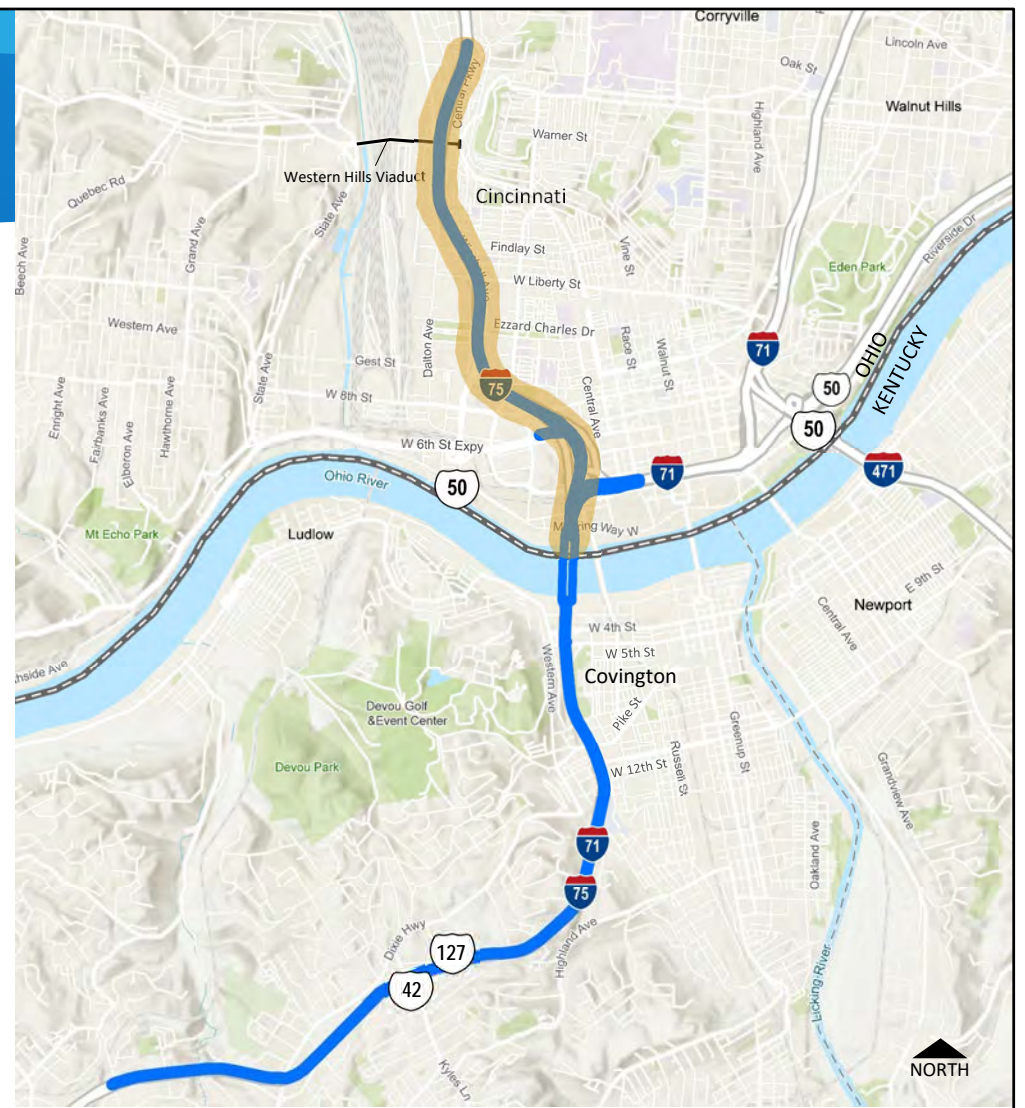
Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, KYTC has begun purchasing the land needed to build the project.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E

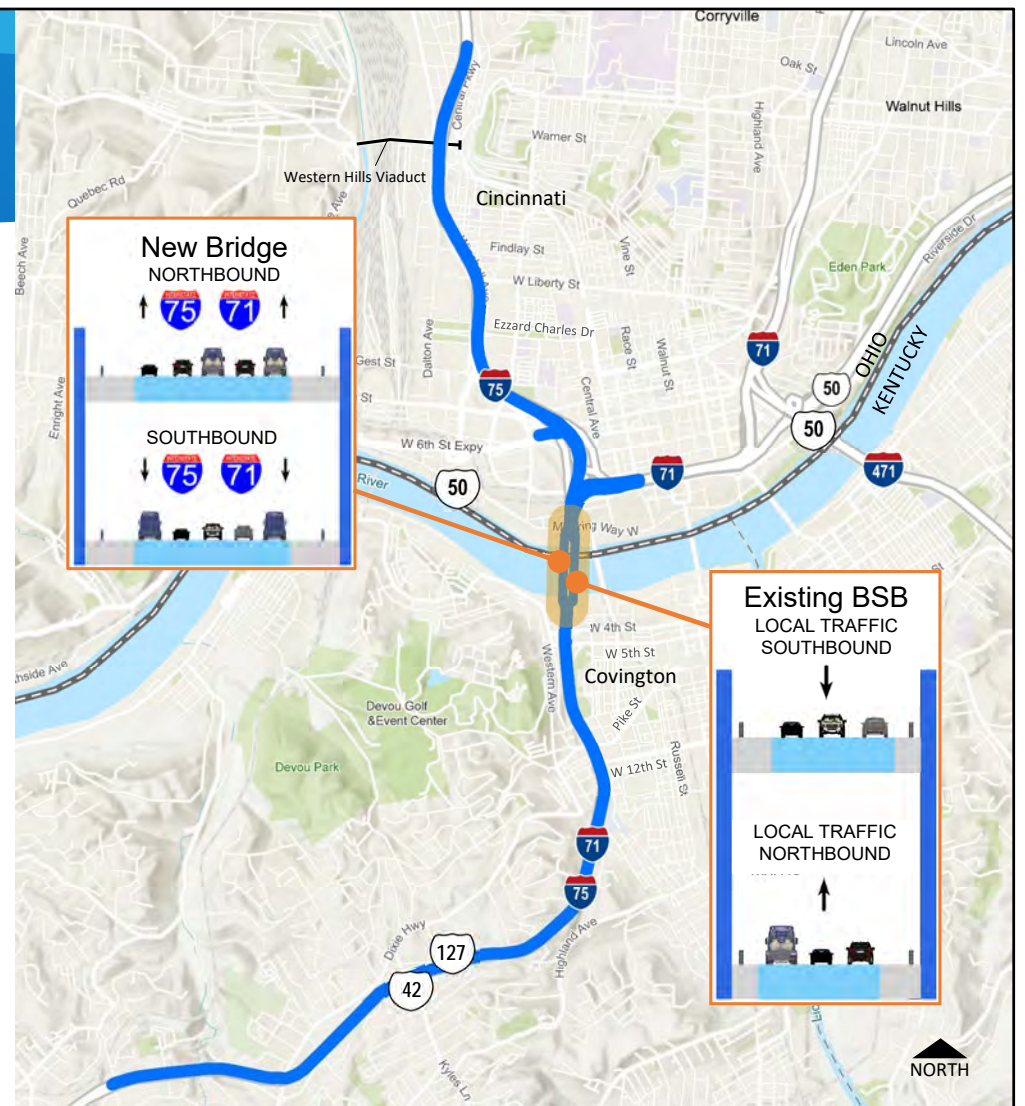


In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

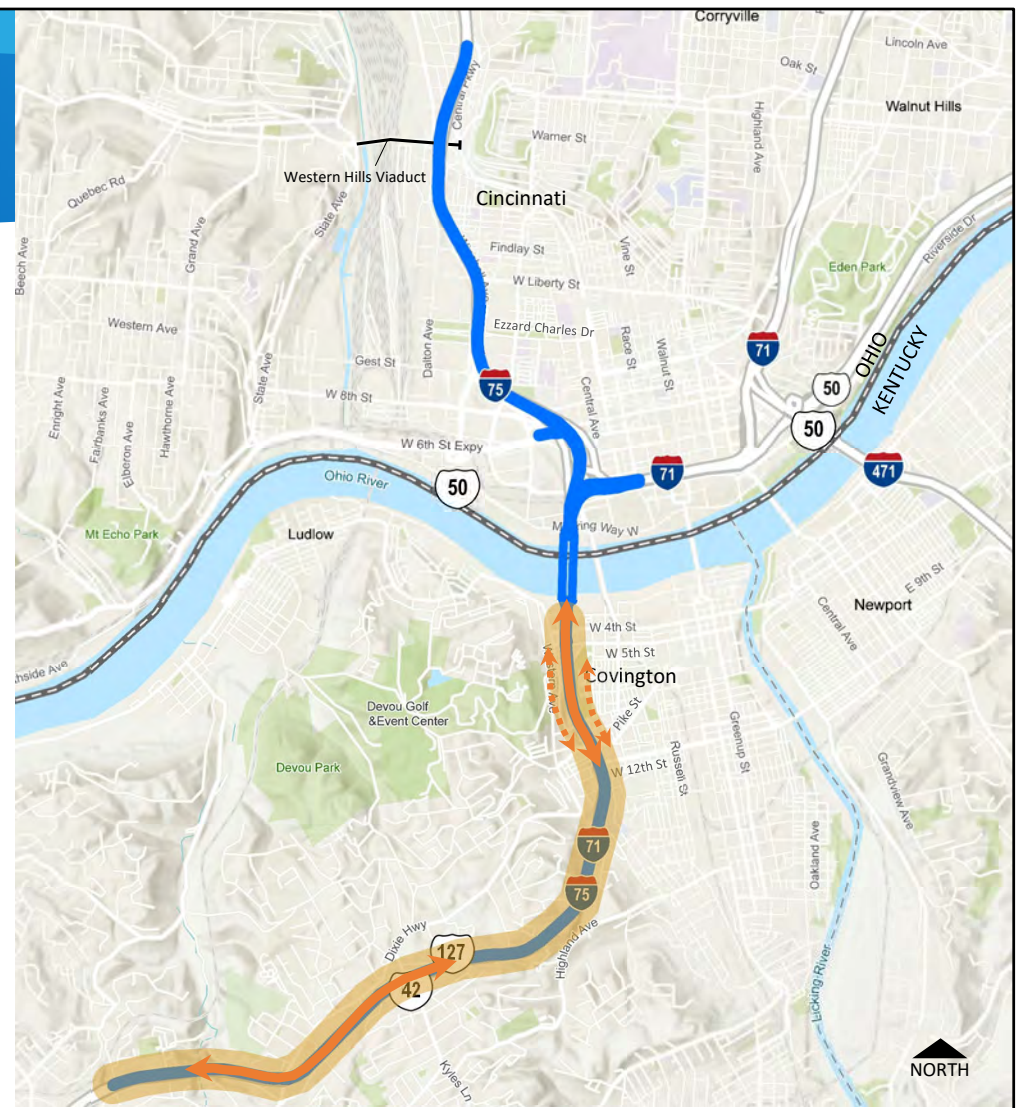
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

# Project Description

## Kentucky

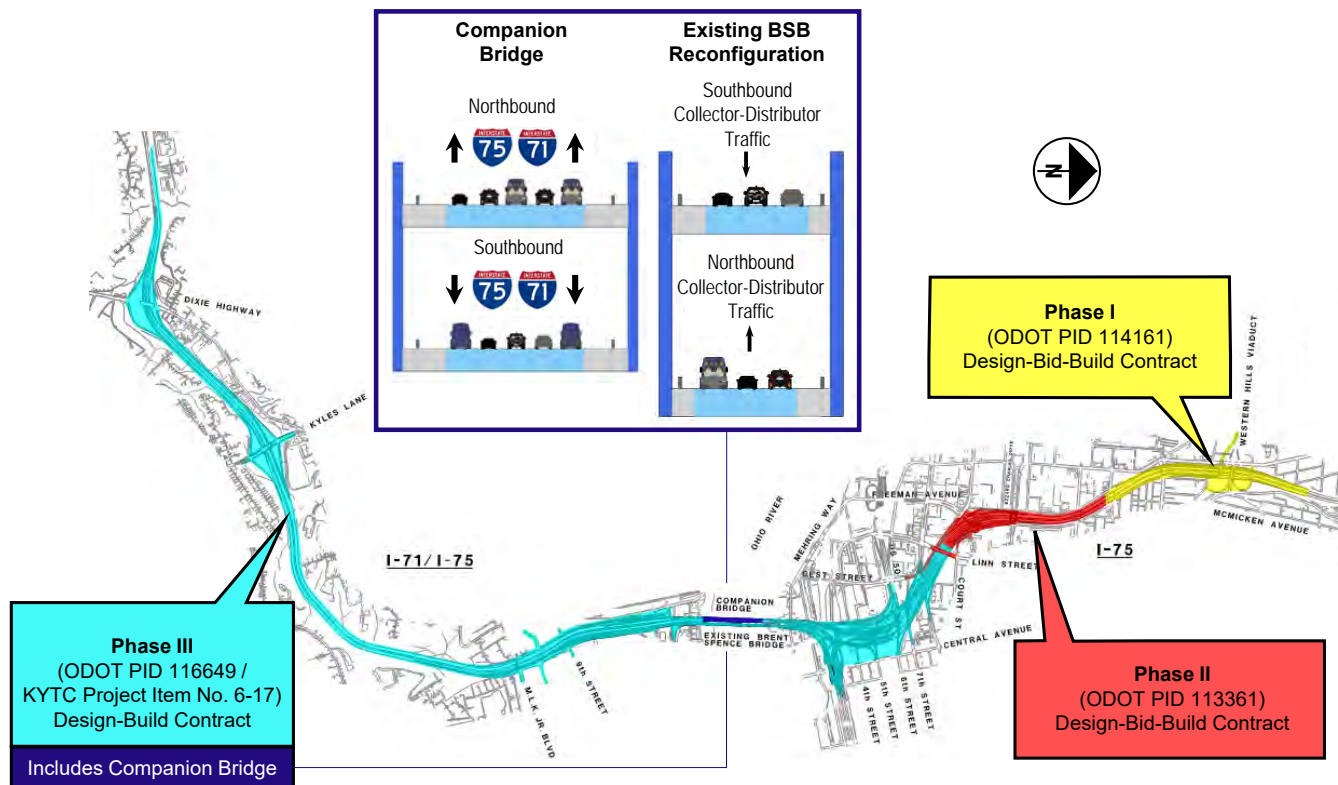
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles Lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

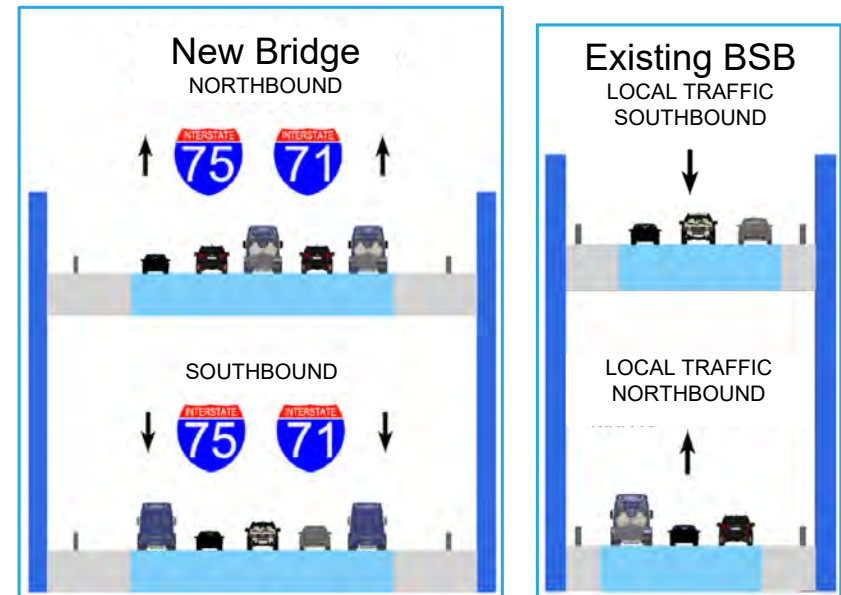
# Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

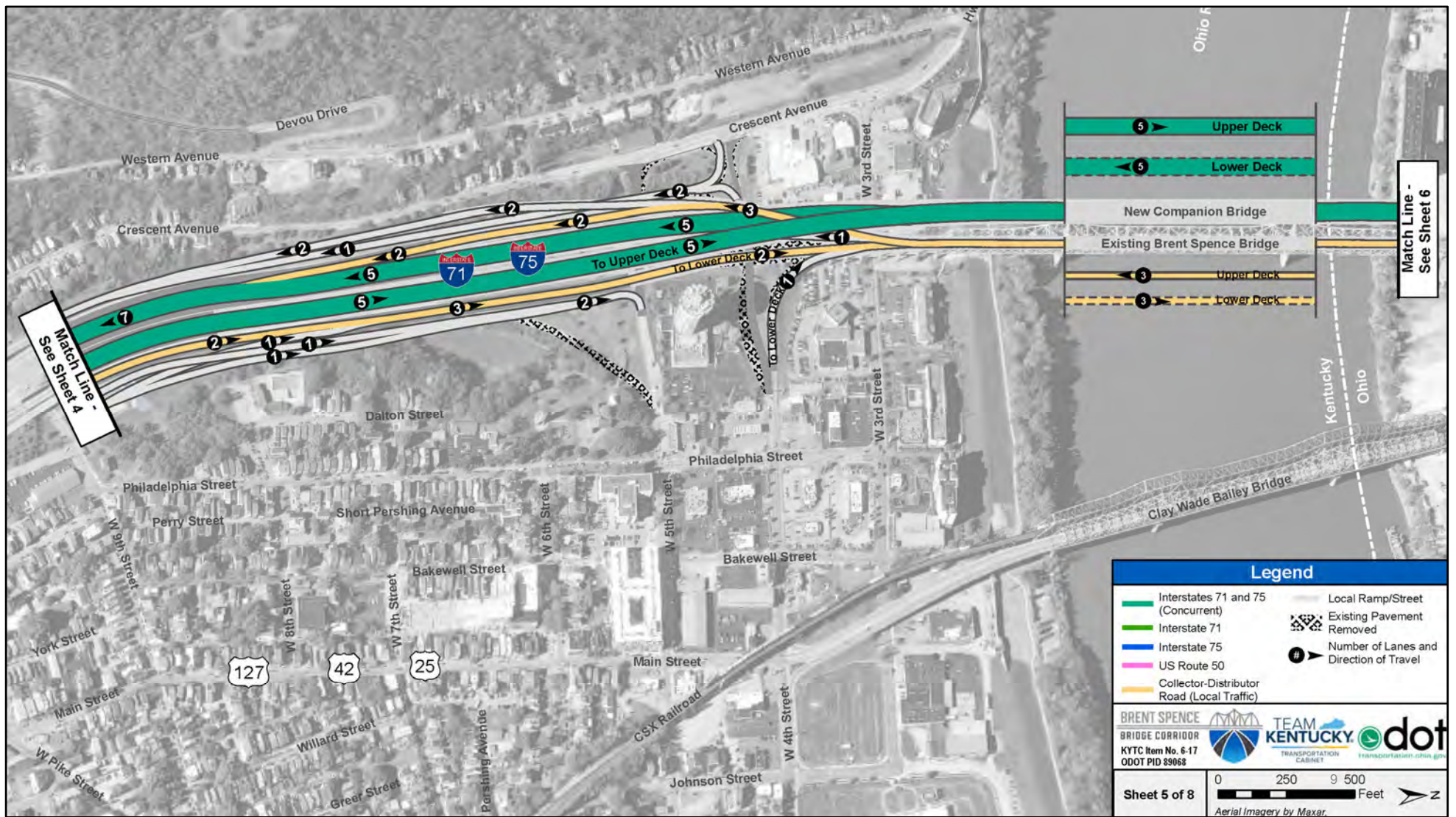
# What Has Changed?

- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



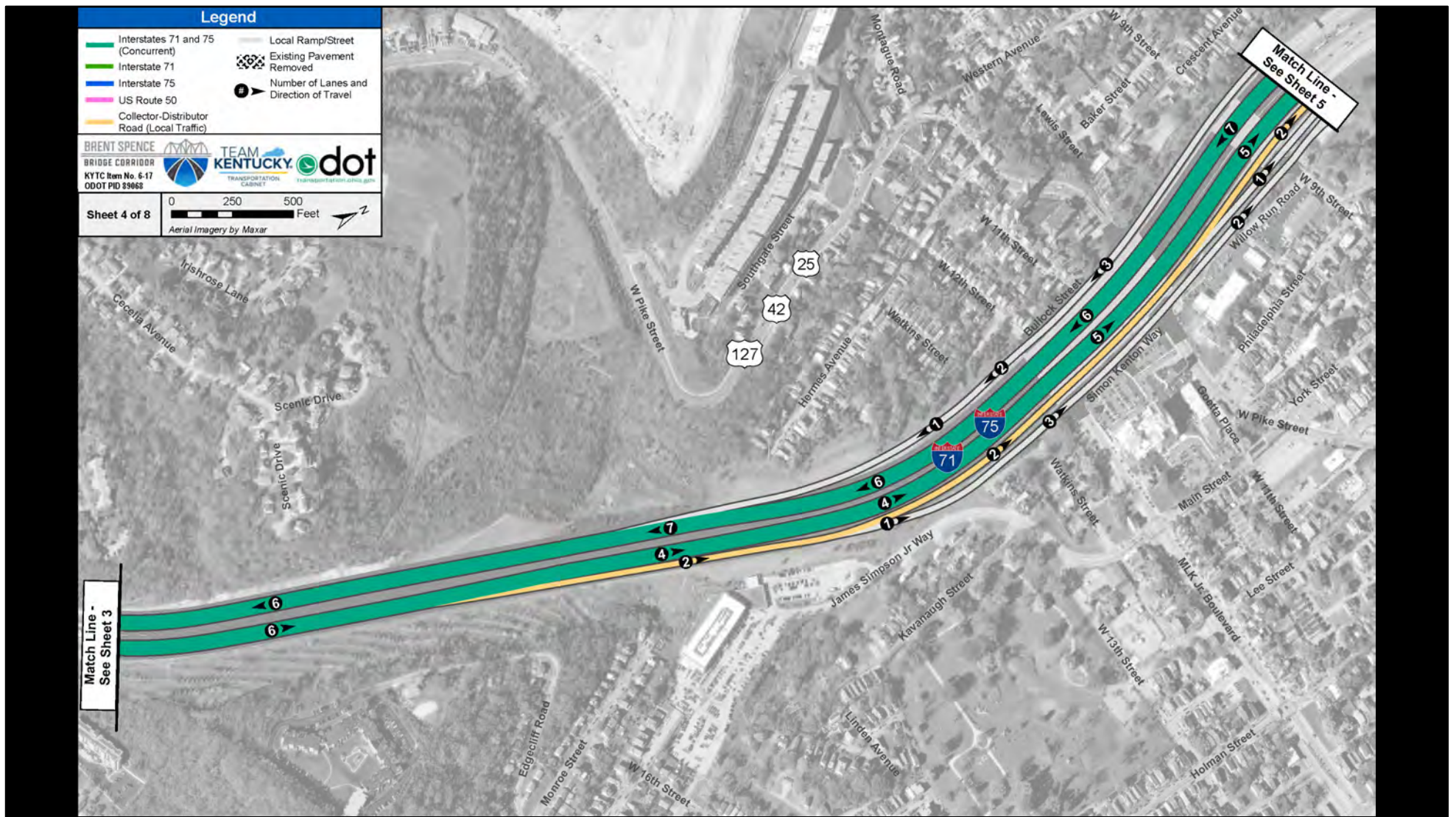
As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.





This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



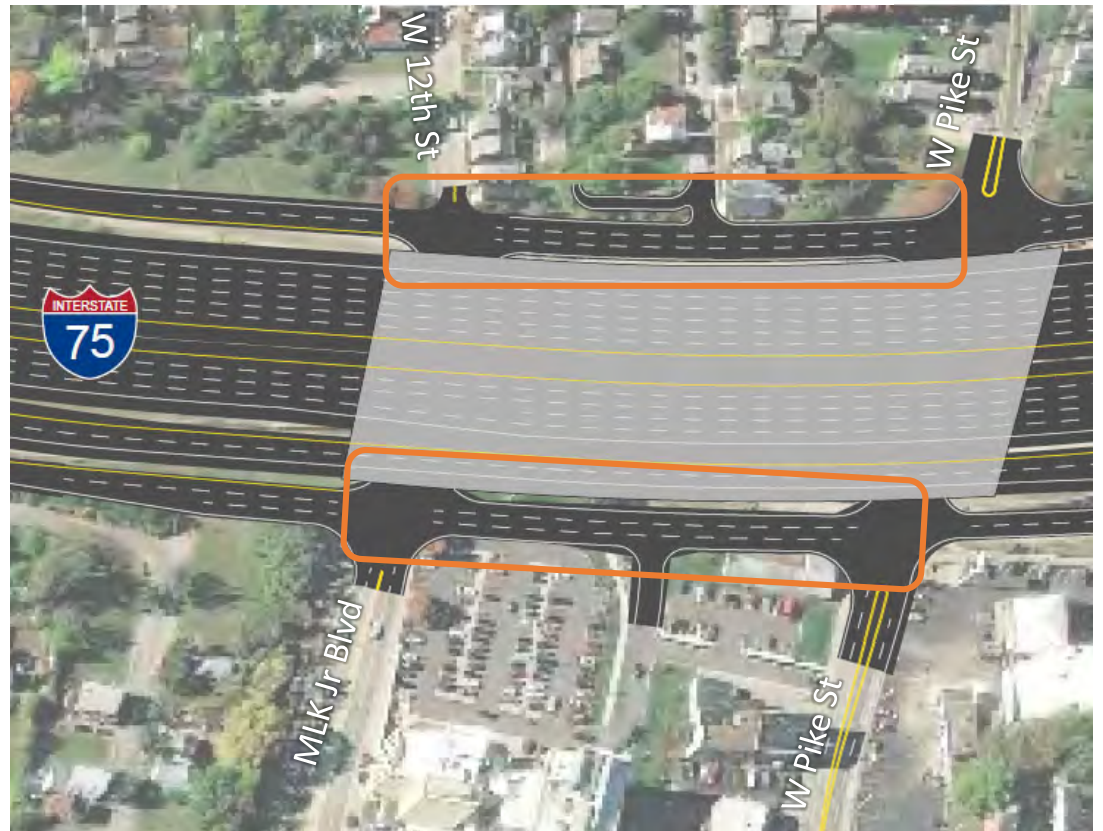
This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.



Here's another view from Covington near Mainstrasse.

# What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 4 vs. 40+
  - Commercial | 5 vs. 6



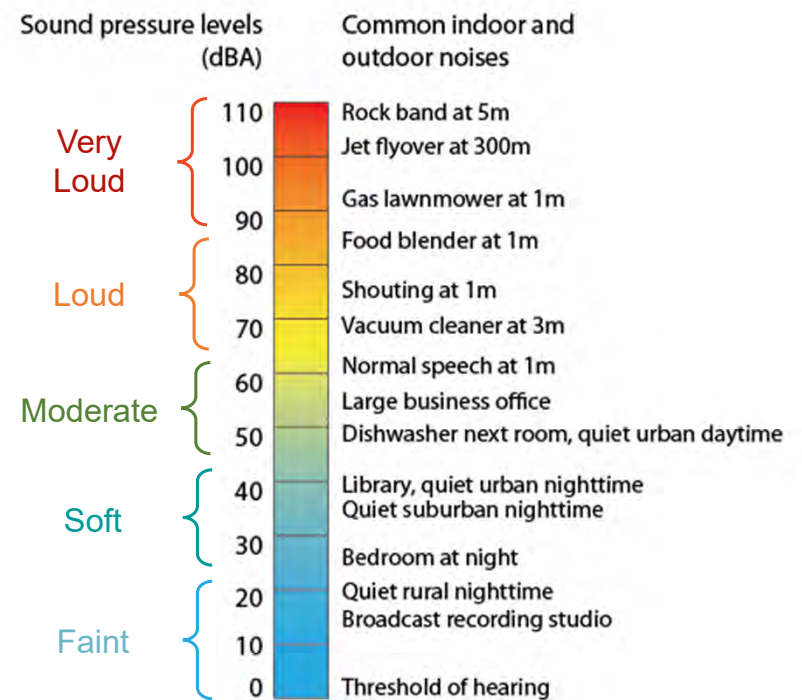
Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound



Source: Minnesota Pollution Control Agency

Now let's dig into how the project will impact Mainstrasse, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

- KYTC Noise Policy
- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.

Kentucky Transportation Cabinet  
Department of Highways  
Division of Environmental Analysis



## NOISE ANALYSIS AND ABATEMENT POLICY

APPROVED BY:

\_\_\_\_\_  
Todd Jeter, Division Administrator  
Federal Highway Administration

\_\_\_\_\_  
Date

\_\_\_\_\_  
James Gray, Secretary  
Kentucky Transportation Cabinet

\_\_\_\_\_  
Date

Revised August 1, 2022  
**EFFECTIVE DATE JULY 1, 2020**  
Revised July 1, 2015  
Original July 2011

KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dBA, while the threshold for a house receptor is 67 dBA. We also determine if there is a substantial increase – specifically a 10 dBA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dBA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

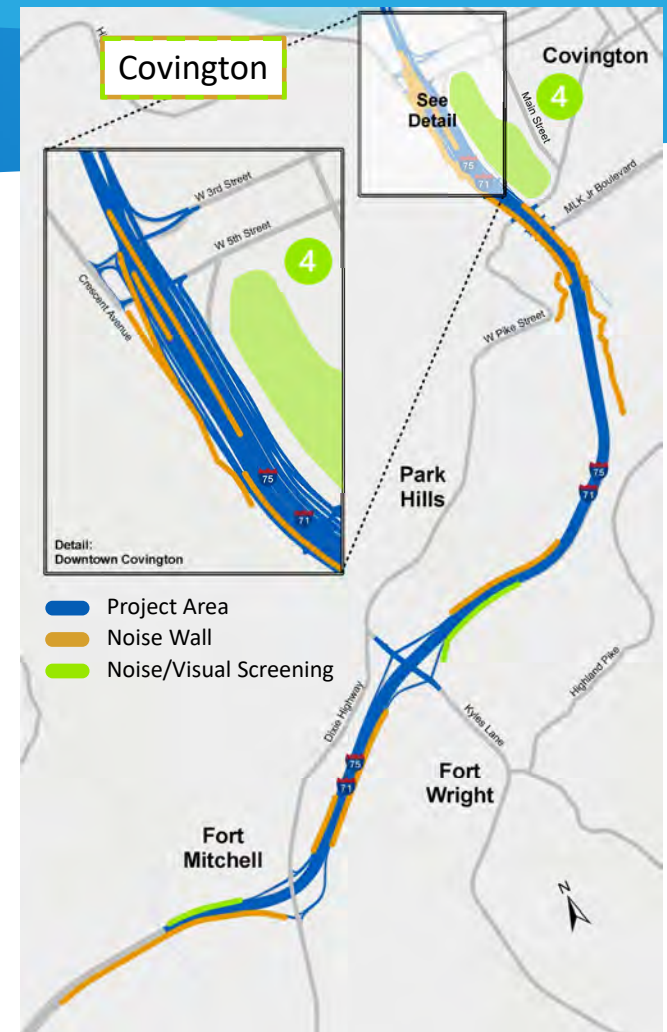
If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

# Noise

Noise walls not reasonable/feasible per KYTC Noise Policy

- 4 • W. 5th St to Pike St (Mainstrasse)
  - Feasible
  - Not cost effective
  - Did not meet design goals
  - KYTC is evaluating further



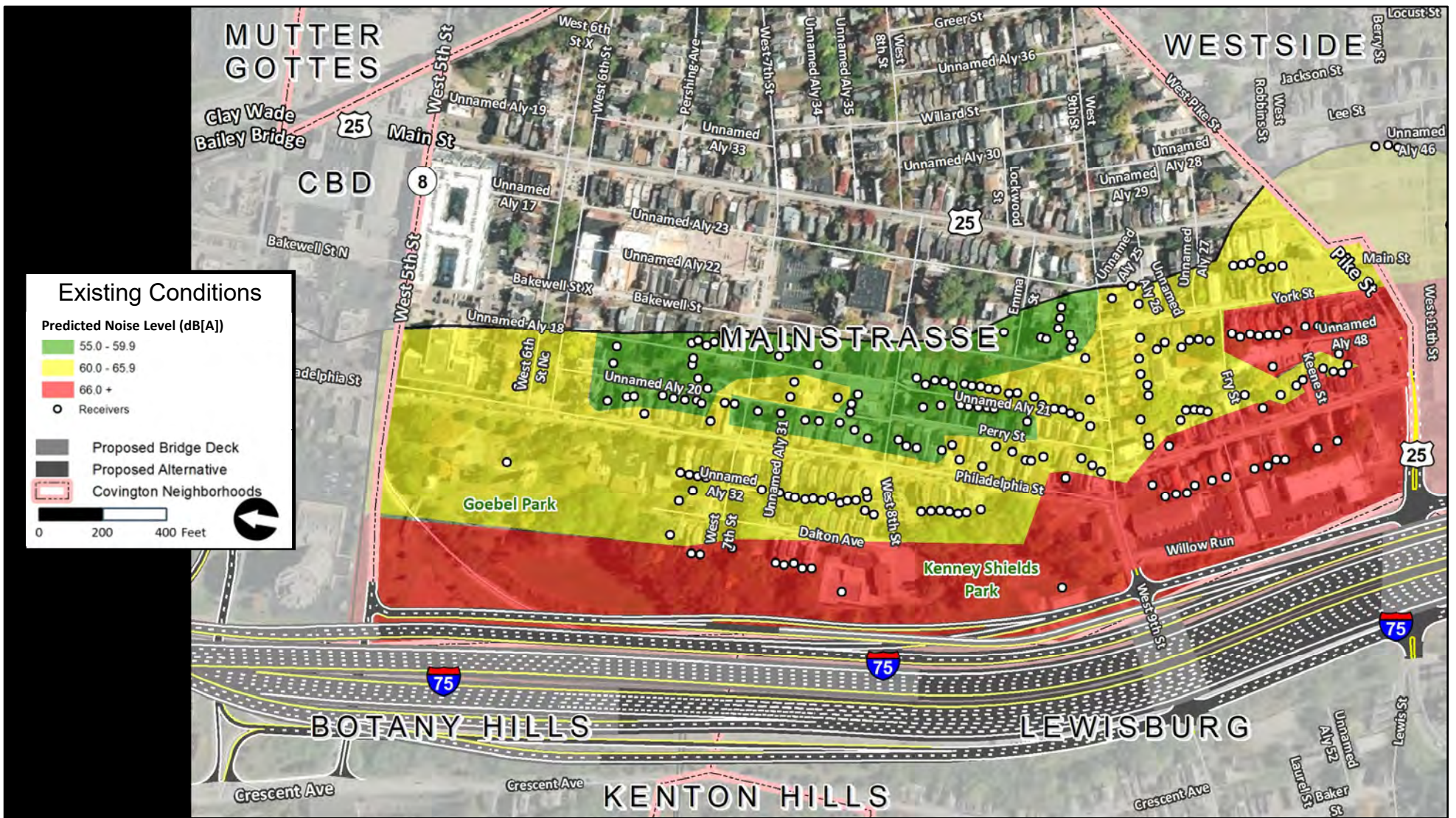
Now that we've provided some noise background, let's review how KYTC is addressing traffic noise in the areas in Mainstrasse. KYTC evaluated several noise walls and noise wall systems in Covington. Some of them met the feasible and reasonable criteria we just discussed. However, in Mainstrasse, a noise wall system benefitting the area labeled with the number 4 was found to be feasible but not reasonable because it was not cost effective and did not meet design goals.

Recognizing that traffic noise is a concern, KYTC is currently conducting a technical study to further evaluate a system of noise/visual screening walls to help provide some noise reduction for residences in Mainstrasse and to shield views of the highway. KYTC will document their decision about this noise wall system in a technical memo based on the technical feasibility and comments received during this meeting and other outreach activities.

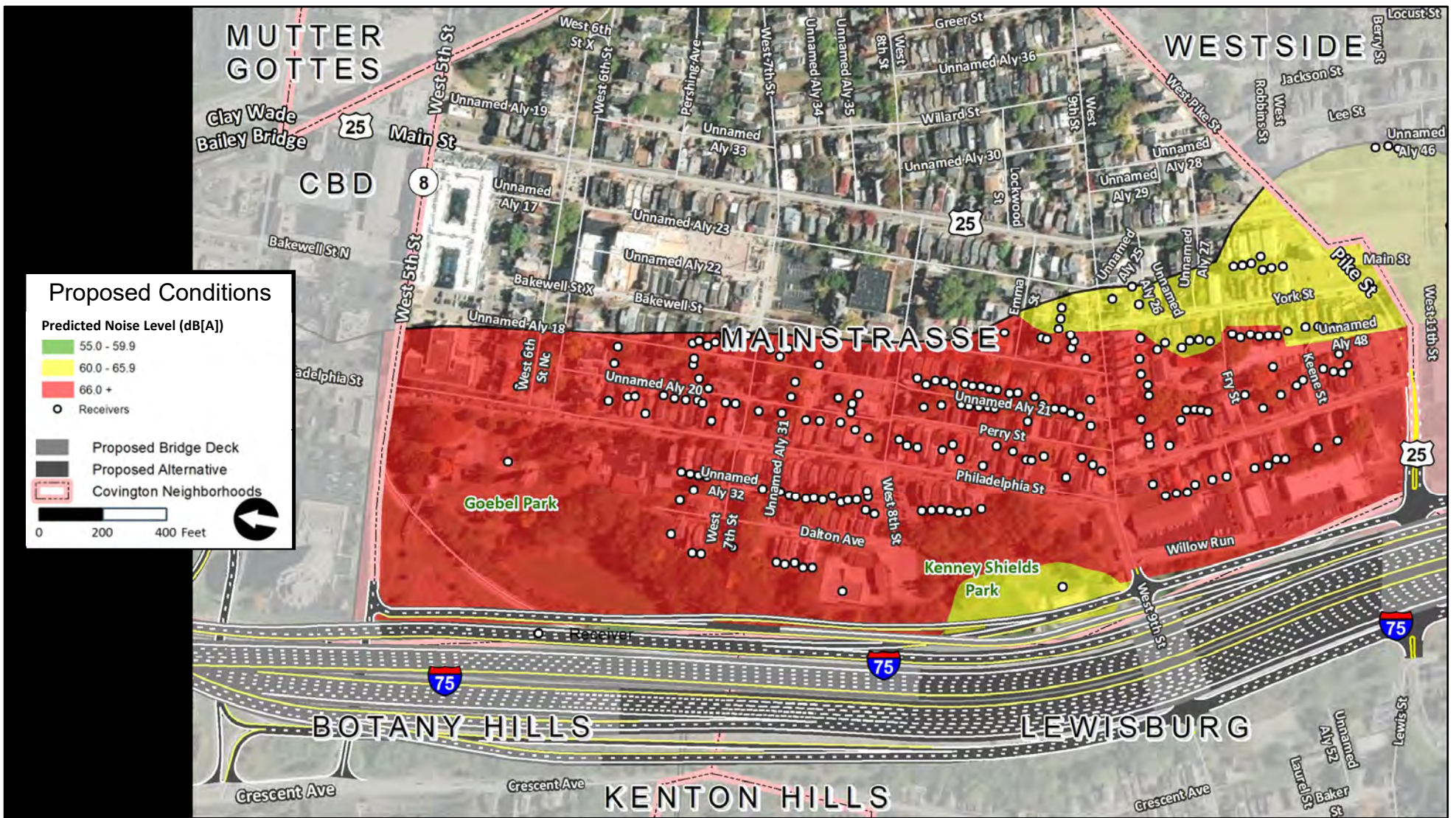


Now let's discuss some specifics from that technical noise study in the vicinity of Mainstrasse.

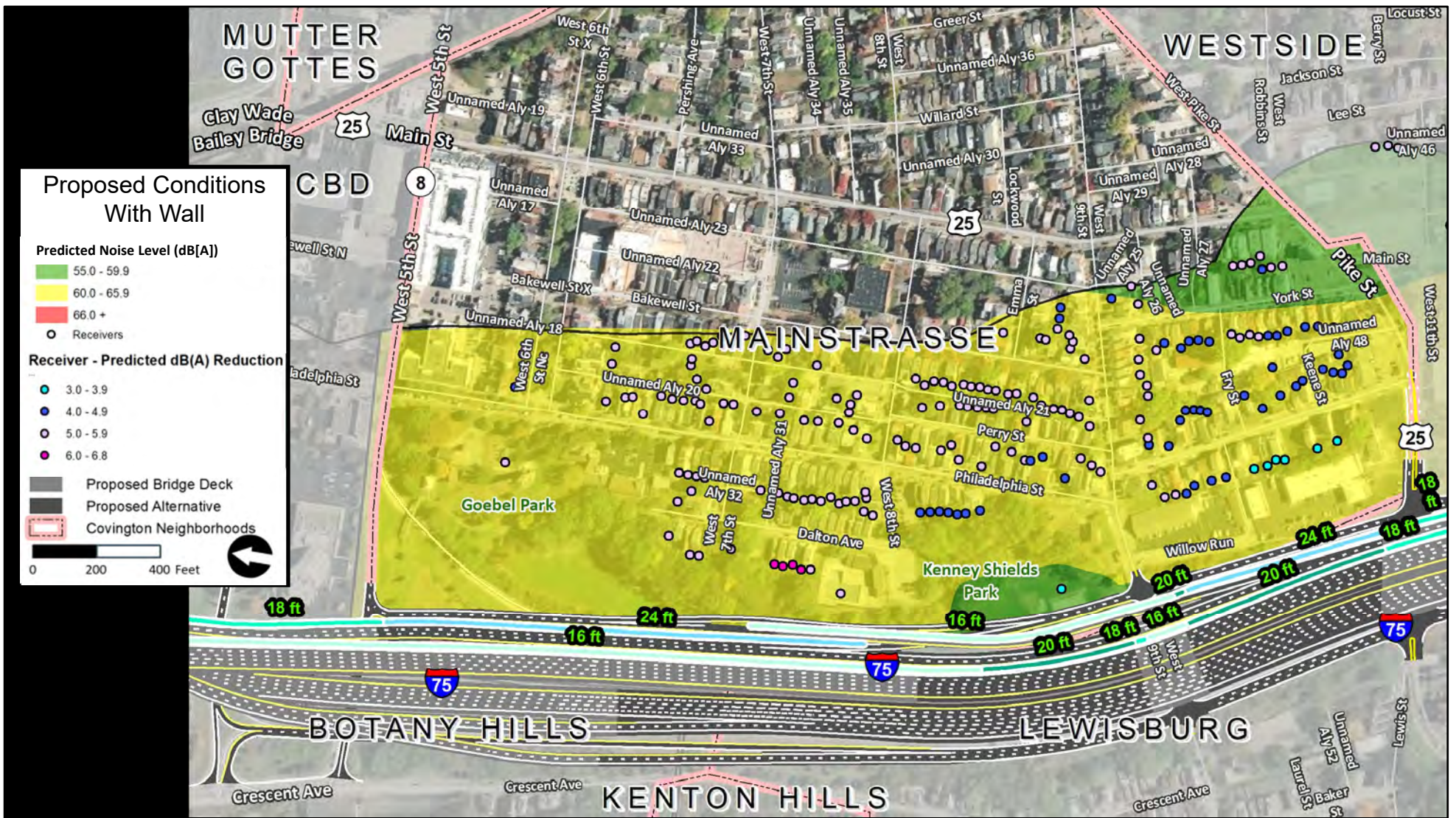




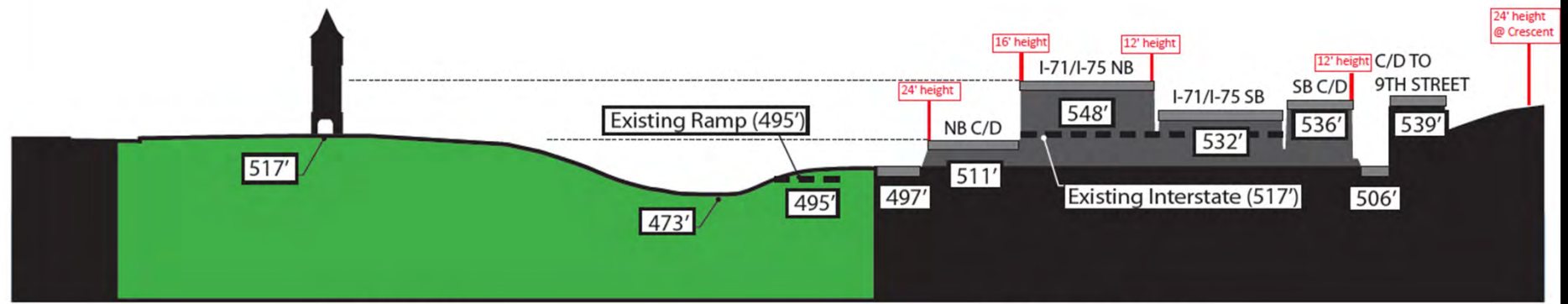
This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



GOEBEL PARK

INTERSTATE CORRIDOR

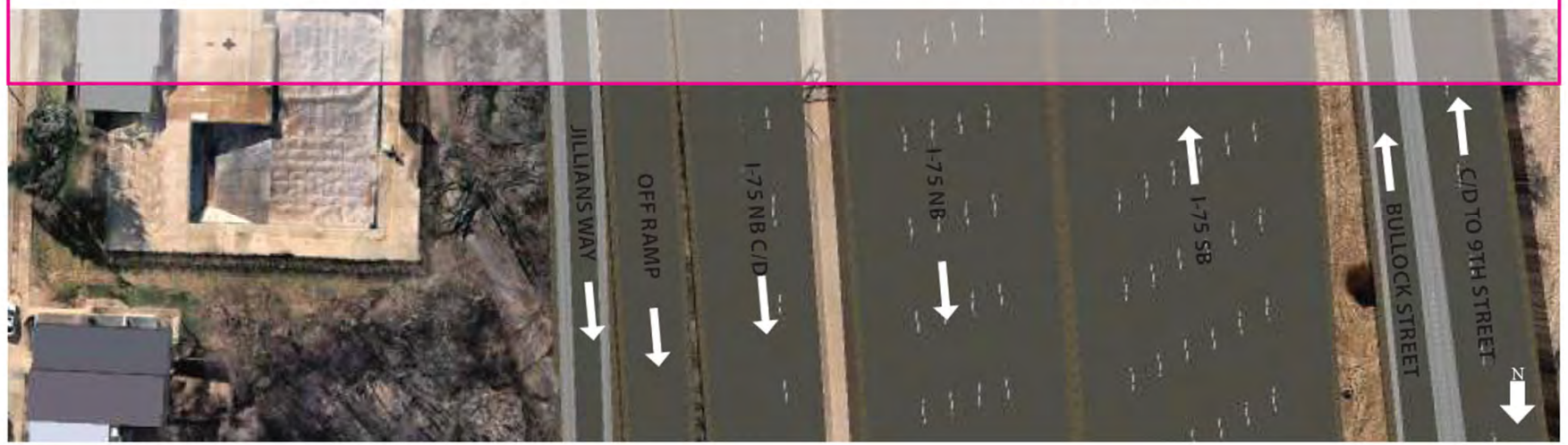
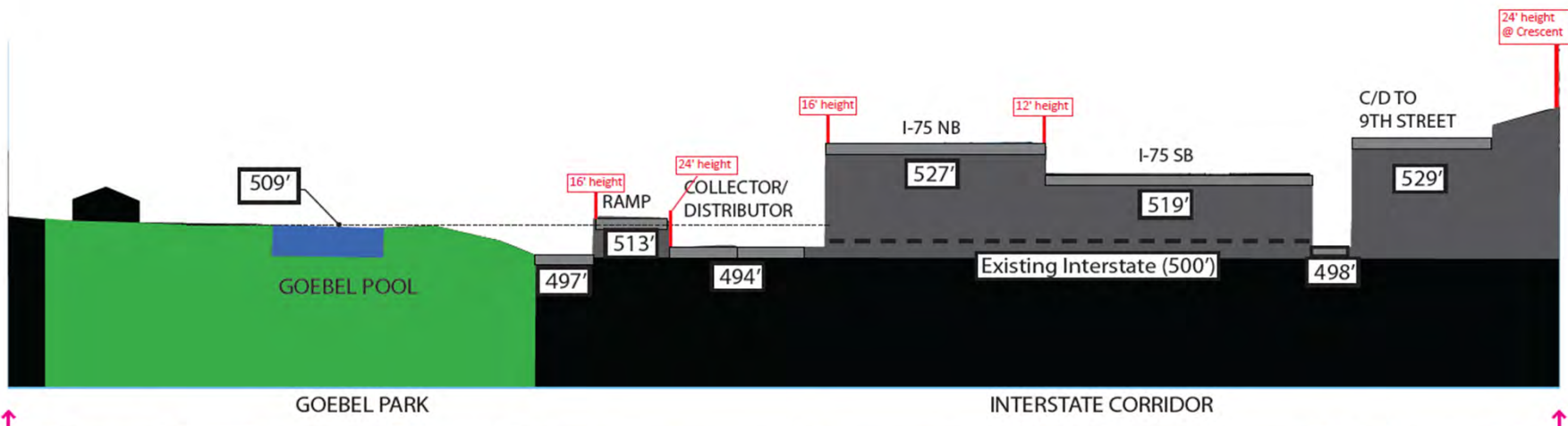


## CROSS SECTION ANALYSIS - CLOCK TOWER

BRENT SPENCE BRIDGE CORRIDOR AESTHETIC GUIDELINE DRAFT | QK4 | HUMAN NATURE | OCTOBER 2022



This slide shows a cross-section of the roadway and noise barriers at the clock tower to demonstrate the overall schematic of the roadway and the height of potential noise walls.



## CROSS SECTION ANALYSIS - GOEBEL POOL

BRENT SPENCE BRIDGE CORRIDOR AESTHETIC GUIDELINE DRAFT | QK4 | HUMAN NATURE | OCTOBER 2022



This slide shows a cross-section of the roadway and noise barriers at the Goebel pool to demonstrate the overall schematic of the roadway and the height of potential noise walls.

# Noise



Pike Street Without Noise Wall (looking west)  
67 dBA (average)



Pike Street With Noise Wall (looking west)  
63 dBA (average)

To the left is a drawing showing what the finished project might look like from Pike Street if a noise wall is not built. To the right is a drawing showing what Pike Street will look like with a noise wall. These photos are looking west on Pike Street.

# Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. As we discussed earlier, KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.

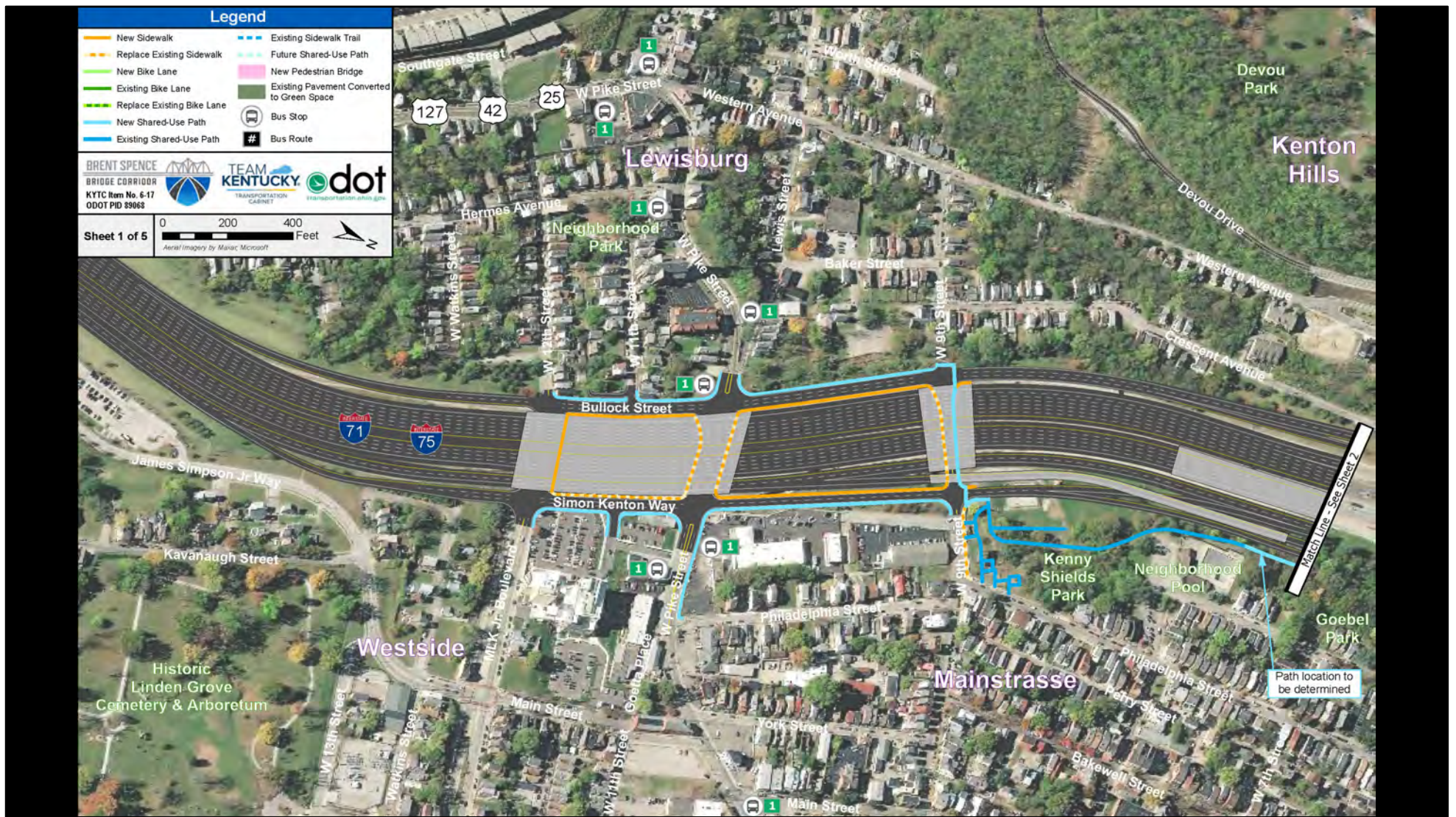
# Enhancement Measures

- Working with Covington to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, and pedestrian paths
- Pedestrians and Bicycles
  - Connections across I-71 and I-75
  - New shared use paths
  - New and rebuilt sidewalks

In addition to coordinating work in Goebel Park, KYTC is also working with Covington to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.



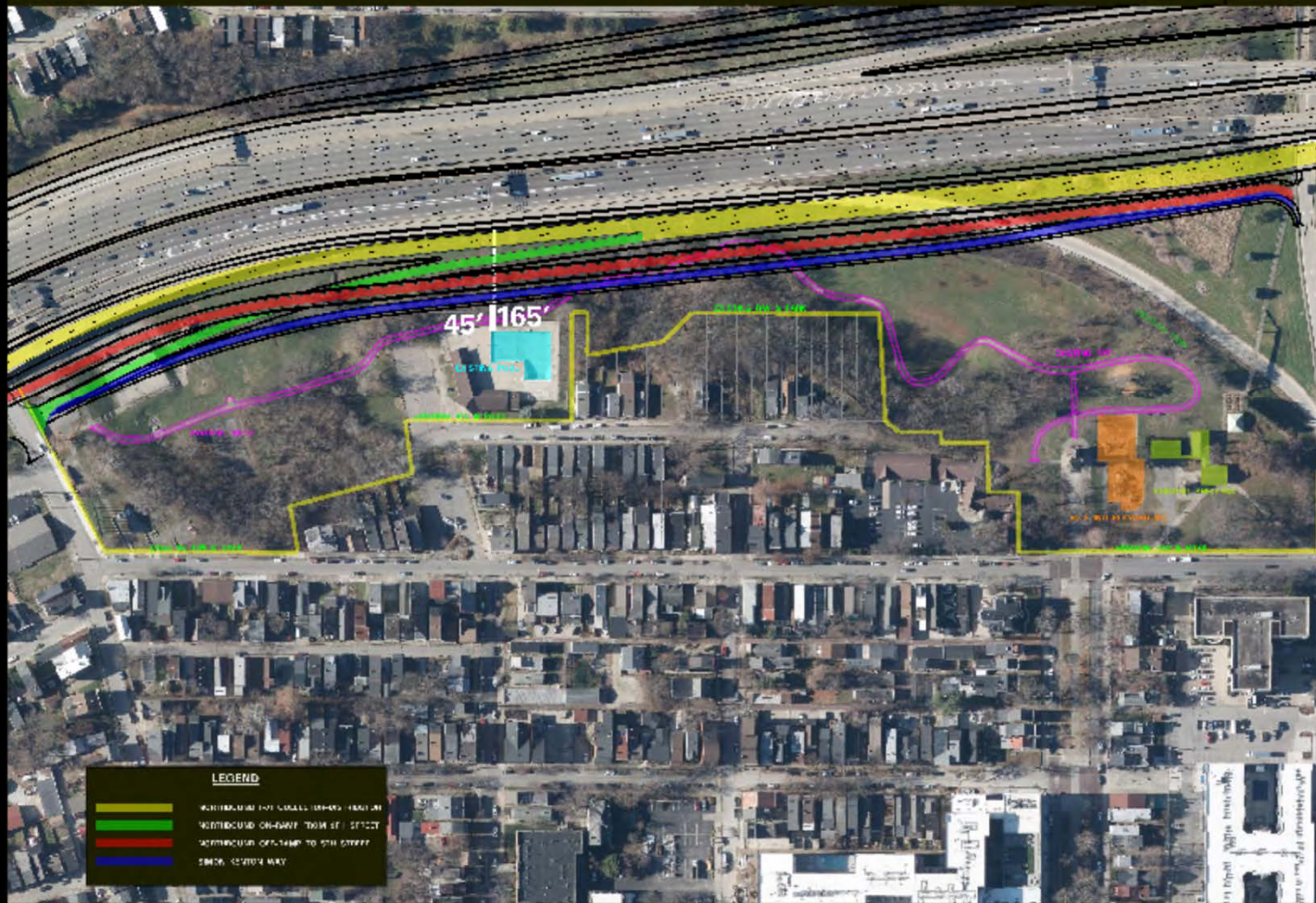


This exhibit shows some of the planned pedestrian and bicycle facilities near Mainstrasse. Note the new shared use paths and sidewalks along the frontage roads. Also notice the new and rebuilt sidewalks under the MLK/West 12th, Pike, and West 9th Street bridges. A new shared use path will be built under the West 9th Street bridge, which will tie into the shared use paths in Goebel Park.



Moving north, note the new shared use path in Goebel Park and under the West 5th Street bridge. The shared use path will be extended along Crescent Avenue to connect to the existing shared use path along the river. Also notice the new and rebuilt sidewalks along West 5th and West 3rd streets. Throughout Covington, the shared use paths and sidewalks will improve connections to existing transit stops.

# GOEBEL PARK EXISTING AND PROPOSED PROPERTY MAP WITH 9th TO 5th ST. CONNECTOR



28

The next topic of discussion is to gather feedback from the community regarding connectivity in Goebel Park. As currently designed, the project includes a local street connector from West 9<sup>th</sup> Street to West 5<sup>th</sup> Street in the northbound direction to provide an enhanced local street network. This roadway is indicated by the blue line.

Also on the diagram:

Red Line: West 5<sup>th</sup> Street off ramp from the collector-distributor road

Green Line: West 9<sup>th</sup> Street on ramp to the collector-distributor road

Yellow Line: Collector-distributor system

# GOEBEL PARK EXISTING AND PROPOSED PROPERTY MAP WITHOUT 9th TO 5th ST. CONNECTOR



29

This graphic shows what the project would look like without the connector between West 9th Street and West 5th Street.

Still shown on the diagram:

Red Line: West 5<sup>th</sup> Street off ramp from the collector-distributor road

Green Line: West 9<sup>th</sup> Street on ramp to the collector-distributor road

Yellow Line: Collector-distributor system



This is a view of what Goebel Park might look like once the highway improvements are constructed, the new shared use paths are built, and agreed upon landscaping is in place.



This view shows how the finished project might appear when looking across I-71 and I-75 toward Mainstrasse.

BRENT SPENCE  
BRIDGE CORRIDOR



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



TEAM  
KENTUCKY  
TRANSPORTATION  
CABINET

This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.



---

Attachment 5: Exhibits





Brent Spence Bridge Corridor - Kentucky  
(Looking Southeast)



Pike Street With Noise Walls (Looking Northwest) - Kentucky



Pike Street Without Noise Wall (Looking West) - Kentucky



Pike Street With Noise Wall (Looking West) - Kentucky



Pike Street Without Noise Wall (Looking East) - Kentucky



Pike Street With Noise Wall (Looking East) - Kentucky

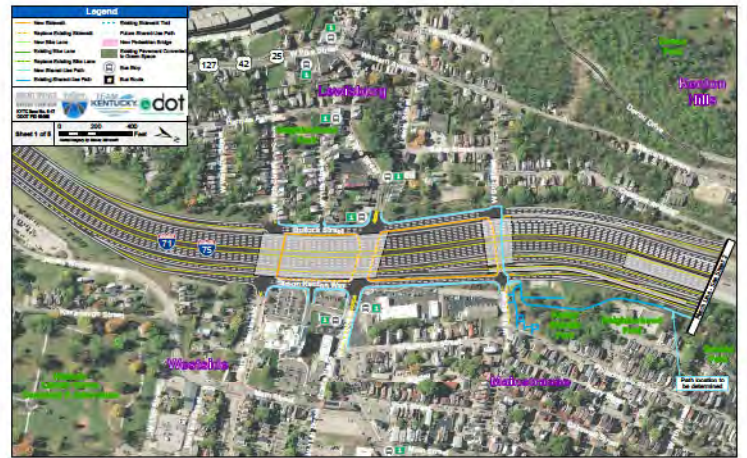




Goebel Park (Looking North) - Kentucky



KYTC (see No. 417) CDOT PG 6688





---

Attachment 6: Comment Forms

# COMMENT FORM

BRENT SPENCE  
BRIDGE CORRIDOR



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident    Area business owner or employee    Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_  
\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

- Please include the noise walls in the plan!
- Please no extension of public road from Pike to 5<sup>th</sup> St.

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Top concerns:  
- Want noise walls!  
- Traffic mitigation and enforcement during construction

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: MBA

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

I APPRECIATE THEY REACHED OUT TO OUR NEIGHBORHOOD, PERHAPS  
FACEBOOK PAGE OR INSTAGRAM?

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Thank you Steve great job!

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: Graduate

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

---

# Brent Spence Bridge Corridor Project Friends of Peaseburg(FOPNA) Neighborhood Association Meeting Summary November 28, 2022

---

## Introduction

The Friends of Peaseburg Neighborhood Association (FOPNA) Neighborhood Meeting was held on November 28, 2022 from 6:00 pm to 8:00 pm. FOPNA conducted their regularly-scheduled neighborhood meeting from 6:00 pm to 7:00 pm. The Brent Spence Bridge (BSB) Corridor Project team presented and answered questions from 7:00 pm to 8:00 pm. The meeting was held at the Covington Firefighter's Hall located at 2232 Howell Street in Covington, Kentucky. It was publicized through the FOPNA Facebook page, the FOPNA email list, and flyers were posted at three local stores, two schools, and a church (see Attachment 1). Attendees at the meeting included the City of Covington, the project team, and members of the public. In total, 22 people attended the meeting, including 6 individuals from the city and the project team (see Attachment 2). The meeting was also streamed via Facebook Live with a maximum of five viewers. Photographs are included in Attachment 3.

The meeting format included introductions by the Mayor of Covington, Joseph U. Meyer and a formal presentation by Stacey Hans (KYTC) (see Attachment 4). During the presentation, the project team responded to questions and comments offered by those present. The mayor concluded the meeting and noted there is an Aesthetics Committee working to support the project. He also noted that the biggest priorities of noise walls and flooding issues are being addressed. He commended KYTC on the way they have managed the project today and when the project will be done. The FOPNA president also commended the presentation and said it was insightful.

Exhibits showing noise wall renderings and lane configurations were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Six (6) comment forms and ten (10) demographic surveys were returned at the meeting.



## Demographics

The following is a breakdown of the responses received from the demographic survey.





---

A write in space was available to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. Comments included:

- Continue meetings throughout our neighborhoods.
- I think you're already going above and beyond on this project.

## Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Clarification / understanding of local versus through traffic and ramp connections.
- Concern over truck traffic and noise.
- Clarification / understanding of the process for noise evaluation and discussion on impacts with / without noise wall.
- Drainage for low lying areas, particularly neighborhoods.
- Clarification of timeline and staging of construction.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.

Comments noted on the survey forms returned at the meeting include:

- Noise / sound walls need to extend south beyond Pike – the current proposed noise walls that extend to Kyles Ln are much needed and wanted.
- Will there be people uprooted? If so, are they getting a fair price for their property? How long will the project take and what about the traffic? Will there be long delays?
- The updated designs are much better than the original plans. We appreciate the effort to retain so many existing homes and businesses. We also appreciate the adjacent infrastructure plans such as storm sewers and other utilities. Thanks for all the extra outreach.
- Great presentation and presenter Stacey; Extremely valuable information and insight; Noise walls area must for Peaselburg residents (thank you); Eliminating Willow Run combined water flow will truly assist in deterring flooding our neighborhood; Timeline seems reasonable; Keep project transparency.



**Table 1: Q & A from Session**

Question	Project Team Response
What have neighbors said about traffic?	The project team is holding meetings in the neighborhoods adjacent to the project to solicit input.
When do you make the decision to go I-71 or I-75 NB?	It will be just past the bridge NB on the Ohio side and will be the only decision to make on the companion bridge. Travelers on the local bridge would have the choice to get on I-71 and can continue on the collector-distributor system to reach I-75.
12 <sup>th</sup> Street will be accessible to both?	All local movements to and from the highway will occur through the collector-distributor system. Local traffic will function similar to how it does today.
Does it affect neighborhoods or Goebel Park?	Some land will be required from Goebel Park, but no residences will be impacted on the east side of the highway.
About how much of the park are you taking?	About 3 acres will be required from Goebel Park, but KYTC will give back 2 acres once the West 5 <sup>th</sup> Street ramp is relocated.
Is the collector-distributor where the storm / sewer line will go?	The presentation provided a map of the storm trunk line.
Will the walking trail through the park be lost? Concerned about trail by Toyota to hospital.	A portion of the walking trail through Goebel Park will be impacted, but it will be replaced. KYTC is committed to providing bike and pedestrian facilities in addition to vehicular traffic.
Question about taking away flood basins. Will they be replaced? We have flooding issues.	Mapping showing flood basins was provided and explained during the presentation.
Where are the residential relocations?	Three residential properties in Covington and one property further to the south will be relocated. The three in Covington are all on the west side of the interstate. The one to the south is on the east side of the interstate. Other properties may have strip right-of-way acquisition. KYTC is currently acquiring right-of-way. Commercial relocations are primarily on the west side of the highway. Billboards are technically considered commercial property.
Jake Braking? Enforcement?	KYTC cannot prohibit jake braking on state-maintained roadways since it is a safety function.
Will the expanded interstate be noisier?	Ideally adding capacity will allow traffic to flow more freely and possibly slightly reduce noise.



Question	Project Team Response
Are Crown Pointe apartments protected / affected?	The Crown Pointe apartments are further north. Interstate may be pulling further away, so noise walls will not be as effective.
Specifically point out where the noise wall starts and stops and where Highland Avenue is.	Noise wall locations were clarified during the presentation.
Round of applause for noise walls!	Comment noted.
Will this help with flooding on 27 <sup>th</sup> and Russell?	Flooding issues in that location are beyond what KYTC can control with the BSB Corridor Project. The local government would need to be contacted about other flooding issues.
Where does the riverfront walkway go? Under / through bridge?	The riverfront walkway has been built under the BSB along the Ohio River. The BSB Corridor Project will connect shared use paths in Goebel Park to this trail.
How will vehicles access Covington going north or south?	All access to and from Covington will be off the collector-distributor.
How much of that time is “no bridge”?	It is anticipated the companion bridge will be built first. Once traffic is transferred to the companion bridge, repair work to the existing bridge will occur. KYTC does not anticipate any full closures of traffic across the river unless a major incident occurs. The project will establish a maintenance of traffic and incident management plan during construction.
Will the Roebling Bridge be protected?	Yes.



---

Attachment 1: Advertising Materials

## Seth Turner

---

**From:** FOPNA Peaselburg  
**Sent:** Tuesday, November 1, 2022 12:02 PM  
**To:** Sherry Kish  
**Cc:** Seth Turner  
**Subject:** Re: Brent Spence Bridge Presentation on 11/28/22

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

Sherry and Seth,

Thank you for reaching out to me regarding spreading the word about our upcoming meeting.

I already informed our residents at our recent meeting of this presentation opportunity. I have marked below the areas that I will be happy to share the information. I also think we can begin your presentation at 7pm....would you kindly update the flyer and resend.

Thanks in advance,

Susan

On Fri, Oct 28, 2022 at 3:54 PM Sherry Kish wrote: [\\_\\_\\_\\_\\_](#)

Susan,

Thank you so much for allowing the Brent Spence Bridge project team to attend the Friends of Peaselburg neighborhood meeting you are setting up on November 28<sup>th</sup>. As you know, the project team wants to share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

Our goal is to be sure all notifications about meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous neighborhood meeting?	X	10/24/22

Email list	Can you email a neighborhood wide or citywide email list with information about BSB presentation at the upcoming meeting?	X I WILL EMAIL BY 11/4/22	
Facebook (private to neighborhood)	Can you share a Facebook message about BSB presentation?	X I WILL POST ON BOTH FOPNA AND THE COVINGTON NEIGHBORHOOD COLLABORATIVE FB PAGES WITH ABILITY TO SHARE	
Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?	X WE HAVE 3 LOCAL STORES I WILL HANG FLYERS	
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?	X WE HAVE TWO ELEMENTARY SCHOOLS AND A CHURCH THAT I WILL SHARE THE FLYER WITH AND ASK THAT THEY SHARE WITH PARENTS.	
Other	Is there another outlet you have available to get the word out?	NA	
Do you need assistance from us with advertising?	We made a flyer for your consideration (attached to this email).	I WILL SHARE VIA EMAIL, FACEBOOK AND HANG LOCALLY.	

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!  
Sherry

**Sherry R. Kish**

**HNTB CORPORATION**

88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

**100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

--

Susan Barnett  
FOPNA Chairperson

--- CONFIDENTIALITY STATEMENT ---

**This e-mail transmission contains information that is intended to be privileged and confidential. It is intended only for the addressee named above....please do not forward without permission!**

\* If you receive this e-mail in error, please do not read, copy or disseminate it in any manner. If you are not the intended recipient, any disclosure, copying, distribution or use of the contents of this information is prohibited. Please reply to the message immediately by informing the sender that the message was misdirected. After replying, please erase it from your computer system. Your assistance in correcting this error is appreciated.

## Seth Turner

---

**From:** FOPNA Peaselburg  
**Sent:** Monday, November 28, 2022 1:48 PM  
**To:** Seth Turner  
**Subject:** Fwd: Brent Spence Bridge Project Meeting hosted by FOPNA 11-28-22  
**Attachments:** KYTC -ODOT - FOPNA Bridge Flyer.pdf

Hello Seth,

Attached is the email that I sent to our neighbors....it was sent to about 50 residents. We also posted it on our FaceBook page - Friends of Peaselburg. In addition, I hung a couple of the flyers in our local businesses.

If you have any additional questions, please just let me know.

Susan

----- Forwarded message -----

**From:** **FOPNA Peaselburg** [\\_\\_\\_\\_\\_](#)  
**Date:** Fri, Nov 4, 2022 at 12:41 PM  
**Subject:** Fwd: Brent Spence Bridge Project Meeting hosted by FOPNA 11-28-22  
**To:**

Hello Neighbors,

As mentioned at our recent FOPNA meeting, I was contacted by the HNTB Corporation which has been contracted by the Kentucky Transportation Cabinet (KTC) and the Ohio Department of Transportation (ODOT) regarding the Brent Spence Bridge Project. Representatives from the Bridge Project Team requested a meeting with FOPNA so they can discuss and share project updates.

Therefore, we have invited them to provide a presentation after our November FOPNA meeting on November 28th. *Our November 28th FOPNA meeting will begin at 6pm and the Brent Spence Bridge presentation will begin at 7pm.*

**Here are some details and links for further information prior to the meeting:**

The presentation should last about 20-30 minutes with the goal to share a little about the project and then answer any questions we may have.

Information boards will be up for display so it's easier to see the proposed changes. Team members will stay after the meeting to discuss, if necessary.

FOPNA hopes to be able to stream this meeting on FaceBook Live via our Friends of Peaselburg Page.

The team is sharing website information in advance to allow our neighbors to formulate questions in advance.



Below is some information and links to website/webpages to allow you to formulate questions in advance:

- The project team has submitted applications for two federal grants.
- Project website link. <https://brentspencebridgecorridor.com/>
- News and past newsletters are linked on this page: <https://brentspencebridgecorridor.com/news/>
- At the bottom of each page of the website is a sign-up form for newsletters and a comment/question form in case someone has a question before the meeting.

We hope you will join us! Please share this information with your neighbors so we are all informed about this important and informative meeting that will effect our neighborhood and city.

Thanks in advance,

Susan

Susan Barnett  
FOPNA Chairperson


--- CONFIDENTIALITY STATEMENT ---

**This e-mail transmission contains information that is intended to be privileged and confidential. It is intended only for the addressee named above....please do not forward without permission!**

\* If you receive this e-mail in error, please do not read, copy or disseminate it in any manner. If you are not the intended recipient, any disclosure, copying, distribution or use of the contents of this information is prohibited. Please reply to the message immediately by informing the sender that the message was misdirected. After replying, please erase it from your computer system. Your assistance in correcting this error is appreciated.

# FOPNA Facebook Page

Facebook.com





**Friends of Peaseburg Neighborhood Association**


### Intro


Peaseburg is nestled in the hills and valleys on the west side of the city of Covington in Northern

---

 **Page** · Community

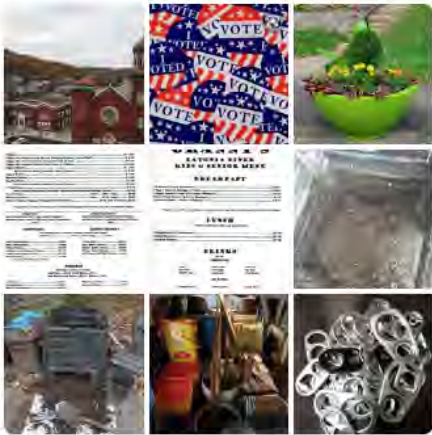
 Peaseburg, KY


 (859) 547-5563

 friendsofpeaseburg@gmail.com

### Photos

[See all photos](#)





**Friends of Peaseburg Neighborhood Association**  
November 8 at 2:48 PM · 🌐

### Brent Spence Bridge Project Discussion 11-28-22

FOPNA was contacted by the HNTB Corporation which has been contracted by the Kentucky Transportation Cabinet (KTC) and the Ohio Department of Transportation (ODOT) regarding the Brent Spence Bridge Project to set a neighborhood meeting to discuss the bridge plan and share updates to the project.

Therefore, we will be hosting them to provide a presentation after our November FOPNA meeting on November 28th.

- FOPNA meeting at 6pm
- Brent Spence Bridge presentation at 7pm.

Here are some details and links for further information prior to the meeting:

The presentation should last about 20-30 minutes to share a little about the project and then answer any questions we may have. Information boards will be up for display so it's easier to see the proposed changes.

FOPNA is planning to stream this meeting on FaceBook Live via this Friends of Peaseburg Page.

Below is information and links to website/webpages to allow you to formulate questions in advance:

- \*The project team has submitted applications for two federal grants
- \*Project website link. <https://brentspencebridgecorridor.com/>
- \*News and past newsletters are linked on this page: <https://brentspencebridgecorridor.com/news/>

NOTE: At the bottom of each page of the website is a sign-up form for newsletters and a comment/question form in case someone has a question before the meeting.

We hope you will join us! Please share this information with your neighbors so we are all informed about this important and informative meeting that will effect our neighborhood and city.



Attachment 2: Sign-In Sheet





SIGN-IN SHEET

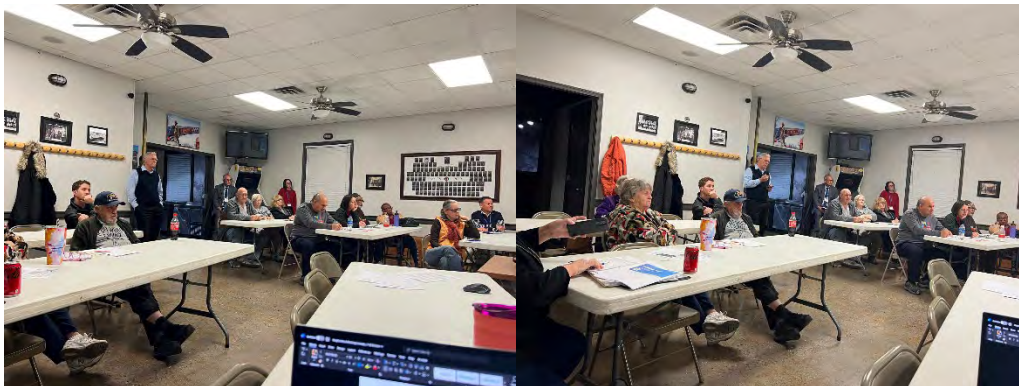
**BRENT SPENCE BRIDGE CORRIDOR PROJECT**  
Neighborhood Outreach Meeting — November 28, 2022

Name	Representing	Address	Phone	Email
1. Susan Barrett	Friends of Peaseburg			
2. JANITA WINDLE	Friends of Peaseburg			
3. Betty Schumacher	"			
4. MIKE TORRE	"			
5. Yola Reagon	"			
6. Larry Kibbgood	"			
7. Randy Fitzwater	"			
8. Deanna Fitzwater	"			
9. DANAN DUMALLIS	FOPNA			
10. Heidi Buchman	FOPNA			
11. RHONDA CLAWNER	FOPNA			
12. Pete Neroni	FOPNA			
13. Jill Corwin	FOPNA			
14. Clay Tenberge	FOPNA			
15. Sheila Gray	FOPNA			
16. Judi Heilig	HNTB			
17. Mark Behreer	HNTB			
18. Sharré Hobb	KYTC			
19. Gary Galister	KYTC			
20. Lindsay Lumbel	HMB			

Please print clearly • Contact information is optional but must be included if you wish to receive updates.  
page \_\_\_\_ of \_\_\_\_  
KYTC Item No. 6-17 | ODOT PID 89068



### Attachment 3: Photographs





---

[Attachment 4: Presentation](#)

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



# Peaseburg Neighborhood Meeting (KY)

November 28, 2022



INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with introductions of the Project Team in attendance (2 min).

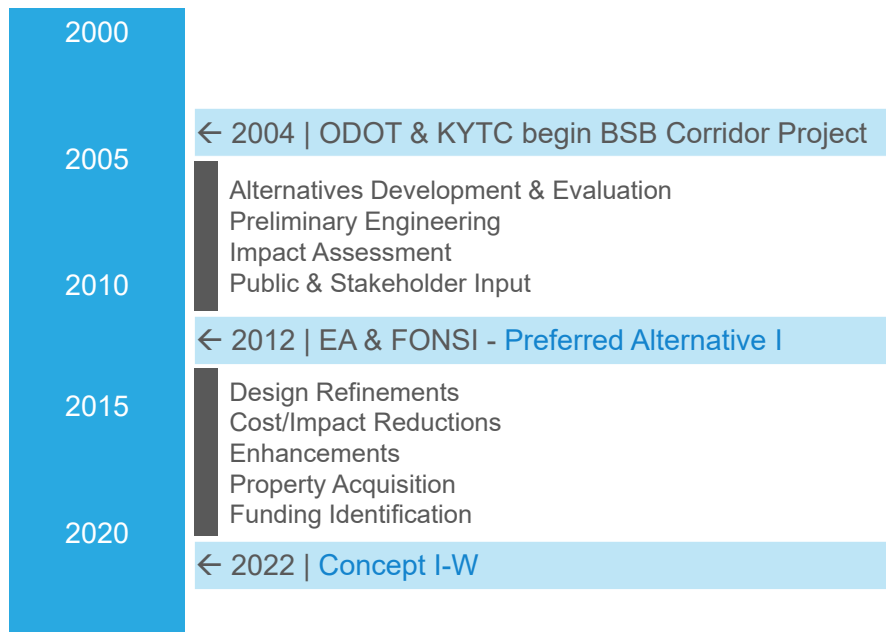
# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the Peasenburg neighborhood the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Peasenburg area
  - Discussion/feedback from Peasenburg residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Peasenburg neighborhood. Most importantly, tonight we're focusing on hearing from the residents of Peasenburg. We're here to answer your questions and to listen to your feedback about the project.

# Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, KYTC has begun purchasing the land needed to build the project.



# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E

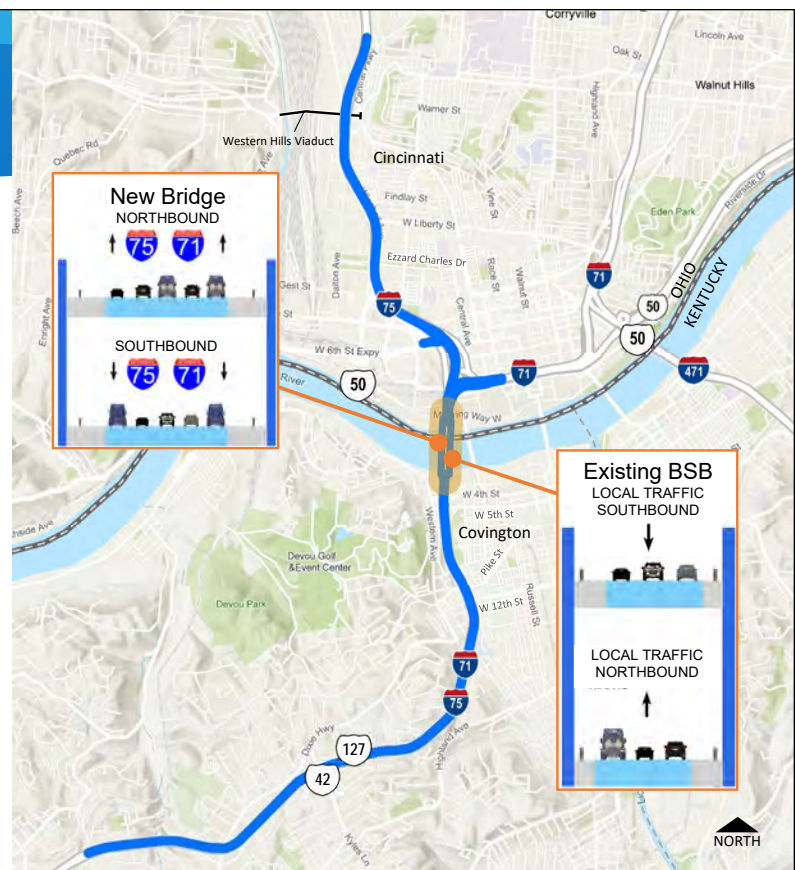


In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

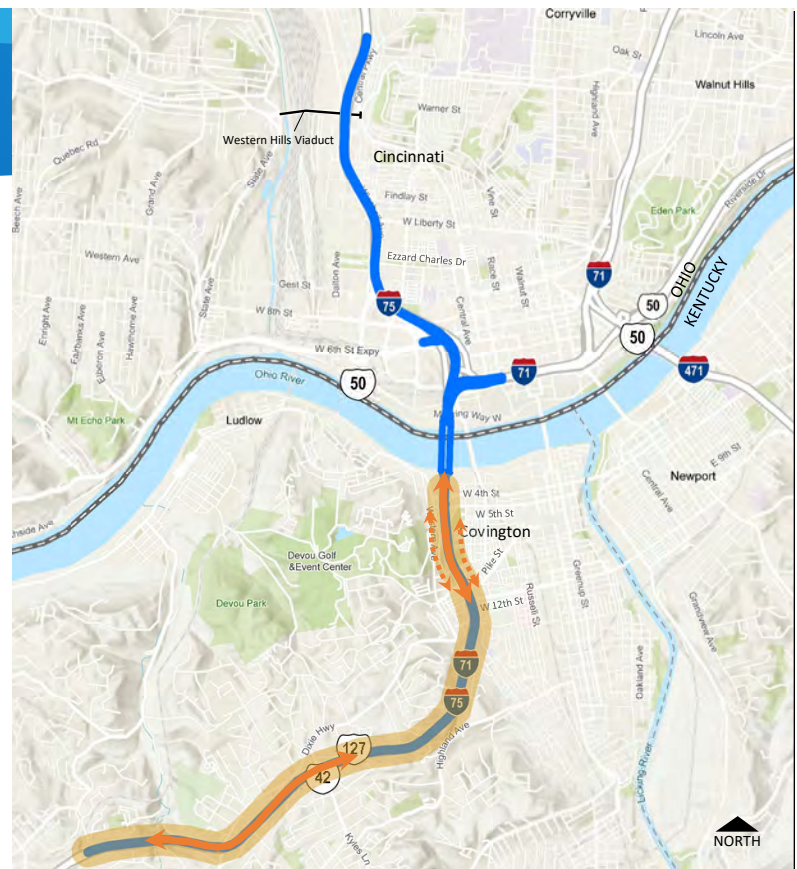
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

# Project Description

## Kentucky

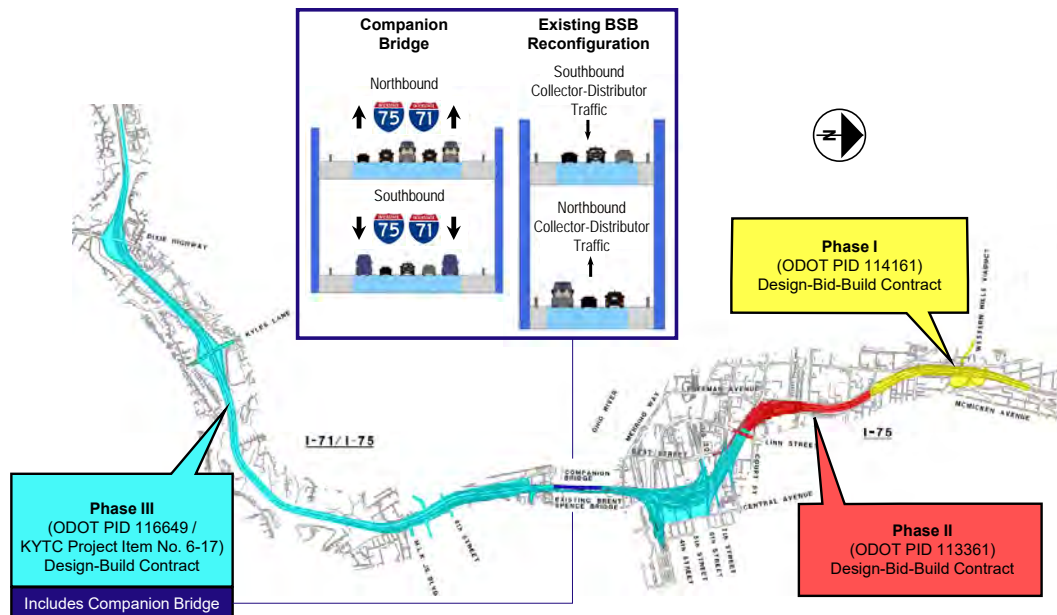
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

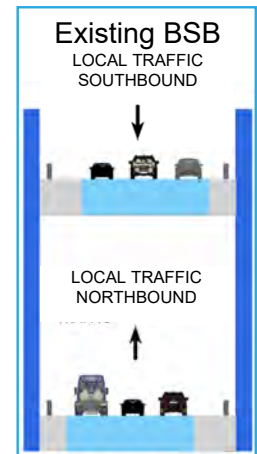
# Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

# What Has Changed?

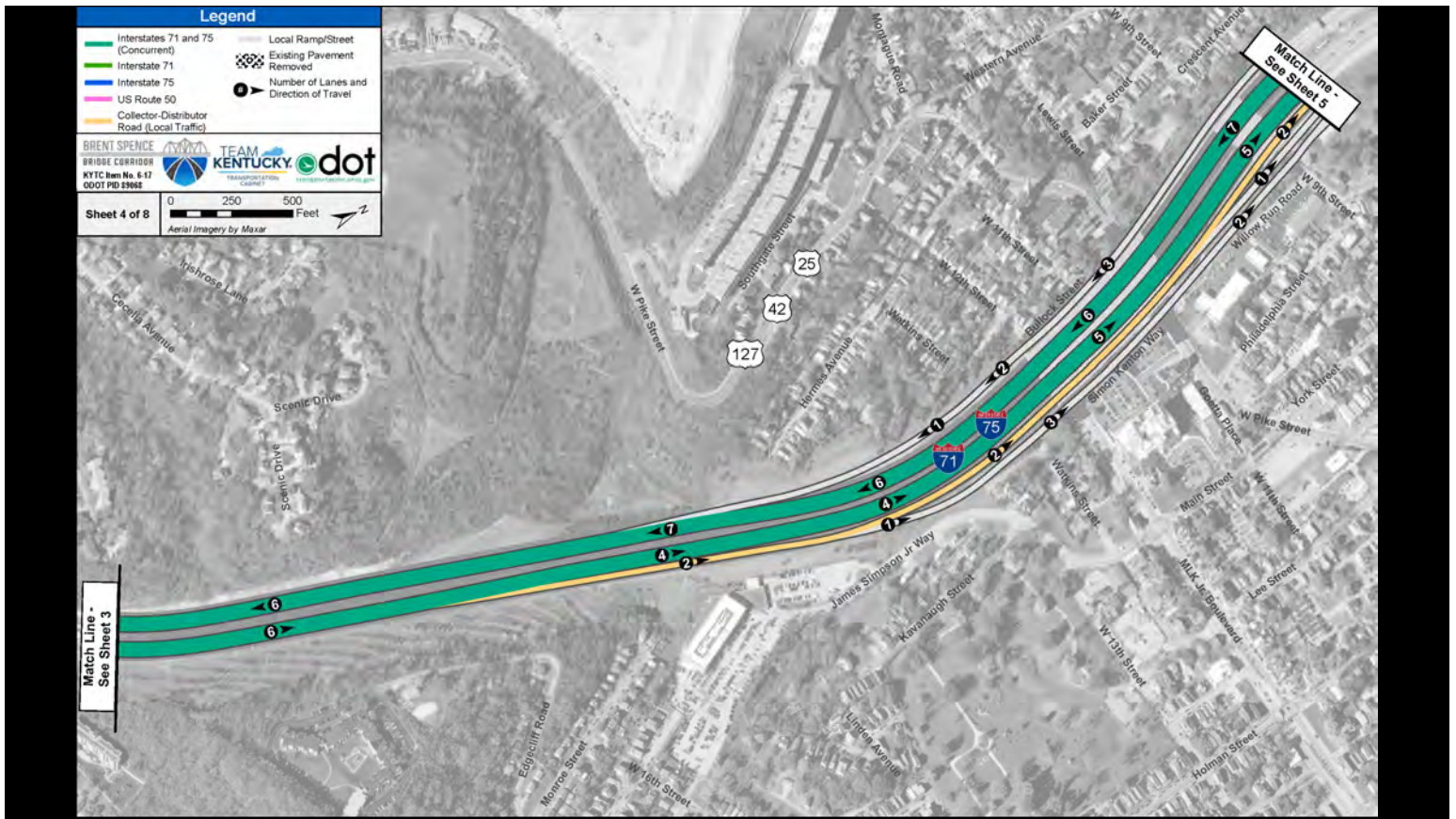
- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



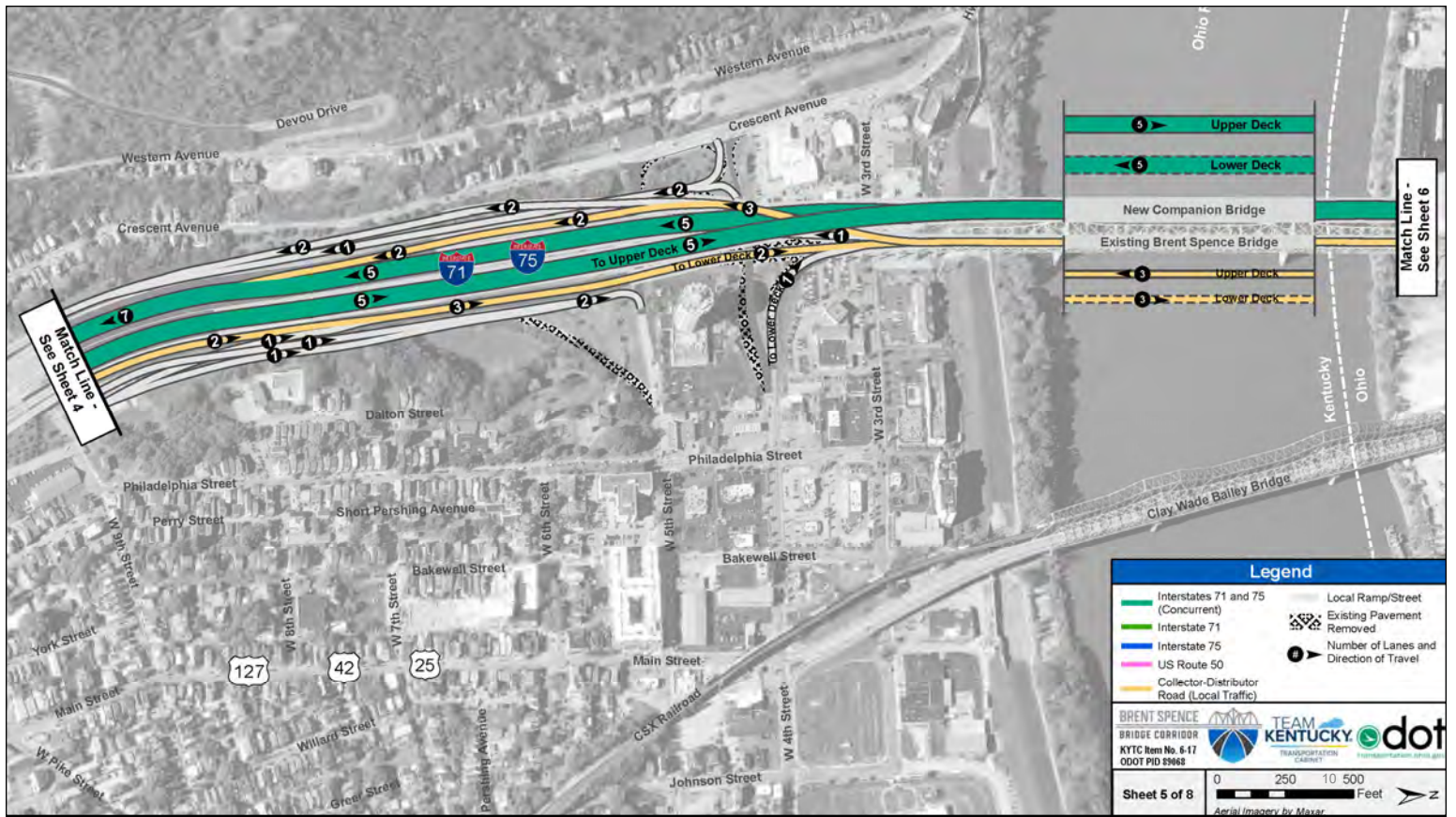
8

As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

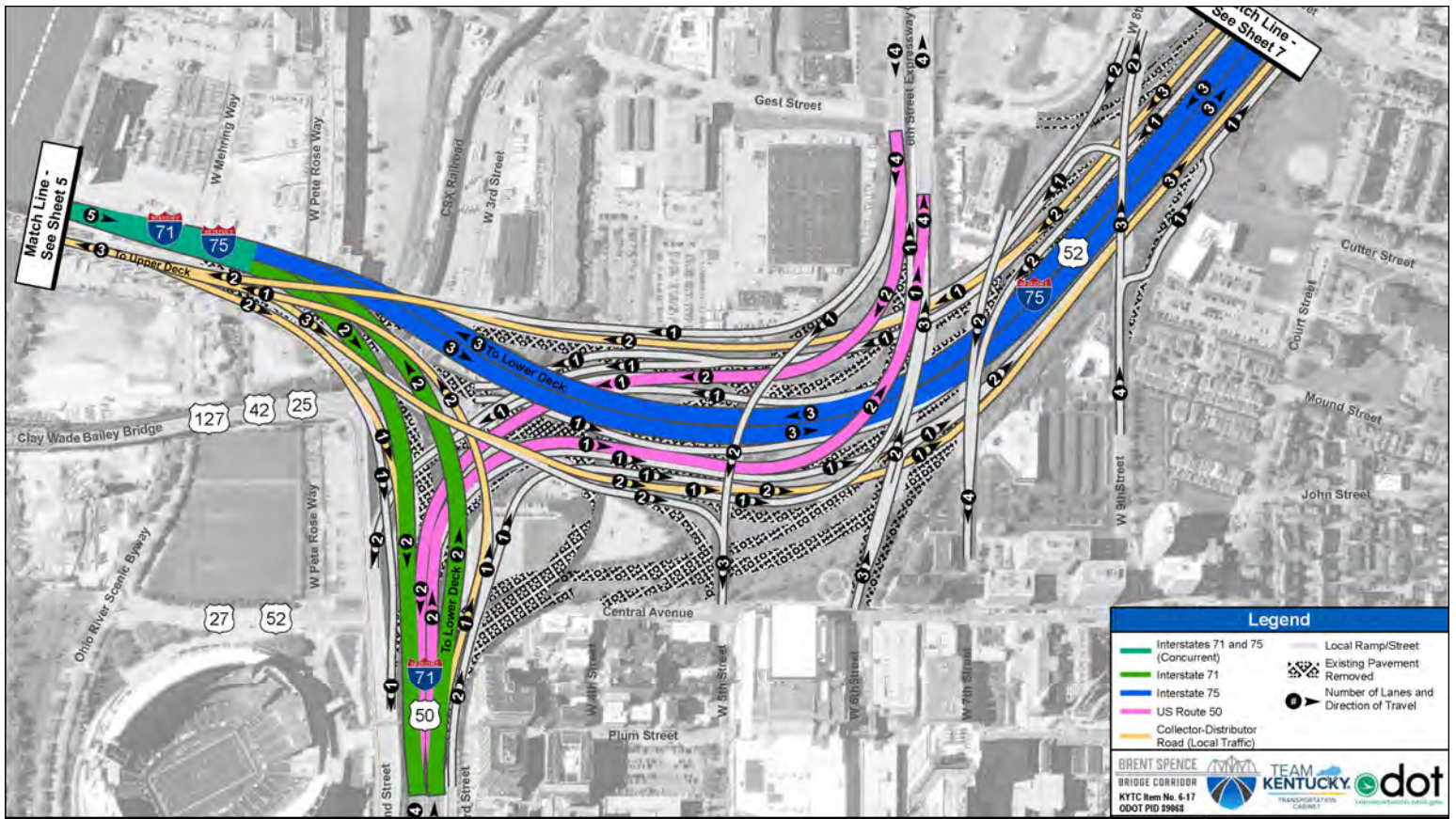
Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.





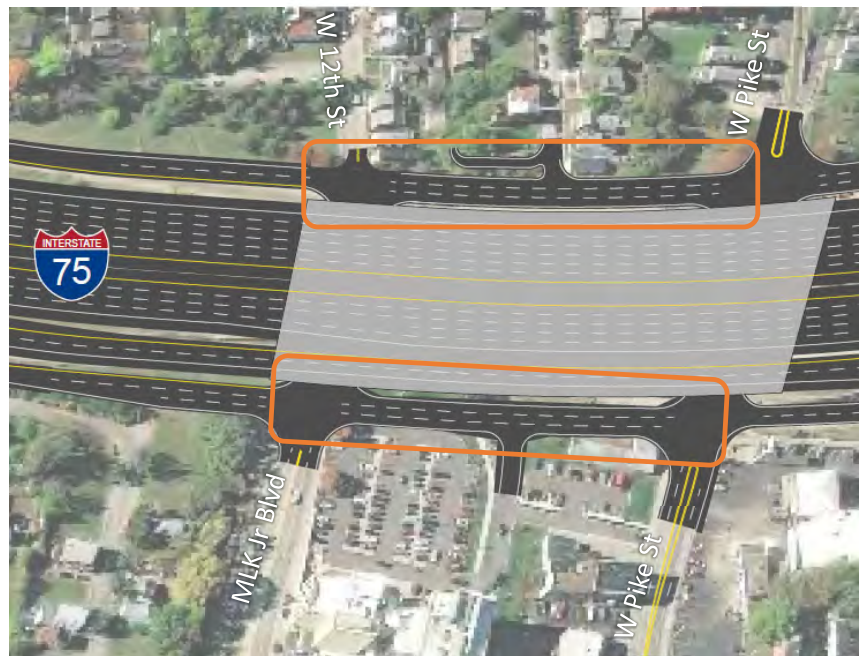
This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.



Here's another view from Covington about ¾ mile north of Peaselburg.

# What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 4 vs. 40+
  - Commercial | 5 vs. 6



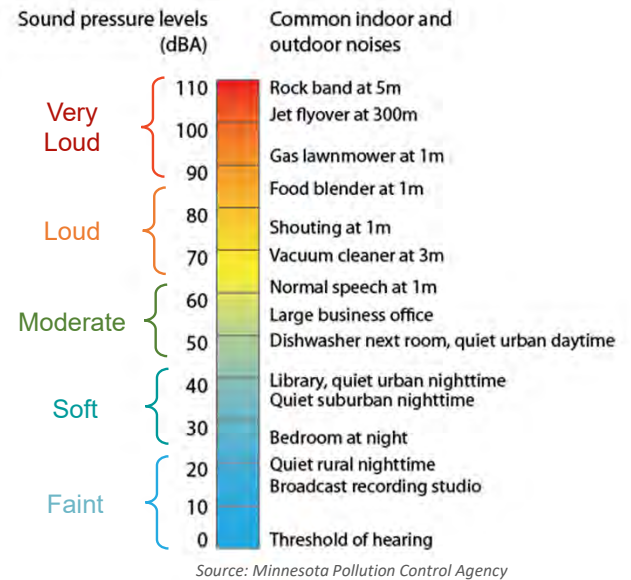
Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound

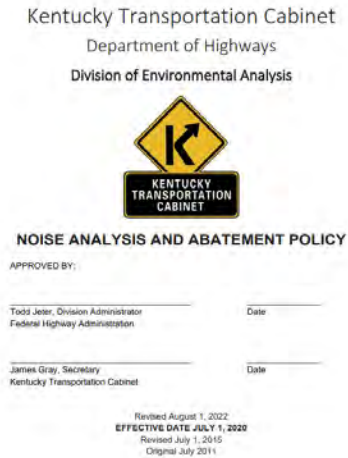


15

Now let's dig into how the project will impact Peaselburg, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

- KYTC Noise Policy
- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.



16

KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dbA. We also determine if there is a substantial increase – specifically a 10 dbA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dbA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

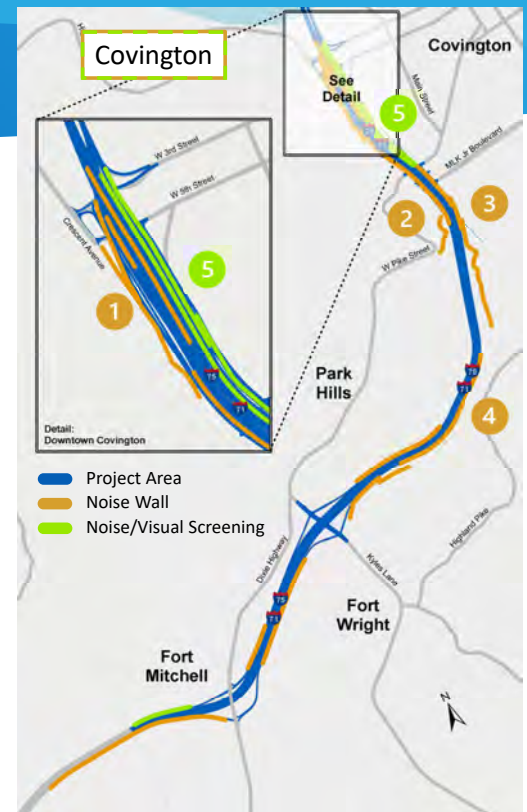
# Noise

Noise walls reasonable/feasible per KYTC Noise Policy

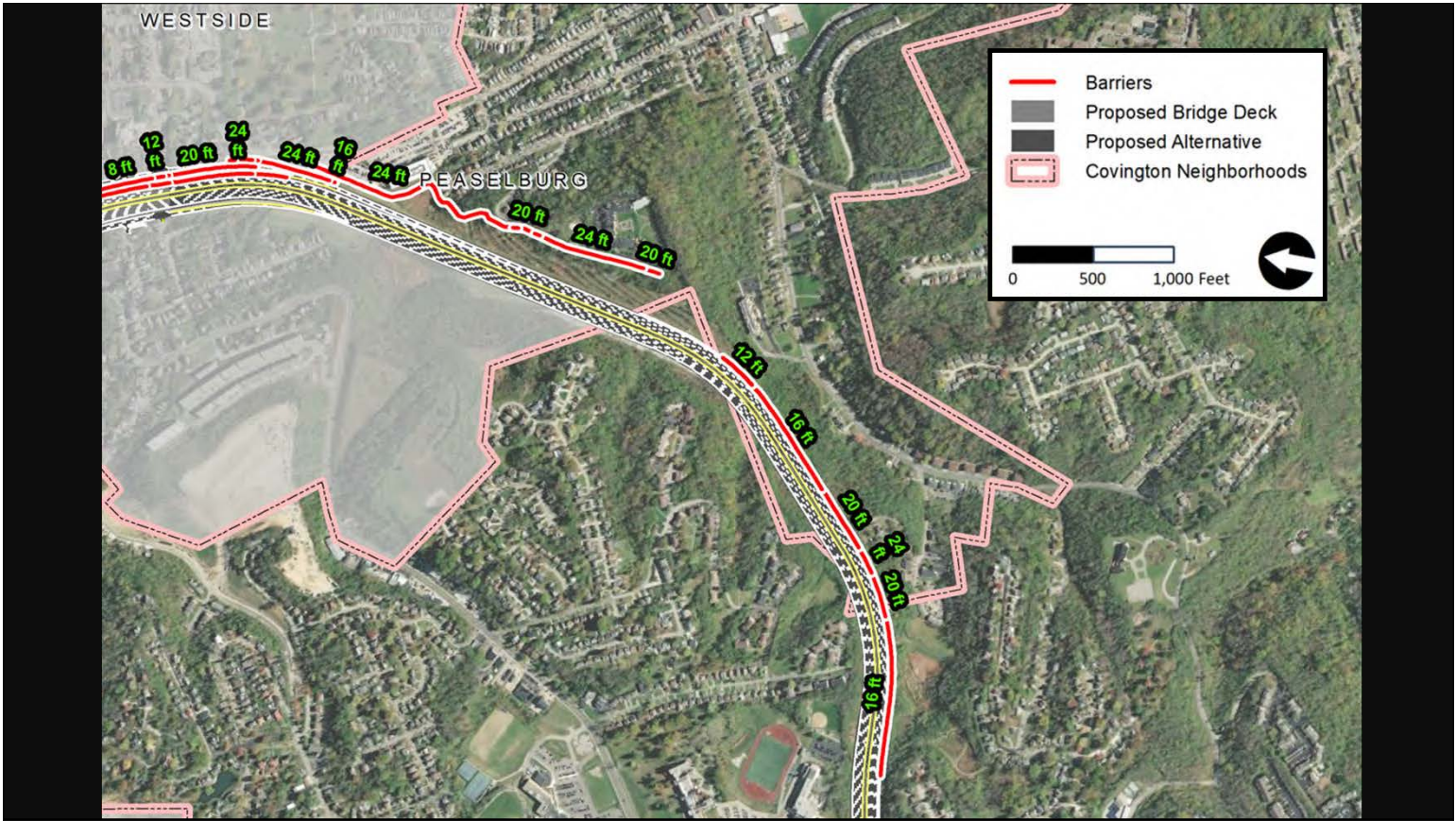
- 3 • Pike St to Edgecliff Rd (Westside/Peaselburg)
  - Without noise wall = 71 dBA
  - With noise wall = 64 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office

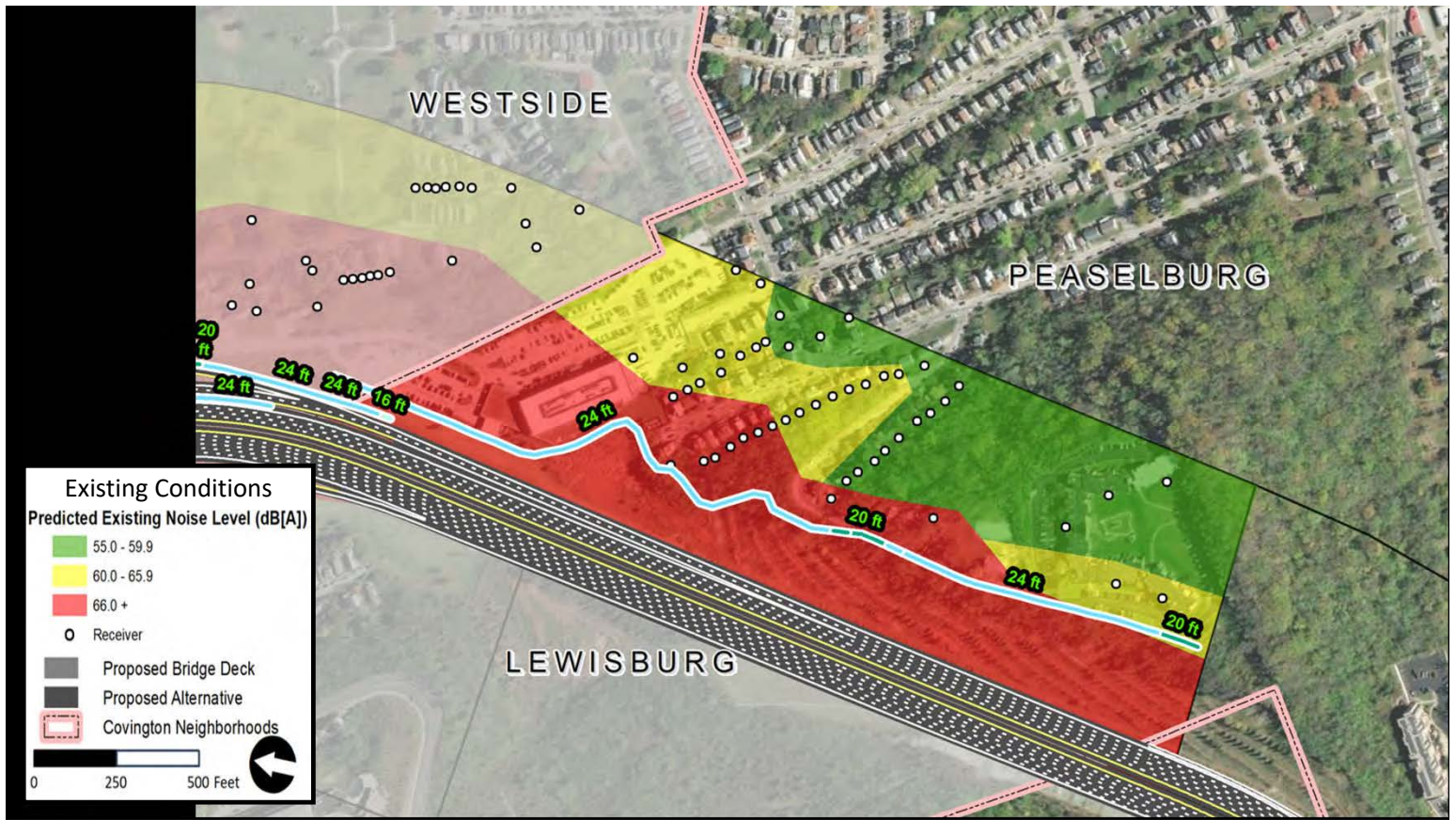
*Note: Sound levels represent the average for the area benefitted by the wall.*



In Peaselburg, a noise wall system extending from Pike Street to Edgecliff Road was found to be reasonable and feasible (shown by the number 3). In this area, average noise levels are predicted to be 71 decibels without a noise wall and 64 decibels with a noise wall. For reference, the average exterior sound levels with noise walls in Covington are predicted to be near what you'd experience in normal conversation.

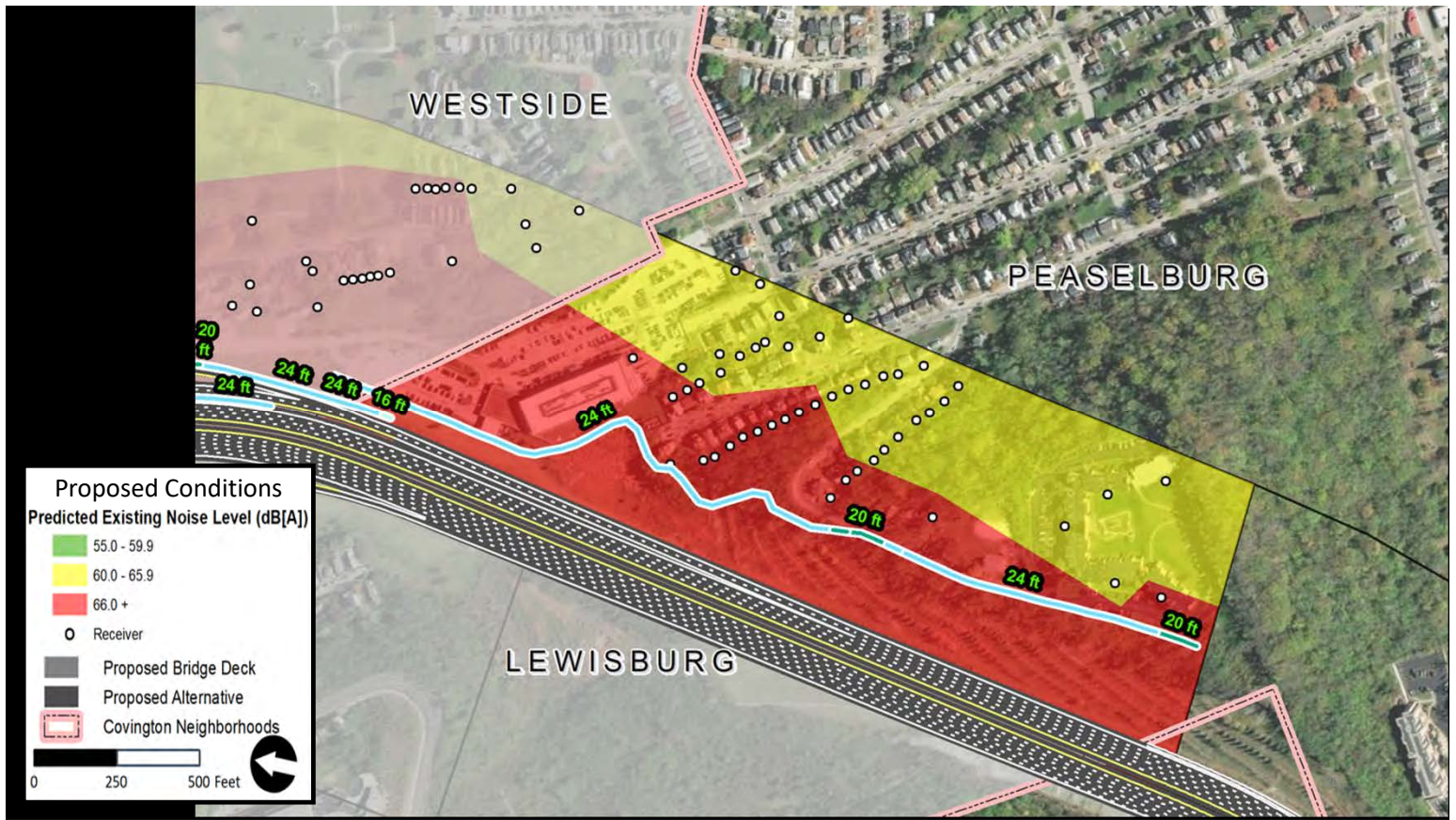


The red lines on this slide show noise walls that were evaluated as part of the noise study for northern Peaseburg. The green numbers show the heights of the different sections of the noise walls.

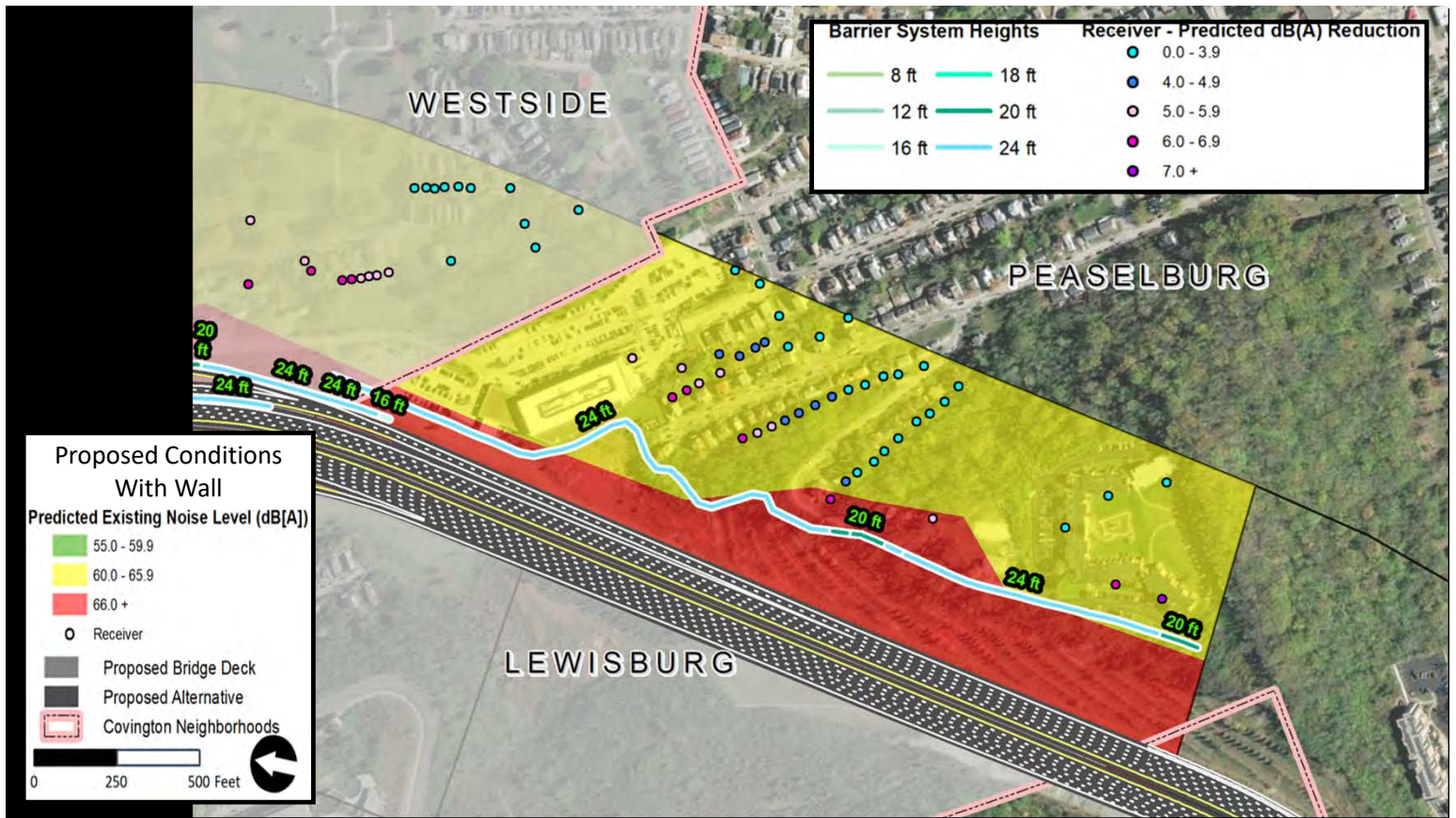


This slide highlights the receptors evaluated as part of the noise analysis for northern Peaseburg. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.





This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

# Noise



Pike Street Without Noise Wall (looking east)  
71 dBA (average)



Pike Street With Noise Wall (looking east)  
64 dBA (average)

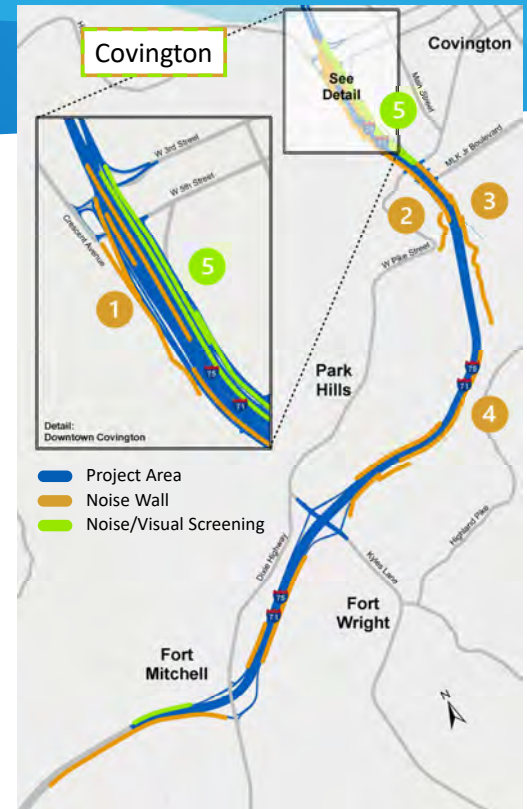
To the left is a drawing showing what the finished project might look like at Pike Street if a noise wall is not built. To the right is a drawing showing what Pike Street might look like with a noise wall. These photos are looking east on Pike Street.

# Noise – Covington (KY)

Noise walls reasonable/feasible per KYTC Noise Policy

- 4 • Highland Pike to Kyles Lane (Peaselburg)
  - Without noise wall = 64 dBA
  - With noise wall = 57 dBA

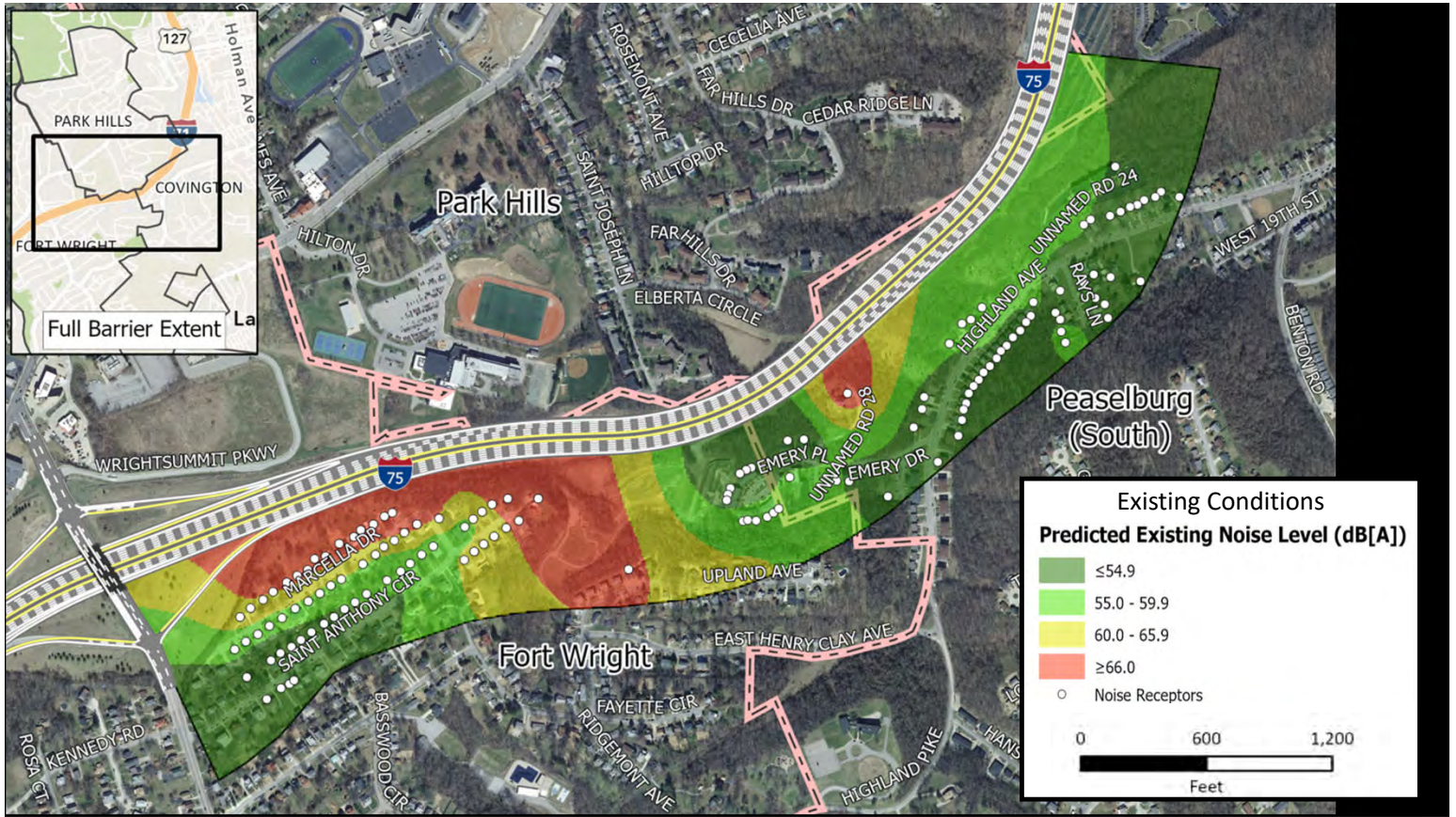
Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office



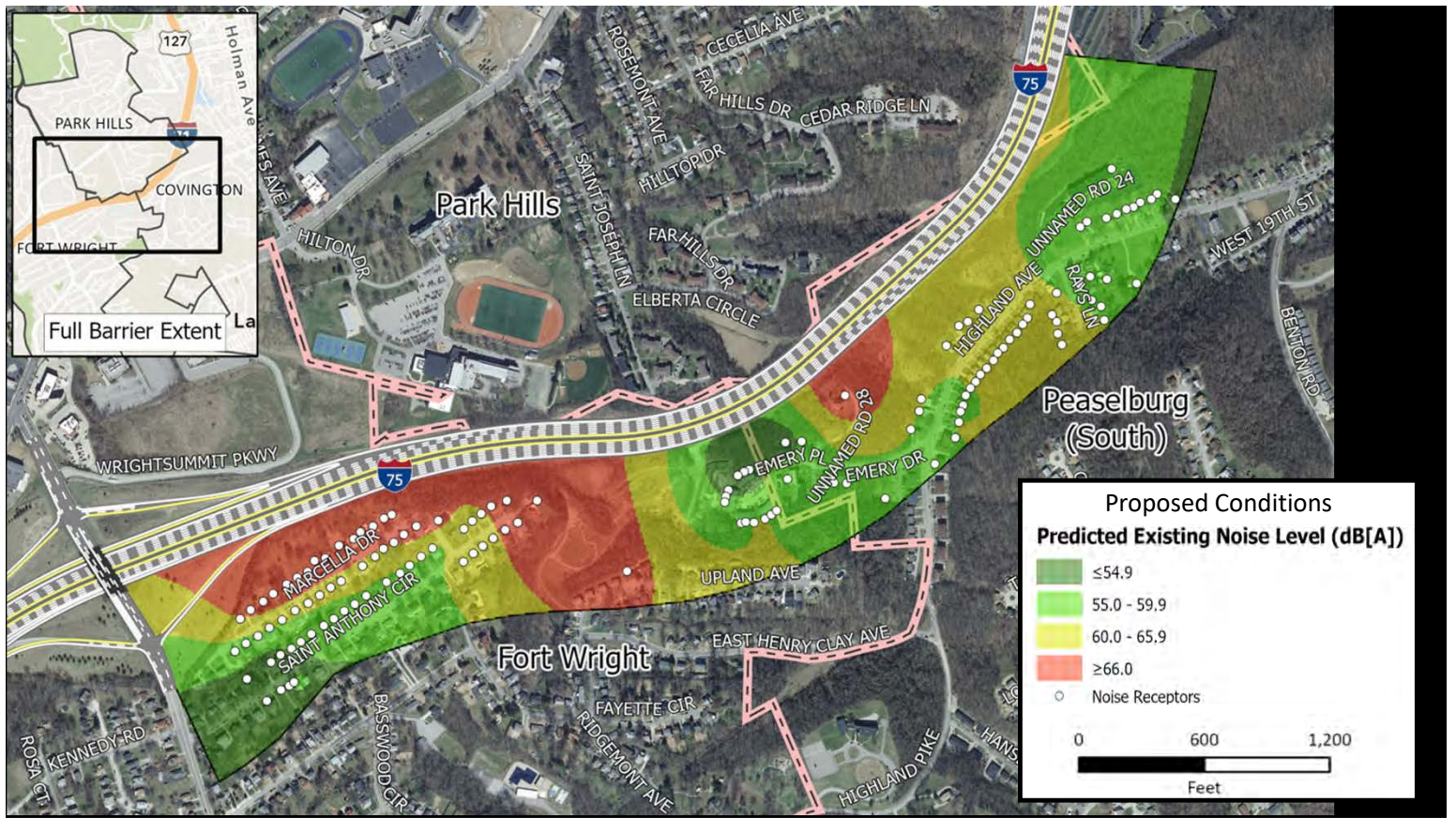
Note: Sound levels represent the average for the area benefitted by the wall in Peaselburg.

In southern Peaselburg, a noise wall system extending from Highland Pike to Kyles Lane was also found to be reasonable and feasible (shown by the number 4). In this area, average exterior noise levels are predicted to be 64 decibels without a noise wall and 57 decibels with a noise wall. The sound levels will vary depending on where each receptor is located. For reference, the average exterior sound levels with noise walls in southern Peaselburg are predicted to be near what you'd experience in normal conversation.

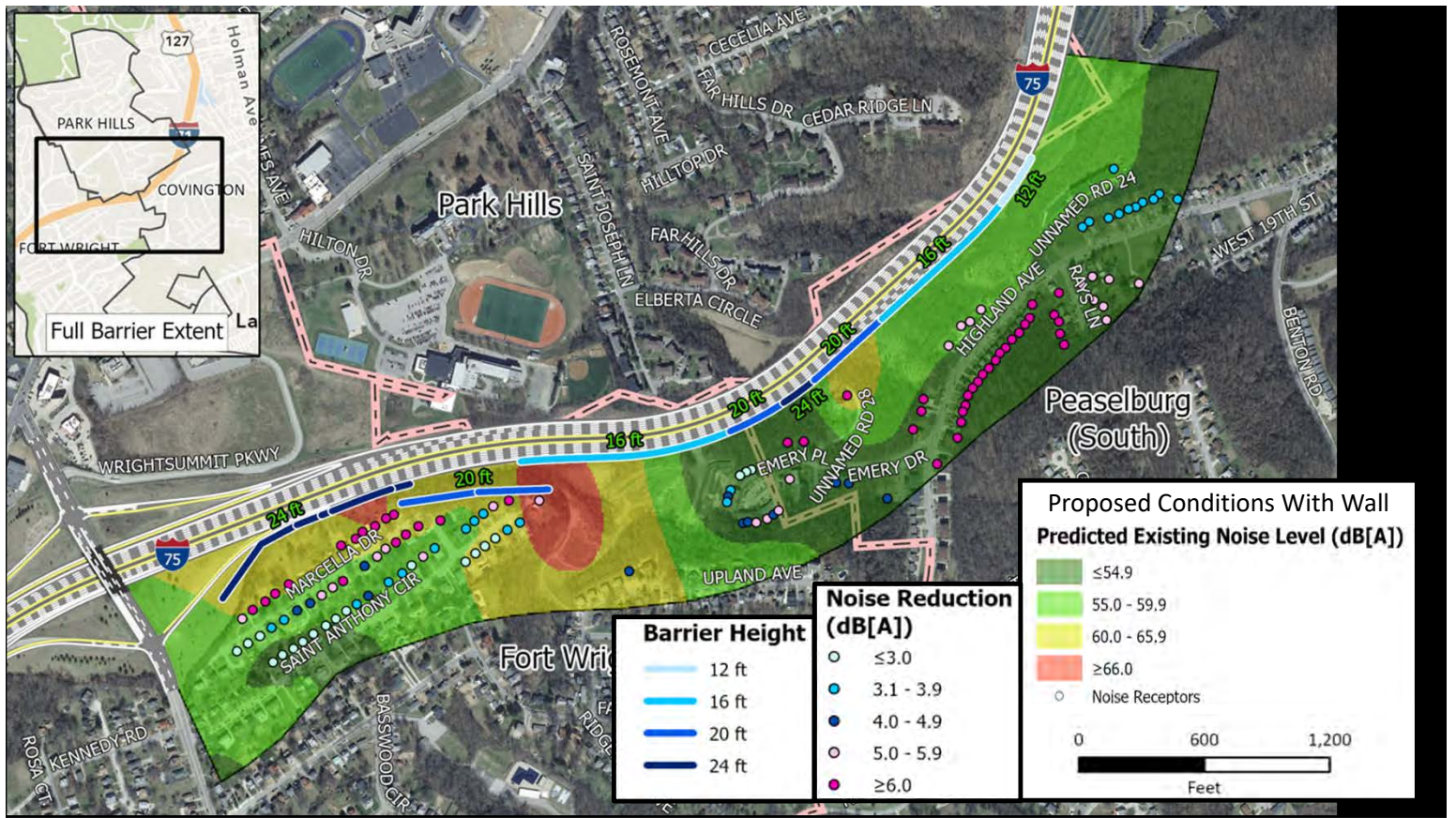




This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



# Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.

# Willow Run Watershed Drainage Improvements

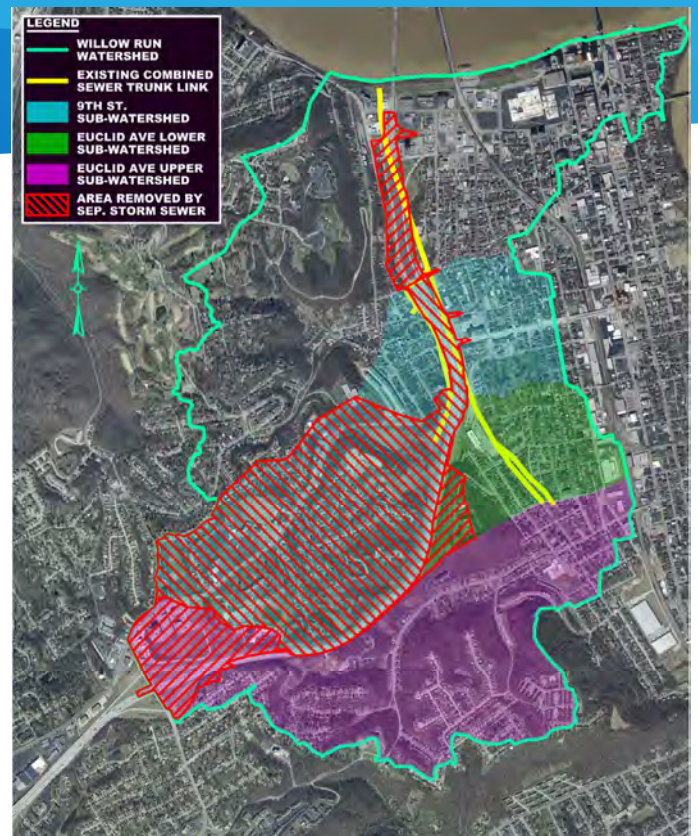
- Project team working with the City of Covington and Sanitation District No. 1
- Euclid Avenue drainage/flooding issues
- Goal
  - Construct a new interstate storm sewer system to remove stormwater runoff from the Willow Run combined sewer system
- Benefits
  - Reduce flooding problems in the Euclid Avenue area
  - Reduce combined sewer overflows



KYTC is working with the City of Covington and Sanitation District No. 1 to address drainage issue in the project area. Existing highway runoff flows in the combined sewer system in Covington, and overflow events negatively affect residents, particularly in the Euclid Avenue area. KYTC is committed to separating the highway drainage from the combined sewer systems as part of the project.

# Willow Run Watershed Drainage Improvements

- New separated storm sewer system along interstate
- Removes drainage areas from the combined Willow Run sewer in the following sub-basins:
  - Euclid Avenue upper basin – 70 acres
  - Euclid Avenue lower basin – 22 acres
  - West 9<sup>th</sup> Street basin – 375 acres
- Replace portion of Willow Run trunk line with new 120-inch pipe from hospital to W. 9<sup>th</sup> Street
- Modeling of the system shows reduction in flooding events in the Euclid Avenue area



(Note: Project details will come in with click (marked by #))

As part of the Brent Spence Bridge Corridor Project, KYTC will be a new separated storm sewer system along the reconstructed interstate. We are not just stiving to separate additional runoff from the combined sewer systems, but we are working to separate all runoff in in Covington, which will substantially reduce overflow events.

The yellow line shows the existing combined storm sewer, and the teal line shows the Willow Run watershed which feeds the sewer. (#) The blue, green, and purple show the Euclid Avenue upper and lower and the West 9th Street sub-basins, which feed the Willow Run sewer. (#) When the new storm sewer system is built, it will the red hatched area from the combined Willow Run system.

In addition, portions of the Willow Run trunk line will be replaced with a larger 120-inch pipe from Saint Elizabeth Hospital to West 9th Street. Modeling of the system shows a reduction in flooding events in the Euclid Avenue area.

# Enhancement Measures

- Working with Covington to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, and pedestrian paths
- Pedestrians and Bicycles
  - Connections across I-71 and I-75
  - New shared use paths
  - New and rebuilt sidewalks

31

In addition to coordinating work in Goebel Park, KYTC is also working with Covington to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.



This exhibit shows some of the planned pedestrian and bicycle facilities near Peasenburg. Note the new shared use paths and sidewalks along the frontage roads. Also notice the new and rebuilt sidewalks under the MLK/West 12th, Pike, and West 9th Street bridges. A new shared use path will be built under the West 9th Street bridge, which will tie into the shared use paths in Goebel Park.





This view shows how the finished project might appear when looking across I-71 and I-75 toward Peasenburg.



This view shows what the project might look like at Pike Street. Notice the potential for aesthetic treatments on the bridge walls and piers. Also notice the sidewalks and shared use paths.





# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)

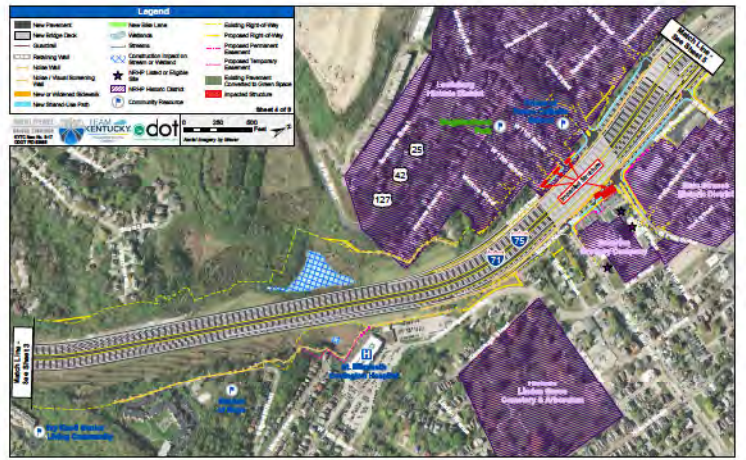


This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.



---

[Attachment 5: Exhibits](#)



Brent Spence Bridge Corridor  
(Looking Northwest from Kentucky)



Brent Spence Bridge Corridor - Kentucky  
(Looking Southeast)



Pike Street With Noise Walls (Looking Northwest) - Kentucky



Pike Street Without Noise Wall (Looking West) - Kentucky





Pike Street With Noise Wall (Looking West) - Kentucky





---

Attachment 6: Comment Forms

# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_  
\_\_\_\_\_

Email address: \_\_\_\_\_ ( Phone: \_\_\_\_\_)

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Noise / Sound walls need to extend south beyond Pike - the current proposed noise walls that extend to Kyles Ln are much needed and wanted

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068





# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: FONA

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Will there be people uprooted?  
If so, are they getting a fair price  
for their property? How long will the  
project take and what about the traffic?  
Will <sup>there</sup> be long delays?

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068





# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

314

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: Friends of Painesburg Neighborhood Association

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

The updated designs are much better than the original plans. We appreciate the effort to retain so many existing homes and businesses. We also appreciate the adjacent infrastructure plans such as storm sewers and other utilities. Thanks for all the extra outreach.

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://www.PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068





# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: Friends of Peasenburg Neighborhood Assn.

Business/Organization Address: same

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: Chairperson of Friends of Peasenburg NA

General Comments (Please attach additional pages, if needed):

- Great presentation + presenter space
- Extremely valuable information & insight
- Noise walls are a must for Peasenburg residents (Thank you)
- Eliminating Wilbur Run combined water flow will truly assist in deterring flooding in our neighborhood.
- Timeline seems reasonable
- Keep project transparency!

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Continue meetings throughout our neighborhoods

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino
- White     Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

### How many people live in your household?

- 1 - 2     3 - 5     6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

### What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999
- \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

### What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university
- Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

KYTC Item No. 6-17 | ODOT PID 89068

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

### How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

### What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

### What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

I think you're already going above and beyond  
on this project.

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

### How many people live in your household?

- 1 - 2     3 - 5     6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

### What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

### What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518





# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

### How many people live in your household?

- 1 - 2     3 - 5     6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

### What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

### What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

### How many people live in your household?

- 1 - 2     3 - 5     6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

### What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

### What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

### How many people live in your household?

- 1 - 2     3 - 5     6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

### What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

### What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518





---

# Brent Spence Bridge Corridor Project CBD-Riverfront Meeting Summary November 29, 2022

---

## Introduction

The CBD-Riverfront meeting was held on November 29, 2022 from 7:00 pm to 9:00 pm. The meeting was held at First Financial Community at One East Fourth Street in Cincinnati, Ohio. It was publicized through various CBD-Riverfront neighborhood resources including: a newsletter, via an email to all neighborhood members, and an announcement at a prior neighborhood meeting (see Attachment 1). Attendees at the meeting included the City of Cincinnati, the project team, members of the CBD-Riverfront Neighborhood Association, and members of the public. In total, 32 people attended the meeting, including five individuals from the Ohio Department of Transportation and three individuals from the Federal Highway Administration (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by the Ohio Department of Transportation (see Attachment 4). After the presentation, the project team responded to questions and comments offered by those present.

Exhibits showing the renderings of the Firefighters Memorial, the corridor, bike and pedestrian facilities, a view of the bridges from Ohio, and a view of the bridges from Kentucky were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. One comment form was returned at the meeting; however, no demographic surveys were returned at the meeting.

## Comments

The written comment received at the meeting stated that the project should include planning for passenger trains and expressed opposition to the project due to its dependence on fossil fuels.

Below is a summary of comments/questions received during presentation:

- How does the project impact driving time? Will this project add time to my commute? How does adding extra lanes affect travel time?
  - The travel times at various locations are unknown at this time. The collector-distributor (C-D) system will accommodate and help with moving traffic through the area. The signal timing will also be adjusted to account for the changes associated with the proposed project. ODOT studied the levels of service (LOS) for the project corridor and are correcting LOS deficiencies at locations experiencing LOS E and F. This project is expected to improve the LOS at the locations with LOS E and F to LOS D and E.
- What about the City streets? How are these analyzed?
  - The City staff responded that it reviews the queues at intersections rather than focusing solely on LOS.



- 
- The project needs to maintain connections walking across the highway. Will there be a shared-use path at 5<sup>th</sup> Street? There needs to be a shared-use path connection around 4<sup>th</sup>/5<sup>th</sup> Street (somewhere south of 6<sup>th</sup> Street).
    - A shared-use path is not currently included on 5<sup>th</sup> Street because the connection from US-50 to the southbound collector-distributor system would need to be crossed. This connection is a 50-mph expressway connection, and pedestrian crossings here would not be as safe as others being provided along 3<sup>rd</sup> Street (existing path) and 6<sup>th</sup> Street (proposed shared-use path). ODOT will further examine what is being requested.
  - How does the project change if we removed all of the connections? Would this require more right-of-way? How much has the project footprint changed?
    - The proposed project improves a corridor, and all changes in access would need to reconsider impacts along the entire corridor. Changing access in one location may result in an impact somewhere else. Evaluating all these factors is how we arrived at the current preferred alternative. The project footprint has been reduced, including 10 acres of excess ODOT property adjacent to Cincinnati's Central Business District that will be transferred to the City of Cincinnati upon the project's completion. ODOT will investigate development of an overlay of the 2013 vs. 2022 changes for the public.
  - When was the last traffic analysis completed? Did the traffic forecasting take into COVID and post-COVID?
    - The last traffic analysis was completed in 2015, with some additional analysis done in 2019 and 2020. ODOT is in the process of updating traffic forecasts and will be reviewing this data and validating lanes are needed as shown. ODOT relies on the MPO's regional travel demand model. ODOT is balancing transportation needs with current developments and has seen traffic is coming back to predicted levels after the pandemic and anticipates the corridor will reach the projected 2050 traffic levels.
  - The City's Master Plan doesn't document the need nor includes the Brent Spence Bridge Corridor project in the plan. What about a mass transit plan?
    - The major investment study completed by Ohio-Kentucky-Indiana Regional Council of Governments (OKI), The North South Transportation Plan ([North South Transportation Plan - OKI Regional Council of Governments](#)), did identify the need for the Brent Spence Bridge project. A mass transit plan was identified as part of a long-term solution for the I-75 corridor, and this part of the solution would need to be driven by regional entities. The Brent Spence Bridge Project is only addressing the highway component of the overall solution presented in OKI's plan.
  - Can we eliminate the 6<sup>th</sup> Avenue connection? What about US-50 going through the downtown area? Can US-50 be eliminated?
    - Connections to US-50 need to be maintained. US-50 is part of the National Highway System and is designated an intermodal connector. A primary goal of the project is to maintain connections to key regional and national corridors, and the east-west connection for US-50 is one of these corridors.
  - Will there be pedestrian traffic or transit on the bridges between Ohio and Kentucky?
    - Pedestrian traffic will not be allowed on the bridges. Currently, there is no plan for additional mass transit on the bridges.



- 
- Did the previous traffic projections meet the 2020 estimates?
    - Yes.
  - The best option is the No Build Option. This area does not need to be fixed.
    - Comment noted.
  - What are the maintenance costs on the existing bridges?
    - These costs are not readily available at the meeting. However, ODOT did perform some operation and maintenance cost development as part of previous efforts associated with tolling [[Options Analysis.pdf \(brentspencebridgecorridor.com\)](#)].
  - What is the project cost, and how was it determined?
    - The current estimated cost is about \$3.6 billion. A meeting between KYTC, ODOT, and the Federal Highway Administration estimated the cost between \$3.0 to \$3.8 billion.
  - Can we reduce lanes at exits?
    - The proposed project is a long project, and all changes in access would need to reconsider impacts along the entire corridor. Changing access in one location may result in an impact somewhere else. Evaluating all these factors is how we arrived at this alternative. There are other communities involved with this project, and the overall project needs to take into consideration all of these communities.
  - How will the grade at the Brent Spence Bridge be addressed in Kentucky?
    - The grades associated with the finished project will be very similar what exists today.
  - The east-west pedestrian connection is hostile. Please make it safer.
    - Comment noted.
  - This project does not help with climate change and putting more cars on the road.
    - As part of the environmental process, an air quality analysis has been completed and coordinated with the Environmental Protection Agency. This agency did not find the project affects air pollutants in the region and may improve air pollutants since there will be less stop-and-go.
  - This project supports more traffic moving through the downtown area. Should we do this?
    - This project is improving a long corridor, and the needs of the entire corridor must be considered. ODOT studied the Level of Service (LOS) for the project corridor and are correcting LOS deficiencies in locations experiencing LOS E and F. This project is expected to improve the LOS at the locations with a LOS E and F to a LOS D and E.
  - Please listen to the public and let us know the changes made. The responses to public comments are inadequate. The public wants to work with ODOT as the project moves forward.
    - Comment noted.
  - What other types of meetings are planned?
    - There are a series of neighborhood meetings in Ohio and Kentucky, and some have already occurred. These meetings are focused on the neighborhoods in which they are occurring. Additionally, a public hearing will occur in July/August 2023 in Ohio and Kentucky where the entire public may attend.
  - Can community input still affect the design of the bridge or the project?
    - While a preferred alternative for the Brent Spence Bridge Corridor has been identified, there are opportunities to refine the design in response to community concerns. There are a series of neighborhood meetings in Ohio and Kentucky, and some have already occurred.





- 
- Why are some of the pedestrian crossings so long (Note: This question pertained to the pedestrian bridge connection near Freeman)?
    - All pedestrian crossings need to be ADA compliant, and the development of necessary ramp connections increase the length.
  - Is any part of the project set in stone?
    - The project needs to address the identified purpose and need. ODOT considers the current alternative to be the best way to meet the project purpose and need. If the individual provides a written comment with details for modifications, ODOT can evaluate and react to it.
  - Can the City make changes to US-50? Can the City request changes for fewer ramps?
    - Changes to US-50 are not part of this project and would need to be pursued by the City as part of another project. The ramps are part of the highway system and administered by ODOT and the FHWA.
  - Is the C-D system part of ODOT?
    - Yes, the C-D system would be part of the highway and, therefore, part of ODOT.
  - If we remove the ramps would there still be a need for a new bridge?
    - Yes. The new bridge will help keep vehicles moving through the area.
  - Can we toll for vehicles using the bridge?
    - No. Kentucky law does not allow tolls.
  - Can we restripe the bridge to three lanes?
    - Restriping the bridge to three lanes would not improve the congestion and safety along the project corridor.
  - Can the new proposed 3<sup>rd</sup> Street ramp to the northbound collector-distributor be eliminated and have traffic on 3<sup>rd</sup> Street enter the proposed connection at the end of the Clay Wade Bailey Bridge?
    - This is possible pending further traffic analysis and can be looked at as part of the final traffic analysis.
  - Can Western Avenue and Winchell Avenue be raised?
    - The project team can evaluate raising Winchell Avenue between 9<sup>th</sup> Street and Ezzard Charles Drive.

Most of the questions asked during and after the presentation were answered by the project team. In general, the comments revolved around:

- Opposition to the project;
- Requests to modify connections into and out of the City consistent with concepts previously submitted as part of other public comments; and
- Existing and projected traffic volumes.

The comment sheet returned at the meeting is included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.

## Attachment 1: Advertising Materials

## Seth Turner

---

**From:** Sherry Kish  
**Sent:** Monday, October 31, 2022 9:26 AM  
**To:** Seth Turner; Jodi Heflin  
**Subject:** FW: Brent Spence Bridge Presentation on 11/29/22

FYI

### Sherry R. Kish

Manager Business Development

**Office:** (614) 493-5510

**Cell:** (614) 507-0504

**Email:** [skish@hntb.com](mailto:skish@hntb.com)

### HNTB CORPORATION

88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

 **100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

---

**From:** Alan Bunker <[alanbunker@yahoo.com](mailto:alanbunker@yahoo.com)>  
**Sent:** Friday, October 28, 2022 6:02 PM  
**To:** Sherry Kish <[skish@HNTB.com](mailto:skish@HNTB.com)>  
**Subject:** Re: Brent Spence Bridge Presentation on 11/29/22

Hi, Sherry!

We can do the first three items and the last item. We have no school in the Central Business District. I will make the announcement at our November 8th membership meeting as well as send an eblast with the announcement to all the members more than two weeks before the meeting and a reminder a few days before the meeting.

Alan

On Friday, October 28, 2022 at 03:58:45 PM EDT, Sherry Kish <[skish@hntb.com](mailto:skish@hntb.com)> wrote:

Alan,

Thank you so much for working with us to get a meeting schedule allowing the Brent Spence Bridge project team to talk with the CBD Riverfront Neighborhood on November 29<sup>th</sup>. Jim Curtis was **wonderful** to work with to schedule the meeting. As you know, the project team wants to share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

Our goal is to be sure all notifications about the meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous neighborhood meeting?		
Email list	Can you email a neighborhood wide or citywide email list with information about BSB presentation at the upcoming meeting?		
Facebook (private to neighborhood)	Can you share a Facebook message about BSB presentation?		
Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?		
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?		
Other	Is there another outlet you have available to get the word out?		
Do you need assistance from us with advertising?	We made a flyer for your consideration (attached to this email).		

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!  
Sherry

**Sherry R. Kish**

Office: (614) 493-5510

Cell: (614) 507-0504

Email: [skish@hntb.com](mailto:skish@hntb.com)

**HNTB CORPORATION**

88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

**100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

## Seth Turner

---

**From:** Alan Bunker <alanbunker@yahoo.com>  
**Sent:** Monday, November 28, 2022 3:27 PM  
**To:** Seth Turner  
**Subject:** Re: Brent Spence Bridge Presentation

Hi, Seth!

Here it is!

*Hello!*

*As was announced at our November membership meeting last Tuesday evening, the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of neighborhood outreach meetings about the Brent Spence Bridge Project. A meeting for the residents and businesses of the Central Business District (CBD) and Riverfront Neighborhood will be held on Tuesday, November 29, 2022, at 7:00 p.m., at the First Financial Center at 4th and Vine Streets. Although it will be held at the location of our DRC meetings, the meeting is not a DRC-sponsored event. I am simply informing you as I do with other announcements of interest. Further information about this meeting is on the attached flyer.*

*Regards,*

*Alan Bunker, DRC President*

[Brent Spence Bridge Invitation Flyer CBD Riverfront.pdf](#)

Alan

On Wednesday, November 2, 2022 at 03:19:27 PM EDT, Seth Turner <sturner@hntb.com> wrote:

Hi Alan,

Yeah that'd be great if you could copy me on the e-blast, I appreciate that!

Thank you,

**Seth Turner**

Transportation Engineer I

**Direct** (216) 377-5836 **Tel** (614) 228-1007 Ext. 75836 **Email** [sturner@hntb.com](mailto:sturner@hntb.com)

---

**From:** Alan Bunker <alanbunker@yahoo.com>  
**Sent:** Wednesday, November 2, 2022 10:55 AM  
**To:** Seth Turner <sturner@HNTB.com>  
**Subject:** Re: Brent Spence Bridge Presentation

Hi, Seth!

I send an e-blast to our 300 or so members announcing the meeting several weeks before the meeting. I also will announce it at our November membership meeting. I can send you a copy of the e-blast for your records, if you like. Just let me know.

Alan

On Wednesday, November 2, 2022 at 08:40:08 AM EDT, Seth Turner <[sturner@hntb.com](mailto:sturner@hntb.com)> wrote:

Good Morning Alan,

Thank you for your help in getting the word out about the Brent Spence Bridge project. You may of seen an earlier email from Sherry Kish stating that I'll be the new point of contact for outreach assistance. If there is anything I can do that would make advertising to your community easier, please let me know.

When you get a chance, could you please drop me an email letting me know how you advertised so we can be sure to include a record in the environmental documentation we're preparing for the project?

Thank you,  
**Seth Turner**

Transportation Engineer I

Direct (216) 377-5836 Tel (614) 228-1007 Ext. 75836 Email [sturner@hntb.com](mailto:sturner@hntb.com)

**HNTB CORPORATION**

1100 Superior Ave, Suite 1701 | Cleveland, OH 44114 | [hntb.com](http://hntb.com)

 **100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

[Home](#)[About Us](#)[Meetings & Activities](#)[Calendar](#)[Get Involved](#)[Links](#)[Contact/Feedback](#)

## Upcoming Events

### **Brent Spence Bridge Meeting**

**7 pm Nov 29, 2022**

First Financial Bank, 4th and Vine

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of neighborhood outreach meetings about the Brent Spence Bridge Project. A meeting for the residents and businesses of the Central Business District (CBD) and Riverfront Neighborhood will be held on Tuesday, November 29, 2022, at 7:00 p.m., at the First Financial Center at 4th and Vine Streets. Although it will be held at the location of the DRC meetings, the meeting is not a DRC-sponsored event.

### **Annual Holiday Party**

**6 pm Dec 05, 2022**

Prime, Sixth and Walnut

Open to all DRC members and their guests (there is a fee for guests), the annual Holiday Party offers a great opportunity to meet and enjoy our downtown neighbors! This party is in place of the usual monthly meeting.

### **DRC Monthly Meeting**

**6 pm Jan 10, 2023**

First Financial Bank, 4th & Vine

Please join us for our regularly scheduled monthly meeting. We will meet on the first floor in the bank's Innovation Room, which seats around 70. We will hear reports describing recent downtown activities as well as from Cincinnati police regarding safety issues downtown.







---

## Attachment 2: Sign-In Sheets



## BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting

Name	Representing	Address	Phone	Email
1. Kathleen Fuller	ODOT			
2. Ryan Minnich	Cincinnati Citizens			
3. Vince Terry	MOODY WOLAN INC.			
4. ALAN BUNKER	DOWNTOWN RES. CO.			
5. Joe Bredestage	Bridge Forward			
6. Kay Sunderman	DRC			
7. Michael Moline	mm/vc			
8. Mike James				
9. Brian Bland	Bridge Forward			
10. Jeff Dui	ODOT			
11. Jeff Worden	Self			
12. MACH BECHER	KENTR			
13. Alex Browne	over the Rhine			
14. DYLAN LURK	CBP			
15. Alex Wainwright	CBP			
16. ADAM PLAKER	Self			
17. Matt Brunny	ODOT			
18. Erica Schneide	ODOT			
19. Jacque Annarino	"			
20. Tim Hill	"			

Please print clearly • Contact information is optional but must be included if you wish to receive updates.

page 1 of 1



## BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting

Name	Representing	Address	Phone	Email
1. <del>Kevin Shaw</del> Kevin Shaw	Self	[REDACTED]	[REDACTED]	[REDACTED]
2. John Wettersel	Bridge Forward			
3. James McEnatt	Bridge Forward / UWB			
4. Casey Klein	Self			
5. CHRISTOPHER WYATT	—			
6. Jonah Snyder	Self			
7. Megan Conburn	FHWA			
8. Tim Long	FHWA			
9. Melissa Toni	FHWA			
10. Richard Wendel	Self			
11. Douglas L. Hunt	Bridge Forward			
12. Anthony Isaacs	Self			
13.				
14.				
15.				
16.				
17.				
18.				
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



---

## Attachment 3: Photographs

**CBD-Riverfront Neighborhood Meeting**

**November 29, 2022**



Photograph 1. CBD-Riverfront Neighborhood Meeting held on November 29, 2022.



Photograph 2. CBD-Riverfront Neighborhood Meeting held on November 29, 2022.

## CBD-Riverfront Neighborhood Meeting

November 29, 2022



Photograph 3. CBD-Riverfront Neighborhood Meeting held on November 29, 2022.



Photograph 4. CBD-Riverfront Neighborhood Meeting held on November 29, 2022.

**CBD-Riverfront Neighborhood Meeting**

**November 29, 2022**



Photograph 5. CBD-Riverfront Neighborhood Meeting held on November 29, 2022.



Photograph 6. CBD-Riverfront Neighborhood Meeting held on November 29, 2022.

## Attachment 4: Presentation



**BRENT SPENCE**  
**BRIDGE CORRIDOR**



# CBD Riverfront Neighborhood Meeting (OH)

November 29, 2022



INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with remarks by the City of Cincinnati (5 min) and introductions of the Project Team in attendance (2 min).

# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer individuals in the CBD Riverfront neighborhood the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the CBD Riverfront area
  - Discussion/feedback from the CBD Riverfront



2

The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the CBD Riverfront. Most importantly, tonight we're focusing on hearing from the individuals in CBD Riverfront. We're here to answer your questions and to listen to your feedback about the project.

# Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Ohio, that includes I-75 from the Brent Spence Bridge to just north of the Western Hills Viaduct. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, ODOT has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, ODOT has purchased most of the land needed to build the project. In addition, we've begun detailed design from Linn Street to the northern project limits.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



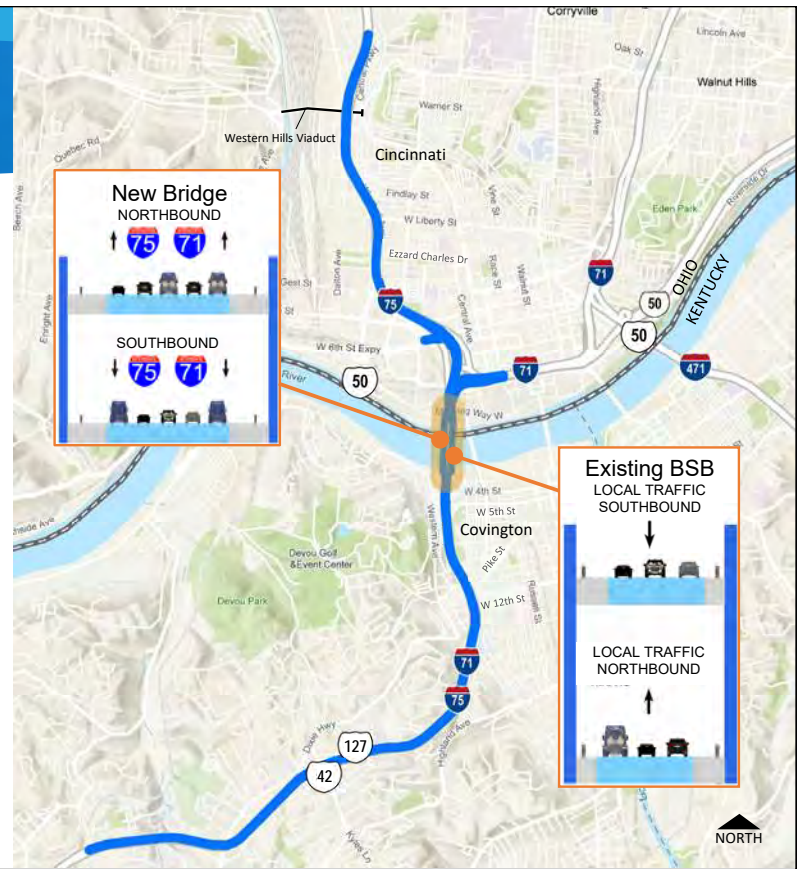
Note: Individual project elements will come in with click (marked by #).

In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. (#) Beginning near Ezzard Charles Drive, a collector-distributor system will be added to connect I-75 traffic to and from the local street network and US-50 West. (#) In the north, it will tie into the recently completed Mill Creek Expressway-Hopple Street Interchange project. (#) The project will rebuild the I-75 interchange at the Western Hills Viaduct and tie into the new bridge replacement project being developed by the City of Cincinnati and Hamilton County. (#) A new northbound exit will be built at Ezzard Charles Drive to improve access to Union Terminal, TQL Stadium, and Over-the-Rhine. Lastly, it will connect to I-71 and US-50 East.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

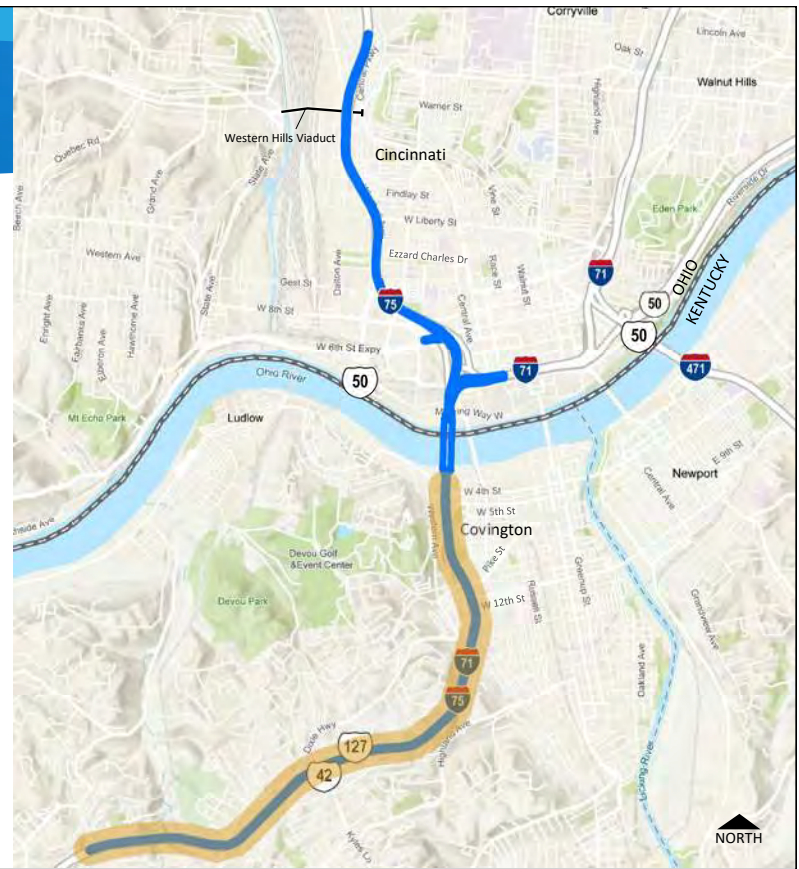
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

# Project Description

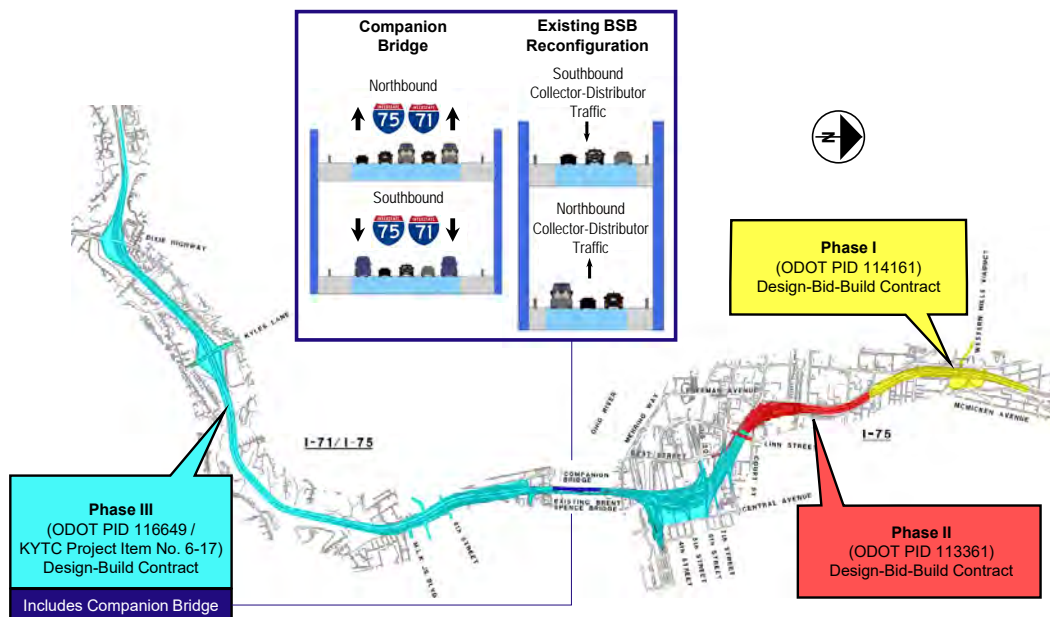
## Kentucky

- Widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct collector-distributor systems in Covington and Ft. Mitchell/Ft. Wright



In Kentucky, the project will widen I-71 and I-75 and rebuild all overpass bridges and interchanges. The project will also extend existing frontage roads to improve connectivity in Covington. A collector-distributor system will also be built beginning in Covington to connect interstate traffic to and from the local street network. Lastly, collector-distributor ramps will be built in Fort Mitchell and Fort Wright to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

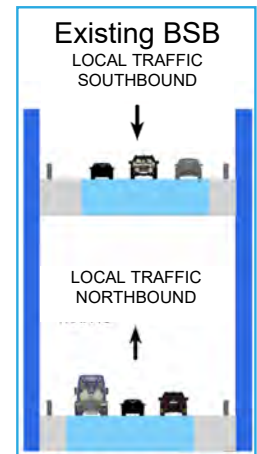
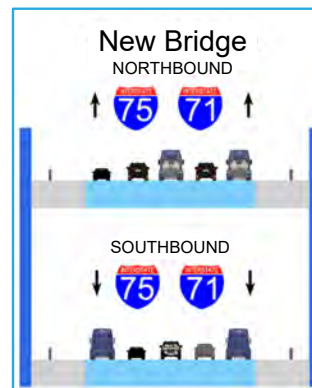
# Project Description



The project is going to be built in three phases. Phase I (shown in yellow) will stretch from Findlay Street to the north. Phase II (shown in red) will stretch from Linn Street to Findlay Street. Phase III (shown in blue) will build everything else, including the new companion bridge. Phase I is currently under design with construction expected to begin in 2028. Although, the construction time frame may adjust based on on-going coordination with the City's Western Hills Viaduct Project. Phase II is also under design with construction expected to begin in 2025. Phase III, which includes the portions in CBD Riverfront, will be built under a progressive design-build contract which will begin in 2023.

# What Has Changed?

- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced

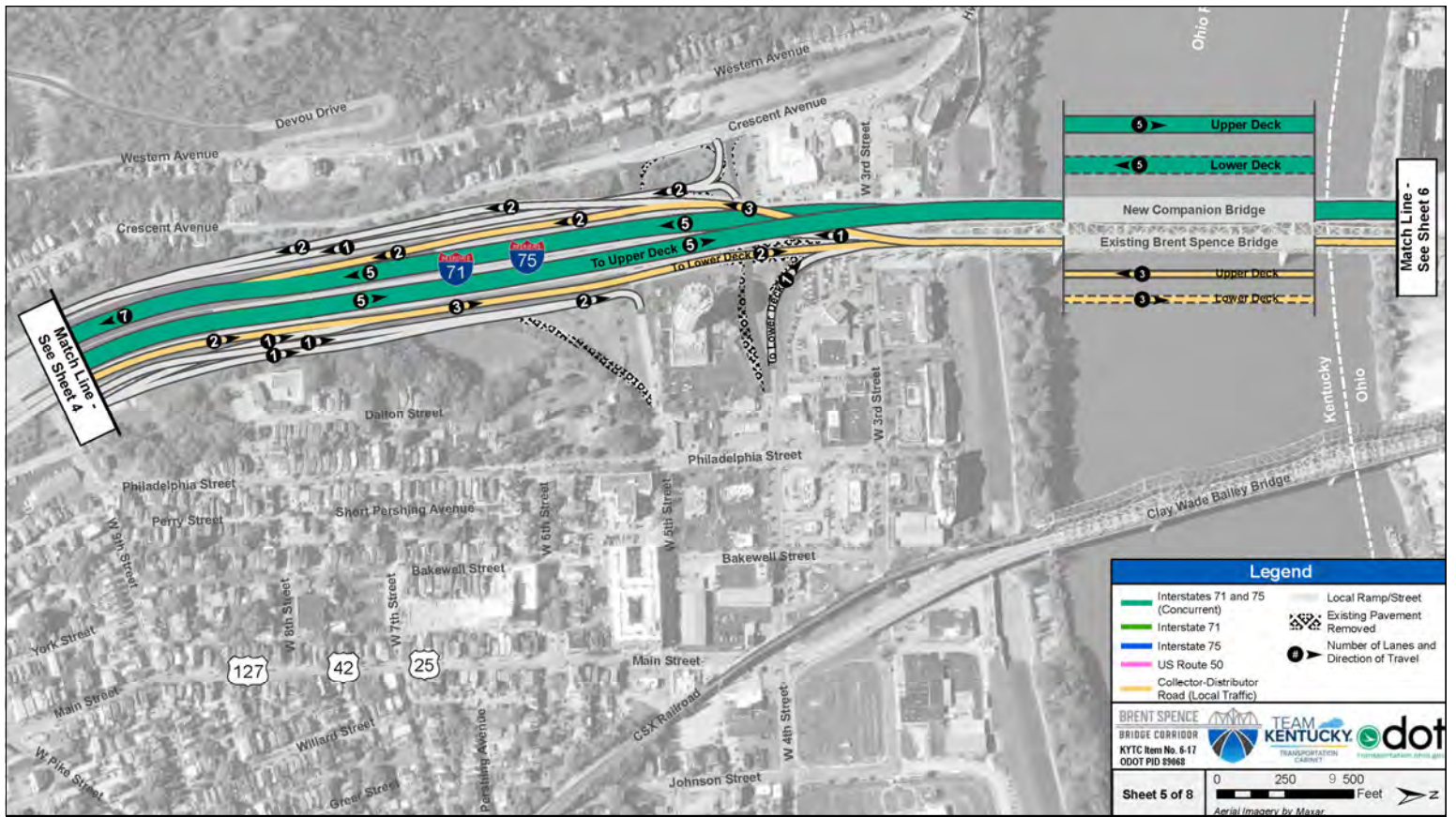


8

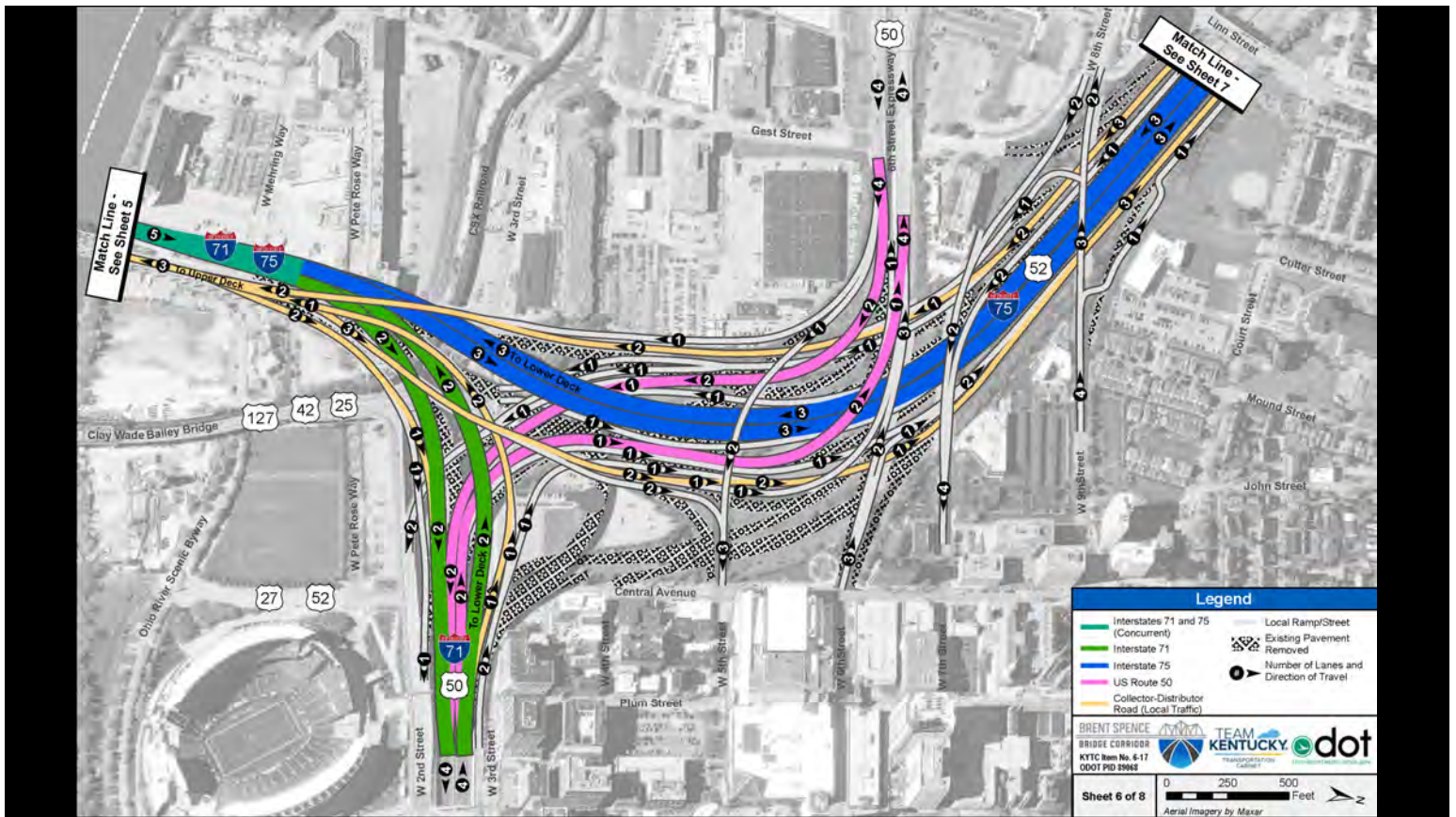
As mentioned earlier, since the 2012 EA and FONSI, ODOT has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.





This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will through Cincinnati. The blue lines show how I-75 traffic will travel to and from the companion bridge. The green lines show how traffic on I-71 will be routed to and from the companion bridge. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets. Finally, the pink lines show how traffic will flow on US 50.

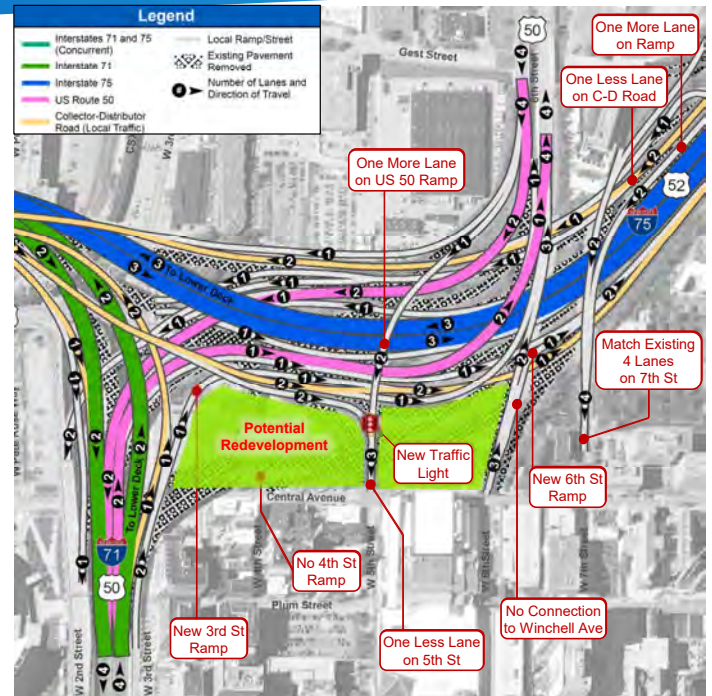
Other changes since 2012 include reducing shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet. In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design and allows us to reduce the overall area needed to build the roadways. We also reduced the number of lanes on some of the frontage roads in Kentucky.



This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

# What Has Changed?

- Reconfigured Downtown Cincinnati Ramps



**Note: Click to walk through ramp changes (marked by #)** | Another change is the layout of the ramps in Downtown Cincinnati. First, ODOT optimized the interchange layout to use land formerly occupied by the Dunnhumby building. More recently, the City asked ODOT to evaluate ramp changes to open up additional land for redevelopment. (#) Based on the City's request, the 4th street ramp to NB I-75 was removed. (#) To provide access for traffic that would have used 4th Street, a new entrance ramp to NB I-75 was added to 3rd Street. (#) The SB exit to 5th Street was removed, which also resulted in fewer lanes on the SB collector-distributor road and at the Central Avenue intersection. The 7th Street exit was also widened to provide additional lanes for traffic that would have used the 5th Street exit. (#) The NB exit to 5th Street was moved closer to the highway to intersect the US 50 ramp at a traffic light, and one more lane was added to the US 50 ramp to make sure all traffic could move smoothly through the light. (#) The 6th Street connection to Winchell Avenue was removed and replaced with a new connection between 6th Street and the NB collector-distributor road. (#) All these changes will open up about 9.5 acres of land for redevelopment, which has been a consistent comment we've been hearing from the public in the last several months.



This drawing shows what the Downtown interchange area might look like once the project is built.

# What Has Changed?

- Northbound I-75 entrance ramp moved from Freeman to Winchell
- One Ezzard Charles Drive bridge
- Minimize work along Winchell



Note: Changes will come in with click (marked by #).

Another change ODOT's made since 2012 involves moving the northbound entrance ramp to I-75 from its existing location at (#) Freeman Avenue (south of Ezzard Charles Drive) (#) to Winchell Avenue (north of Ezzard Charles Drive). Also, (#) the two existing one-way bridges on Ezzard Charles Drive will be replaced with one, two-way bridge. These changes were also made in coordination with the City of Cincinnati to reduce project impacts and costs and improve local access to the interstate.

We've also refined the roadway layouts so that work will not occur along Winchell Avenue beyond what is needed to build the northbound entrance ramp shown in this drawing.

# What Has Changed?

- Interchange at the Western Hills Viaduct

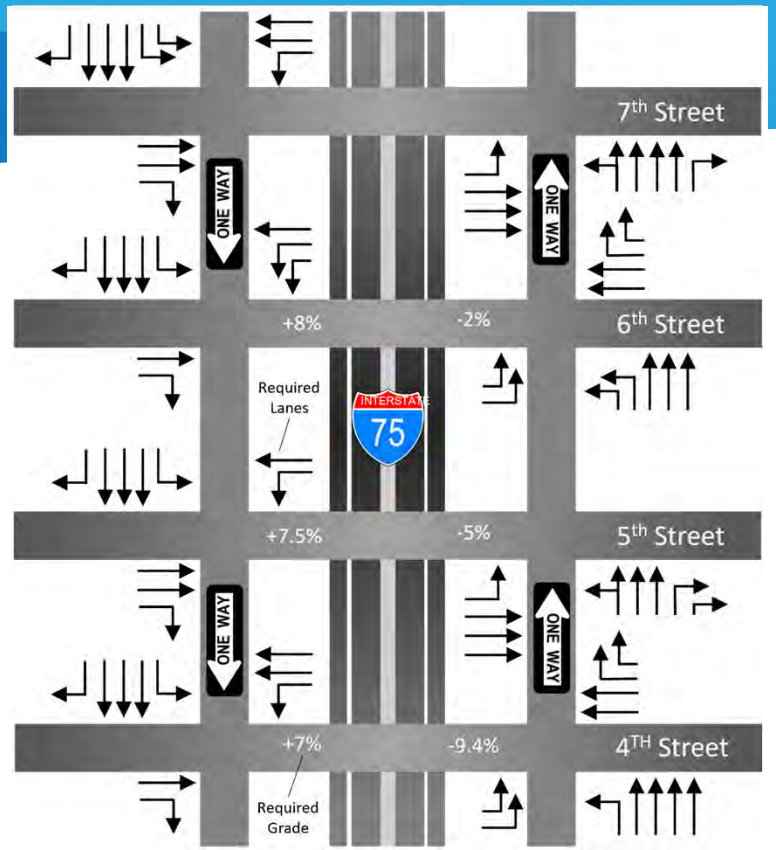


In addition, the layout of the project has been refined to tie into the new Western Hills Viaduct, which is a separate, stand-alone project being developed by the City and County. The existing ramps at the I-75 interchange will be removed and replaced with new ramps that will provide direct access to and from the new Western Hills Viaduct bridge. The ramps will also connect I-75 to the local street system at Findlay Street for southbound traffic and Bank Street for northbound traffic.

## Other Concepts

Additional changes were evaluated but not incorporated into the design

- Depressing I-75 and extending downtown streets to form an urban street grid
  - Cannot meet current design criteria
  - Introduces safety concerns due to steep grades



ODOT has also received several comments about depressing I-75 through downtown Cincinnati similar to Fort Washington Way. This graphic illustrates schematically what that concept would look like. I-75 would be depressed, and the downtown streets would be reworked so that the east-west streets connected across the highway. Two, one-way frontage roads would be built on either side of the highway to move north-south traffic. The combination of these north-south frontage roads and the east-west streets would form a grid, also called an “urban street grid.”

ODOT supports concepts like this when the site conditions lend themselves to this type of design. However, the conditions on and around I-75 do not support this type of design. Just north of the Ohio River, I-75 must over a railroad, and it would require very steep grades (around 8%) in order to be depressed through downtown. This doesn't meet current design standards which state the maximum grade must be 5%. In addition, such steep grades would introduce safety concerns, particularly given the large number of trucks that travel on I-75.



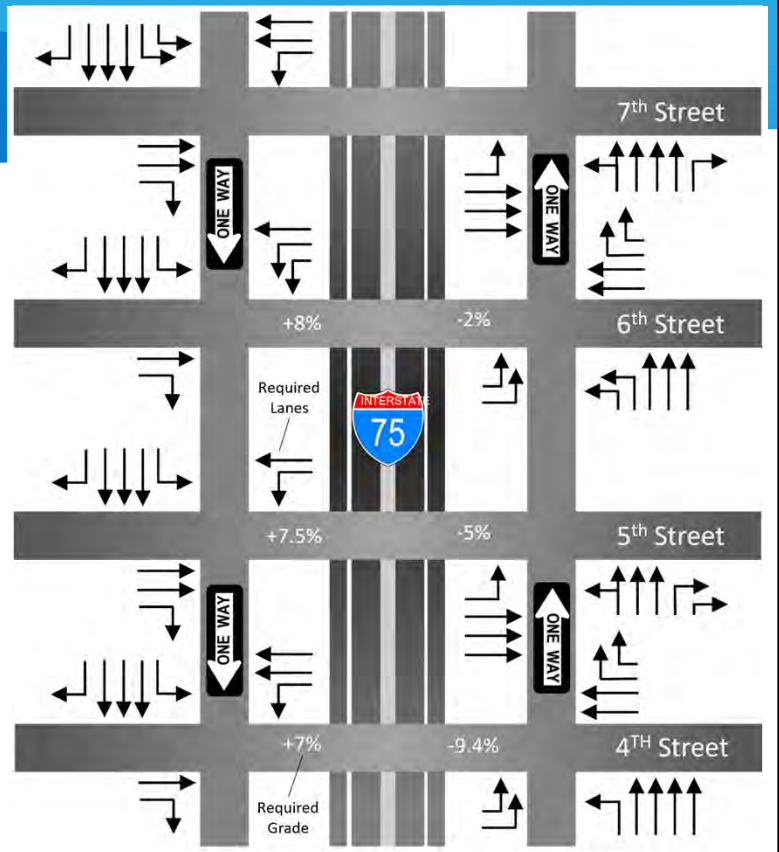


Any design must accommodate a complicated system of mainline and ramp movements to pass over the railroad and provide local access and continuity along I-71, I-75, and US 50. Weaving these ramps through the project area requires a complex tiered bridge system with the highest tier rising over 50 feet above the ground. To meet these geometric constraints, the preferred alternative for the BSB Corridor Project currently descends toward downtown Cincinnati at a 4- to 5.5-percent grade. Depressing the roadway any further would require substantially steeper roadway grades (8% or more). For reference, ODOT's *Location and Design Manual, Volume 1* lists the maximum grade for urban interstates in similar terrain as 6 percent.

## Other Concepts

Additional changes were evaluated but not incorporated into the design.

- Depressing I-75 and extending downtown streets to form an urban street grid
  - Cannot meet current design criteria
  - Introduces safety concerns due to steep grades
  - Increases project footprint with steep local streets
  - Increases traffic through downtown Covington



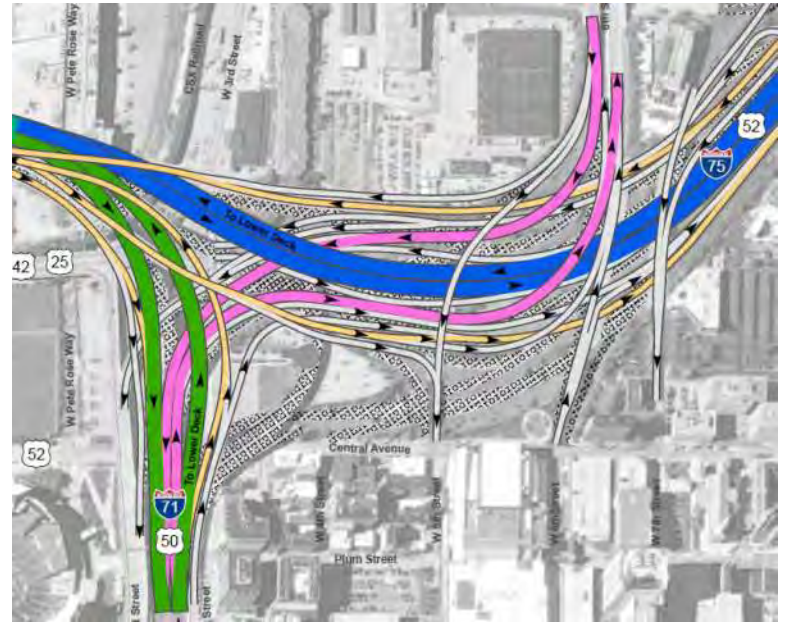
Since I-75 cannot be lowered, local streets would need to be raised to form an urban street grid across I-75. This would increase the project footprint because the east-west streets would need to be rebuilt well beyond where they intersect the highway. This additional footprint would result in new impacts to existing businesses and potential developable land. For example, the elevated cross streets would create steep slopes that do not meet design criteria for local streets (as shown in this graphic) and would negatively affect potential development opportunities for urban, street-facing development on 2.5 to 3.0 acres of land east of I-75 that is currently slated to be transferred to the City of Cincinnati once the project is completed.

Moving all local traffic to an urban street grid as opposed to the collector-distributor system currently included in Concept I-W would also substantially increase traffic on the local streets in Covington.

## Other Concepts

Additional changes were evaluated but not incorporated into the design.

- Depressing I-75 and extending downtown streets to form an urban street grid
  - Cannot meet current design criteria
  - Introduces safety concerns due to steep grades
  - Increases project footprint with steep local streets
  - Increases traffic through downtown Covington
  - Does not maintain through traffic on US 50



Finally, building an urban street grid would not allow US 50 – which is shown in pink above – to remain where it is today. To make room for the new “grid,” US 50 would have to start and stop on both sides of I-75. ODOT’s also received several comments about improving north-south connections and east-west connections across I-75. However, as illustrated by this graphic, Concept I-W maintains north-south connectivity through the collector-distributor system shown in orange. East-west connectivity across I-75 is provided by local streets shown in grey and US 50 shown in pink.

# Aesthetics

- Corridor-wide aesthetic treatments
  - Ashlar stone treatments for abutments, piers, and walls
  - Decorative bridge parapets
  - Translucent screen walls and planters on bridges
  - Other features (lighting, sidewalks, etc.) to match City standards



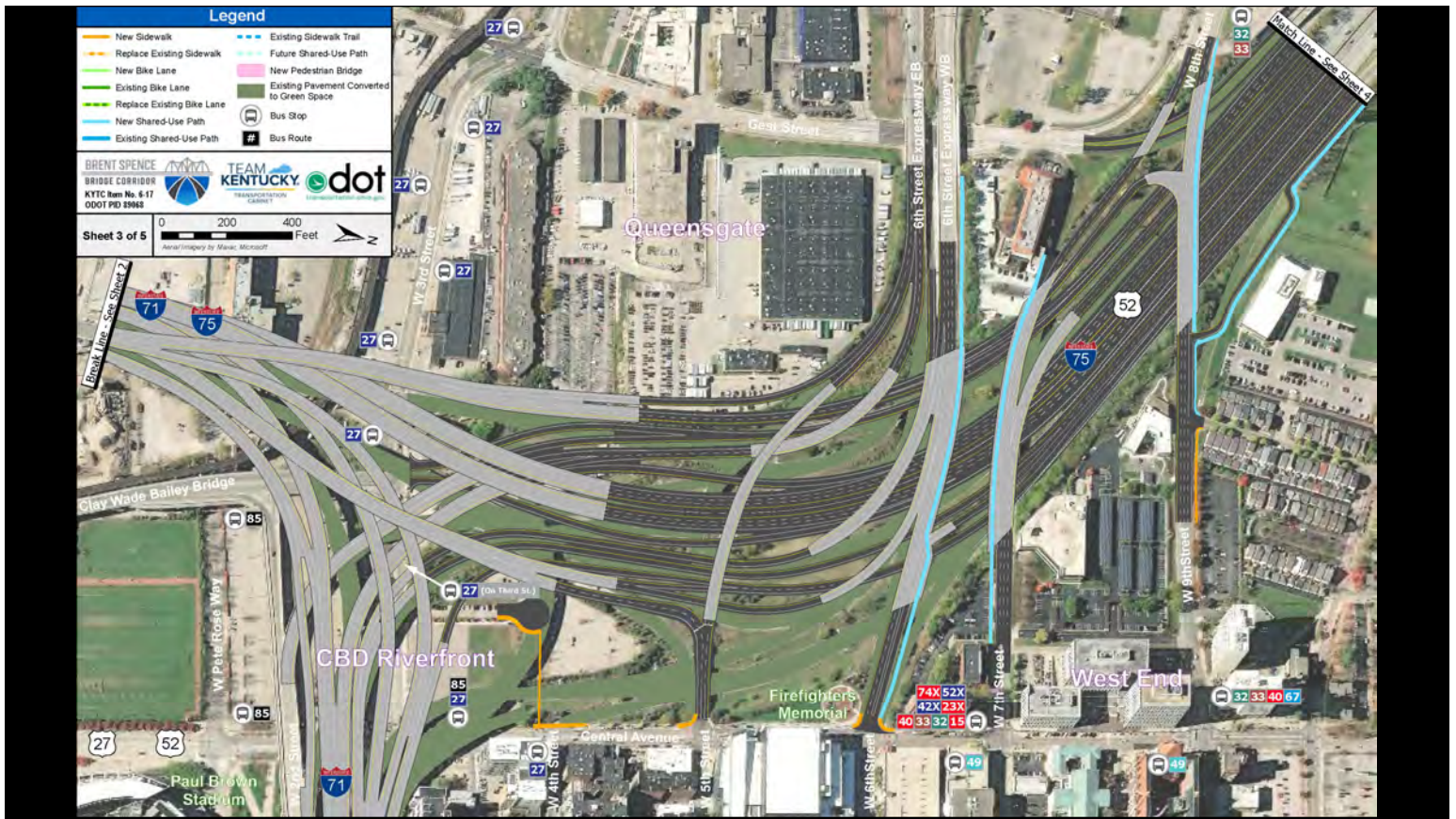
ODOT has worked with the City and the project Aesthetic Committee to develop an Aesthetic Design Checklist that will guide what the Brent Spence Bridge Corridor will look like. For example, ODOT will add aesthetic treatments in a pattern called “Ashlar Stone” too all bridge abutments, parapets, and piers. Retaining walls and the extended traffic safety barriers we just discussed will also have ashlar stone treatments. Overhead bridge parapets will have end treatments with the bridge identification name, construction completion data, and rustification design features. Examples of similar bridge parapets are shown in the images on this slide. In addition, overpass bridges will have wide sidewalks or shared-use paths, planters, and translucent screen walls. Other features throughout the corridor will match City aesthetic standards, including lighting, sidewalks, tree lawns, and others. We will show some examples of what specific locations in the project area will look like in just a few slides.

# Pedestrians and Bicycles

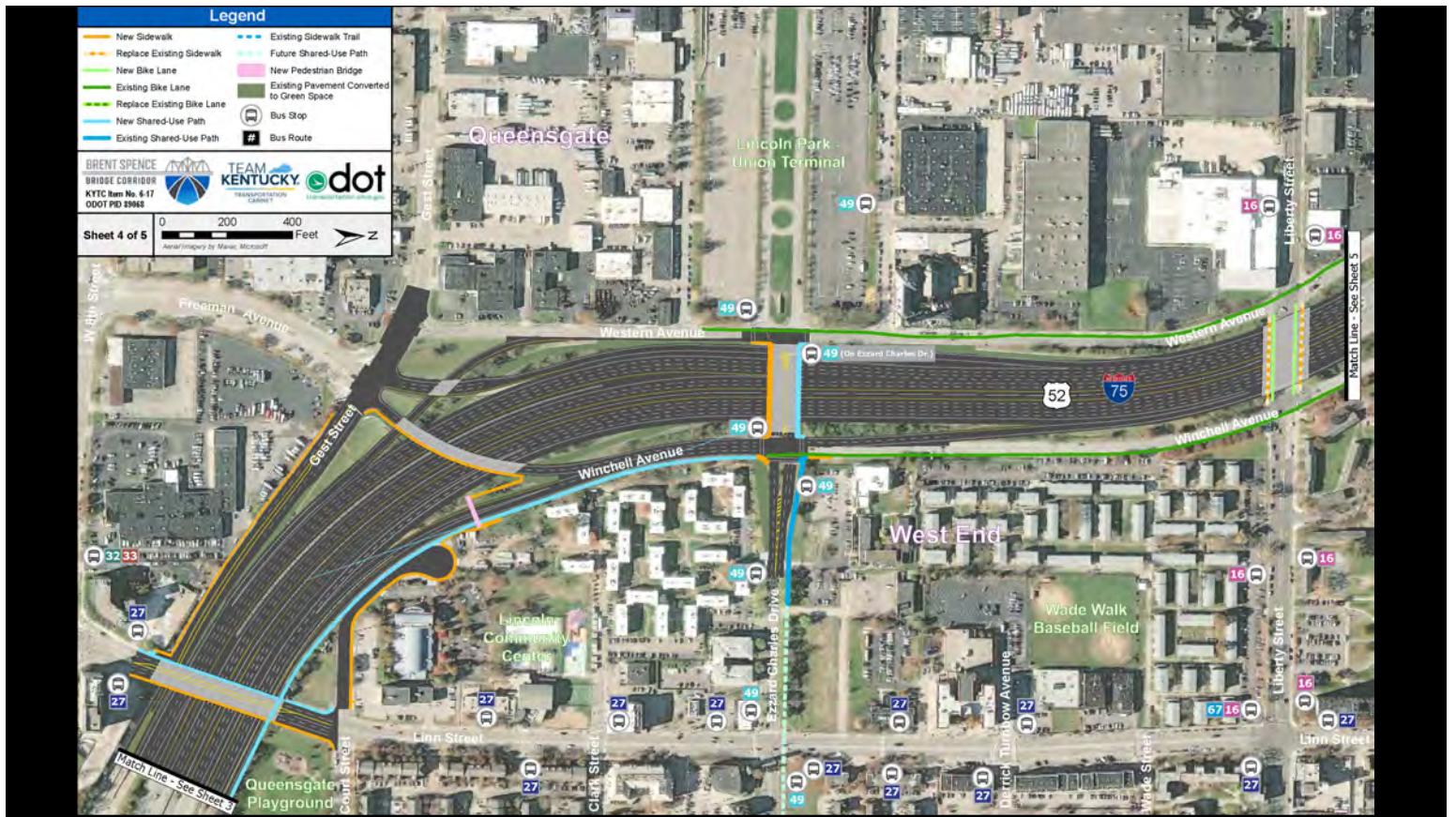
- Connections across I-75
  - Sidewalks
  - Shared use paths
  - Bike lanes
- New shared use path along Winchell Avenue
- Improved connections to local destinations



The project will install bicycle and pedestrian infrastructure on connections across I-75 like what is shown here for 6th, 7th, and 9th streets. Pedestrian and bicycle connections will also be provided on Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue. To promote safety for bicycles and pedestrians, the ramp connections with local streets are being designed as lower-speed urban intersections in accordance with City of Cincinnati design standards. The pedestrian and bicycle infrastructure included in the project will improve connectivity to transit, employment, healthcare, cultural, recreational, and commercial destinations.



This exhibit shows some of the planned pedestrian and bicycle facilities near CBD Riverfront. Note the new shared use paths on 6th, 7th, and 9th streets and Winchell Avenue and new sidewalk connections on 9th.



Continuing north, we see how the new shared use path on Winchell Avenue continues all the way to Ezzard Charles Drive. Also notice new shared use paths on Linn Street and Ezzard Charles Drive, new sidewalks along Guest Street, Court Street, Freeman, and Ezzard Charles, a new pedestrian bridge connection over Winchell Avenue, and new bike lanes on Liberty Street.



As we move further north, notice the new bike lanes on Findlay Street, the new shared use path on Harrison Avenue, and the replaced sidewalks on Harrison, Bank, and Findlay. These will tie into the existing bike lanes on Western, Winchell, and Bank. Throughout the corridor the new and improved pedestrian and bicycle facilities will provide connections to existing transit stops.





This is a view of what the Firefighters Memorial will look like when the project is built. Notice the highway and ramps in the background and the ashlar stone treatments on the retaining walls. Also notice the areas currently shown as green space that are opened up for potential development.



This view shows what the new, two-way bridge on Ezzard Charles Drive will look like. Notice the wide shared use path on the right and the sidewalk on the left. Also notice the planters, screenwall, and decorative lighting on the bridge. The bridge lights and traffic signal supports will be black and meet City aesthetic standards.



This view shows what the finished project will look like from Ezzard Charles Drive looking north. Notice the ashlar stone treatments on the retaining walls.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.



---

## Attachment 5: Exhibits



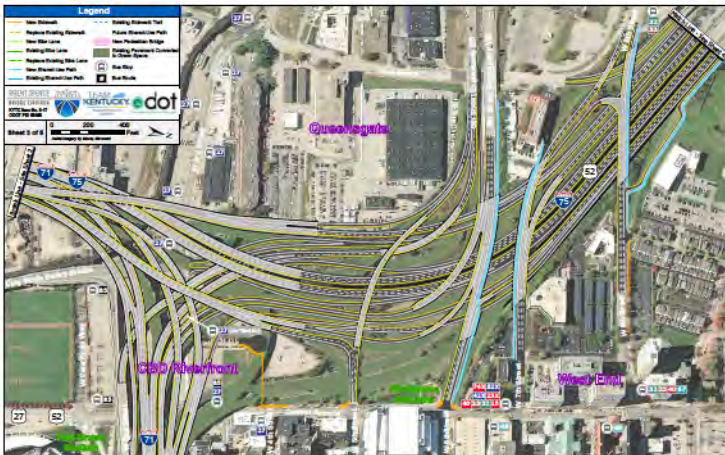
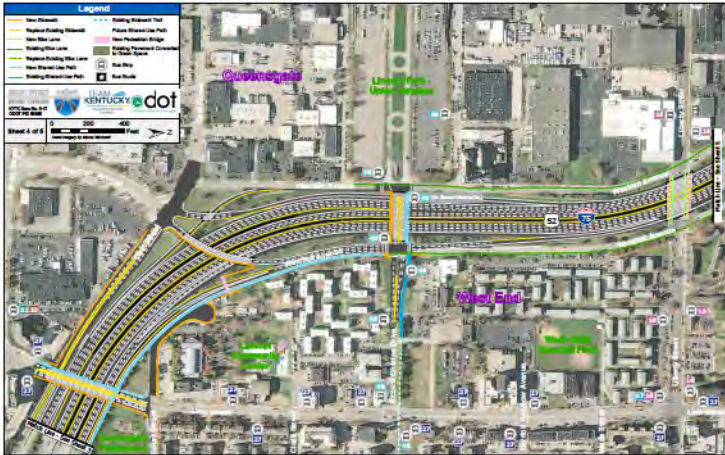
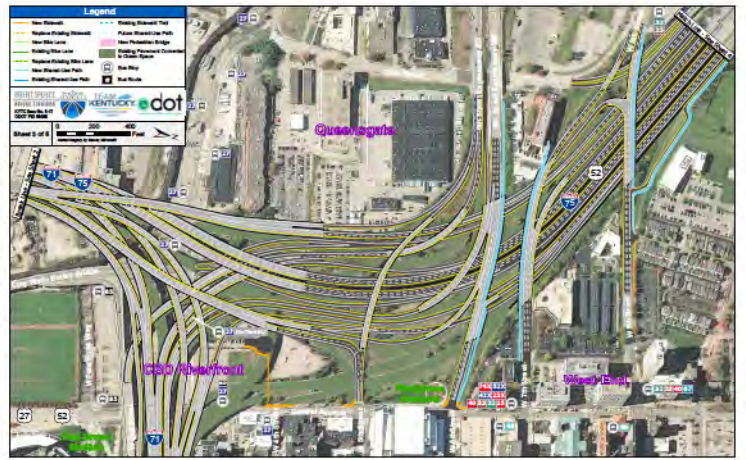
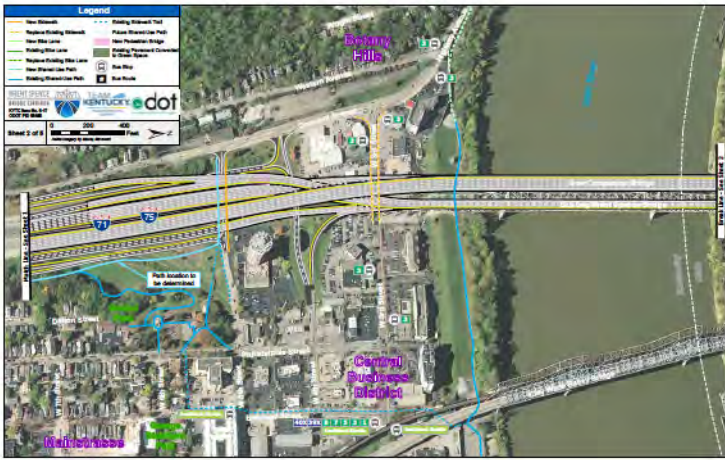
BREIT SPENCE BRIDGE CORRIDOR  
**Brent Spence Bridge Corridor (Looking North from Kentucky)**  
 TEAM KENTUCKY dot  
 KYTC Item No. 9-17 | CDOT PG 0608

BREIT SPENCE BRIDGE CORRIDOR  
**Brent Spence Bridge Corridor (Looking Northwest from Kentucky)**  
 TEAM KENTUCKY dot  
 KYTC Item No. 9-17 | CDOT PG 0608



BREIT SPENCE BRIDGE CORRIDOR  
**Firefighters Memorial (Looking Southwest) - Ohio**  
 TEAM KENTUCKY dot  
 KYTC Item No. 9-17 | CDOT PG 0608

BREIT SPENCE BRIDGE CORRIDOR  
 TEAM KENTUCKY dot  
 KYTC Item No. 9-17 | CDOT PG 0608





---

Attachment 6: Comment Form



# COMMENT FORM

BRENT SPENCE  
BRIDGE CORRIDOR



Contact information is not required but will ensure you receive a response, should one be required.

Name: Mike James

Mailing address (or nearest cross streets): [REDACTED]

Email address: [REDACTED] Phone: [REDACTED]

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

If this projects intended strategy is to plan for the year 2050 and does not include planning for trains then it has entirely missed the mark. It is clearly working on behalf of fossil fuel industry directives and all of the work that has been done is irrelevant and waste ful  
Scrap your entire plan and start over. o

**COMMENTS DUE BY  
January 5, 2023**

**Comments may be submitted:**

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
- By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# Brent Spence Bridge Corridor Project Westside Covington Neighborhood Meeting Summary November 29, 2022

## Introduction

The Westside Covington Neighborhood Meeting was held on November 29, 2022 from 6:30 pm to 7:40 pm. The meeting was held at the Center for Great Neighborhoods located at 321 West 12<sup>th</sup> Street in Covington, Kentucky. It was publicized through the West Covington Facebook page and an announcement was posted at the meeting location (see Attachment 1). Attendees at the meeting included the Covington police, the project team, and members of the public. In total, 20 people attended the meeting, including 7 individuals from the project team (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). During the presentation, the project team responded to questions and comments offered by those present.

Exhibits showing noise wall renderings, lane configurations, and a rendering of Goebel Park were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](https://www.PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Six (6) comment forms and nine (9) demographic surveys were returned at the meeting.

## Demographics

The following is a breakdown of the responses received from the demographic survey.





A write in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. Comments included:

- Keep involving westside residents including residents on planning committees for projects.

## Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Clarification / understanding of local versus through traffic and ramp connections.
- Concern over relocations / property impacts.
- Clarification / understanding of the process for noise evaluation and discussion of impacts with / without noise wall.
- Clarification of timeline and staging of construction.
- Information of cost / funding.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.

Comments noted on the survey forms returned at the meeting include:

- Sound barrier essential.
- I personally like the noise wall.
- Great presentation – thanks for this opportunity!

**Table 1: Q & A from Session**

Question	Project Team Response
On the south side of the bridge, will all construction be on the west?	Not completely. There will be some construction on the east side to build the local network.
Will most of buildings that will be affected be on the west side?	The majority of the relocations will be on the west side, although one business is on east side.
Will park lands will be lost?	Some land from Goebel Park will be used, but KYTC will return some area to the park by reconfiguring the West 5 <sup>th</sup> Street exit ramp.
Will the Texas turnaround be part of this project?	The 4 <sup>th</sup> Street ramp that is going to be restricted to emergency vehicle access only will be reopened for this project. The Texas turnaround will be eliminated once the project is built, but it will be helpful for maintenance of traffic.



Question	Project Team Response
Will there be emergency lanes on the existing (local) bridge?	There will be 8-foot shoulders.
If you are going north and want to go to I-71, do you have to take the local bridge?	I-71 can be accessed from both the companion (interstate) and the existing (local traffic) bridges. The only decision on the Ohio side of the companion bridge will be whether to stay on I-75 or to take I-71.
Is existing 5 <sup>th</sup> Street still going to go under the interstate?	Yes.
Once it gets into Covington will it be a double decker?	Both the companion and existing bridges will be double-decker structures. In Covington, the southbound lanes from the existing bridge will weave between the lanes for the upper and lower decks of the companion bridge.
What will be the impact in 20-30 years? Does reducing the project footprint now have ramifications 20-30 years down the road?	Most of the reductions in the project footprint are due to the reconfiguration of lanes on the companion bridge and reductions on the local street network. Traffic analyses consider traffic operations 20 years into the future.
If you end up in interstate / express lanes where is your next decision point going southbound once you've crossed the bridge?	Kyle's Lane will be the next opportunity to exit going southbound in the interstate lanes.
Has there been an economic study to determine if there will be negative impacts on businesses with the express lanes?	The concept of express / local lanes is not unique and is in use throughout the country. KYTC has already developed conceptual signing plans to help address concerns about wayfinding and access to local businesses.
Some communities want the wall and some don't? How does that work?	Those that benefit from the wall will be given more say in whether or not walls will be built.
Will there be walls on the other side of the interstate? Will noise reflect off the walls and make it louder on the other side?	These conditions are considered in the noise model.
What do you mean by enhanced sidewalks?	West 9 <sup>th</sup> Street was used as an example. Today, the walks are narrow, and it's dark under the bridge. The walks can be made wider to accommodate multi-use and there are opportunities for lighting under bridges and improved crossings.
Would the vegetative median be a rain garden?	Treatments for medians have not been specifically decided yet. The aesthetics subcommittee is exploring options.



Question	Project Team Response
Is there a projected start date?	KYTC and ODOT have applied for Megaproject and Bridges Grants established by the Bipartisan infrastructure law. The announcement of grant recipients will likely occur by the end of year. Some construction could begin in late 2023, but heavy construction will not begin until 2024.
Is the project being funded with City or Federal dollars?	The project has a significant price tag. Federal funding is required. Neither state can fund the project independently.
What is estimated cost?	The current cost estimate is over \$3.2 billion, but prices can / will fluctuate due to inflation. This is one of the reasons the project team decided to use progressive design / build.
What is the bridge type? Cable stay rendering? Will this look like the arch bridge?	Do not have current renderings of both bridge types. Renderings from 2012 are available on <a href="http://PublicInput.com/bsbc">PublicInput.com/bsbc</a> .



---

## Attachment 1: Advertising Materials

**Westside Facebook Page**

Facebook.com



**Westside Covington Neighborhood**



**Reyna Larie**

November 5 at 9:52 AM · 🌐



This event is intended for any westside resident..

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of neighborhood outreach meetings for the Brent Spence Bridge (BSB) Corridor Project

Purpose: To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team ... [See more](#)



👍 2

4 Shares

👍 Like

💬 Comment

➦ Share



Submit your first comment...







Attachment 2: Sign-In Sheet

SIGN-IN SHEET

### BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – November 29, 2022

Name	Representing	Address
1. <i>David Arnold</i>	<i>Westside</i>	
2. <i>Aaron Winters</i>		
3. <i>Ryan Smith</i>	<i>Westside</i>	
4. <i>Bill Wells</i>	<i>CCC</i>	
5. <i>Michael Campbell</i>	<i>Westside</i>	
6. <i>Heather Butler</i>	<i>Westside</i>	
7. <i>Susan Allen</i>	<i>Neighborhood</i>	
8. <i>Lindsay Allen</i>		
9. <i>Dave Gordon</i>	<i>Westside</i>	
10. <i>Mike Nesbitt</i>	<i>Westside</i>	
11. <i>Mike Spivey</i>	<i>Westside</i>	
12. <i>Mike Spivey</i>	<i>Westside</i>	
13. <i>Sam Fekert</i>	<i>West Side</i>	
14. <i>Rechal White</i>	<i>Covington Police</i>	
15. <i>Miss Burrell</i>	<i>Covington Police</i>	
16. <i>Heather Patten</i>		
17. <i>Jodi Hill</i>	<i>HUB</i>	
18. <i>Steve Hays</i>	<i>KYC</i>	
19. <i>Greg Vahlsing</i>	<i>KYC</i>	
20. <i>Lindsay Weller</i>	<i>HUB</i>	

Please print clearly. Contact information is optional but must be included if you wish to receive updates.

page \_\_\_ of \_\_\_  
KYTC Item No. 6-17 (08/17) (08/17) (08/17)



### Attachment 3: Photographs





---

[Attachment 4: Presentation](#)

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



# Westside Covington Neighborhood Meeting (KY)

November 29, 2022



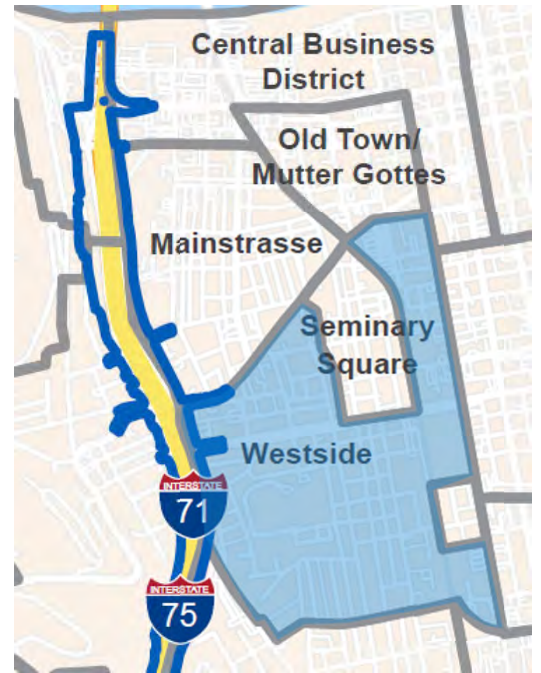
INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with introductions of the Project Team in attendance (2 min).

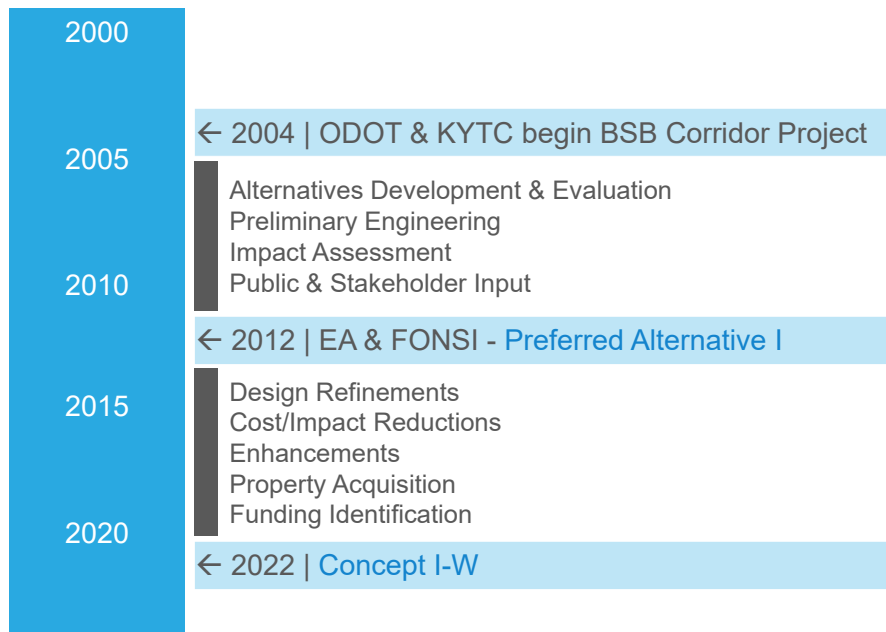
# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the Westside Covington neighborhood the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Westside area
  - Discussion/feedback from Westside residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Westside Covington neighborhood. Most importantly, tonight we're focusing on hearing from the residents of Westside. We're here to answer your questions and to listen to your feedback about the project.

# Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, KYTC has begun purchasing the land needed to build the project.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E

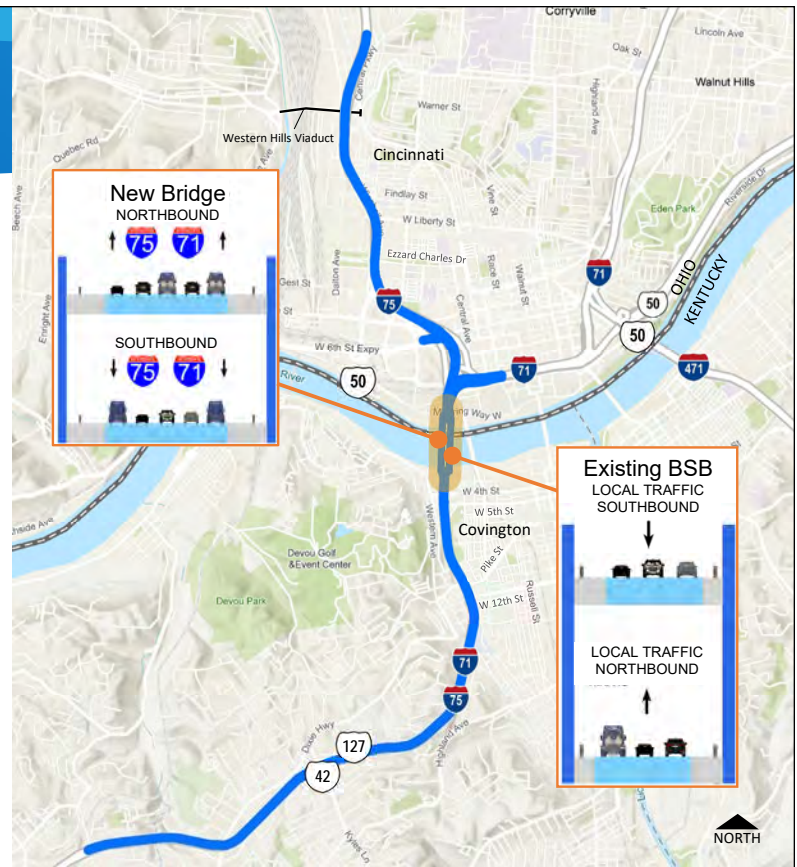


In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

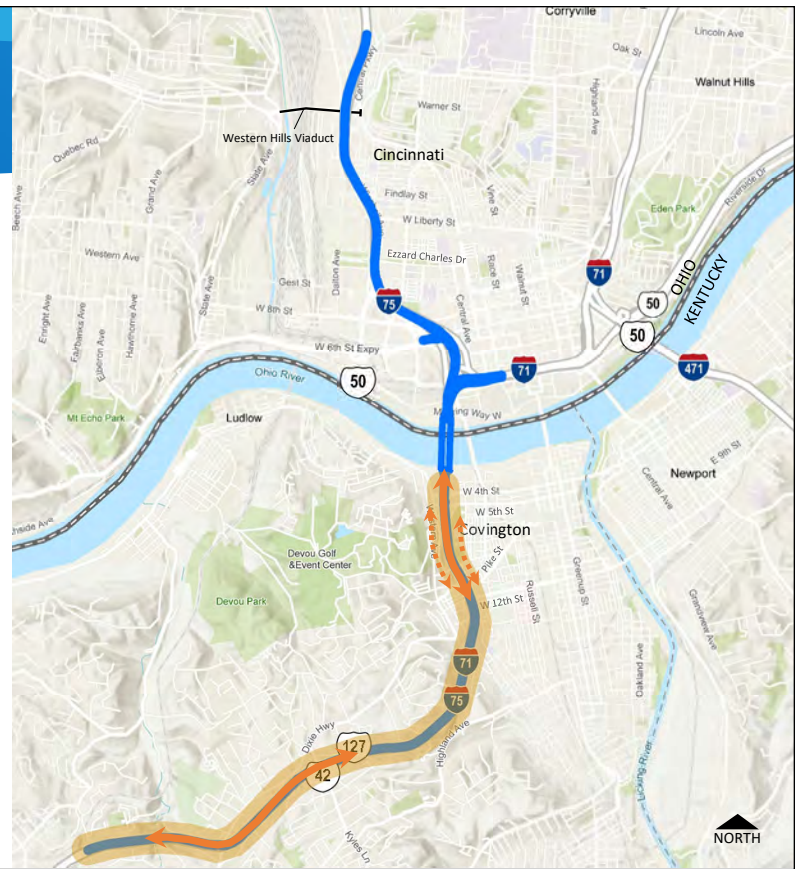
The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.



# Project Description

## Kentucky

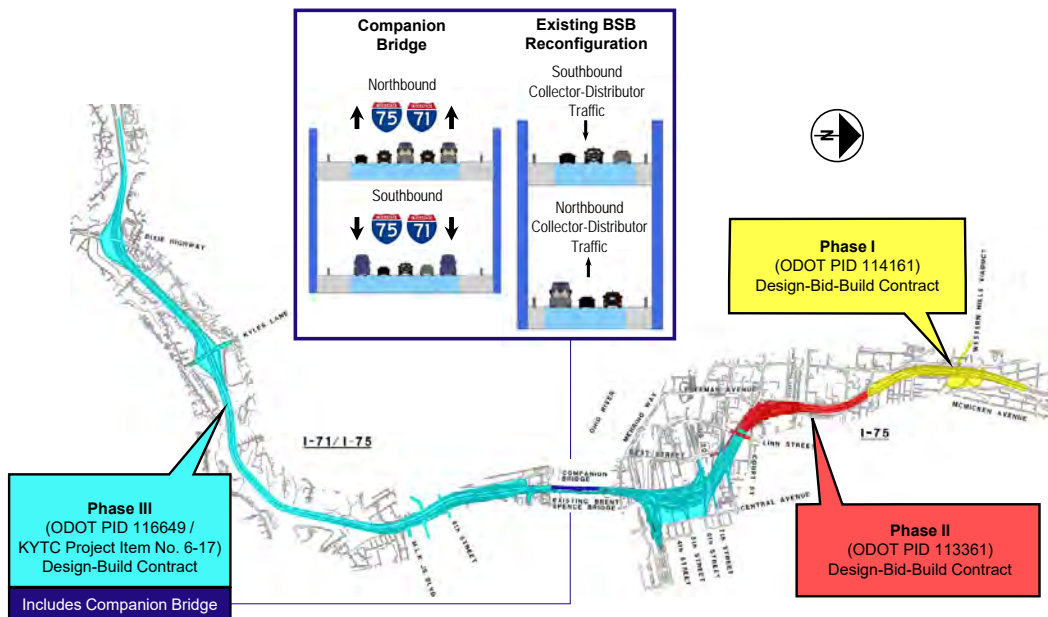
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles Lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

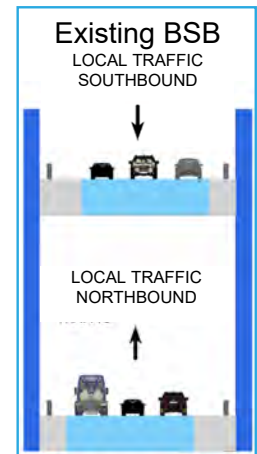
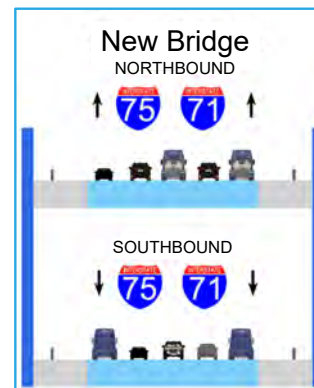
# Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

# What Has Changed?

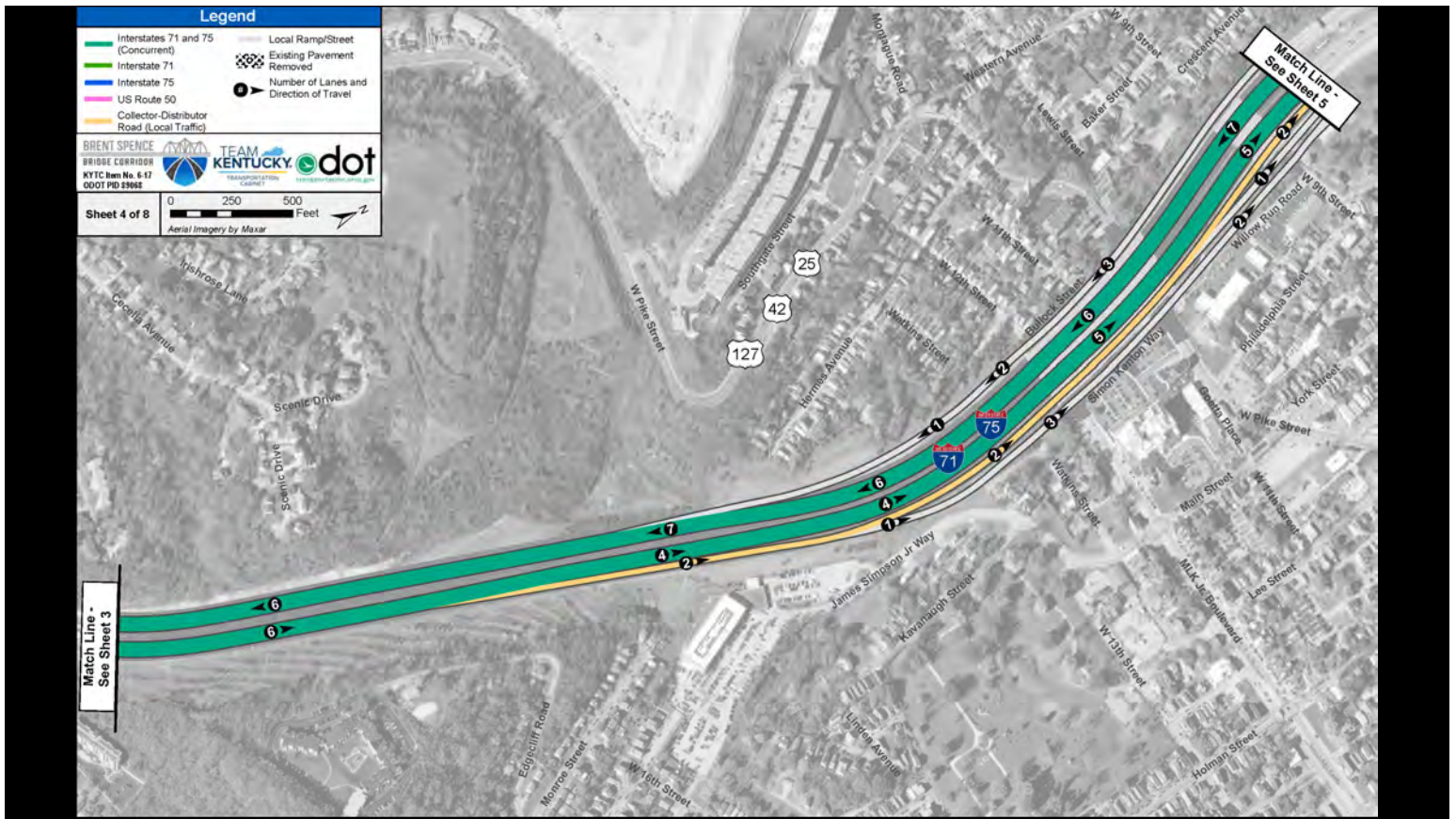
- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



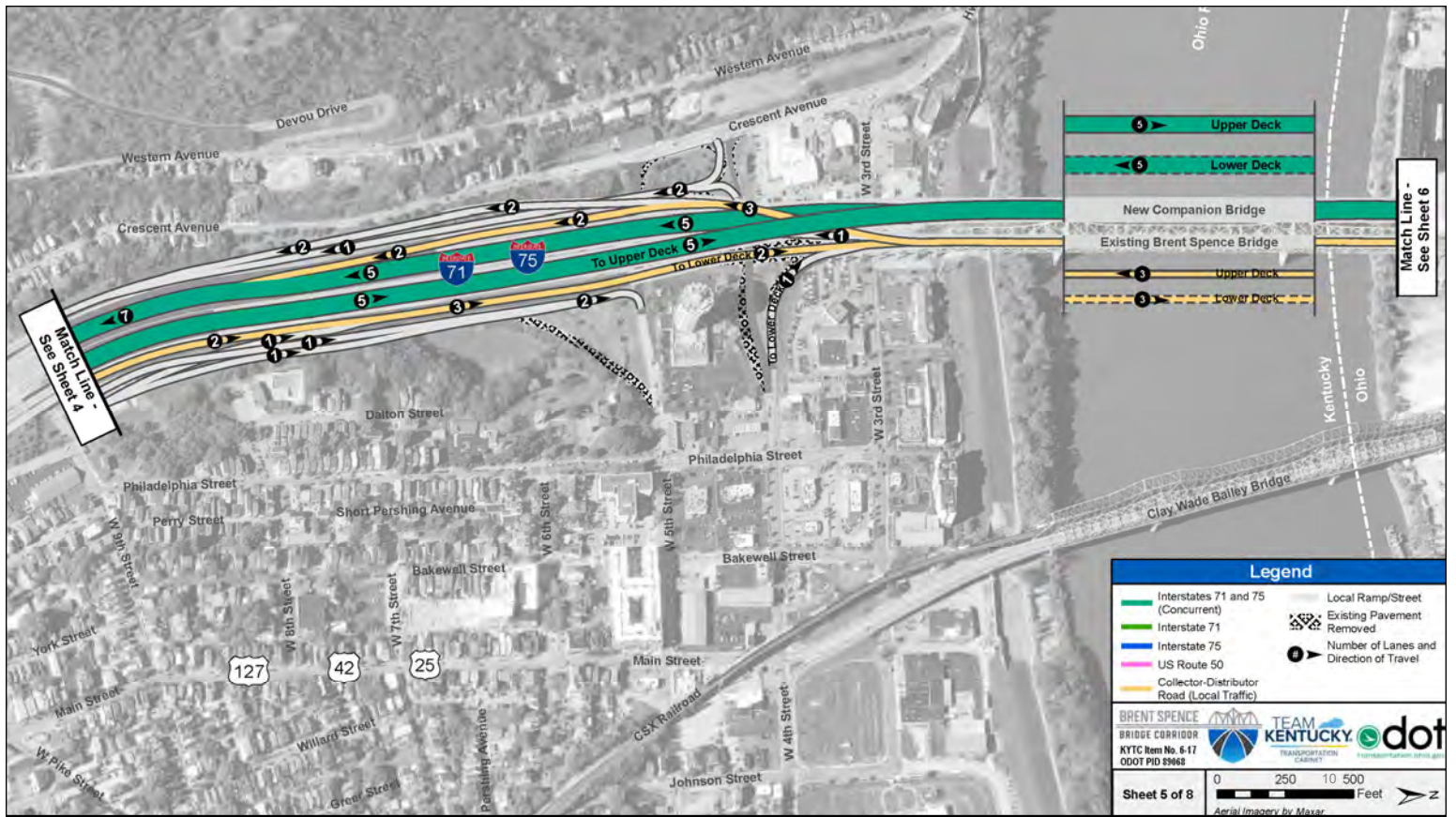
8

As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

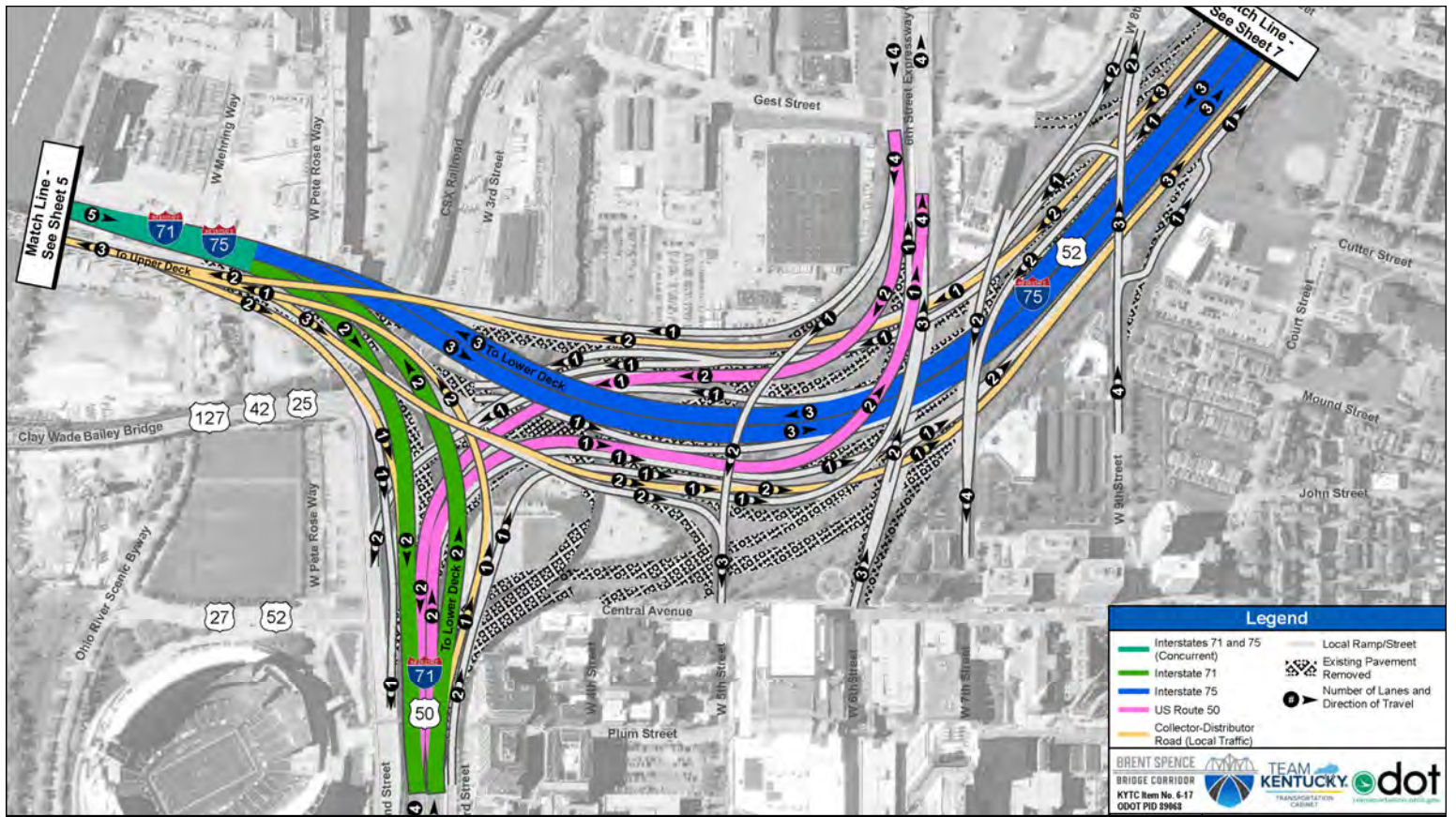
Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

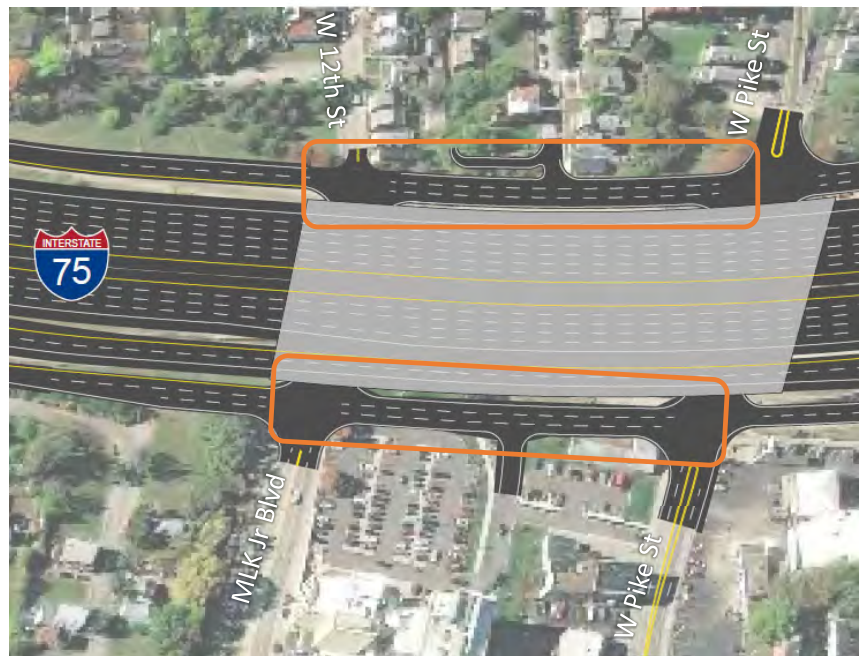


Here's another view from Covington north of Westside.



# What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 40+ then vs. 4 now
  - Commercial | 6 then vs. 5 now



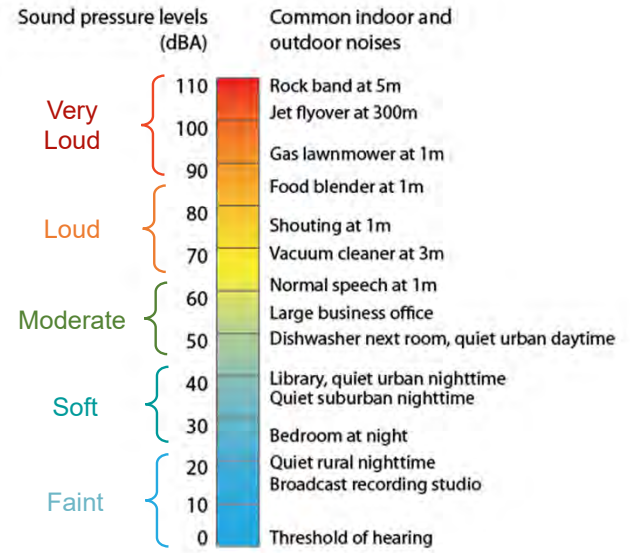
Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound



Source: Minnesota Pollution Control Agency

15

Now let's dig into how the project will impact Westside Covington, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

- KYTC Noise Policy
- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.

Kentucky Transportation Cabinet  
Department of Highways  
Division of Environmental Analysis



## NOISE ANALYSIS AND ABATEMENT POLICY

APPROVED BY:

Todd Jeter, Division Administrator  
Federal Highway Administration

Date

James Gray, Secretary  
Kentucky Transportation Cabinet

Date

Revised August 1, 2022  
**EFFECTIVE DATE JULY 1, 2020**  
Revised July 1, 2016  
Original July 2011

16

KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dbA. We also determine if there is a substantial increase – specifically a 10 dbA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dbA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

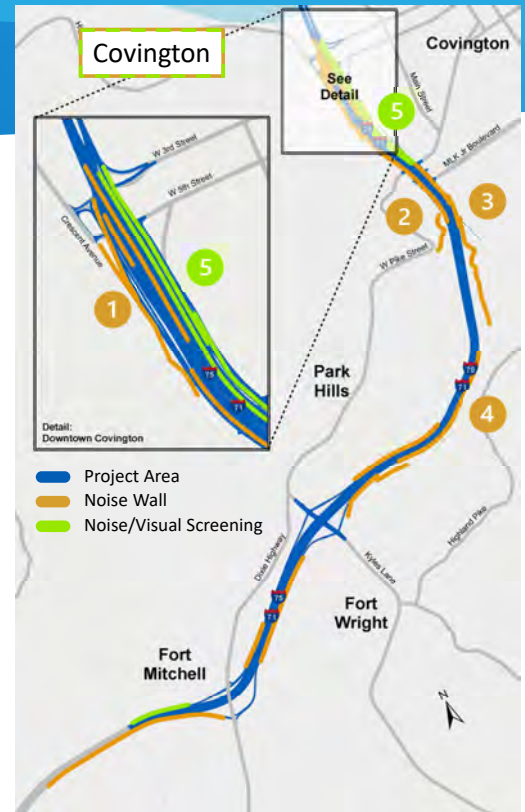
# Noise

Noise walls reasonable/feasible per KYTC Noise Policy

- 3 • Pike St to Edgecliff Rd (Westside/Peaselburg)
  - Without noise wall = 71 dBA
  - With noise wall = 64 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office

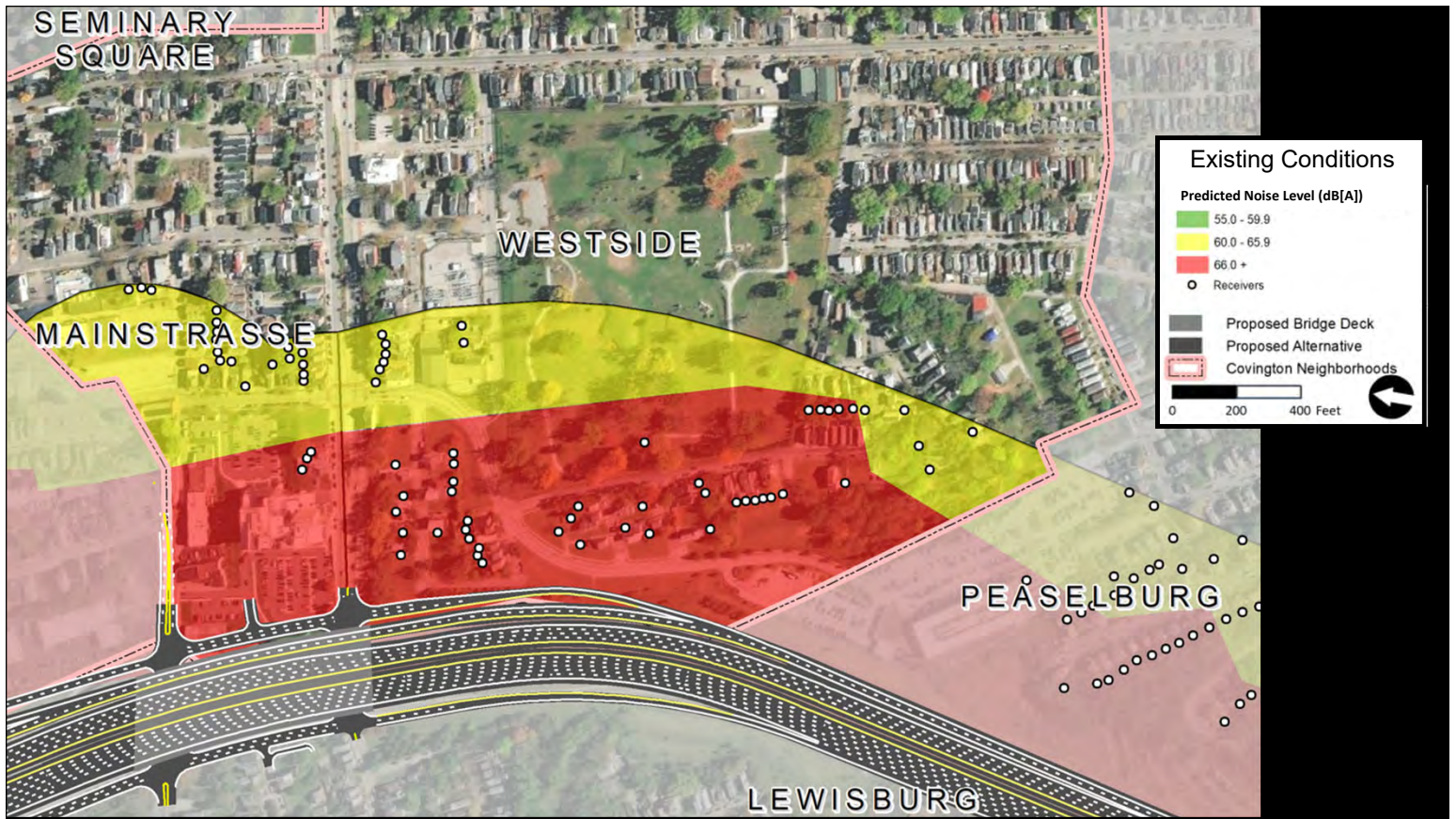
*Note: Sound levels represent the average for the area benefitted by the wall.*



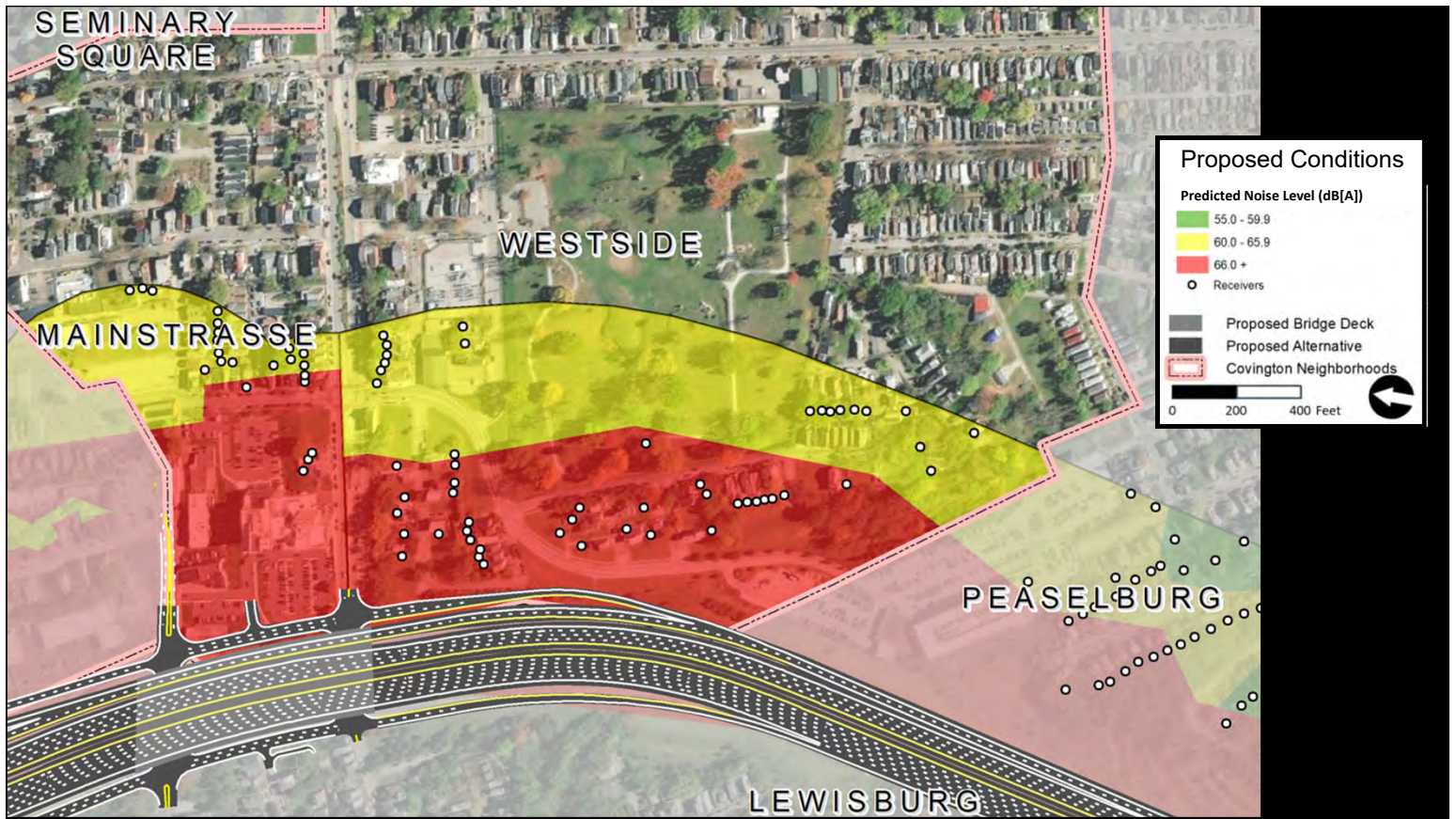
In Westside Covington, a noise wall system extending from Pike Street to Edgecliff Road was found to be reasonable and feasible (shown by the number 3). In this area, average noise levels are predicted to be 71 decibels without a noise wall and 64 decibels with a noise wall. For reference, the average exterior sound levels with noise walls in Westside are predicted to be near what you'd experience in normal conversation.



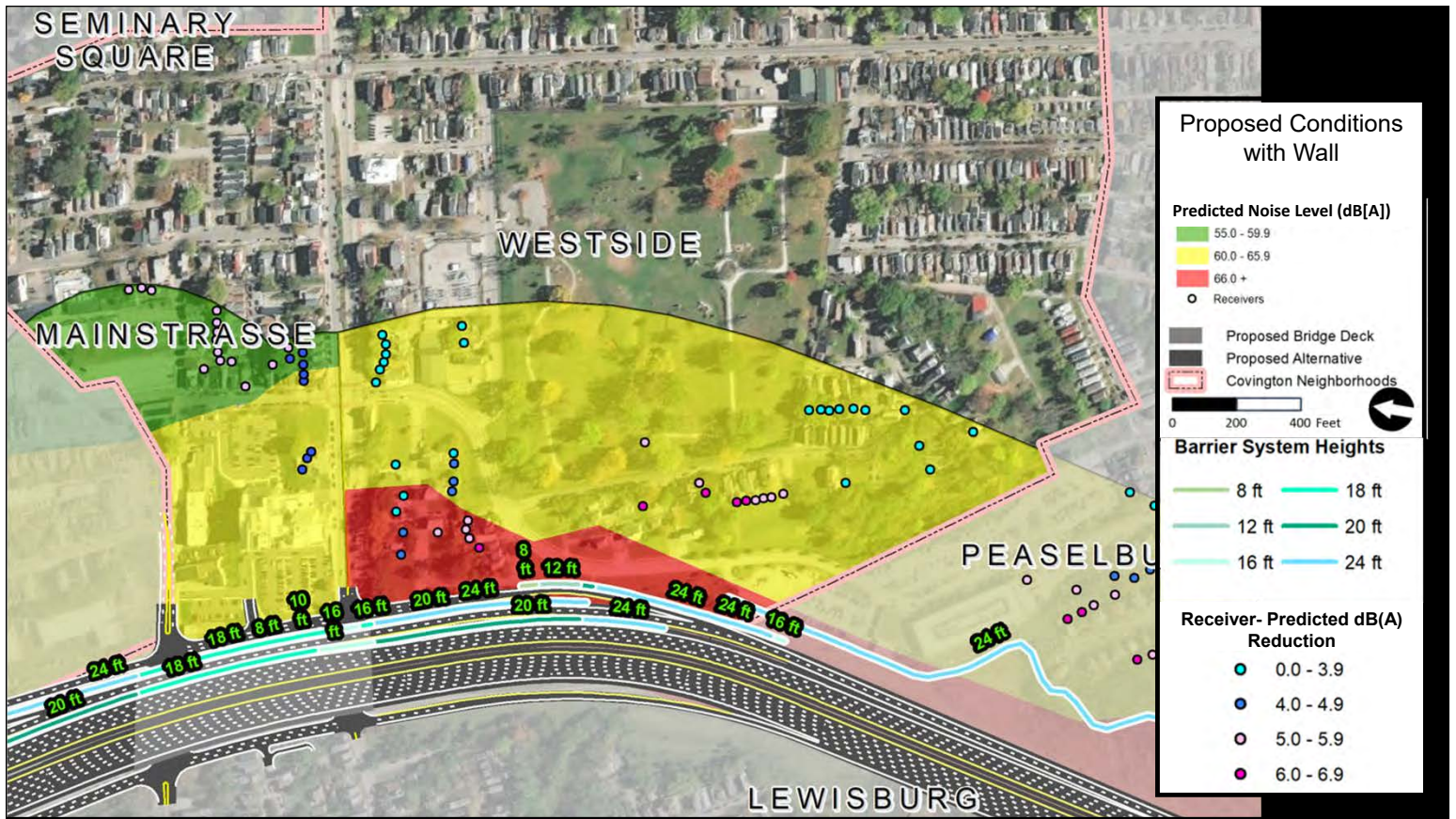
The red lines on this slide show noise walls that were evaluated as part of the noise study for in the Westside Covington area. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a noise wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



# Noise



Pike Street Without Noise Wall (looking east)  
71 dBA (average)



Pike Street With Noise Wall (looking east)  
64 dBA (average)

To the left is a drawing showing what the finished project might look like at Pike Street if a noise wall is not built. To the right is a drawing showing what Pike Street might look like with a noise wall. These photos are looking east on Pike Street.

# Noise



Pike Street Without Noise Wall (looking west)  
71 dBA (average)



Pike Street With Noise Wall (looking west)  
64 dBA (average)

These views are looking west on Pike Street. Again, you can see what the project would look like with and without noise walls.

# Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.

# Enhancement Measures

- Working with Covington to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, and pedestrian paths
- Pedestrians and Bicycles
  - Connections across I-71 and I-75
  - New shared use paths
  - New and rebuilt sidewalks
- Stormwater
  - Separate highway runoff from combined sewer system
  - Reduce overflow event
  - Working with the City of Covington and Sanitation District No. 1

25

In addition to coordinating work in Goebel Park, KYTC is also working with Covington to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.

Existing highway runoff flows in the combined sewer system in Covington, and overflow events negatively affect area residents. KYTC is committed to separating the highway drainage from the combined sewer systems as part of the project. It should be noted that we are not just striving to separate additional runoff from the MS4 systems, but we are working to separate all runoff in in Covington, which will substantially reduce overflow events.





Moving north, note the new shared use path in Goebel Park and under the West 5th Street bridge. The shared use path will be extended along Crescent Avenue to connect to the existing shared use path along the river. Also notice the new and rebuilt sidewalks along West 5th and West 3rd streets. Throughout Covington, the shared use paths and sidewalks will improve connections to existing transit stops.



This is a view of what Goebel Park might look like once the highway improvements are constructed, the new shared use paths are built, and agreed upon landscaping is in place. The project's efforts to separate highway runoff from the existing MS4 system will also reduce the number of overflow events affecting Goebel Park.



This view shows how the finished project might appear when looking across I-71 and I-75 with Westside in the distance.





This view shows what the project might look like at Pike Street. Notice the potential for aesthetic treatments on the bridge walls and piers. Also notice the sidewalks and shared use paths.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)

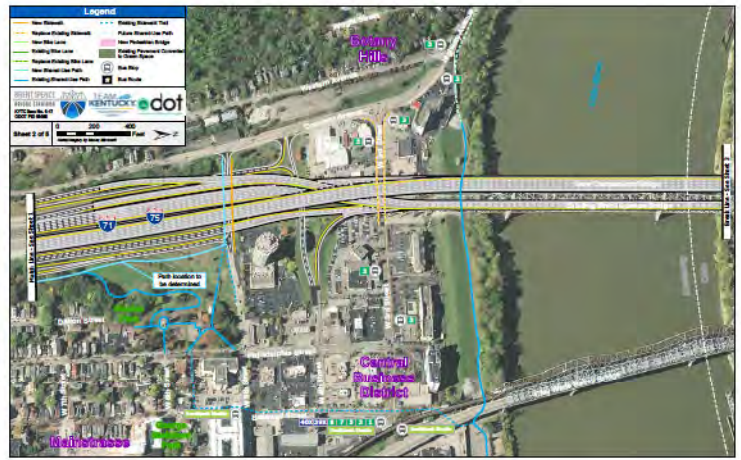
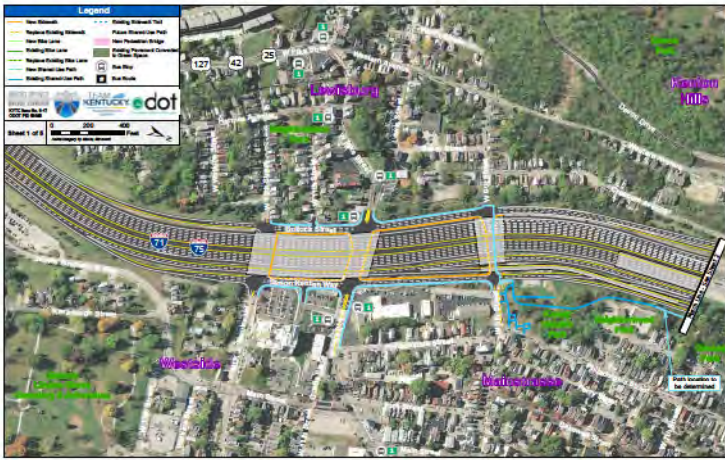


This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.



---

[Attachment 5: Exhibits](#)



Brent Spence Bridge Corridor  
(Looking Northwest from Kentucky)



KYTC Item No. 9-17 | CDOT PID 8888



Brent Spence Bridge Corridor - Kentucky  
(Looking Southeast)



KYTC Item No. 9-17 | CDOT PID 8888



Pike Street With Noise Walls (Looking Northwest) - Kentucky



KYTC Item No. 9-17 | CDOT PID 8888



Pike Street Without Noise Wall (Looking East) - Kentucky



KYTC Item No. 9-17 | CDOT PID 8888



Pike Street With Noise Wall (Looking East) - Kentucky



Goebel Park (Looking North) - Kentucky





---

Attachment 6: Comment Forms

# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

## How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

## What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

## What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518





# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university

Other: Graduate degrees

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1-2    3-5    6+

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

- Keep involving westside residents including residents on planning committees for project

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

TEAM  
KENTUCKY  
TRANSPORTATION  
CABINET

  
transportation.ohio.gov

# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

KYTC Item No. 6-17 | ODOT PID 89068

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://PublicInput.com/bsbc)
  - By mail using address on back





# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

\_\_\_\_\_  
SOUND BARRIER ESSENTIAL  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident    Area business owner or employee    Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068

# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

*I personally like the noise wall -*

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_

(Phone: COV.)

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Great presentation -  
thanks for this opportunity!

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



---

# Brent Spence Bridge Corridor Project City of Fort Mitchell Meeting Summary November 30, 2022

---

## Introduction

The City of Fort Mitchell (COFM) meeting was held on November 30, 2022, from 6:30 pm to 9:00 pm. The meeting was held at the city administrative building located at 2355 Dixie Highway in Fort Mitchell, KY. It was publicized through the COFM Instagram page, COFM Twitter page, and the COFM email list (see Attachment 1). Attendees at the meeting included:

- The City of Fort Mitchell
- Members of the public
- Representatives of Highland Cemetery
- Representatives of DeMaria Dental
- The City of Crescent Springs
- The City of Covington
- Representatives from the Blessed Sacrament Church
- Representatives of ServPro
- The project team

In total, fifty-four (54) people attended the meeting, including eight (8) individuals from the City of Fort Mitchell and the project team (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). After the meeting, the project team responded to questions and comments offered by those present.

Exhibits showing plan views of the project through the COFM and renderings of the proposed bridge were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and a postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Five (5) comment forms and five (5) demographic surveys were returned at the meeting.

## Demographics

The following is a breakdown of the responses received from the demographic survey.



**WHAT IS YOUR RACE OR ETHNICITY?**

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe



**WHAT IS YOUR PRIMARY LANGUAGE SPOKEN IN YOUR HOME?**

- English
- Spanish
- Other (Please specify)



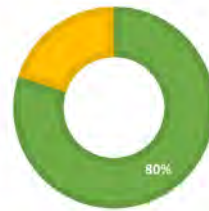
**WAS PROJECT INFORMATION TRANSLATED INTO OTHER LANGUAGES APPROPRIATELY?**

- Yes
- No
- Not Applicable



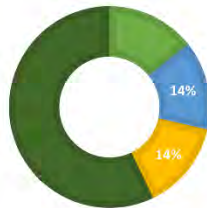
**HOW MANY PEOPLE LIVE IN YOUR HOUSEHOLD?**

- 1-2
- 3-5
- 6+



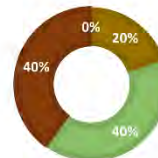
**WHAT ARE THE AGE RANGES OF THOSE LIVING IN YOUR HOUSEHOLD?**

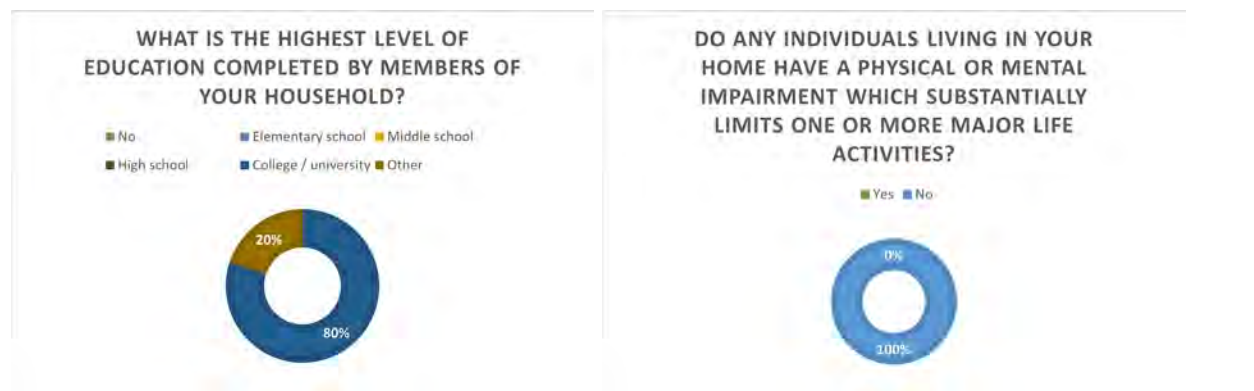
- Under 18
- 19-44
- 45-64
- 65+



**WHAT IS YOUR ANNUAL HOUSEHOLD INCOME?**

- Less than \$10,000
- \$10,000 - \$24,000
- \$25,000 - \$49,000
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000+
- Not Answered





A write-in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. The only comment received was:

- Post Card or letter. We knew nothing about the meeting. A friend told us.

## Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Concern over existing noise levels, clarification / understanding of the process for noise evaluations, and the discussion of impacts with / without noise wall.
- Clarification of timeline and staging of construction.
- Concern over pedestrian access across the interstate (and ramps), particularly for kids wanting to walk to school.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.

Comments noted on the survey forms returned at the meeting include:

- The Ft Mitchel (sic) Point neighborhood is concerned about increased noise from the highway. We are hopeful that some noise abatement structure will be part of the project.
- Need a sound wall.
- Concern regarding [address redacted] on Lewis. Was told property was on the demo list but did not hear back.
- Not everyone is connected to the internet. Perhaps a post card might be helpful.





**Table 1**

Question	Project Team Response
Are the southbound lanes decreasing from 6 to 5 to 4?	Yes, this is to tie down to the existing interstate.
Are you staying in the existing footprint to add the additional lanes?	In general, some amount of property will be required to widen the interstate.
Will the existing bridge be shut down during construction like it has been during previous maintenance work?	When it was worked on previously, there was not a companion bridge. We can't say for certain that it won't be shut down during construction, but we anticipate the companion bridge to be built first, the traffic moved to the new companion bridge, and then the rehabilitation of the existing bridge will take place. KYTC is committed to maintaining traffic across the river throughout construction.
What are the total number of lanes on the bridges?	There will be 5 lanes in each direction on the companion bridge for interstate traffic and the existing bridge will be reduced to 3 lanes in each direction to handle local traffic. The companion bridge will be "stacked" like the existing bridge.
Did you ever consider tunneling?	Yes, but the grade made it undesirable.
Will Kyles Lane have to be reconfigured?	Yes, it will have to be reconstructed. One of the DBT's goals will be to look at cost-saving opportunities to reduce impacts.
Will parts of Fort Mitchell be cut off from the other side during construction?	Maintenance of traffic will be a conversation with the DBT, and will include the local police and other emergency vehicles. Consideration will be made for detour routes, emergency situations, response times, etc.
Did the department consider truck lanes? There are a lot of vehicles cutting off trucks to get to exits.	We have not considered them as part of this project. With the project adding capacity, we hope it won't be as much of a concern.
Is the project widening the interstate south of Dixie Highway?	This is where we're trying to tie down as quick as possible.
How can you add more lanes without cutting into the hill?	We're already at 6 lanes in this section. Some additional right of way will be purchased, but we're staying within the existing footprint as much as possible.
If there's a noise wall, can the noise bounce off and go up and over?	Yes, but things such as wall texture and distance reduce the amount.
Are you taking noise measurements before construction?	Yes. Noise readings were taken this summer.



<p>The noise is already unacceptable. Last time we were told there was no noise abatement but it's gotten worse.</p>	<p>We can't add noise walls for existing roadways, but we reevaluate roadways when there is a new project. What we've done in the past doesn't apply in this situation.</p>
<p>What land does the city own that isn't a part of the Mercedes Benz property? They've cleared off a hillside so people can see the lot, but it's made it louder.</p>	<p>The state owns a portion of the land, but vegetation is not considered a form of noise abatement.</p>
<p>What traffic volumes were used in the noise analysis?</p>	<p>The model is based on the predicted traffic in the year 2050.</p>
<p>So you're saying the wall meets the criteria for sound but that there's not enough people that it affects?</p>	<p>Yes.</p>
<p>Is the Fort Mitchell overpass being rebuilt?</p>	<p>That is currently the plan, but the DBT may come up with an alternative solution.</p>
<p>Was a non-at-grade pedestrian facility considered from Beechwood to West Maple? Is that enhancement possible? There is no bus system in this area, so it is very unsafe for kids walking to school. Most specifically for the southbound on-ramp.</p>	<p>KYTC will investigate that situation more.</p>
<p>What is the cost of the project?</p>	<p>We're looking at north of \$3 billion.</p>
<p>Driving on Dixie Hwy is terrible because of the construction. Any plans for making it better? Sounds like the corridor will be a mess for 5 years.</p>	<p>That will be a part of the Maintenance of Traffic Plan, and KYTC will work on it as we go along. Construction in this area will more likely be 2 construction seasons, as we'll focus on phasing and how to construct in portions to help move traffic.</p>
<p>Whose noise policy do you follow and who sets it?</p>	<p>It is KYTC's policy, and it is set by KYTC in coordination with FHWA.</p>
<p>Has the team considered alternative surface types? There was an increased noise when the surface was changed from asphalt to concrete.</p>	<p>Yes, we are considering it. We have the Kentucky Transportation Center looking into quiet pavement types for this project.</p>
<p>Who does this project serve? Does it help the locals? What about air pollution?</p>	<p>It helps local residents who work in Cincinnati, and it helps reduce congestion in the area. Air pollution will actually be improved since vehicles will be moving instead of idling due to traffic backups.</p>
<p>Any thoughts on diverting truck traffic?</p>	<p>There are a significant number of motor carriers with destinations in this area, so policing the truck traffic to determine who belongs and who doesn't would be a hard task. Also, it's worth noting that this is an interstate roadway and consideration must be taken for the purpose of the interstate.</p>



---

Attachment 1: Advertising Materials



# Upcoming

November 2022

WED  
30

November 30 @ 6:30 pm - 8:30 pm

## Brent Spence Bridge Corridor Project Meeting

Fort Mitchell City Building (Council Chamber)  
2355 Dixie Highway, Fort Mitchell, KY

Brent Spence Bridge Corridor Project Meeting 11-30-22



December 2022

THU  
1

December 1 @ 5:30 pm - 6:30 pm

## LiveWell Fort Mitchell Produce Popup

Fort Mitchell City Building (Parking Lot) 2355 Dixie Highway, Fort Mitchell, KY 41017, Fort Mitchell, KY



SUN  
4

December 4 @ 1:00 pm - 5:00 pm

## Holly Jolly Days (Fort Mitchell Day)



Privacy - Terms

**Seth Turner**

---

**From:** [REDACTED]  
**Sent:** Tuesday, November 29, 2022 9:24 AM  
**To:** [REDACTED]  
**Subject:** Fw: Brent Spence Bridge Meeting

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

[Sent from Yahoo Mail for iPhone](#)

Begin forwarded message:

On Wednesday, November 2, 2022, 10:41 AM, City of Fort Mitchell <[news@fortmitchell.com](mailto:news@fortmitchell.com)> wrote:

No images? [Click here](#)



**Brent Spence Bridge Meeting**

The [Ohio Department of Transportation](#) (ODOT) and [Kentucky Transportation Cabinet](#) (KYTC) will hold a neighborhood outreach meeting for the Brent Spence Bridge (BSB) Corridor Project on Wednesday, November 30 at 6:30pm at the Fort Mitchell City Building, 2355 Dixie Highway.

The purpose of the public meeting is to share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team. For more information, go to

[https://link.edgepilot.com/s/65f0be72/bHoDe\\_5fB0aWFtnQwN4n2Q?u=https://brentspencebridgecorridor.com/](https://link.edgepilot.com/s/65f0be72/bHoDe_5fB0aWFtnQwN4n2Q?u=https://brentspencebridgecorridor.com/).



## You're Invited

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of **neighborhood outreach meetings** for the Brent Spence Bridge (BSB) Corridor Project

**Purpose:** To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team

**When:** November 30, 2022 | 6:30pm

**Where:** Fort Mitchell City Building (Council Chamber)  
2355 Dixie Highway, Fort Mitchell, KY 41017

## Hope to see you there!

If you need assistance attending this meeting, please call Stacey Hans at [\(859\) 341-2700](tel:8593412700).

View the most recent project newsletter at the following link:  
<https://mailchi.mp/aae28ef3744d/brent-spence-bridge-e-newsletter-16606655>

ODOT PID 89068 | KYTC Project Item No. 6-17

**City Communication**



\* Go to our Website - [fortmitchell.com](https://fortmitchell.com)

\* Like us on Facebook - [@fortmitchell](https://www.facebook.com/fortmitchell)

\* Follow us on Instagram - [fortmitchellgov](https://www.instagram.com/fortmitchellgov)

\* Follow us on Twitter - [fortmitchellgov](https://twitter.com/fortmitchellgov)

\* Watch us on YouTube - [Fort Mitchell](https://www.youtube.com/Fort Mitchell)

\* Subscribe to our [E-Newsletter](#)

#### **Next City Council Meeting:**

**Monday, November 14, 2022**

**6:30pm**

**City Building (Council Chamber)**

**2355 Dixie Highway, Fort Mitchell, KY 41017**

**For more information, visit the City of Fort Mitchell [website](#).**

Meetings provide an opportunity to bring comments, suggestions, and concerns before City Council. Agenda, [future scheduled meetings](#), and past minutes are available on the City's [website](#). Televised meetings can be viewed through [TBNK](#).



City of Fort Mitchell  
2355 Dixie Highway  
Fort Mitchell, KY 41017  
[fortmitchell.com](http://fortmitchell.com)

Monday - Friday, 8:00am - 4:30pm

City Office: 859.331.1212  
Fire Administration: 859.331.1267  
Police Administration: 859.331.2823  
24/7 Non-Emergency: 859.356.3191  
Public Works Administration: 859.331.1212

[Unsubscribe](#)

Links contained in this email have been replaced. If you click on a link in the email above, the link will be analyzed for known threats. If a known threat is found, you will not be able to proceed to the destination. If suspicious content is detected, you will see a warning.



## Seth Turner

---

**From:** [REDACTED]  
**Sent:** Monday, October 31, 2022 9:52 AM  
**To:** Sherry Kish  
**Cc:** [REDACTED]  
**Subject:** RE: Brent Spence Bridge Presentation on 11/30/22

Hi Sherry,

Please see the X'ed boxes below. We will get to work on it.

Thanks,

Edwin



**Edwin King**

City Administrative Officer  
City of Fort Mitchell

[REDACTED]

2355 Dixie Highway  
Fort Mitchell, KY 41017

[www.fortmitchell.com](http://www.fortmitchell.com)



This message (and any attachments) is intended solely for the use of the named individual(s) to whom it is directed. This message may contain information that is confidential and/or is exempt from disclosure under applicable law. If you are not the intended recipient, you are hereby notified that any dissemination, distribution or duplication of this transmission, or files attached to this transmission, is strictly prohibited. If you have received this message in error, please immediately notify the sender and delete this message and all attachments from your computer.

---

**From:** [REDACTED]  
**Sent:** Friday, October 28, 2022 4:02 PM  
**To:** [REDACTED]  
**Cc:** [REDACTED]  
**Subject:** Brent Spence Bridge Presentation on 11/30/22

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Edwin,

Thank you so much for working with us to get a meeting scheduled to allow the Brent Spence Bridge project team to talk with the City of Fort Mitchell residents on November 30<sup>th</sup>. As you know, the project team wants to share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

Our goal is to be sure all notifications about the meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous city/neighborhood meeting?	X	
Email list	Can you email a neighborhood wide or citywide email list with information about BSB presentation at the upcoming meeting?	X (We can send to our newsletter distribution list)	
Facebook (private to neighborhood/city)	Can you share a Facebook message about BSB presentation?	X	
Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?	X	
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?	X	
Other	Is there another outlet you have available to get the word out?		
Do you need assistance from us with advertising?	We made a flyer for your consideration (attached to this email).	Thanks for the Flyer	

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!  
Sherry

**Sherry R. Kish**



**HNTB CORPORATION**

88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

**100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

# Instagram

Log In Sign Up



## You're Invited

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of **neighborhood outreach meetings** for the Brent Spence Bridge (BSB) Corridor Project

**Purpose:** To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team

**When:** November 30, 2022 | 6:30pm

**Where:** Fort Mitchell City Building (Council Chamber)  
2355 Dixie Highway, Fort Mitchell, KY 41017

### Hope to see you there!

If you need assistance attending this meeting, please call Stacey Hans at [\(859\) 341-2700](tel:8593412700).

View the most recent project newsletter at the following link:  
<https://mailchi.mp/aae28ef3744d/brent-spence-bridge-e-newsletter-16506655>

ODOT PID 89068 | KYTC Project Item No. 6-17



**fortmitchellgov** • Follow

Fort Mitchell, Kentucky



**fortmitchellgov** The Ohio Department of Transportation (ODOT) and Kentucky Transportation Cabinet (KYTC) will hold a neighborhood outreach meeting for the Brent Spence Bridge (BSB) Corridor Project on Wednesday, November 30 at 6:30pm at the Fort Mitchell City Building, 2355 Dixie Highway.

The purpose of the public meeting is to share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with



5 likes

1 DAY AGO

Log in to like or comment.

### More posts from fortmitchellgov



Log into Instagram

Log in to see photos and videos from friends and discover other accounts you'll love.



# Explore

Settings

← Tweet



City of Fort Mitchell  
@fortmitchellgov



The @ODOT\_Statewide and @KYTC will hold a neighborhood outreach meeting for the Brent Spence Bridge Corridor Project tonight (Wednesday, November 30) at 6:30pm at the Fort Mitchell City Building, 2355 Dixie Highway. For more information, go to [brentspencebridgecorridor.com](https://brentspencebridgecorridor.com).



## You're Invited

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of **neighborhood outreach meetings** for the Brent Spence Bridge (BSB) Corridor Project.

**Purpose:** To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team.

**When:** November 30, 2022 | 6:30pm

**Where:** Fort Mitchell City Building (Council Chamber)  
2355 Dixie Highway, Fort Mitchell, KY 41017

**Hope to see you there!**



---

Attachment 2: Sign-In Sheets



# BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – November 30, 2022

Name	Representing	Address	Phone	Email
1. <u>Jim Danneman</u>	<u>Ft Mitchell Property Owner</u>			
2. <u>PETER + IRENE HESS</u>	<u>FT MITCHELL PROPERTY</u>			
3. <u><del>James D.</del></u>	<u>_____</u>			
4. <u>James D. Waincott</u>	<u>Ft. Mitchell Property</u>			
5. <u>Man Cuy</u>	<u>" " "</u>			
6. <u>Jamie and Michelle Keller</u>	<u>" " "</u>			
7. <u>Kevin Dicke</u>	<u>Ft. Mitchell Resident</u>			
8. <u>Bryson Law</u>	<u>Ft Mitchell resident</u>			
9. <u>Dick Bruns</u>	<u>Ft Mitchell Resident</u>			
10. <u>Tom Howebrock</u>	<u>Highland Cemetery</u>			
11. <u>Amy &amp; Mark Higgins</u>	<u>home owner</u>			
12. <u>Wayne Tarr + Ke</u>	<u>_____</u>			
13. <u>Chris + Amy Derry</u>	<u>Homeowner</u>			
14. <u>Michael Schoech</u>	<u>Ft. Mitchell Resid</u>			
15. <u>Martha + Michael Murphy</u>	<u>(Ft. M. Property</u>			
16. <u>Carrie + Mick Schoech</u>	<u>Ft. Mitchell,</u>			
17. <u>Kim Moore</u>	<u>Homeowner</u>			
18. <u>Denise Halpin</u>	<u>134 W. Maple Ave. Ft. Mitchell</u>			
19. <u>Mo Halpin</u>	<u>Homeowner</u>			
20. <u>Dr <del>A</del> DAVID P DENMARI</u>	<u>110 W. Maple Ave</u>			
	<u>"</u>			
	<u>De Maria Dental</u>			

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



# BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – November 30, 2022

Name	Representing	Address	Phone	Email
1. Jerry Deatherage	City of Ft Mitchell			
2. Mary Barks	self			
3. Roger & Martha Kuehle	self / property owner			
4. Mike Dely	City of Crescent Springs			
5. Carol Rector	Blessed Sacrament Church			
6. L. Roger Cole	City of Ft. Mitchell			
7. SUSAN M. JOHNSON	SELF/PROP OWNER			
8. MICHAEL Stoeckle	City of Ft Mitchell			
9. Ed O'Connell				
10. Bill Reis	SELF			
11. Carol Beirne	self			
12. Joe McCafferty	self			
13. MICK & LOU ANN HOLTZLEIFER	SELF and HOA			
14. Patty Kaiser	self			
15. Nolan Nicaise	City of Covington			
16. Matt & Kim Schamer	self			
17. Kevin Kunkle	Self			
18. Dwight Seidel	self			
19. Joe Hartbe	Serupro			
20. Sodi Heflin	HNTB			
Mark Becherer	HNTB			

Please print clearly • Contact information is optional but must be included if you wish to receive updates.

# BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 1, 2022

Name	Representing	Address	Phone	Email
1. <u>Scott Scholtz</u>	_____	_____	_____	_____
2. <u>Michael Leathers</u>	<u>HMB</u>	_____	_____	_____
3. <u>Stacey Hans</u>	<u>KYTC</u>	_____	_____	_____
4. <u>Gary Valentine</u>	<u>KYTC</u>	_____	_____	_____
5. _____	_____	_____	_____	_____
6. _____	_____	_____	_____	_____
7. _____	_____	_____	_____	_____
8. _____	_____	_____	_____	_____
9. _____	_____	_____	_____	_____
10. _____	_____	_____	_____	_____
11. _____	_____	_____	_____	_____
12. _____	_____	_____	_____	_____
13. _____	_____	_____	_____	_____
14. _____	_____	_____	_____	_____
15. _____	_____	_____	_____	_____
16. _____	_____	_____	_____	_____
17. _____	_____	_____	_____	_____
18. _____	_____	_____	_____	_____
19. _____	_____	_____	_____	_____
20. _____	_____	_____	_____	_____

Please print clearly • Contact information is optional but must be included if you wish to receive updates.





---

Attachment 3: Photographs



### Attachment 3: Photographs





---

Attachment 4: Presentation

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



**Fort Mitchell Meeting (KY)**  
November 30, 2022



INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with introductions of the Project Team in attendance (2 min).

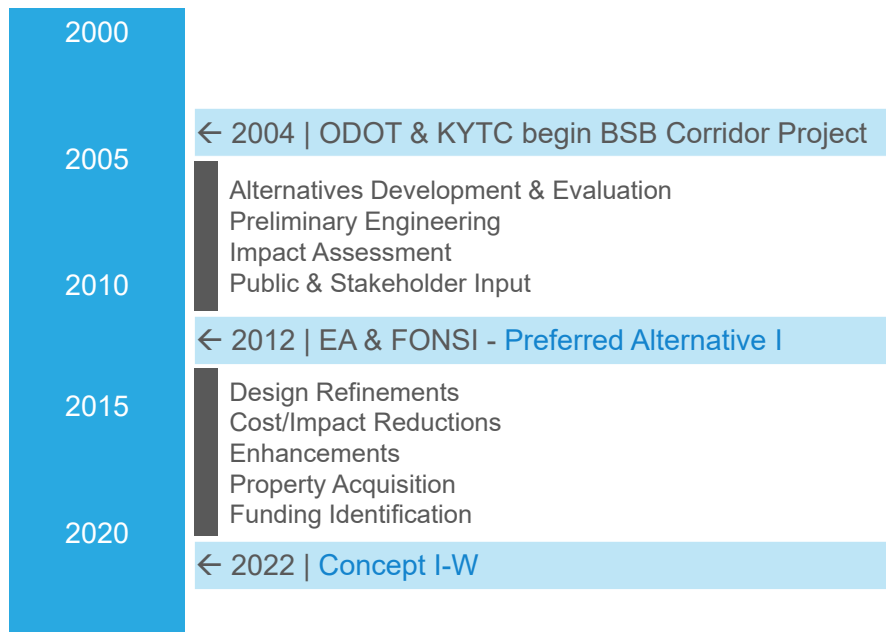
# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in Fort Mitchell the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Fort Mitchell area
  - Discussion/feedback from Fort Mitchell residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near Fort Mitchell. Most importantly, tonight we're focusing on hearing from the residents of Fort Mitchell. We're here to answer your questions and to listen to your feedback about the project.

# Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, KYTC has begun purchasing the land needed to build the project.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

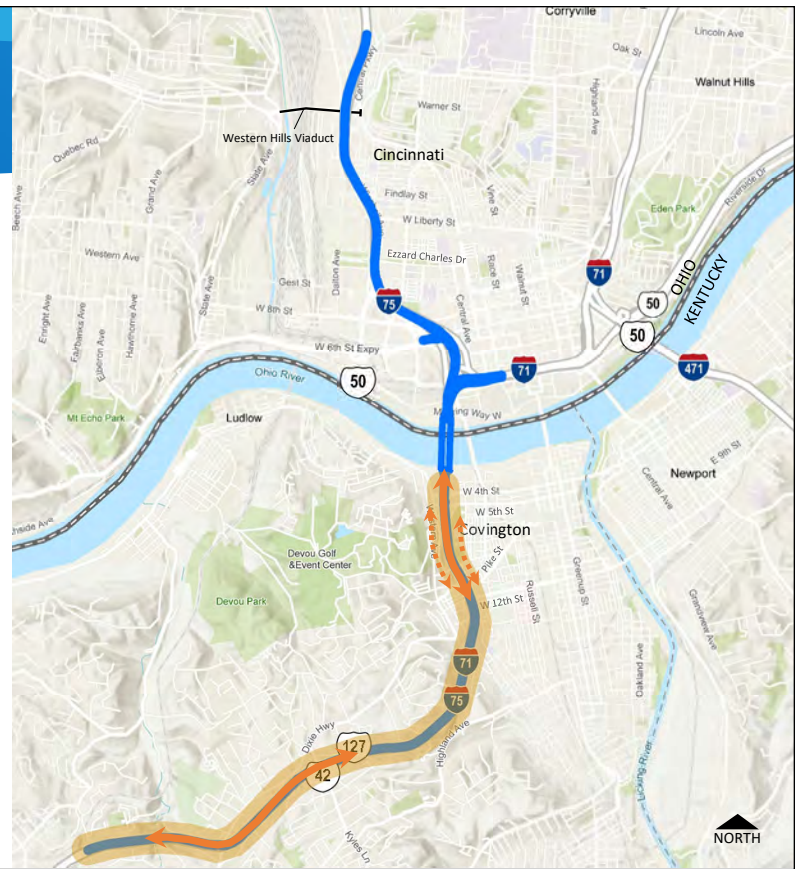
The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.



# Project Description

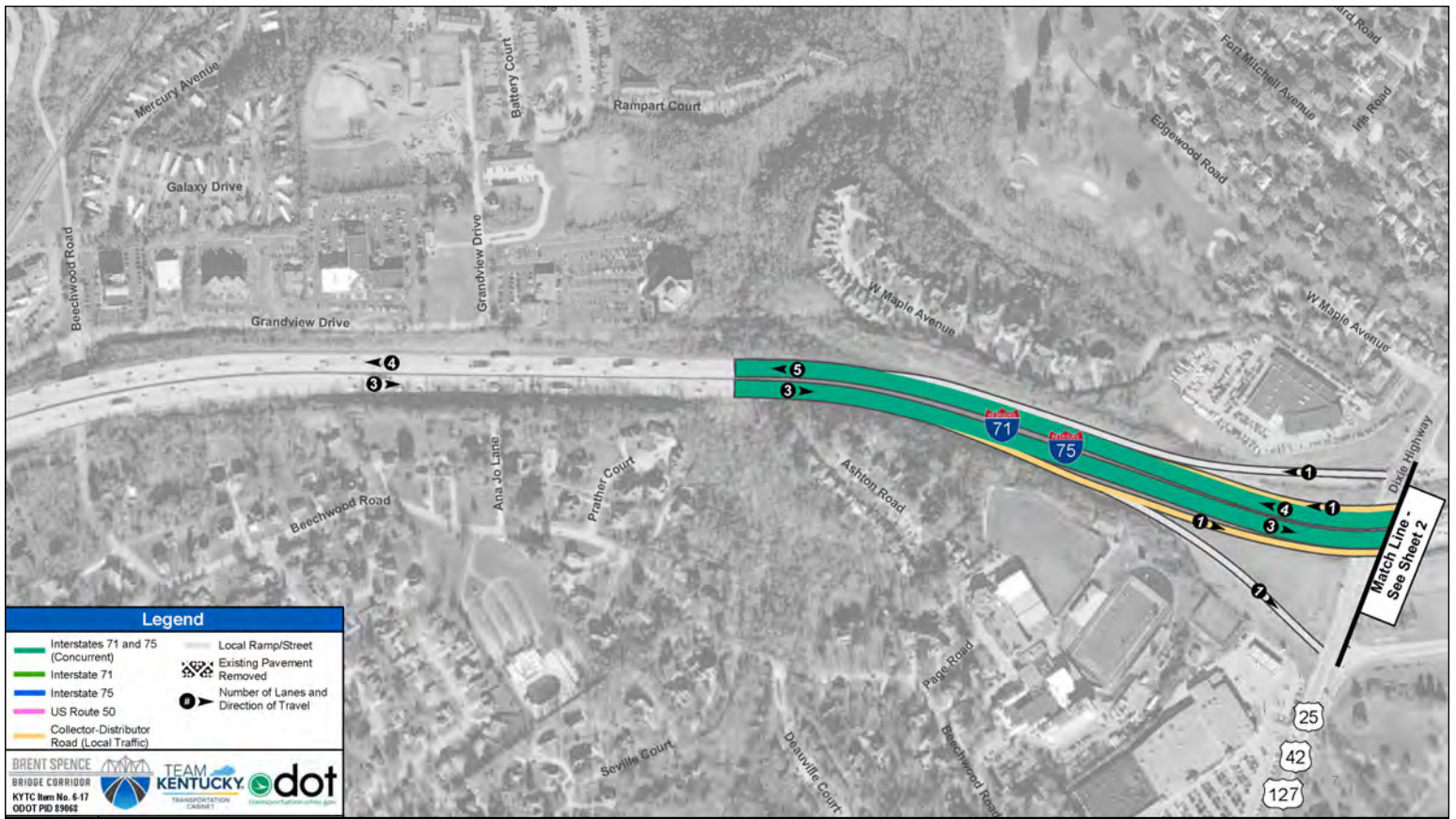
## Kentucky

- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.



This slide shows how traffic will move through Fort Mitchell. The green shows through traffic on the interstate. As shown in the orange, a collector distributor road will start just south of Dixie Highway. The purpose of the collector-distributor road is to reduce the number of places where traffic is entering and exiting the freeway to reduce the number of times vehicles weave in and out of the through travel lanes to access local roads. As shown here, in the northbound direction, traffic traveling to Dixie Highway and Kyles Lane will exit onto the collector-distributor road south of Dixie Highway. Vehicles traveling to Dixie Highway will then leave the collector-distributor road and take a ramp to Dixie Highway (shown in grey). Traffic traveling to Kyles Lane will continue north on the collector-distributor road.



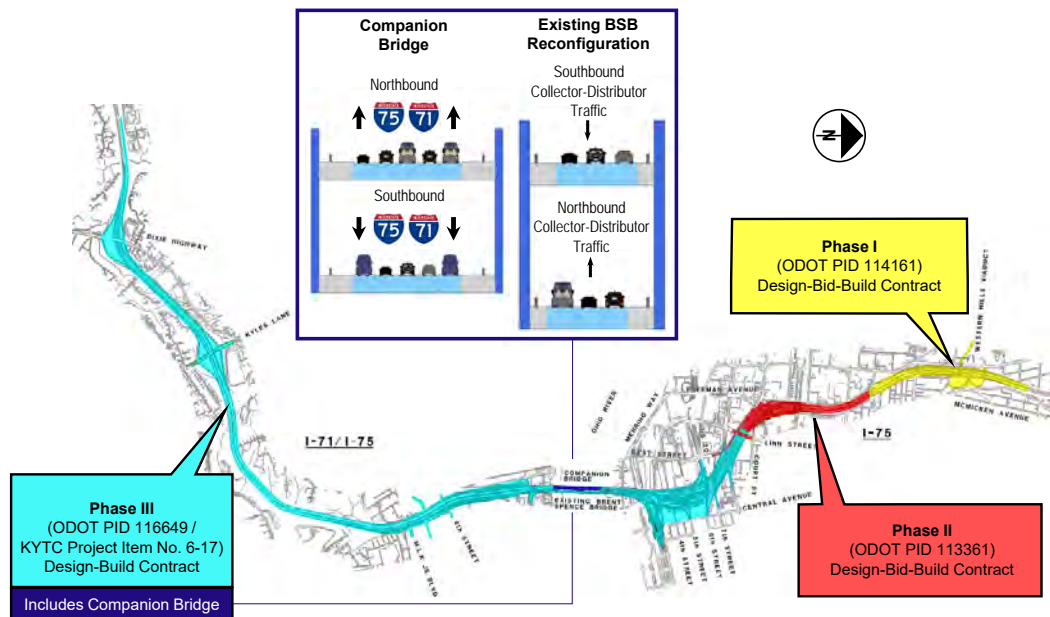
Moving further north, traffic from Dixie Highway will travel down a ramp to enter the northbound collector-distributor road. Next, traffic traveling to Kyles Lane will leave the collector-distributor road and use a ramp to reach Kyles Lane. Northbound traffic will travel through on the collector-distributor road, and more traffic will enter from Kyles Lane.



Finally, the traffic on the collector-distributor road will join with traffic from the Kyles Lane ramp and enter the interstate going northbound. Notice that there will be only one northbound exit and one entrance between Dixie Highway and Kyles Lane instead of one exit and one entrance for each road. Consolidating these access points will improve traffic flow and reduce high-speed crashes on the interstate.

The traffic movements we just walked through will be reversed in the southbound direction, providing the same access scheme and improvements in traffic flow and safety.

# Project Description

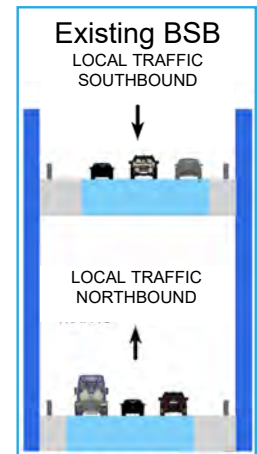
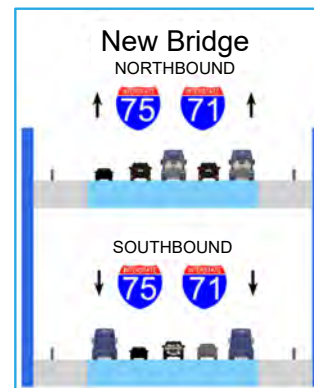


10

The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

# What Has Changed?

- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



11

As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

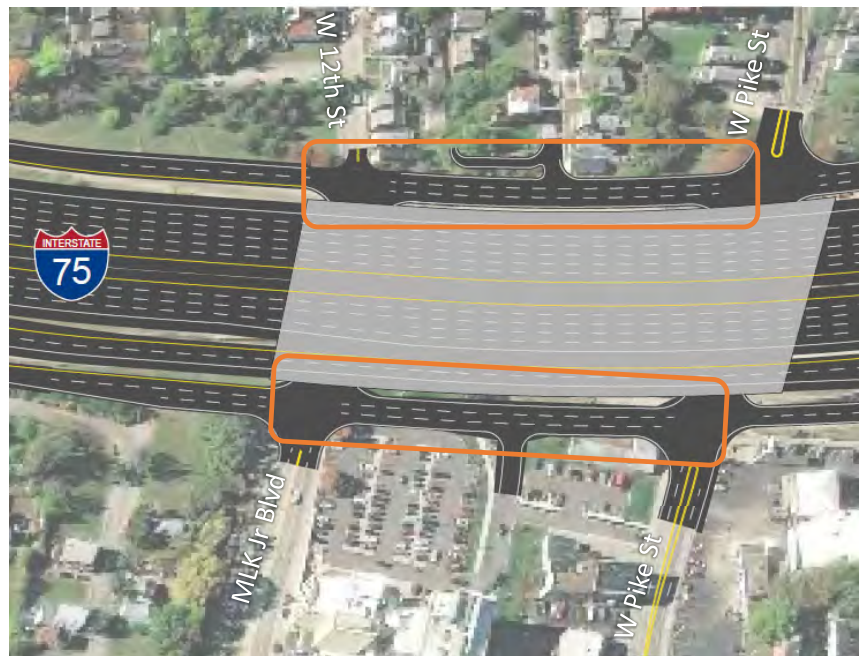


Here's another view from Covington.



# What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 40+ then vs. 4 now
  - Commercial | 6 then vs. 5 now

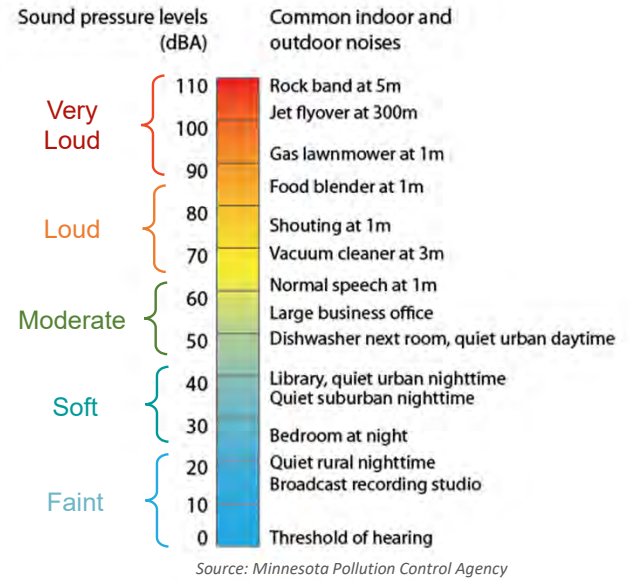


Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet. In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street in Covington. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound



15

Now let's dig into how the project will impact Fort Mitchell, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

- KYTC Noise Policy
- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.

Kentucky Transportation Cabinet  
Department of Highways  
Division of Environmental Analysis



## NOISE ANALYSIS AND ABATEMENT POLICY

APPROVED BY:

Todd Jeter, Division Administrator  
Federal Highway Administration

Date

James Gray, Secretary  
Kentucky Transportation Cabinet

Date

Revised August 1, 2022  
**EFFECTIVE DATE JULY 1, 2020**  
Revised July 1, 2016  
Original July 2011

16

KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dbA. We also determine if there is a substantial increase – specifically a 10 dbA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, “Can it effectively block noise?” In Kentucky, that means determining if a barrier can provide a minimum 5 dbA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

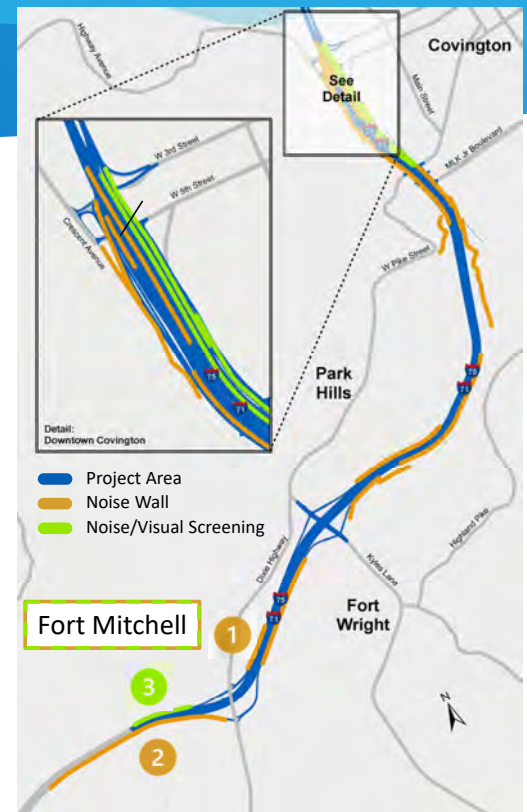
# Noise

Noise walls reasonable/feasible per KYTC Noise Policy

- 1 • North of Dixie Highway
  - Without noise wall = 69 dBA
  - With noise wall = 61 dBA
- 2 • South of Dixie Highway (East of I-71/75)
  - Without noise wall = 66 dBA
  - With noise wall = 59 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office

Note: Sound levels represent the average for the area benefitted by the wall.



In Fort Mitchell, two noise walls were found to be reasonable and feasible. The first wall (shown by the number 1) is located north of Dixie Highway and west of the Interstate. The average exterior noise levels are predicted to be around 69 decibels without a noise wall and 61 decibels with a noise wall. These sound levels are averages for all the people who will benefit from the walls. The sound levels will vary depending on where each receptor is located.

The second wall (shown by the number 2) is located south of Dixie Highway and east of the Interstate. The average exterior noise levels are predicted to be around 66 decibels without a noise wall and 59 decibels with a noise wall.

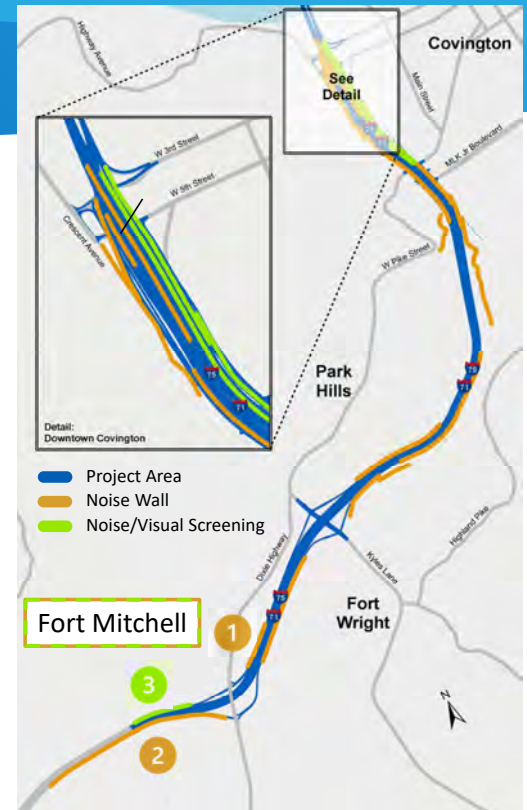
For reference, the sound levels with these noise walls will be slightly quieter than normal conversation.

# Noise

Noise wall not reasonable/feasible per KYTC Noise Policy

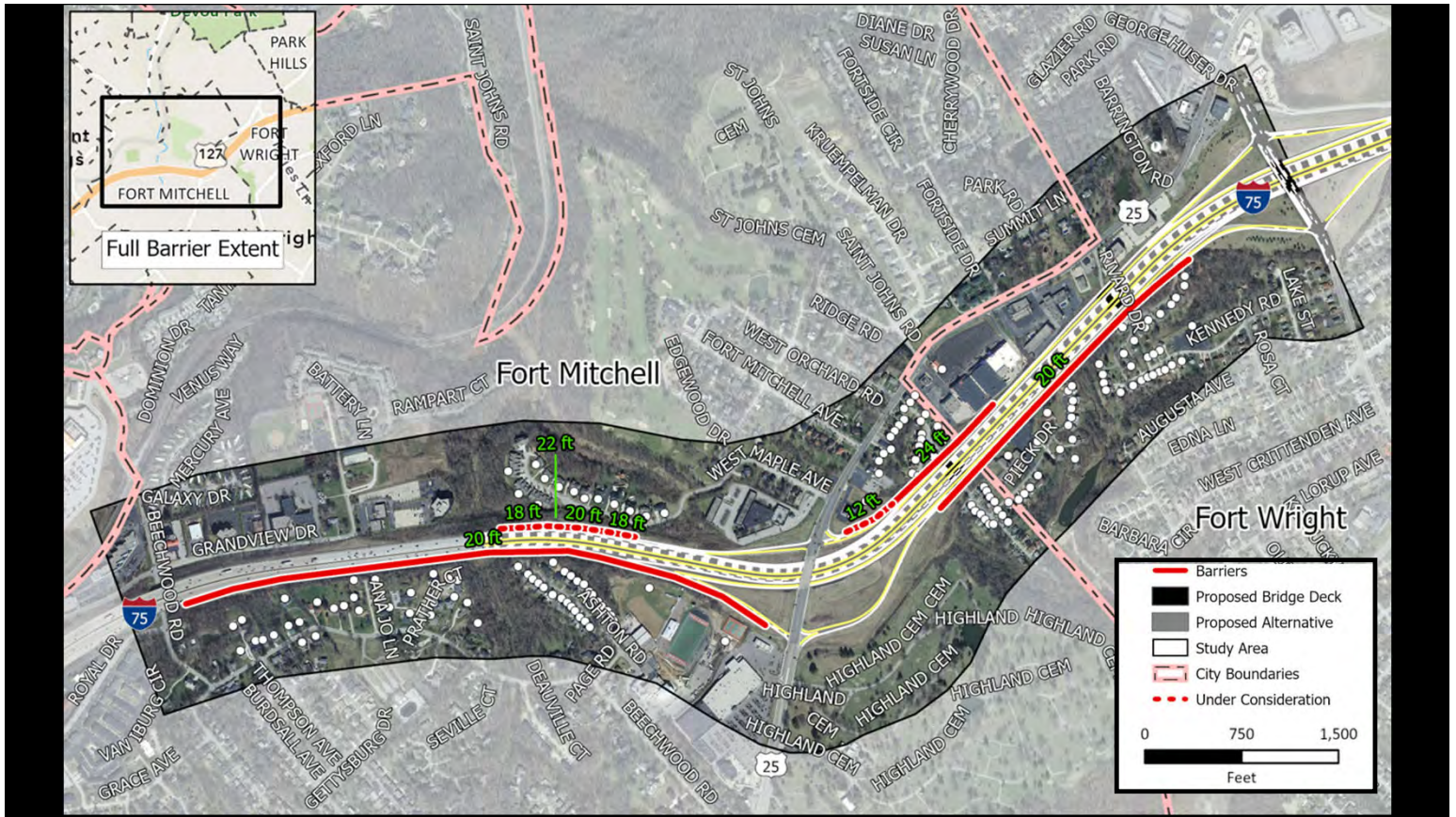
- 3 • South of Dixie Highway (West of I-71/I-75)
  - Noise wall is feasible and meets design goals
  - Noise wall is not cost effective
  - Without wall = 70 dBA
  - With wall = 64 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office

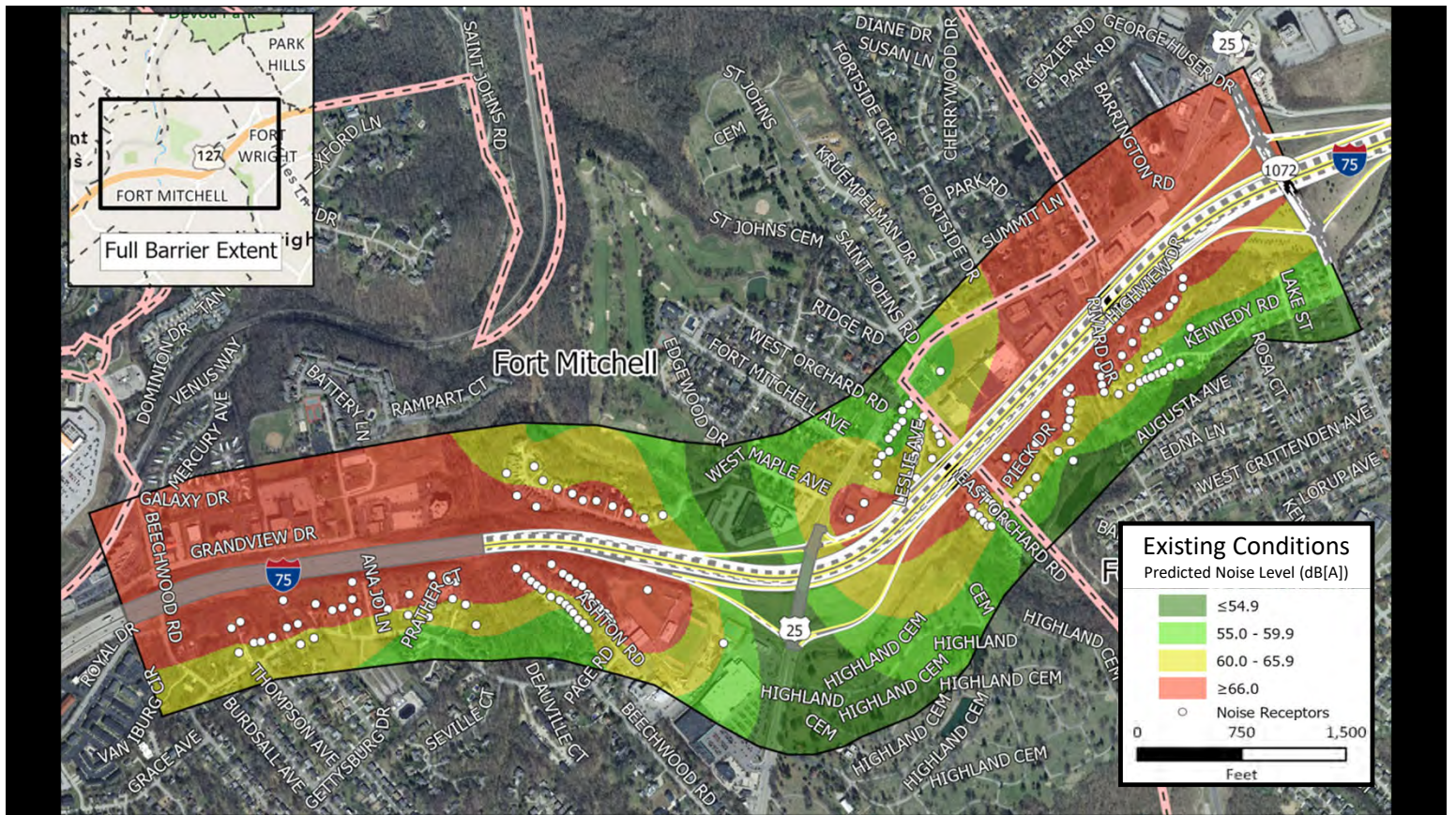


Note: Sound levels represent the average for the area benefitted by the wall.

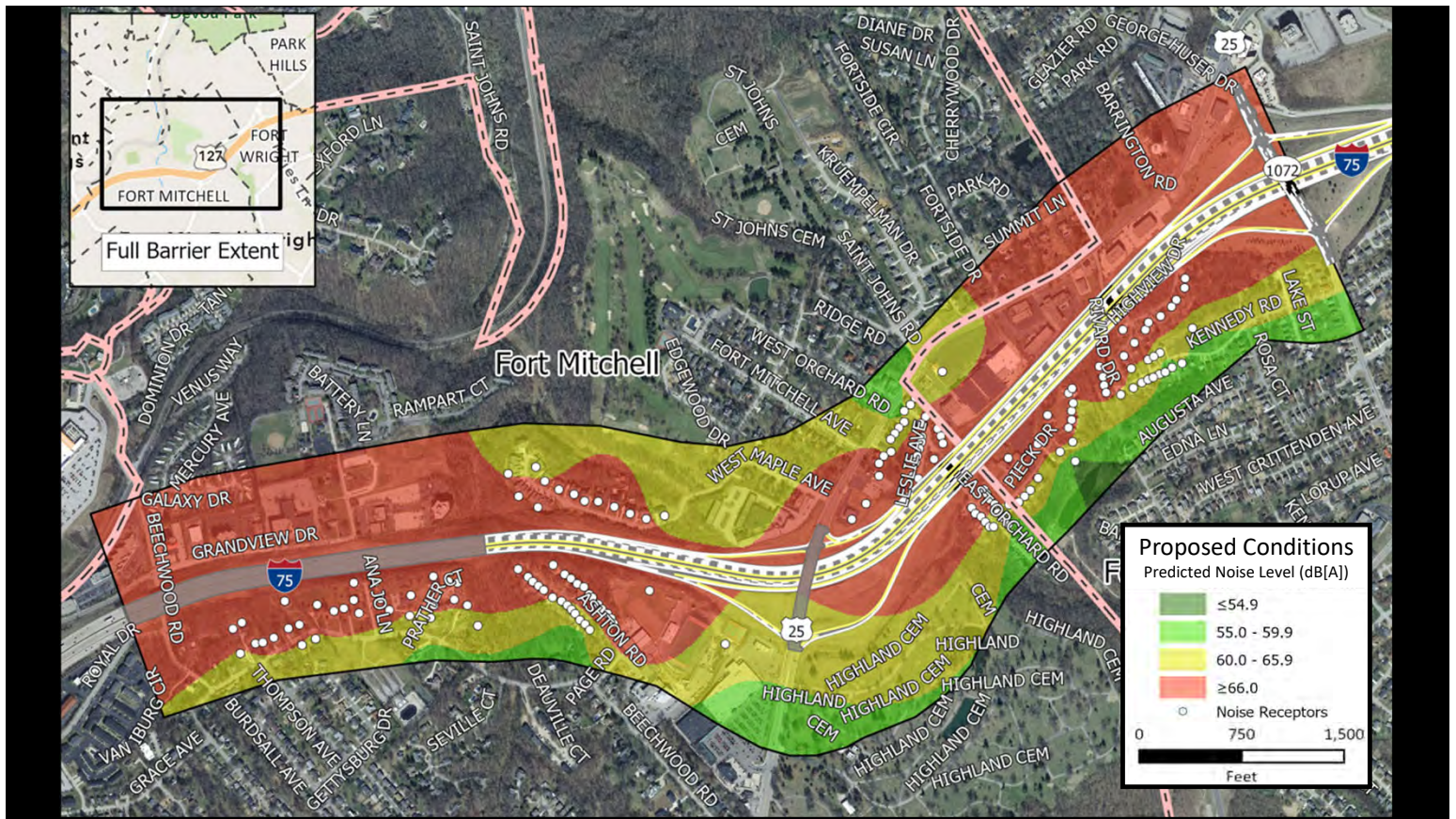
In Fort Mitchell, another noise wall south of Dixie Highway and west of the Interstate (shown by the number 3) was found to be feasible and meet design goals. However, it was not found to be reasonable because it was not cost effective. Recognizing that traffic noise is a concern, KYTC is currently conducting a technical study to further evaluate a system of noise/visual screening walls to help provide some noise reduction for residences in Fort Mitchell and to shield views of the highway. KYTC will document their decision about this noise wall system in a technical memo based on the technical feasibility and comments received during this meeting and other outreach activities.



The red lines on this slide show noise walls that were evaluated as part of the noise study for northern Peaseburg. The green numbers show the heights of the different sections of the noise walls.

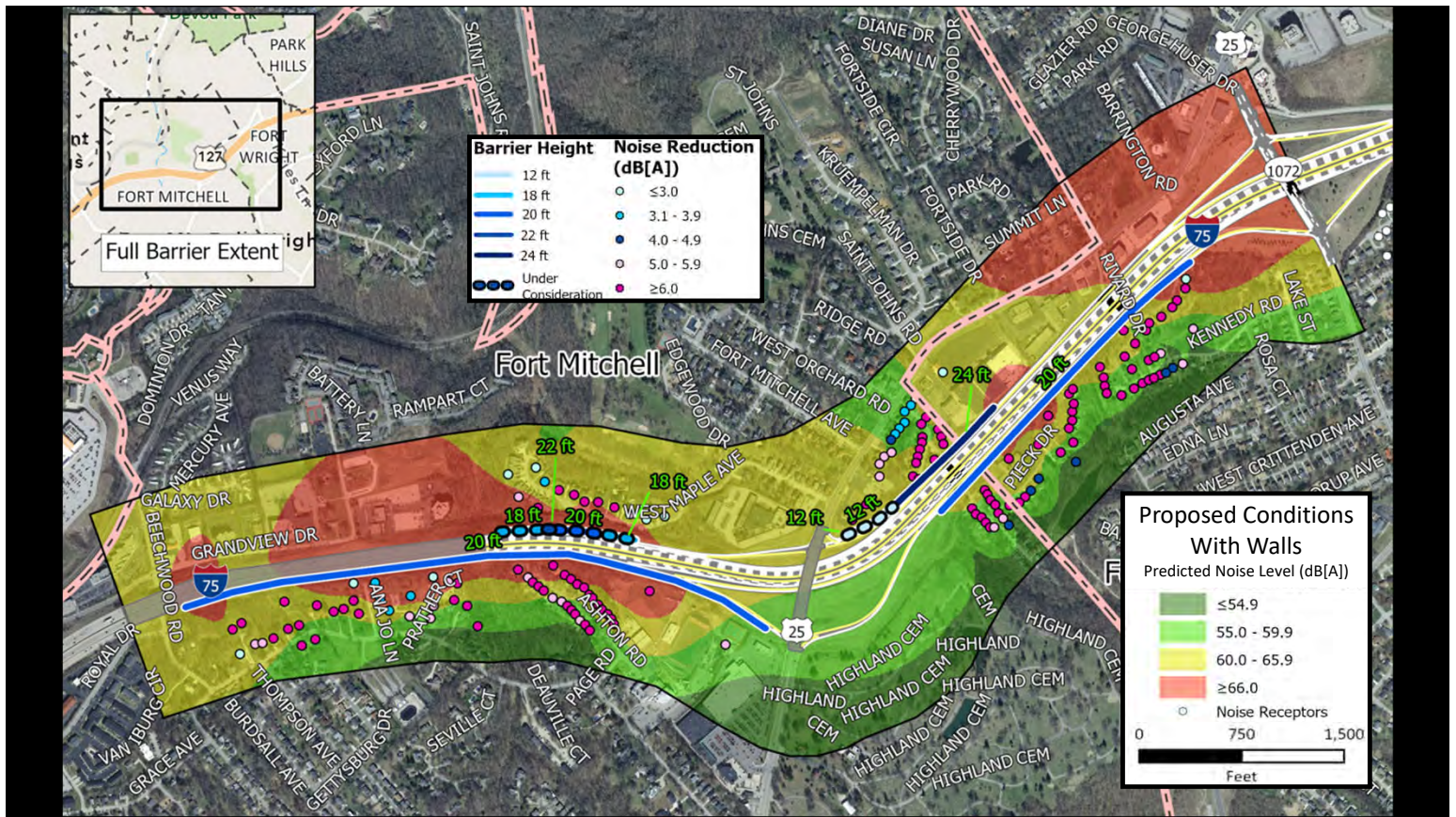


This slide highlights the receptors evaluated as part of the noise analysis for Fort Mitchell. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.





This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a noise wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

# Enhancement Measures

- Working with Fort Mitchell to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Treatments for piers, abutments, retaining walls, noise walls, and pedestrian paths
  - Gateways

23

KYTC is working with Fort Mitchell to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, noise walls, and pedestrian paths. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction.

# Cohesive Interstate Treatments



This slide shows some of the potential locations of the enhancements discussed on the previous slide as it relates to Fort Mitchell. While it is an opportunity for a unique gateway at the Dixie Highway interchange, it is important that those treatments do not conflict with those selected for the Kyles Lane interchange.

# Gateway Opportunities



Existing Community Signage



Planted Median

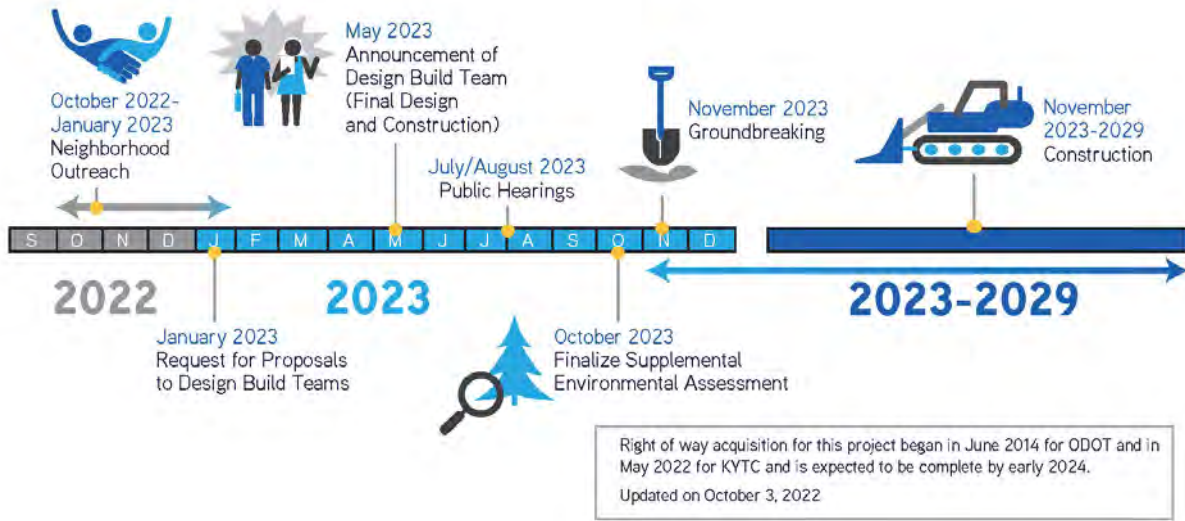


The City of Fort Mitchell provided the project team with information from a 2021 Interchange Study, which became the baseline for enhancements and gateway opportunities. Examples include neighborhood signage and planted medians.



This view shows how the finished project might appear when looking north on I-71 and I-75 at Dixie Highway. Notice the noise walls on both sides, including near Beechwood School. Also notice the collector-distributor system separated by a traffic barrier. Also notice the opportunities for ramp buffer areas and landscaping in open areas for unique gateways. Finally, note the proximity of Kyles Lane to the north and the need for aesthetic treatments that work together to unify the corridor.

# Schedule



This slide shows the project schedule from today to the completion of construction.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.



---

[Attachment 5: Exhibits](#)





Brent Spence Bridge Corridor  
(Looking Northwest from Kentucky)



Brent Spence Bridge Corridor - Kentucky  
(Looking Southeast)





---

[Attachment 6: Comment Forms](#)

# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

THE FT MITCHEL POINT NEIGHBORHOOD  
IS CONCERNED ABOUT INCREASED NOISE  
FROM THE HIGHWAY  
WE ARE HOPEFUL THAT SOME NOISE  
ABATEMENT STRUCTURE WILL BE PART OF  
THE PROJECTS

COMMENTS DUE BY  
January 5, 2023

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

## How many people live in your household?

- 1 - 2     3 - 5     6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

## What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

## What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: [REDACTED]

Mailing address (or nearest cross streets):  
[REDACTED]

Email address: [REDACTED] Phone: [REDACTED]

Business/Organization Name: self

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

\_\_\_\_\_  
Need a sound wall  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

COMMENTS DUE BY  
January 5, 2023

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# COMMENT FORM

BRENT SPENCE  
BRIDGE CORRIDOR



Contact information is not required but will ensure you receive a response, should one be required.

Name: [REDACTED] \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: [REDACTED] Phone: [REDACTED]

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

*Concern regarding [REDACTED]*

*Was told property was on the demo list, but did not hear back.*

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068





# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

**What is your interest in the proposed project? (Select all that apply.)**

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

**General Comments (Please attach additional pages, if needed):**

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

**COMMENTS DUE BY  
January 5, 2023**

**Comments may be submitted:**

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
- By mail using address on back



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: Graduate

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident    Area <sup>property</sup> business owner or employee    Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Not everyone is connected to the internet. Perhaps a post card might be helpful.

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

COMMENTS DUE BY  
January 5, 2023

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

### How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

### What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

### What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: Bachelor Degrees

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

[Redacted] we knew nothing about the meeting.  
A friend told us

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518





---

# Brent Spence Bridge Corridor Project Fort Wright Neighborhood Meeting Summary December 1, 2022

---

## Introduction

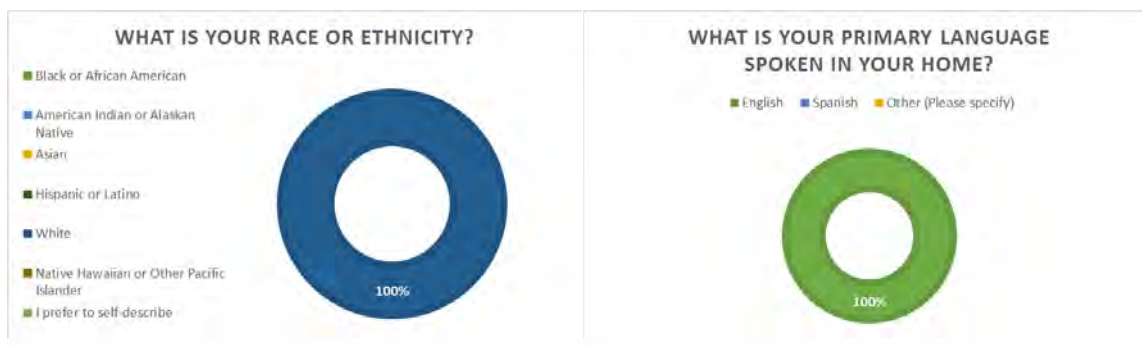
The Fort Wright Neighborhood Meeting was held on December 1, 2022 from 7:00 pm to 8:00 pm. The meeting was held at the South Hills Civic Club located at 10 Bluegrass Avenue in Fort Wright, Kentucky. It was publicized through Fort Wright's Facebook page, email list, and website. In addition, information about the meeting was placed on a digital display sign in front of City Hall (see Attachment 1). Attendees at the meeting included the City of Fort Wright, the project team, and members of the public. In total, 57 people attended the meeting, in addition to members of the project team (see Attachment 2). Two news / media outlets and Fort Wright Police were also present. Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). Following the presentation, the project team responded to questions and comments offered by those present.

Exhibits showing project renderings and lane configurations were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Twelve (12) comment forms and twenty-one (21) demographic surveys were returned at the meeting.

## Demographics

The following is a breakdown of the responses received from the demographic survey.





A write in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. Comments included:

- Send us an e-mail once a month on status and definitive details.



---

## Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Clarification / understanding of local versus through traffic and ramp connections.
- Discussion of impacts of construction, staging, duration, and value of homes during construction period.
- Clarification / understanding of the process for noise evaluation and discussion on impacts with / without noise wall.
- Routing of truck traffic and sound / noise associated with truck traffic.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. The project team will send responses to individuals who provided contact information.

Comments noted on the survey forms returned at the meeting include:

- Please keep public informed of progress.
- Our concern is the impact on our home – value of our home. This will hurt us.
- Heat maps were very effective for noise impacts.
- This will reduce the value of my home because of the traffic nightmares and construction until at least 2029. This does not appear to be fair to Ft. Wright residents.
- I am concerned about the disruption to businesses and residences with this project. I'm also concerned about the merging on and off of the interstates – e.g., where the split occurs – N or S – what kind of signage / electronic device will be in place so that it is clear what road to take? Can cars get off the corridor bridge to go into downtown Cincinnati or Covington? Decrease in sound for those who live close by?



**Table 1: Q & A from Session**

Question	Project Team Response
Impacts – Cork and Bottle? Urban Thelma's?	The project is not even close to impacting these businesses. KYTC has narrowed the footprint as much as possible. Currently, there is not even an impact to Lewis Street.
Does the wall go all the way down the hill to Covington? So all houses in Fort Wright will have a wall?	Yes – an upcoming slide shows that.
Will the project reconstruct the overpasses?	Yes. The project will reconstruct overpasses on Kyles Lane and Dixie Highway. There will be opportunities for community input on what the interchange areas and overpasses will look like.
Between Cincinnati and Kyles Lane, will the road change much? Will the highway move one way or the other?	The interstate will get wider. However, it will mostly stay within the existing right-of-way.
Where do you have to choose which bridge to use when crossing the river?	Going northbound, motorists will choose whether to get into the local or express lanes near the hospital when coming down the cut-in-the-hill.
Do the noise analyses include truck traffic?	All noise studies included truck traffic.
Can first responders get to someone stuck in the chute [barrier-separated lanes between Dixie Highway and Kyles Lane] without having to get off at the next exit and turn around for response.	The project team will be developing a maintenance of traffic plan and an emergency response plan in collaboration with first responders. The project plans will also be coordinated with emergency responders to make sure access is provided for incident response.
Part of solution to ensuring access for emergency responders would be to eliminate the barrier wall between the through and collector-distributor lanes.	Removing the wall would not help with the weaving movements in this area. The wall prevents weaving movements and will reduce crashes.
Maps need to be labeled on renderings.	Comment noted.
This is a federal problem. This is the largest interstate in country, and trucks should be rerouted. Why is the federal government not doing anything with this?	The grants the project team has applied for are federal funding. The state will still have to cover a portion of the project costs. This is one of the best opportunities to get funding.
Is everything shown tonight going to be on PublicInput.com/bsbc? Will information from Covington be on the site?	The presentation for Fort Wright will be available on the website tomorrow. The presentations for other neighborhoods and project exhibits are also on PublicInput.com/bsbc.
How long will the PublicInput.com/bsbc site be available?	Comments are being accepted until January 5, 2023. The site will be taken down after this.
Can you get the City a copy of the presentation to put on their website?	Yes.





Question	Project Team Response
What are you going to call the through traffic bridge? Bypass? Express?	The new companion bridge does not have a name yet. Signage will be specific and determined during the design / build phase.
What is the timeframe for purchasing right-of-way?	Right-of-way acquisition is anticipated to be completed in mid-2024. All offers for Fort Wright have been made.
What about the acquisition of properties in Covington?	KYTC has started right-of-way acquisition for commercial properties in Covington. The environmental document needs to be finished before the residential acquisition can start.
I have concern over selling my home with the coming project.	KYTC can discuss individual concerns one-on-one after the formal presentation.



---

Attachment 1: Advertising Materials

## Seth Turner

---

**From:** Jill Bailey  
**Sent:** Friday, November 11, 2022 7:38 PM  
**To:** Sherry Kish; Seth Turner  
**Cc:** Jodi Heflin; David Hatter; Jeff Bethell  
**Subject:** Brent Spence Bridge Presentation on 12/1/22

Hi Sherry,

Thank you so much for the flyer. I sent it via email to a list of residents who subscribe to our email distribution list (approximately 1000). Additionally, I posted it to all four (4) of our FB pages, and it will appear next week on our website and digital display sign in front of City Hall. We will also post on LINKEDIN periodically. The first notice was sent out tonight at 7:15 pm. I have scheduled additional postings/emails to occur on the following dates and times:

November 22 at 10 am  
November 30 at 3 pm  
December 1 at 7:30 am

FYI...our 11/30 and 12/1 email and post will also be sent to the local media.

The day of the event we will make sure that you have access to the building at 6:30 pm. If you need it sooner please let me know. On the day of the event your contact will be Jeff Bethell. You may reach him at . I have also copied him on this email.

The SHCC will accommodate approximately 80 people seated, and we have a television set that you can plug into to assist in your presentation. You and/or your staff will need to bring a laptop to facilitate it along with any easels for your display boards.

Please let me know if we can be of any further assistance to you. Thank you.

Jill

Jill Cain Bailey, CAO  
City of Fort Wright

---

**From:** Sherry Kish  
**Sent:** Friday, November 11, 2022 4:19 PM  
**To:** Seth Turner; Jill Bailey **Cc:** Jodi Heflin  
**Subject:** RE: Brent Spence Bridge Presentation on 12/1/22

**\*\* WARNING: This e-mail is from an EXTERNAL sender. Be wary of any links or attachments. If this e-mail is unexpected and you are being asked to open an attachment or enter information or credentials into an online form STOP NOW. Call the sender via a known phone number to determine if this is legitimate. Even if you expected this email, if it is regarding any type of financial transaction like a wire, call the sender via phone to validate the information and talk to your supervisor before conducting any financial transaction. \*\***

Jill,  
 Thanks so much for your call this afternoon and for all of your assistance. Here is the updated flyer. I have already updated the meeting location with the project team. If you find you need additional information to assist with advertising, please let us know!  
 Thanks,  
 Sherry

**From:** Seth Turner \_\_\_\_\_  
**Sent:** Friday, November 11, 2022 2:58 PM  
**To:** Jill Bailey \_\_\_\_\_  
**Cc:** Jodi Heflin ; Sherry Kish \_\_\_\_\_  
**Subject:** RE: Brent Spence Bridge Presentation on 12/1/22

Good Afternoon Jill,

Thank you again for scheduling a meeting with our Brent Spence Bridge project team to talk with the City of Fort Wright residents on December 1<sup>st</sup>.

When you get a chance, please let us know your capabilities and what assistance you might need in getting the word out to your neighborhood.

Thank you,  
**Seth Turner**  
 Transportation Engineer I

**From:** Sherry Kish \_\_\_\_\_  
**Sent:** Tuesday, November 1, 2022 7:01 PM  
**To:** Jill Bailey \_\_\_\_\_  
**Cc:** Jodi Heflin ; Seth Turner \_\_\_\_\_  
**Subject:** Brent Spence Bridge Presentation on 12/1/22

Jill,  
 Thank you so much for working with us to get a meeting scheduled to allow the Brent Spence Bridge project team to talk with the City of Fort Wright residents on December 1<sup>st</sup>. As you know, the project team wants to share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

Our goal is to be sure all notifications about the meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous city/neighborhood meeting?		

Email list	Can you email a neighborhood wide or citywide email list with information about BSB presentation at the upcoming meeting?		
Facebook (private to neighborhood/city)	Can you share a Facebook message about BSB presentation?		
Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?		
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?		
Other	Is there another outlet you have available to get the word out?		
Do you need assistance from us with advertising?	We made a flyer for your consideration (attached to this email).		

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!  
Sherry

**Sherry R. Kish**

Office:  
Cell:  
Email:

**HNTB CORPORATION**

88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

**100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

Please note: If your message in any way pertains to City of Fort Wright, KY business, your message and any replies to it may be a public record under Kentucky laws and may be made publicly available to anyone who requests it.

Please note: If your message in any way pertains to City of Fort Wright, KY business, your message and any replies to it may be a public record under Kentucky laws and may be made publicly available to anyone who requests it.

SHARE:

[Join Our Email List](#)

## Mark Your Calendars NOW!



### You're Invited

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of **neighborhood outreach meetings** for the Brent Spence Bridge (BSB) Corridor Project.

**Purpose:** To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team

**When:** December 1, 2022 | 7:00pm

**Where:** South Hills Civic Club  
10 Bluegrass Avenue, Fort Wright, KY 41011

### Hope to see you there!

If you need assistance attending this meeting, please call Stacey Hans at [\(859\) 341-2700](tel:8593412700).

View the most recent project newsletter at the following link:  
<https://mailchi.mp/3ae28ef3744d/brent-spence-bridge-e-newsletter-16606655>

Si desea que los materiales para esta reunión son traducidos a español, contacte a Domingo Martinez tan pronto que sea posible a [Domingo.Martinez@dot.ohio.gov](mailto:Domingo.Martinez@dot.ohio.gov) o por teléfono a (513) 933-6136.

ODOT PID 89068 | KYTC Project Item No. 6-17

City of Fort Wright | 409 Kyles Lane, Fort Wright, KY 41011

[Unsubscribe](#)



# Fort Wright Facebook Page

Facebook.com

**Fort Wright Public Works Department**

Welcome to the Fort Wright Public Works page. Proudly serving the residents and businesses of the City of Fort Wright.

**Page** · Public & Government Service

409 Kyles Lane

(859) 331-1700

info@fortwright.com

fortwright.com

Not yet rated (0 Reviews)

Add diversity info

**Fort Wright Public Works Department**  
November 11 at 7:11 PM

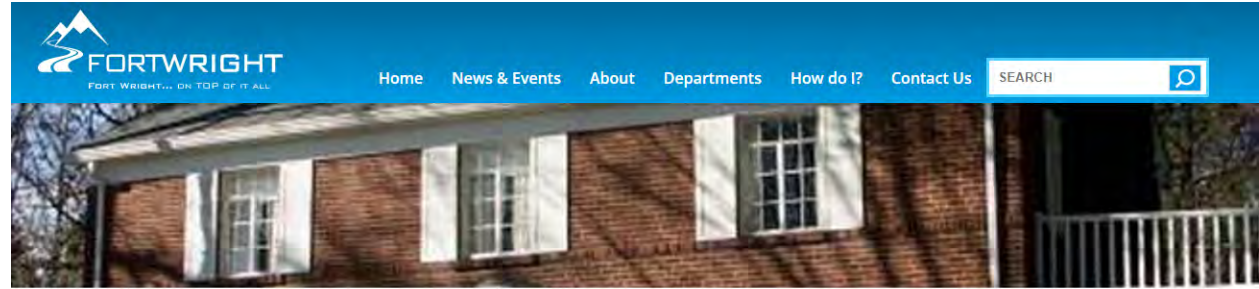
<https://conta.cc/3A9HRBq>

Mark Your Calendars Now!  
Brent Spence Bridge Corridor Community Outreach Meeting  
Thursday, December 1, 2022 | 7 PM | South Hills Civic Club

**Brent Spence Bridge Corridor Community Outreach Meeting**  
Thursday, December 1, 2022 | 7:00 PM | South Hills Civic Club - 10 Bluegrass Avenue, Fort Wright, KY Mark Your Calendars NOW! City of Fort Wright | 409 Kyles Lane, Fort Wright, KY 41011 Unsubscribe

Like Comment Share





## News

### Brent Spence Neighborhood Meeting

Posted on November 14, 2022



You're Invited! The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of neighborhood outreach meetings for the Brent Spence Bridge (BSB) Corridor Project. The Fort Wright Neighborhood Meeting will be on Thursday, December 1st, at 7:00 PM at the South Hills Civic Club, 10 B...

[Read More >](#)

#### News & Events

[> News](#)

[> Newsletters](#)

[> Events](#)

#### Categories

[> Finance \(2\)](#)

[> Taxes \(6\)](#)

[> Museum \(18\)](#)

[> COVID-19 \(16\)](#)

[> Amsterdam Road Project \(1\)](#)

#### News Archives



Attachment 2: Sign-In Sheet

SIGN-IN SHEET



**BRENT SPENCE BRIDGE CORRIDOR PROJECT**  
Neighborhood Outreach Meeting – December 1, 2022

Name	Representing	Address	Phone	Email
1. <i>[Signature]</i>				
2. MARK DUBOCHET	Phar			
3. BENJAMIN DESSAUS	City of Fort Wright			
4. Phoebe Wolitzel				
5. LOU ANN WAINMAN	FORT WRIGHT			
6. James J. Westman	myself - neighborhood			
7. NAREY HOFFMAN	Neighborhood - Marcelle De			
8. JULIE POWERS	MYSELF			
9. Elaine Demoret	Self			
10. BRUCE DEMORET	"			
11. Bill + Claudia Enckling	self			
12. MIKE & LYNN JONES	SELF			
13. TIM SLEAZYS				
14. DGS BLOOM				
15. Carolyn Zembrod	Self Ft Wright			
16. KEN SCHMIDERS	RESIDENT			
17. Tanner Boone Foster	Resident			
18. Ros Clifton	Resident			
19. Jay Weber	Resident			
20. Steve Lorenz	Resident			

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



**BRENT SPENCE BRIDGE CORRIDOR PROJECT**

Neighborhood Outreach Meeting – December 1, 2022

Name	Representing	Address	Phone	Email
1. Gregory Cain	NEIGHBORHOOD			
2. STEVE MYERS				
3. VANIA GROPAX	MARCELA DL-			
4. SUEY QIU OSTERHAGE	DURSELVES			
5. Gina Oaden	neighborhood			
6. MARK HADIG				
7. DONNA BROWN				
8. Johnny Raleigh				
9. C. Humphreys				
10. ALYSON BROWN				
11. RICHARD ATKINSON				
12. Jan Huoba	at			
13. Dave HARR	COFW			
14. Bill Nester				
15. Joe Newaben				
16. ROGER SCHWARTZ				
17. GARY KATLUN	liberal			
18. TIM WALLACE				
19. Jim Cobb				
20. Jason Collins				

Please print clearly. • Contact information is optional but must be included if you wish to receive updates.



SIGN-IN SHEET



**BRENT SPENCE BRIDGE CORRIDOR PROJECT**

Neighborhood Outreach Meeting – December 8 | 2022

	Name	Representing	Address	Phone	Email
1.	Ray Williams	Home owner			
2.	Kirk Collins	Neighbor			
3.	Jessie Baitman	Home owner			
4.	Kinrossa Nylman	Home owner			
5.	Joel Freiburger	Home owner			
6.	Katie Baker	Home Owner			
7.	Jeff Crosby	Home Owner			
8.	David Witt	Home Owner			
9.	Graves Warren	Home Owner			
10.	Daniel Manthey	Citizen			
11.	Jacki Mellic	ANWB			
12.	Marta Barber	ANWB			
13.					
14.					
15.					
16.					
17.					
18.					
19.					
20.					

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



### BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 6, 2022

	Name	Representing	Address	Phone	Email
1.	Karen + Dana Tisdell				
2.	ANDREW ELLIS				
3.					
4.					
5.					
6.					
7.					
8.					
9.					
10.					
11.					
12.					
13.					
14.					
15.					
16.					
17.					
18.					
19.					
20.					

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



### Attachment 3: Photographs





---

[Attachment 4: Presentation](#)

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



**Fort Wright Meeting (KY)**  
December 1, 2022



INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with introductions of the Project Team in attendance (2 min).



# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in Fort Wright the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Fort Wright area
  - Discussion/feedback from Fort Wright residents



2

The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near Fort Wright. Most importantly, tonight we're focusing on hearing from the residents of Fort Wright. We're here to answer your questions and to listen to your feedback about the project.

# Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, KYTC has begun purchasing the land needed to build the project.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E

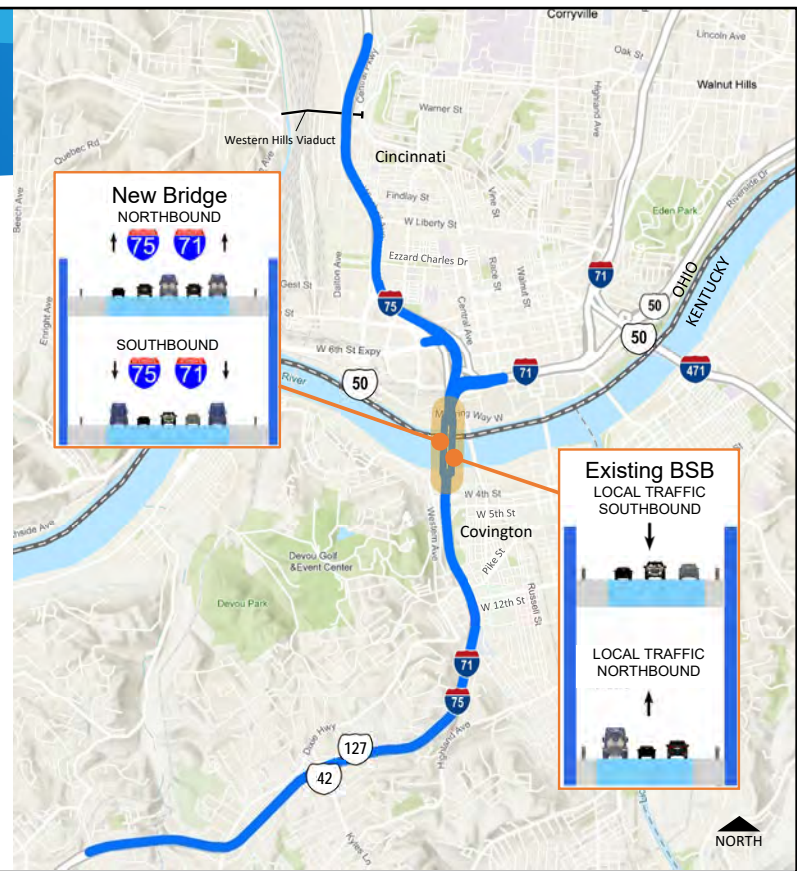


In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

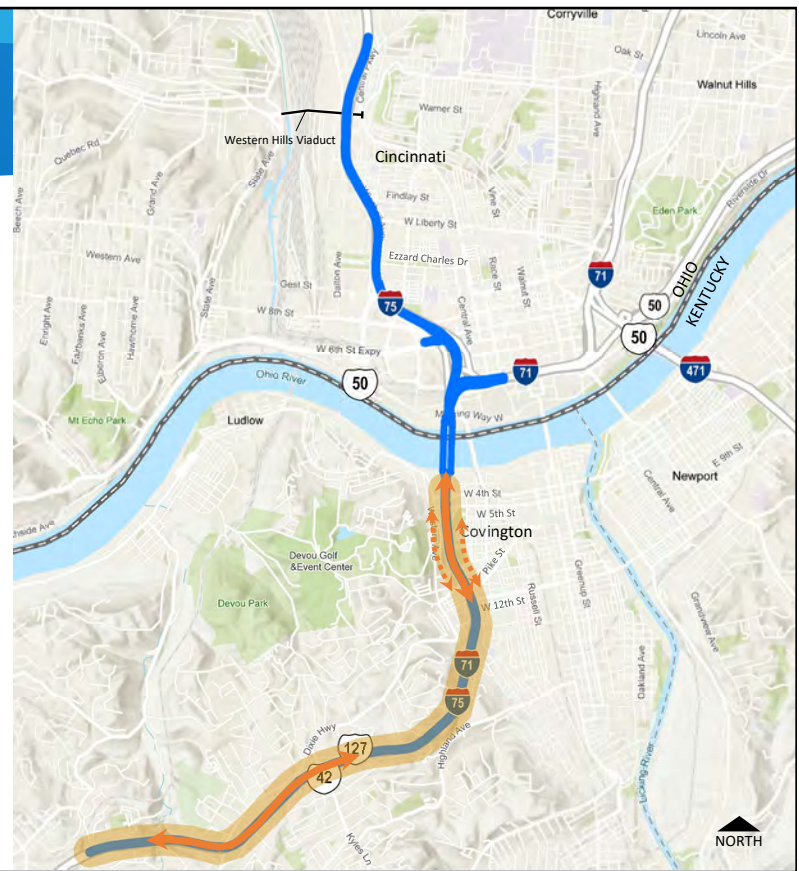
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

# Project Description

## Kentucky

- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.



This slide shows how traffic will move through Fort Mitchell and Fort Wright. The green shows through traffic on the interstate. As shown in the orange, a collector distributor road will start just south of Dixie Highway. The purpose of the collector-distributor road is to reduce the number of places where traffic is entering and exiting the freeway to reduce the number of times vehicles weave in and out of the through travel lanes to access local roads. As shown here, in the northbound direction, traffic traveling to Dixie Highway and Kyles Lane will exit onto the collector-distributor road south of Dixie Highway. Vehicles traveling to Dixie Highway will then leave the collector-distributor road and take a ramp to Dixie Highway (shown in grey). Traffic traveling to Kyles Lane will continue north on the collector-distributor road.



Moving further north, traffic from Dixie Highway will travel down a ramp to enter the northbound collector-distributor road. Next, traffic traveling to Kyles Lane will leave the collector-distributor road and use a ramp to reach Kyles Lane. Northbound traffic will travel through on the collector-distributor road, and more traffic will enter from Kyles Lane.

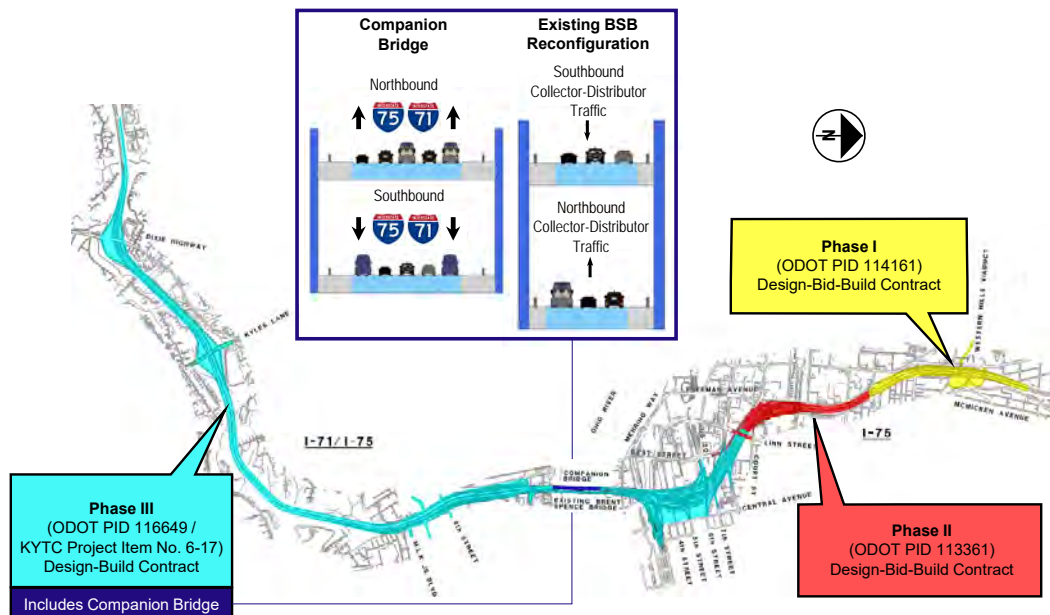


Finally, the traffic on the collector-distributor road will join with traffic from the Kyles Lane ramp and enter the interstate going northbound. Notice that there will be only one northbound exit and one entrance between Dixie Highway and Kyles Lane instead of one exit and one entrance for each road. Consolidating these access points will improve traffic flow and reduce high-speed crashes on the interstate.

The traffic movements we just walked through will be reversed in the southbound direction, providing the same access scheme and improvements in traffic flow and safety.



# Project Description

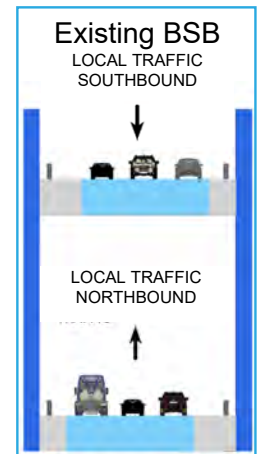
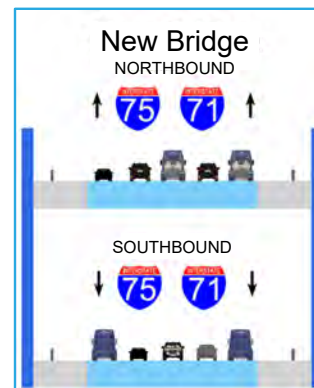


10

The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

# What Has Changed?

- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



11

As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



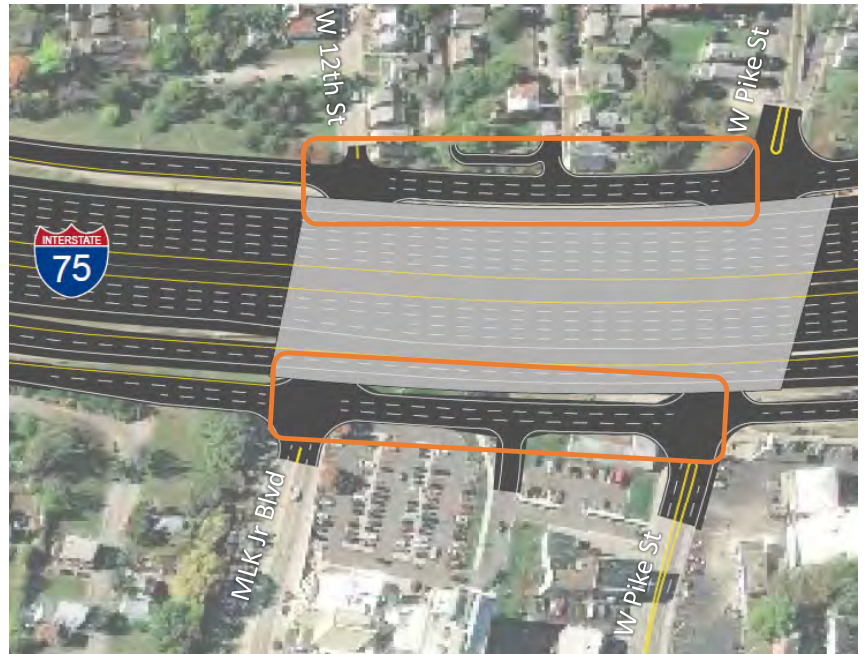
This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.



Here's another view from Covington.

# What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
  - Residential | 40+ then vs. 4 now
  - Commercial | 6 then vs. 5 now
- Reduced relocations



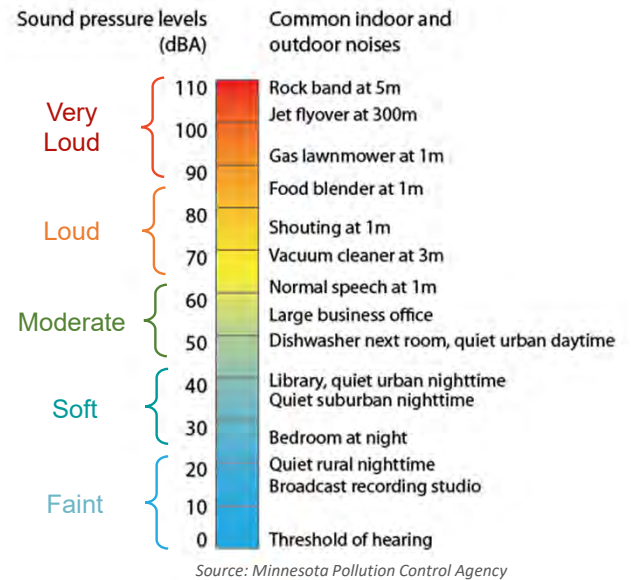
Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street in Covington. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound



15

Now let's dig into how the project will impact Fort Wright, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

- KYTC Noise Policy
- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.

Kentucky Transportation Cabinet  
Department of Highways  
Division of Environmental Analysis



## NOISE ANALYSIS AND ABATEMENT POLICY

APPROVED BY:

Todd Jeter, Division Administrator  
Federal Highway Administration

Date

James Gray, Secretary  
Kentucky Transportation Cabinet

Date

Revised August 1, 2022  
**EFFECTIVE DATE JULY 1, 2020**  
Revised July 1, 2015  
Original July 2011

16

KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dbA. We also determine if there is a substantial increase – specifically a 10 dbA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dbA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

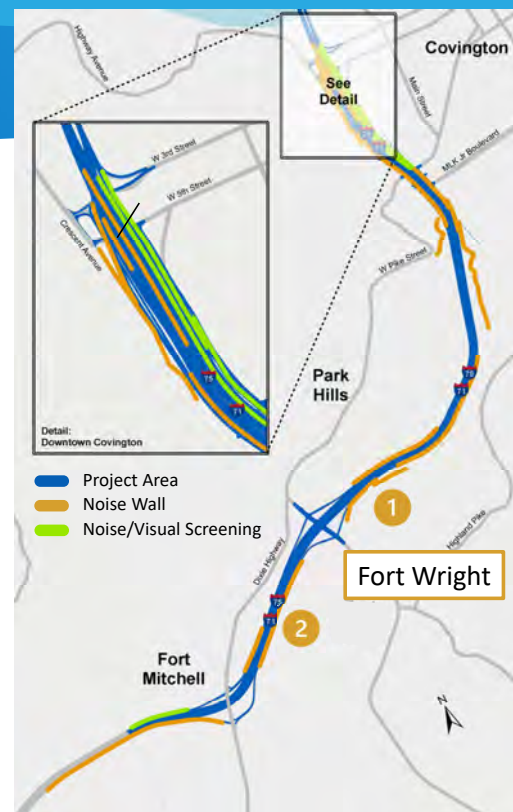
If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

# Noise – Ft. Wright (KY)

Noise wall reasonable/feasible per KYTC Noise Policy

- 1 • Highland Pike to Kyles Lane (Ft. Wright/Peaselburg)
  - Without wall = 64 dBA
  - With wall = 57 dBA
- 2 • South of Kyles Lane
  - Without noise wall = 68 dBA
  - With noise wall = 59 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office



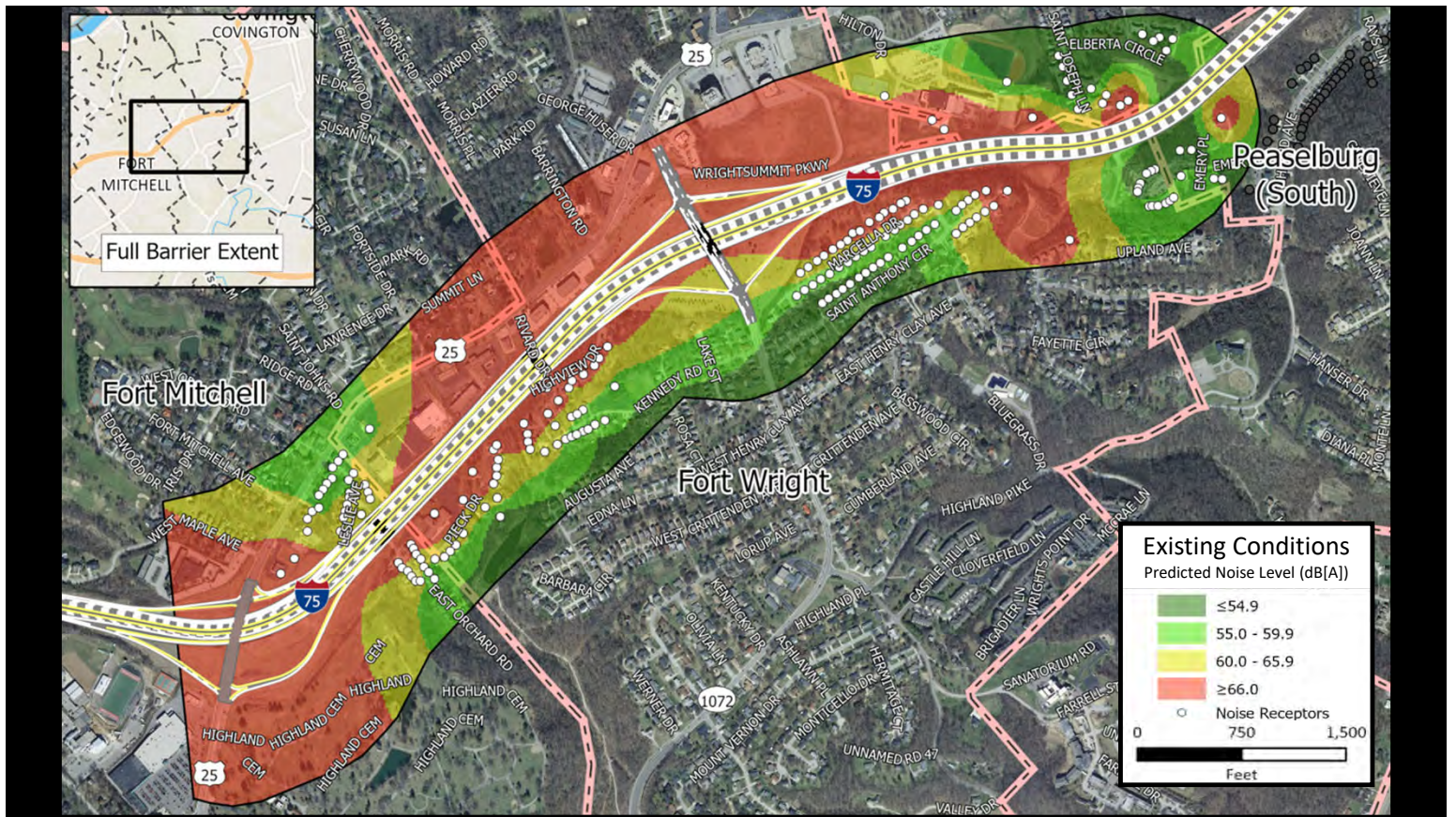
Note: Sound levels represent the average for the area benefitted by the wall.

In Fort Wright, a noise wall system north of Kyles Lane (shown by the number 1) was found to be feasible and reasonable. The average exterior noise levels are predicted to be around 64 decibels without a noise wall and 57 decibels with a noise wall. Another noise wall south of Kyles Lane (shown by the number 2) was also found to be reasonable and feasible. The average exterior noise levels are predicted to be around 68 decibels without a noise wall and 59 decibels with a noise wall. These sound levels are averages for all the people who will benefit from the walls. The sound levels will vary depending on where each receptor is located. For reference, the average exterior sound levels with the noise wall in both locations are predicted to be slightly less than what you'd experience in normal conversation.

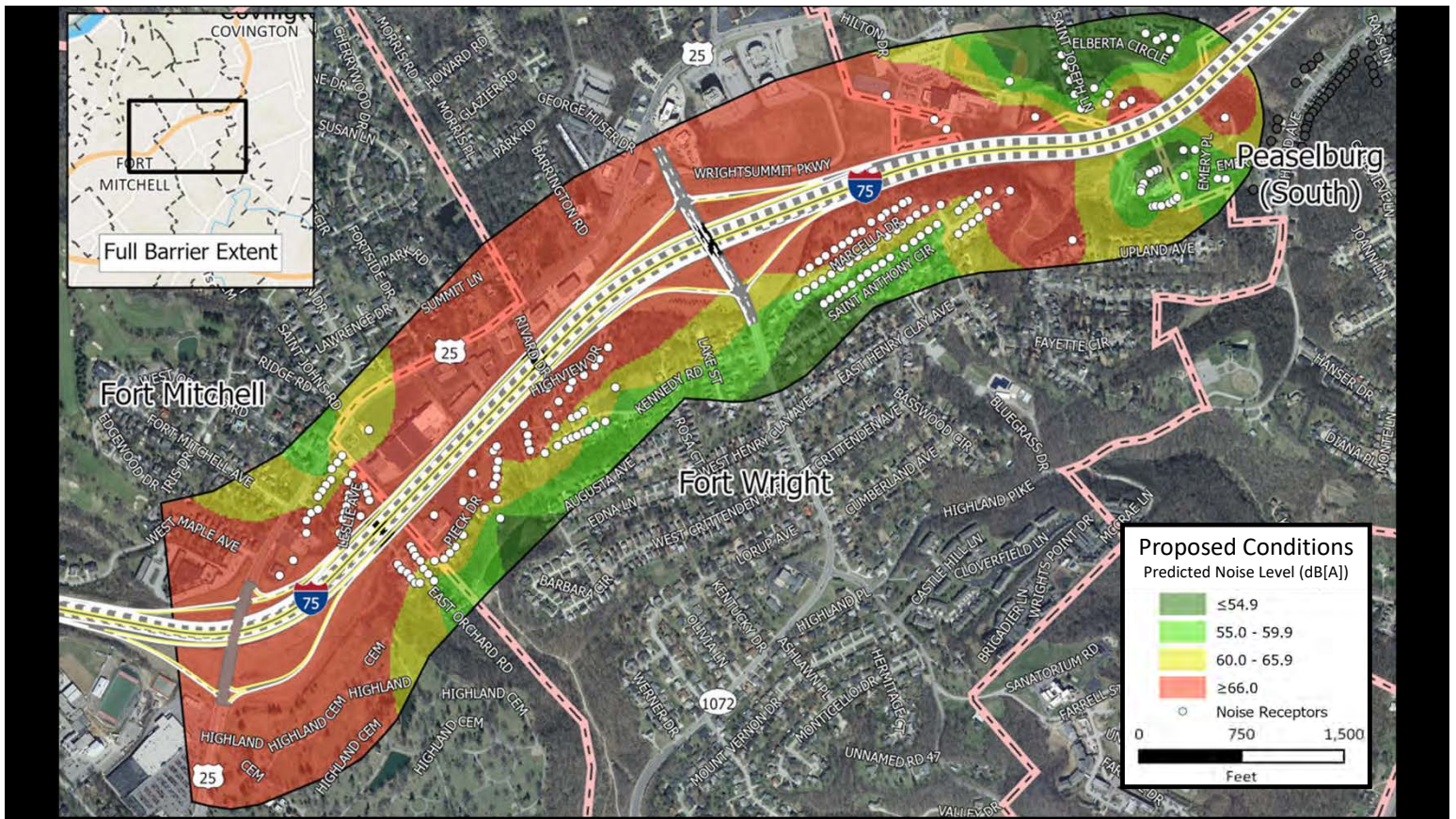




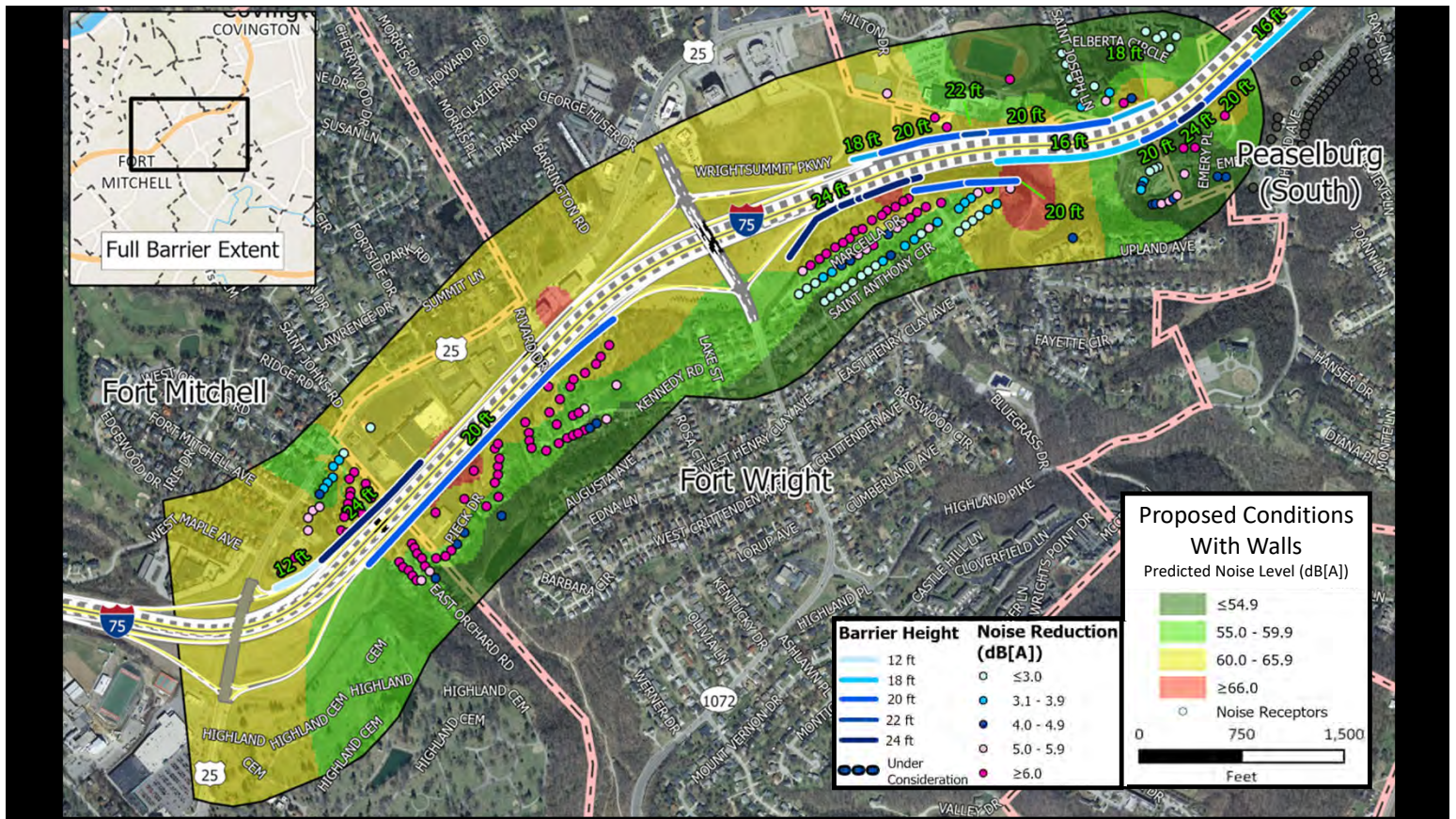
The red lines on this slide show noise walls that were evaluated as part of the noise study for Fort Wright. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis for Fort Wright. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a noise wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

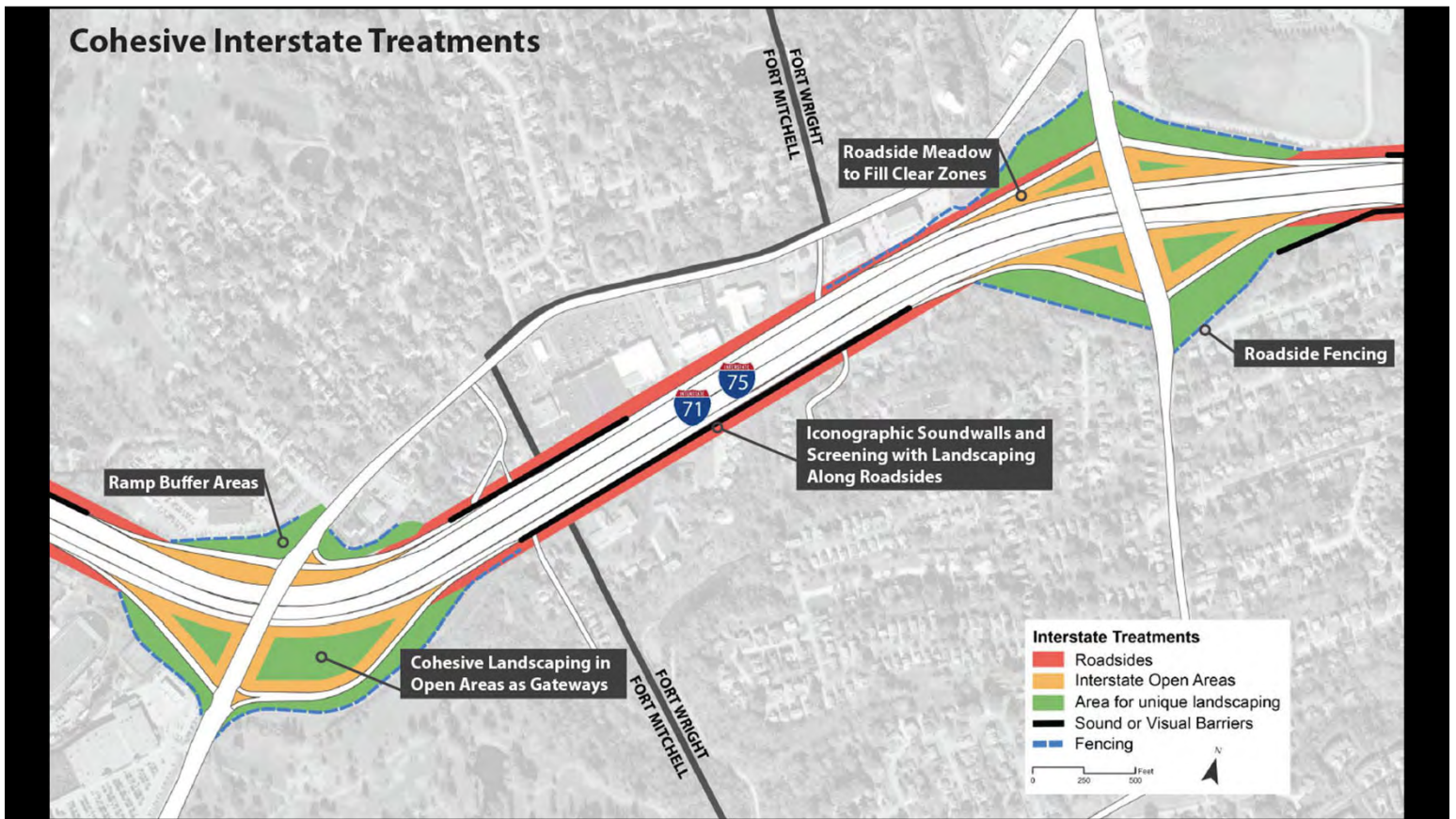
# Enhancement Measures

- Working with Fort Wright to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, noise walls, and pedestrian paths

22

KYTC is working with Fort Wright to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, noise walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction.

## Cohesive Interstate Treatments



This slide shows some of the potential locations of the enhancements discussed on the previous slide as it relates to Fort Wright. While it is an opportunity for a unique gateway at the Kyles Lane interchange, it is important that those treatments do not conflict with those selected for the Dixie Highway interchange.

# Gateway Opportunities



Off-Ramp Signage



Hillside Signage and Landscaping

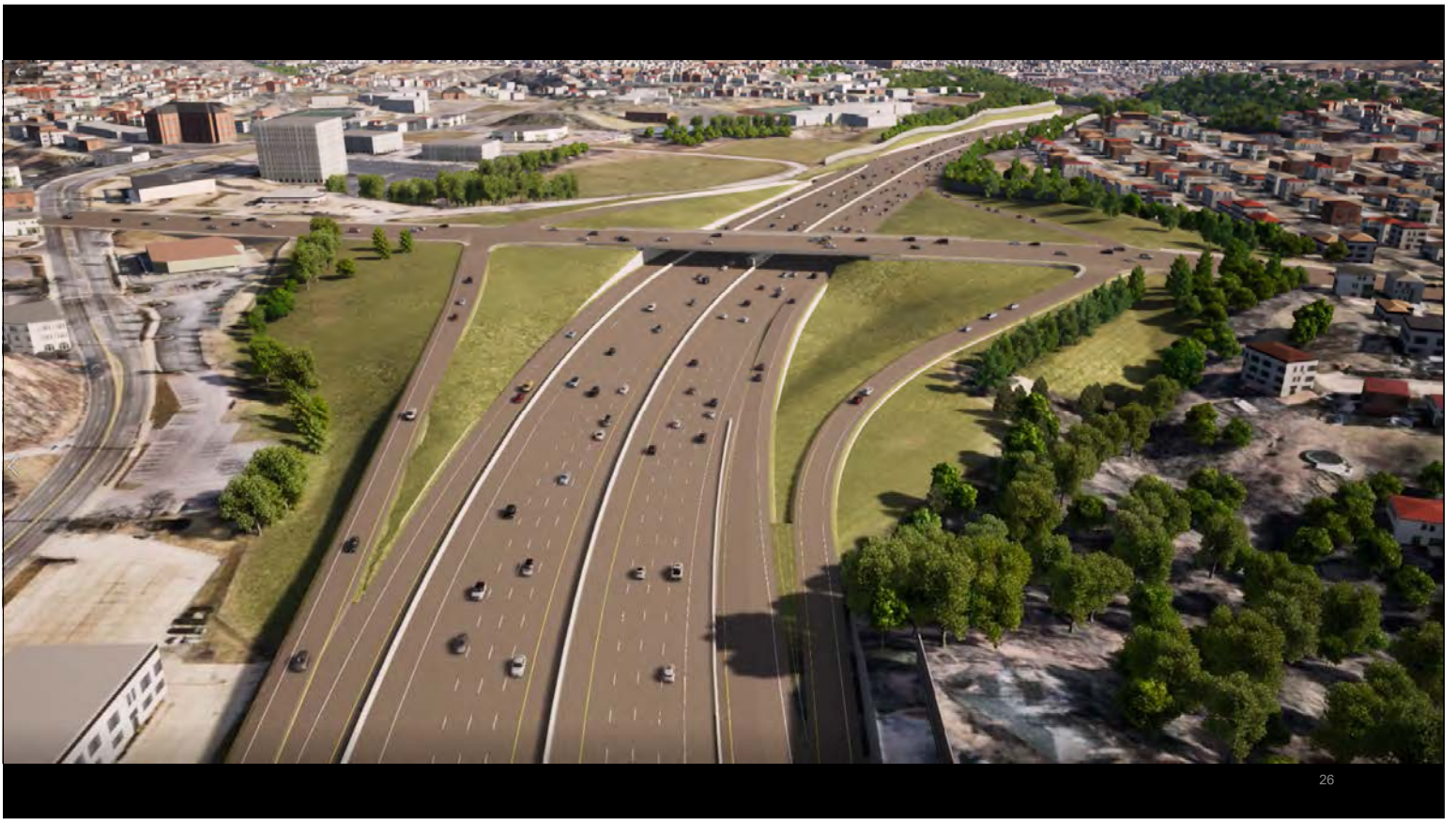


The City of Fort Wright provided the project team with information from a 2021 Interchange Study, which became the baseline for enhancements and gateway opportunities.



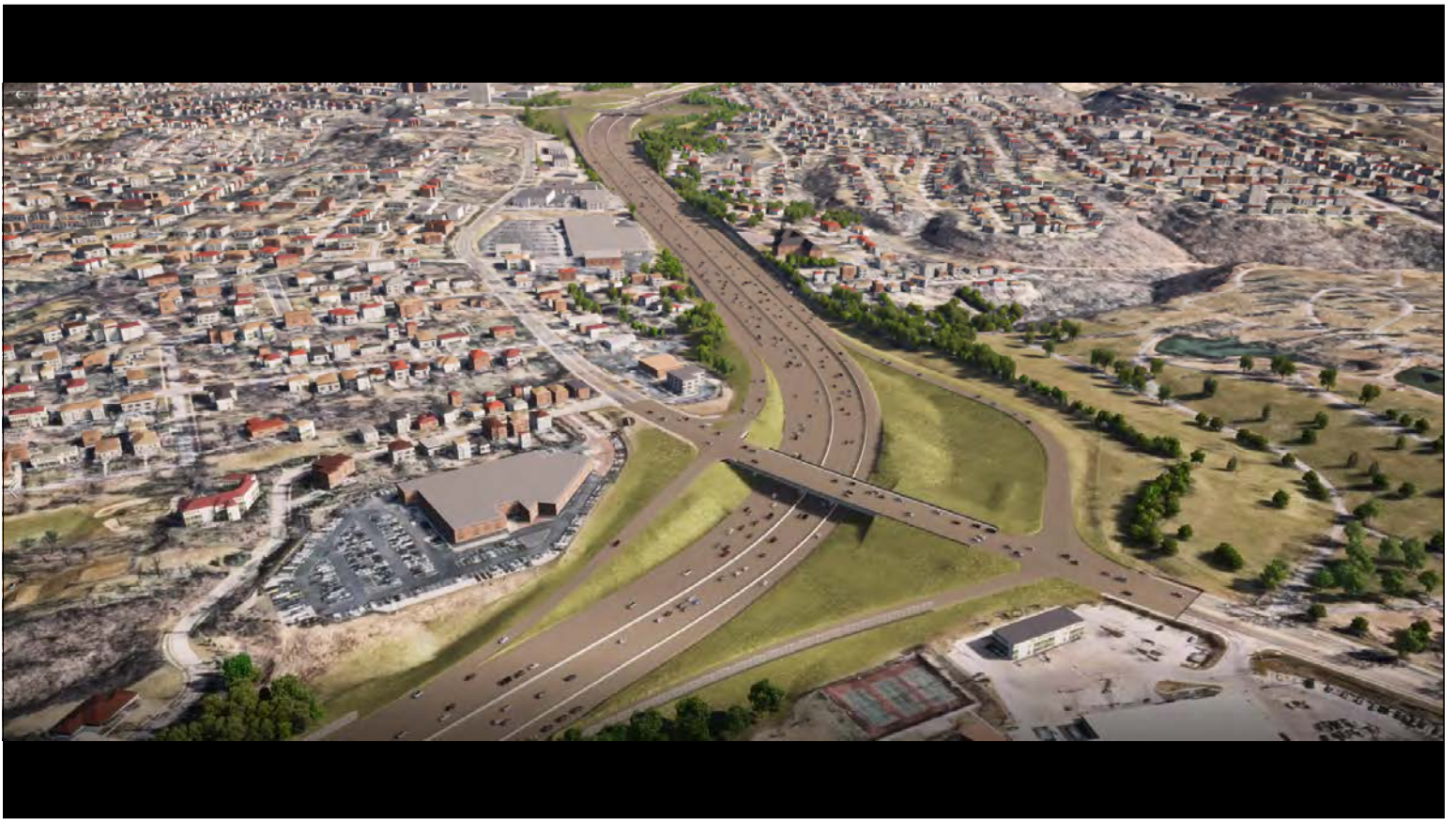
This slide shows what the finished project might look like from Kyles Lane. Notice the noise wall system in Fort Wright along the east side of the highway. On the west side, notice the noise wall constructed at the top of the slope adjacent to Notre Dame Academy.





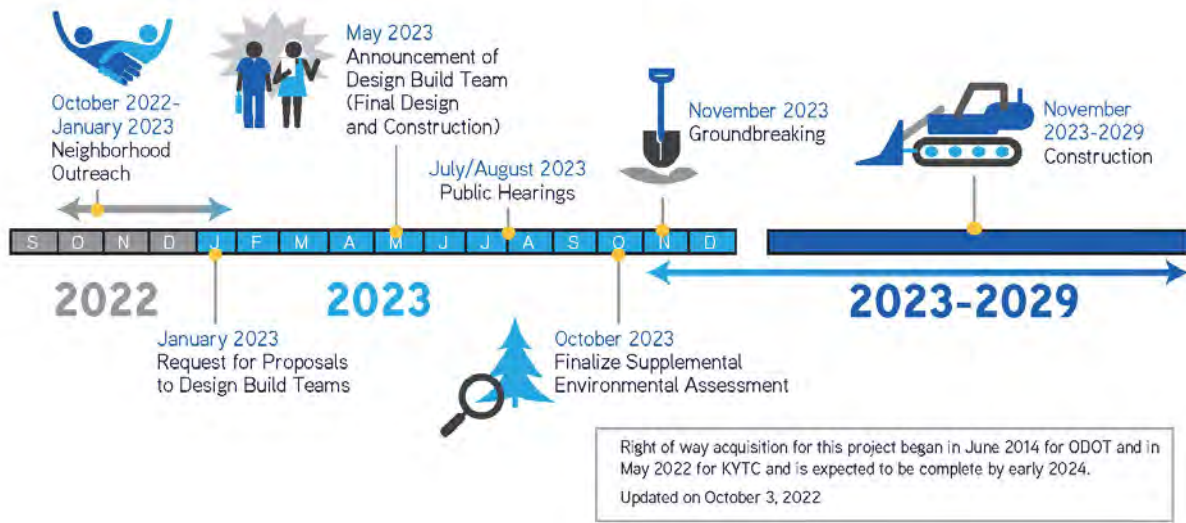
26

This view shows how the finished project might appear when looking north on I-71 and I-75 at towards Kyles Lane. Notice the noise wall on the east. Also notice the opportunities for ramp buffer areas and landscaping in open areas for unique gateways.



This view shows how the finished project might appear when looking north on I-71 and I-75 at Dixie Highway. Note the proximity of Kyles Lane to the north and the need for aesthetic treatments that work together to unify the corridor.

# Schedule



This slide shows the project schedule from today to the completion of construction.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.



---

[Attachment 5: Exhibits](#)



Brent Spence Bridge Corridor  
(Looking North from Kentucky)





Brent Spence Bridge Corridor  
(Looking Northeast from Kentucky)



KYTC Item No. 9-17 | QDOT PG 0608



Brent Spence Bridge Corridor  
(Looking Northwest from Kentucky)



KYTC Item No. 9-17 | QDOT PG 0608



Pike Street With Noise Walls (Looking Northwest) - Kentucky



KYTC Item No. 9-17 | QDOT PG 0608



Pike Street Without Noise Wall (Looking West) - Kentucky



KYTC Item No. 9-17 | QDOT PG 0608



Pike Street With Noise Wall (Looking West) - Kentucky



KYTC Item No. 9-17 | QDOT PG 0608



Pike Street Without Noise Wall (Looking East) - Kentucky



KYTC Item No. 9-17 | QDOT PG 0608



Pike Street With Noise Wall (Looking East) - Kentucky







---

Attachment 6: Comment Forms

# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

PLEASE KEEP PUBLIC INFORMED OF PROGRESS

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: CELL \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: / \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

Area Resident    Area business owner or employee    Commuter

Other: Part of our property was taken for this project.

General Comments (Please attach additional pages, if needed):

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

COMMENTS DUE BY  
**January 5, 2023**

- Comments may be submitted:**
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: N/A

Business/Organization Address: N/A

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Our concern is the impact on our home -- value of our home -- This will hurt us.

**COMMENTS DUE BY  
January 5, 2023**

Comments may be submitted:

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
- By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: N/A

Business/Organization Address: N/A

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068





# COMMENT FORM

BRENT SPENCE  
BRIDGE CORRIDOR



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): / \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

*heat maps were very effective for noise impacts*

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident    Area business owner or employee    Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

This will reduce the value of my home  
B/C OF the traffic nightmares & construction  
until at least 2029. This does not  
~~appear~~ appear to be fair to Ft Wright  
residents

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://www.PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

*I am concerned about the disruption to businesses & residences w/ this project.*

*I'm also concerned about the merging on & off of the ~~Inter~~ Interstates - e.g., where the split occurs - North-South - what kind of signage/electronic device will be in place so that it is clear what road to take?*

*Can cars get off the Corridor Bridge to go into downtown Centre or Cove?*

*Increase in sound for those who live close by?*

**COMMENTS DUE BY  
January 5, 2023**

Comments may be submitted:

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
- By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

## How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

## What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

## What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: some college (for both)

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Send us an e-mail once a month on status + definitive details.

Thank you for helping ODOT and KYTC improve their public involvement practices!

#### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

### How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

### What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

### What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: Master's Degree

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): English

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518





# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

### How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

### What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

### What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: MS

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

## How many people live in your household?

- 1 - 2     3 - 5     6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

## What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

## What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

### How many people live in your household?

- 1 - 2     3 - 5     6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

### What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

### What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

#### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518





# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: MASTERS/PHD PROFESSIONAL DEGREE

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518





# Brent Spence Bridge Corridor Project Mutter Gottes / CBD Meeting Summary December 5, 2022

## Introduction

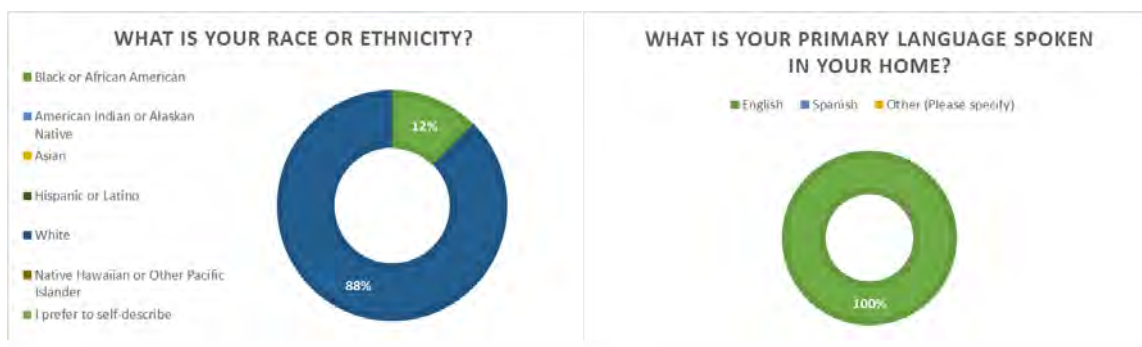
The Mutter Gottes and Covinton Central Business District (CBD) Neighborhood Meeting was held on December 5, 2022 from 7:00 pm to 8:15 pm. The meeting was held at the Kenton County Public Library – Covington Branch located at 502 Scott Street in Covington, Kentucky. It was publicized through the Mutter Gottes Facebook page (private group) and email list. The neighborhood group also distributed 300 flyers (see Attachment 1). Attendees at the meeting included the project team and members of the public. In addition to residents of Mutter Gottes, residents of Edgewood, Mainstrasse, and downtown Cincinnati were also in attendance. In total, 22 people attended the meeting, including 5 individuals from the project team (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). Following the presentation, the project team responded to questions and comments offered by those present.

Exhibits showing project renderings and lane configurations were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Five (5) comment forms and eight (8) demographic surveys were returned at the meeting.

## Demographics

The following is a breakdown of the responses received from the demographic survey.





A write in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. Comments included:

- All Covington neighborhoods are affected. Maybe reach out to other neighborhood groups.
- Post invites in neighborhood Facebook groups to the [sic] public meetings.



---

## Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Clarification / understanding of local versus through traffic and ramp connections.
- Concern over traffic congestion / backup in Covington during bridge closures and / or incidents requiring closure along with concern over access response time by emergency responders.
- Clarification / understanding of the process for noise evaluation and discussion on impacts with / without noise wall.
- Routing of truck traffic and sound / noise associated with truck traffic.
- Clarification of timeline and staging of construction.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. The project team will send responses to individuals who provided contact information.

Comments noted on the survey forms returned at the meeting include:

- Interested in historic preservation.
- Nice presentation.
- Community engagement is critical. As much as practical will be key to adoption. What are projected project cost and tax impacts? If you wish to get in touch with Mutter Gottes neighborhood (MOGNA.cov.mail.com).
- Please protect exits and entrances to and from north Covington, especially at 4<sup>th</sup> and 5<sup>th</sup> Streets.
- Definitely build the sound walls!!! Thx. Is it possible to increase local bridge traffic to 55 mph? 45 mph seems to slow for the bridge. 9<sup>th</sup> Street already has a speed problem especially at 9<sup>th</sup> and Bakewell and 9<sup>th</sup> and Willard. Can you add stop signs at each intersection to help with the added traffic? I'm worried about pedestrian safety on 9<sup>th</sup>. Will the combined Kyles and Dixie exit (south) ramp have 2 lanes? Kyles exit is already very slow, but maybe that's because of the trucks?



**Table 1: Q & A from Session**

Question	Project Team Response
Will there be lights or signals at the project intersections?	The traffic control at each intersection will be decided in the design / build phase.
Will the collector-distributor be northbound or southbound?	The collector-distributor will be in both directions, one on each side of the interstate.
Will there be a barrier between the interstate and collector-distributor road?	Yes.
What is the first exit going south on the collector-distributor road?	It is a left-hand exit for 5 <sup>th</sup> Street.
Can jake braking be prohibited?	KYTC cannot prohibit jake braking on interstate highways because it is a safety feature for trucks.
What if an incident occurs on the interstate?	The new companion bridge provides redundancy in the system. If there is an incident on the interstate, traffic can divert to the existing Brent Spence Bridge rather than the Covington street network.
What are the exits for the collector-distributor road in Cincinnati?	Going northbound, an exit to I-71/2 <sup>nd</sup> Street is first, then 5 <sup>th</sup> Street, 6 <sup>th</sup> Street, and Winchell Drive.
How do you go about determining noise levels?	Noise levels are determined during the highest peak period for traffic volumes. A model is used to determine noise levels, but it is field verified for existing noise levels to make sure it is modeling conditions correctly before it is used to project future noise levels.
Please explain the cost-effective criteria relative to noise walls.	The cost of a wall is divided by the number of people who will benefit from the wall. KYTC has set a threshold for cost per benefitted receptor that determines if a noise wall is cost-effective.
Do noise walls redirect noise upward instead of outward?	A portion of the noise is absorbed by the textures on the wall.
With current conditions and narrow lanes, there may be some trucks that avoid the Brent Spence Bridge. Will there be anything to prohibit / reduce trucks when the project is built?	The additional capacity across the river and splitting through and local traffic will help to improve traffic flow and safety. Trucks will not be restricted or prohibited in the project corridor.
How far south does the corridor extend? Could there be more trucks? I have concern over induced demand.	The project ties back into the existing interstate south of Dixie Highway. When in Covington and Cincinnati, through-traveling trucks will be separated from local traffic. The project has been designed to meet the demand that is anticipated over the next 25 years.



Question	Project Team Response
Did you look at anything like planting trees for noise reduction?	Trees are not considered a viable method of noise reduction in Kentucky. However, KYTC is looking at opportunities for enhancements that could include planting trees in some areas, such as along the shared-use path in Goebel Park.
If you eliminate the local 9 <sup>th</sup> Street to 5 <sup>th</sup> Street connector would there be an exit to 5 <sup>th</sup> Street?	Yes.
Will you have bicycle ramps on / along I-71?	No. KYTC will focus on opportunities to improve east-west movements for bicycles in the project area.
Can anything be done to reduce speeds and improve safety for pedestrians at the end of Bakewell and Willard?	The project team can discuss this further one-on-one after the presentation.
Separating the storm and sanitary is a huge improvement, especially for Peaselburg.	KYTC has been working with neighborhoods to design a project that benefits the communities. This is a concern that has been raised that KYTC has been able to incorporate into the design.
Will it be more complex for local law enforcement and response after the project is built?	KYTC will collaborate with local emergency response and first responders (police / fire / EMT) for all municipalities in the project corridor to develop an Incident Management Plan.
How much of what you are describing is absolute versus what is being tweaked? What are you signing off on? What might change during the design / build phase?	One example of an absolute is the number of lanes. Opportunities for modifications or refinements include pedestrian facilities, landscape design, and others. One of the biggest decisions during the design / build phase could be selecting a bridge type.



---

Attachment 1: Advertising Materials



## Seth Turner

---

**From:** Brandon Galeas  
**Sent:** Friday, November 11, 2022 9:51 AM  
**To:** Sherry Kish; Ginger Dawson  
**Cc:** Jodi Heflin; Seth Turner  
**Subject:** RE: Brent Spence Bridge Presentation on December 5th

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

---

**From:** Sherry Kish  
**Sent:** Wednesday, November 09, 2022 5:02 PM  
**To:** Brandon Galeas; Ginger Dawson  
**Cc:** Jodi Heflin; Seth Turner  
**Subject:** Brent Spence Bridge Presentation on December 5th

Brandon and Ginger,

Thank you so much for working with me to set up a meeting to invite the residents of Mutter Gottes to hear from the Brent Spence Bridge (BSB) project team. The project team will share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

Our goal is to be sure all notifications about meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous neighborhood meeting?		X – 11/2 though we did say it was in the works and shooting for this month which is now not in the cards
Email list	Can you email a neighborhoodwide or citywide email list with information about BSB presentation at the upcoming meeting?	X	
Facebook (private to neighborhood)	Can you share a Facebook message about BSB presentation?	X	

Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?	X – There are some hot spots for businesses in our neighborhood, though there is a lot of cross-pollination exposure of residents as a result of our city’s walkability	
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?		
Other	Is there another outlet you have available to get the word out?		
Do you need assistance from us with advertising?	We have created a flyer that could be helpful (attached).	X – if you can print flyers our distribution team can pass them out to every home	

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!  
Sherry

**Sherry R. Kish**

**HNTB CORPORATION**

88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

**100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

## Seth Turner

---

**From:** Sherry Kish  
**Sent:** Monday, November 21, 2022 11:32 AM  
**To:** Seth Turner; Jodi Heflin  
**Subject:** FW: MGNA Brent Spence Bridge Presentation  
**Attachments:** 11-08-2022\_Invitation Flyer\_Mutter Gottes.pdf

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

FYI

**Sherry R. Kish**  
Manager Business Development

**HNTB CORPORATION**  
88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

 **100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

---

**From:** Brandon Galeas  
**Sent:** Sunday, November 20, 2022 2:55 PM  
**To:** Sherry Kish  
**Subject:** FW: MGNA Brent Spence Bridge Presentation

Sherry,

Here is our email notification for the event. A facebook post was also posted to our page but it's a private group with 423 members from the local community.

Brandon Galeas

---

**From:** Mutter Gottes Neighborhood Association  
**Sent:** Sunday, November 20, 2022 2:52 PM  
**Subject:** MGNA Brent Spence Bridge Presentation

The Kentucky Transportation Cabinet and Ohio Department of Transportation are conducting a series of neighborhood outreach meetings for the Brent Spence Bridge Corridor project. The project team will share updates, answer questions and give the opportunity for residents to share feedback.

Consider joining the Mutter Gottes community meeting on **December 5, 2022 at 7 p.m.** in the **Kenton County Public Library – Covington Branch** Meeting Rooms, 502 Scott Street.



Attachment 2: Sign-In Sheet

SIGN-IN SHEET

**BRENT SPENCE BRIDGE CORRIDOR PROJECT**

Neighborhood Outreach Meeting – December 5, 2022

Name	Representing	Address	Phone	Email
1. Raymond Senlar Jr	SELF			
2. John Paul	Self			
3. Steve Frank	RACIA			
4. Dennis Bullock	Self			
5. Ray Burr	MGNA			
6. Shannon Gathers	MGNA			
7. Anne Mitchell	MGNA			
8. GERRI KING	MGNA			
9. PAULY NAVIN	MGNA			
10. Ginger Dawson	MGNA			
11. JeanneHouck Trifilio	Self			
12. David Averdich	Self			
13. Alex Nmyr	Self			
14. Bridget Cunsiorowski	Self			
15. LAM LUK	SELF			
16. MAREN BECHMAN	HWFB			
17. Catherine Clark	Self			
18. Jamir Davis	Self			
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



### Attachment 3: Photographs





---

[Attachment 4: Presentation](#)

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



# Mutter Gottes/CBD Neighborhood Meeting (KY)

December 5, 2022



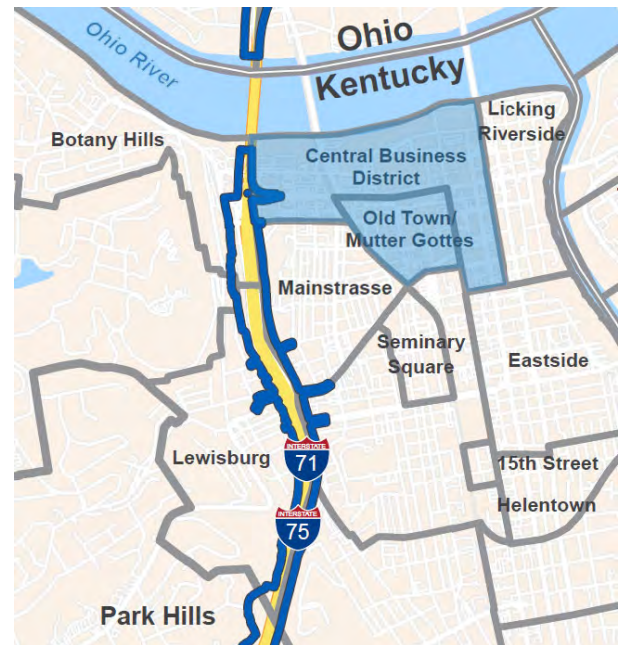
INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with remarks by the City of Covington (5 min) and introductions of the Project Team in attendance (2 min).

# Welcome

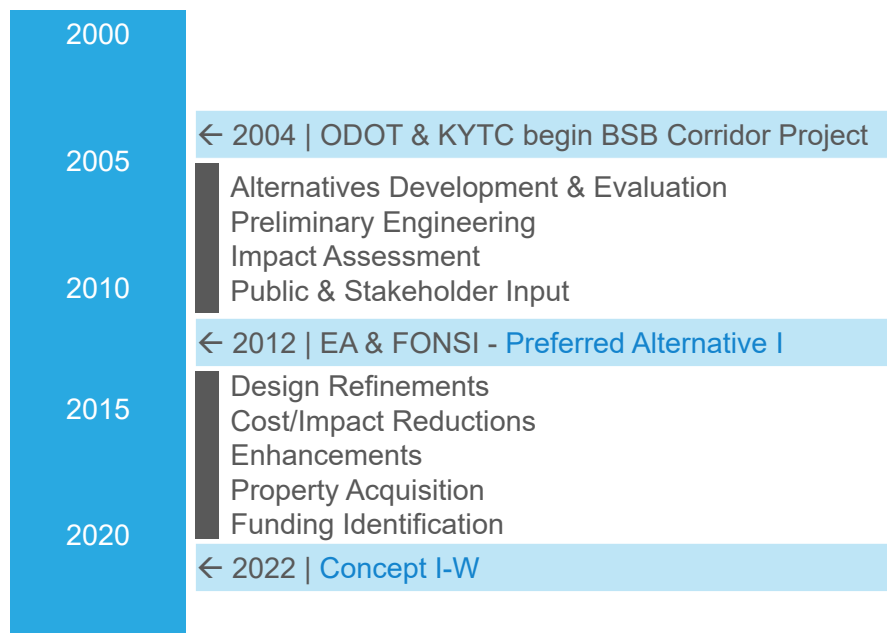
- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the Mutter Gottes / CBD neighborhoods the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Mutter Gottes / CBD area
  - Discussion/feedback from Mutter Gottes / CBD residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Mutter Gottes / CBD neighborhoods. Most importantly, tonight we're focusing on hearing from the residents of Mutter Gottes and the CBD. We're here to answer your questions and to listen to your feedback about the project.



# Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

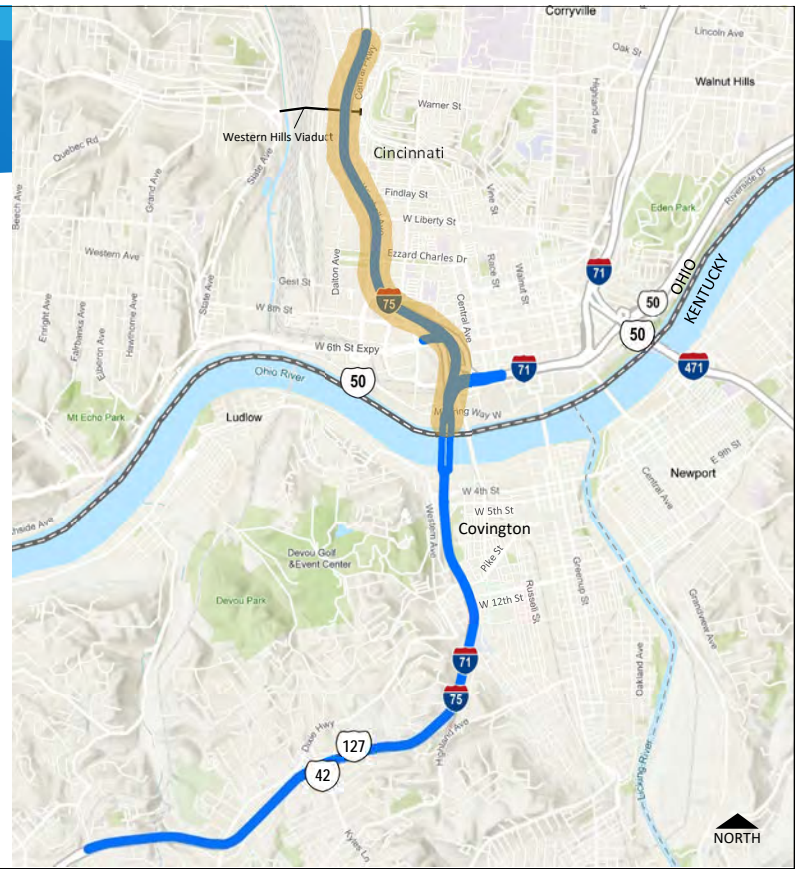
Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2022, KYTC has begun purchasing the land needed to build the project.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E

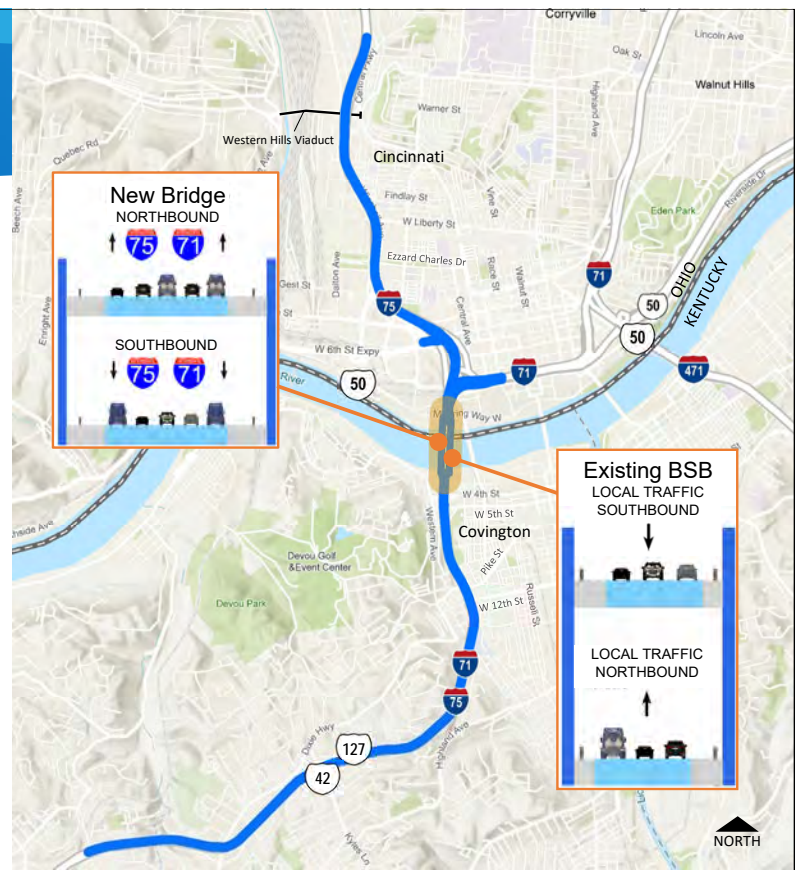


In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

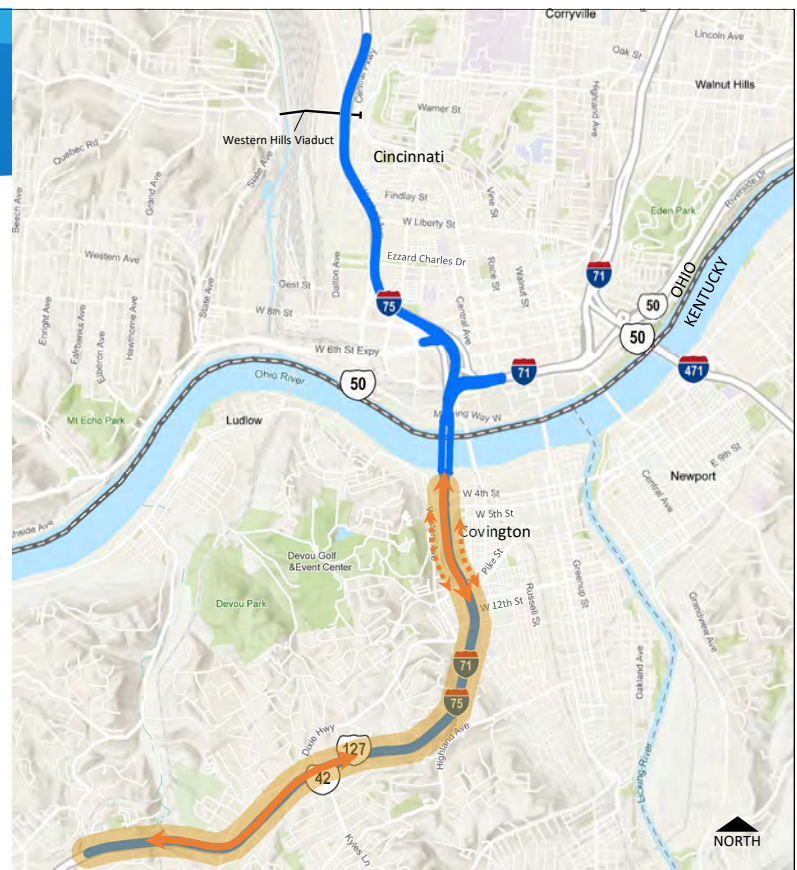
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

# Project Description

## Kentucky

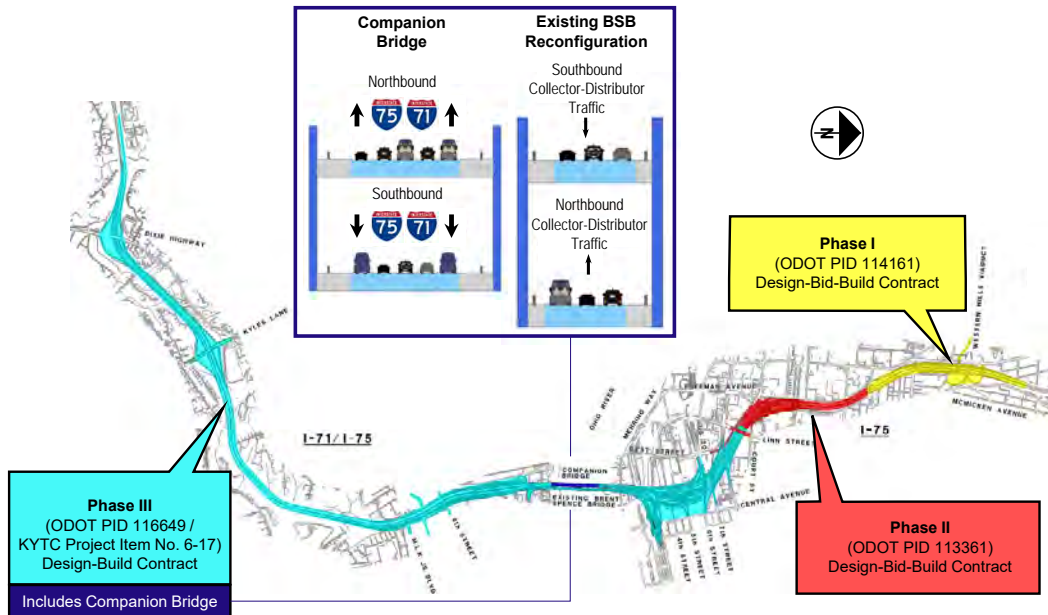
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles Lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

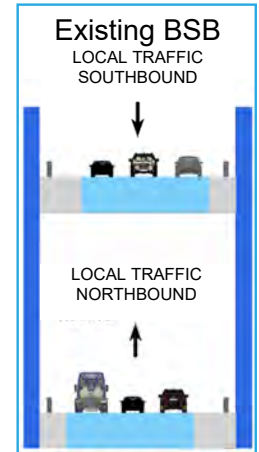
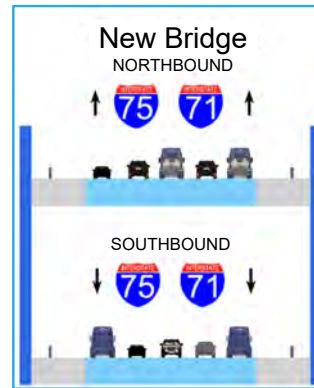
# Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

# What Has Changed?

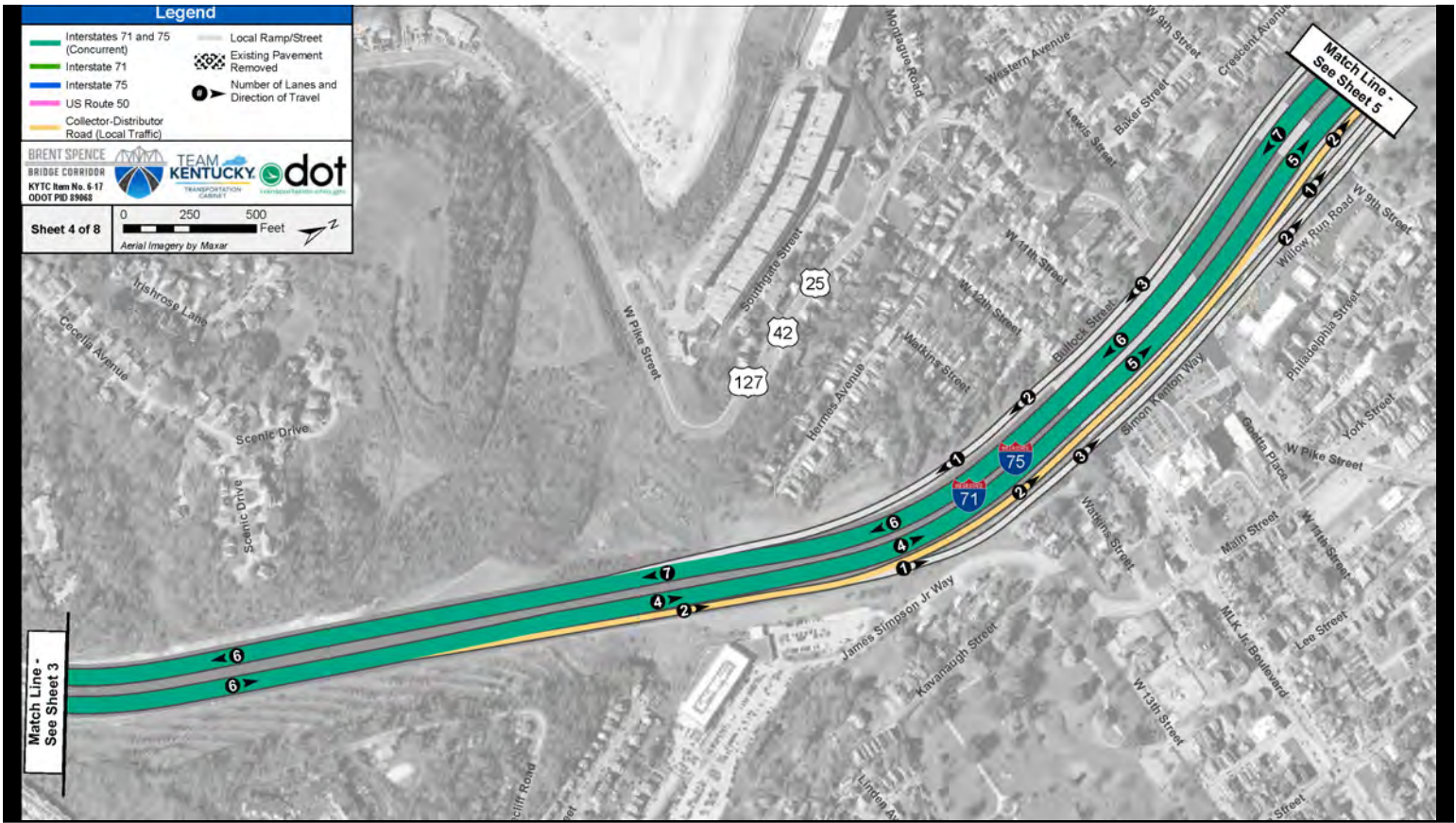
- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



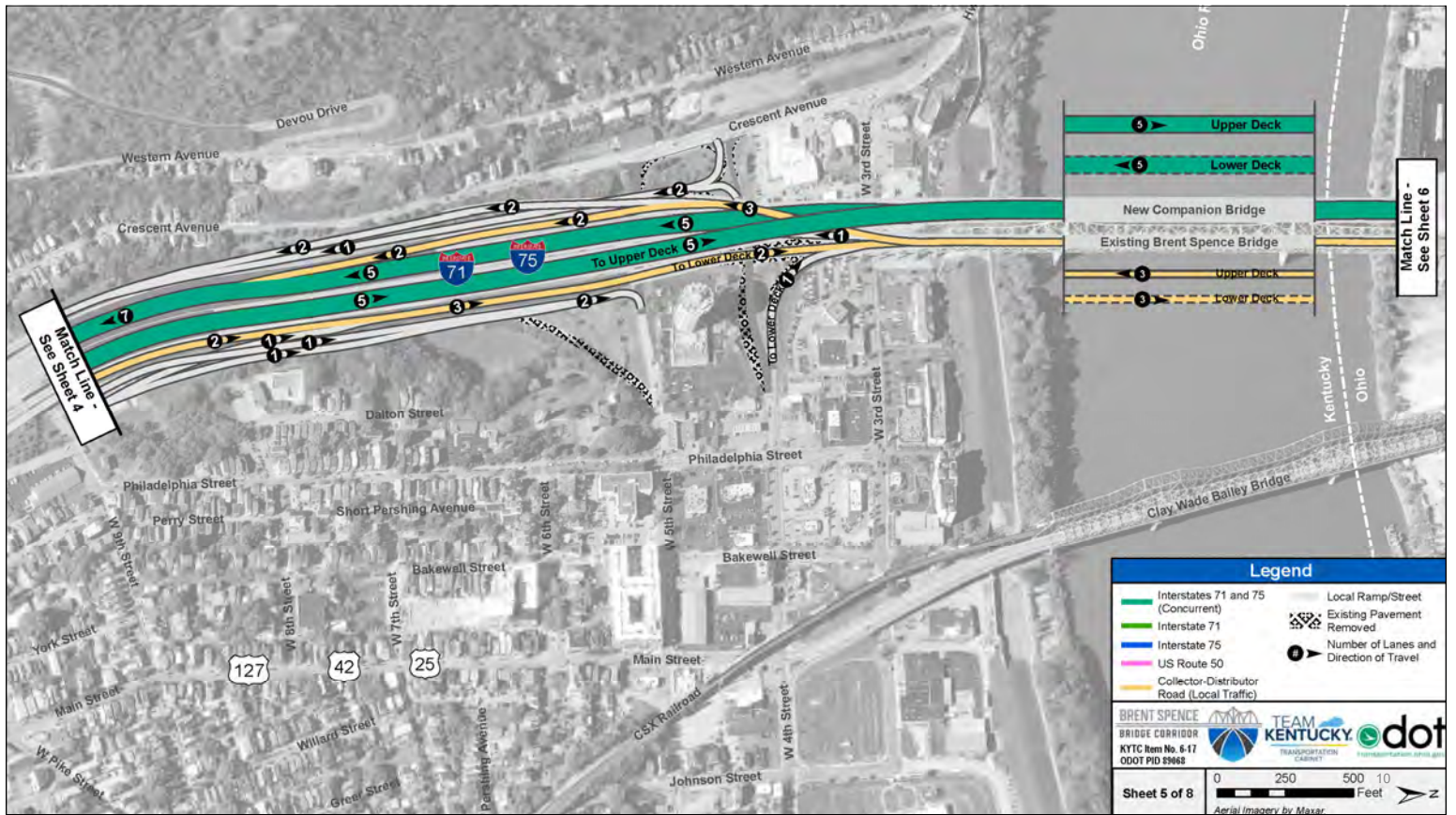
8

As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.

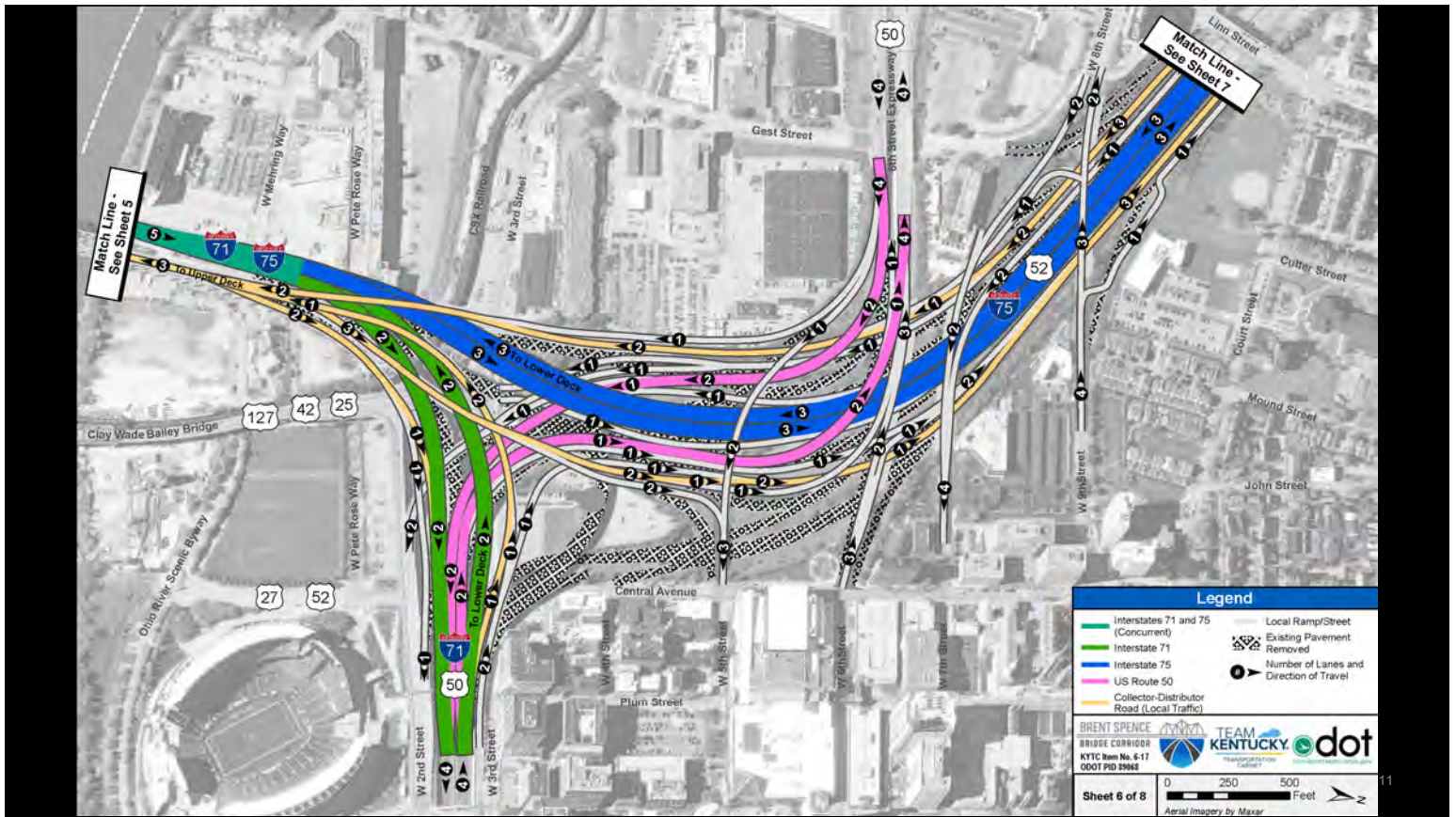


This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.





This slide shows how traffic will move in downtown Cincinnati. The dark blue shows I-75. The dark green shows I-71. The pink shows US-50. The orange shows the collector-distributor roads. The grey shows the local access ramps. The project also provides the opportunity for a direct connection from the Clay Wade Bailey Bridge to I-75.



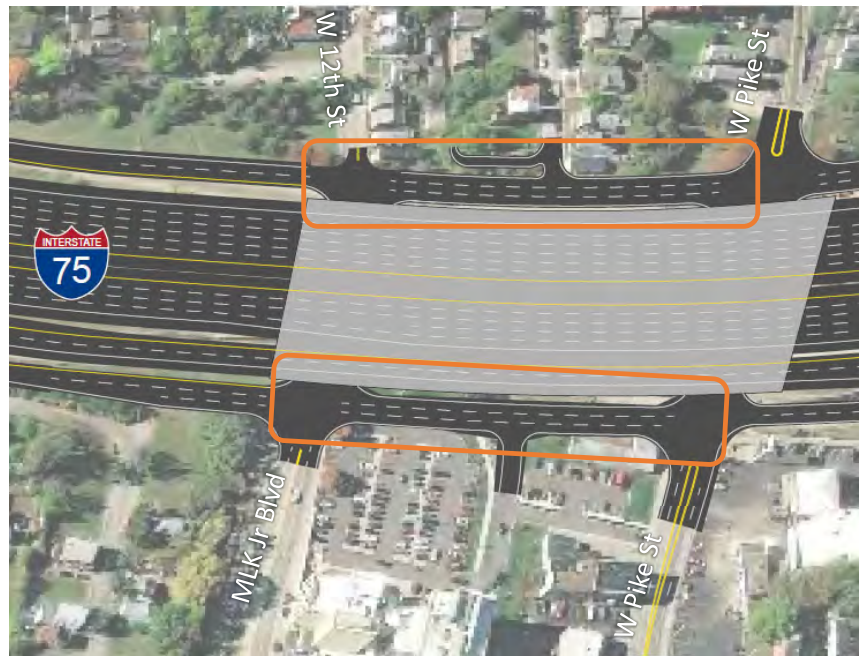
This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.



Here's another view from Covington near Mutter Gottes and the CBD.

# What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 40+ then vs. 4 now
  - Commercial | 6 then vs. 5 now



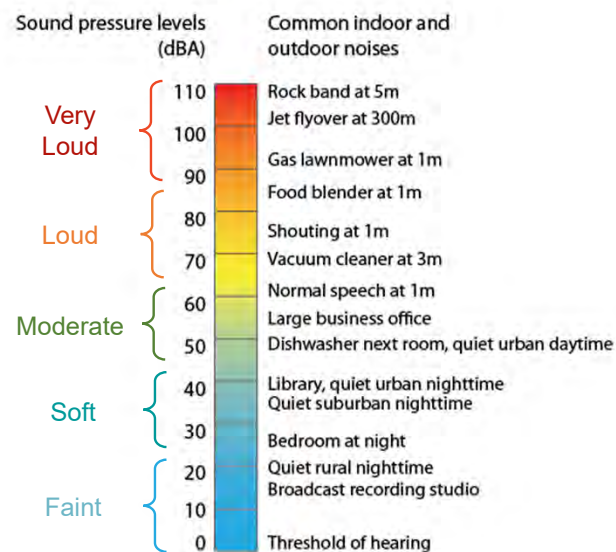
Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound



Source: Minnesota Pollution Control Agency

15

Now let's dig into how the project will impact Mutter Gottes and the CBD, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

## KYTC Noise Policy

- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.

Kentucky Transportation Cabinet  
Department of Highways  
Division of Environmental Analysis



### NOISE ANALYSIS AND ABATEMENT POLICY

APPROVED BY:

\_\_\_\_\_  
Todd Jeter, Division Administrator  
Federal Highway Administration

\_\_\_\_\_  
Date

\_\_\_\_\_  
James Gray, Secretary  
Kentucky Transportation Cabinet

\_\_\_\_\_  
Date

Revised August 1, 2022  
EFFECTIVE DATE JULY 1, 2020  
Revised July 1, 2015  
Original July 2011

16

KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dBA. We also determine if there is a substantial increase – specifically a 10 dBA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, “Can it effectively block noise?” In Kentucky, that means determining if a barrier can provide a minimum 5 dBA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

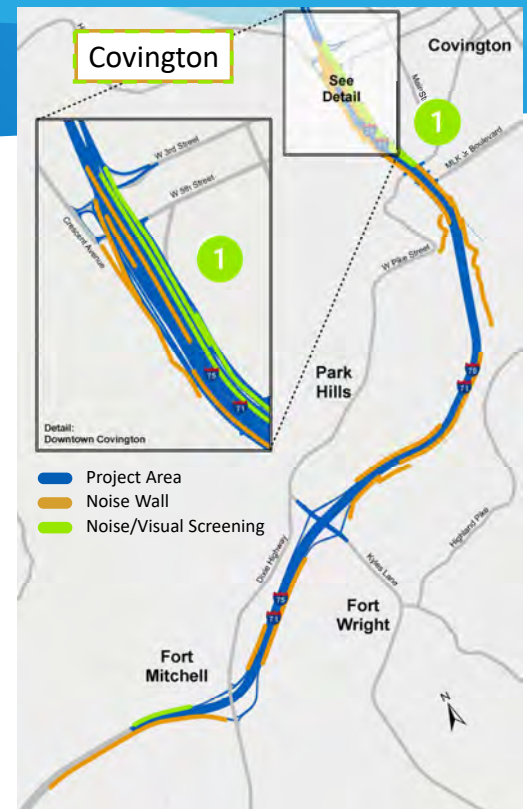
If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

# Noise

## Noise walls not reasonable/feasible per KYTC Noise Policy

- 1 • W. 5th St to Pike St (Mainstrasse)
  - Feasible
  - Not cost effective
  - Did not meet design goals
  - KYTC is evaluating further
  - Without noise wall = 67 dBA
  - With noise wall = 62 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office



Now that we've provided some noise background, let's review how KYTC is addressing traffic noise in the CBD and the areas just west of Mutter Gottes. KYTC evaluated several noise walls and noise wall systems in Covington. Some of them met the feasible and reasonable criteria we just discussed. However, in Mainstrasse and the CBD (just west of Mutter Gottes), a noise wall system benefitting the area labeled with the number 1 was found to be feasible but not reasonable because it was not cost effective and did not meet design goals.

Recognizing that traffic noise is a concern, KYTC is currently conducting a technical study to further evaluate a system of noise/visual screening walls to help provide some noise and to shield views of the highway. KYTC will document their decision about this noise wall system in a technical memo based on the technical feasibility and comments received during this meeting and other outreach activities.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



# Noise



Pike Street Without Noise Wall (looking west)  
71 dBA (average)



Pike Street With Noise Wall (looking west)  
64 dBA (average)

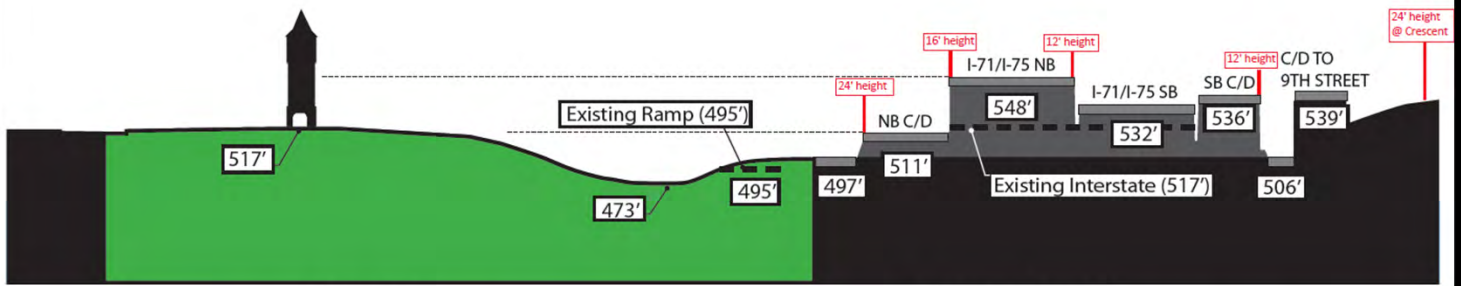
To the left is a drawing showing what the finished project might look like from Pike Street if a noise wall is not built. To the right is a drawing showing what Pike Street will look like with a noise wall. These photos are looking west on Pike Street.

# Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. As we discussed earlier, KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.

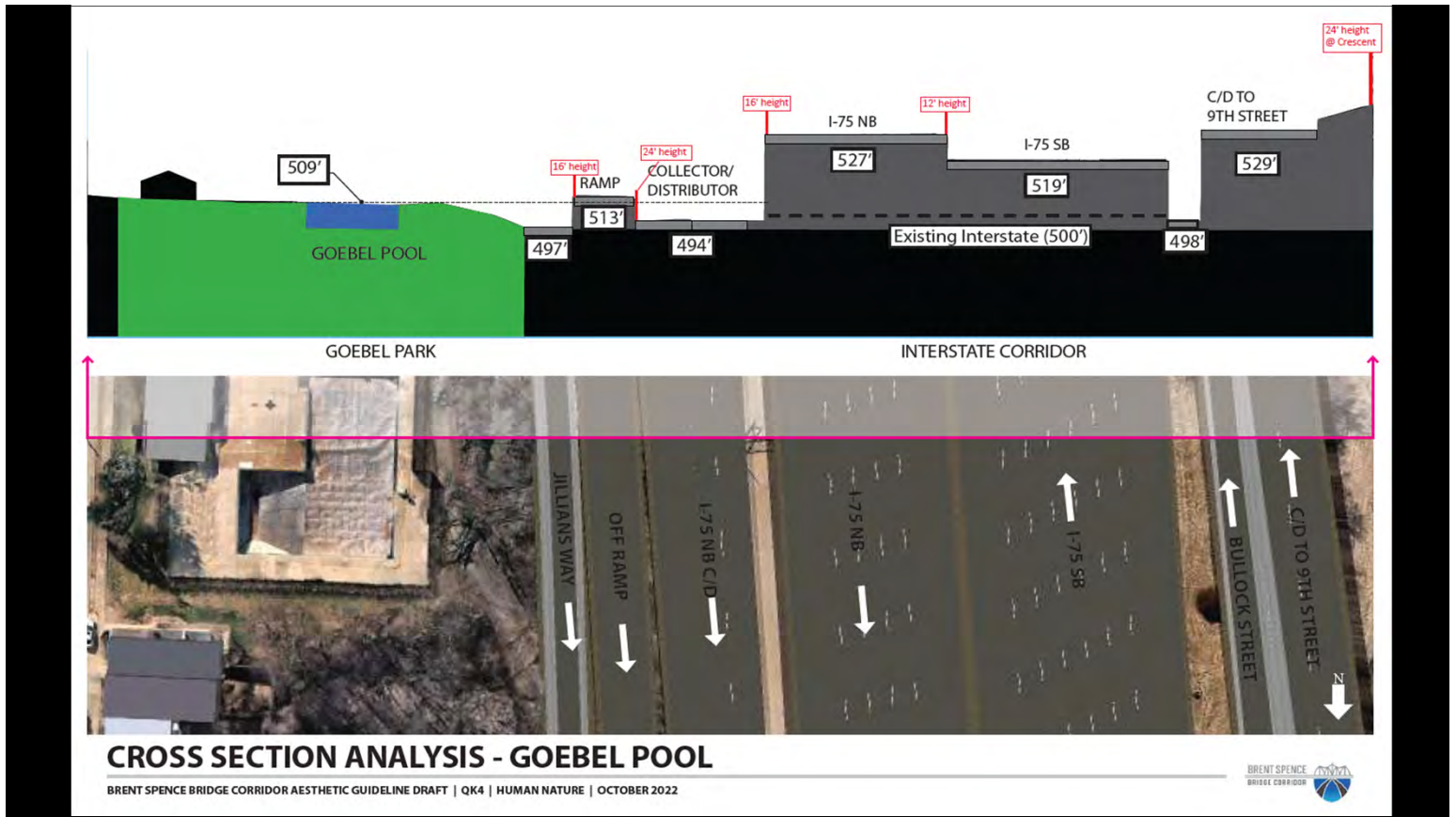


## CROSS SECTION ANALYSIS - CLOCK TOWER

BRENT SPENCE BRIDGE CORRIDOR AESTHETIC GUIDELINE DRAFT | QK4 | HUMAN NATURE | OCTOBER 2022

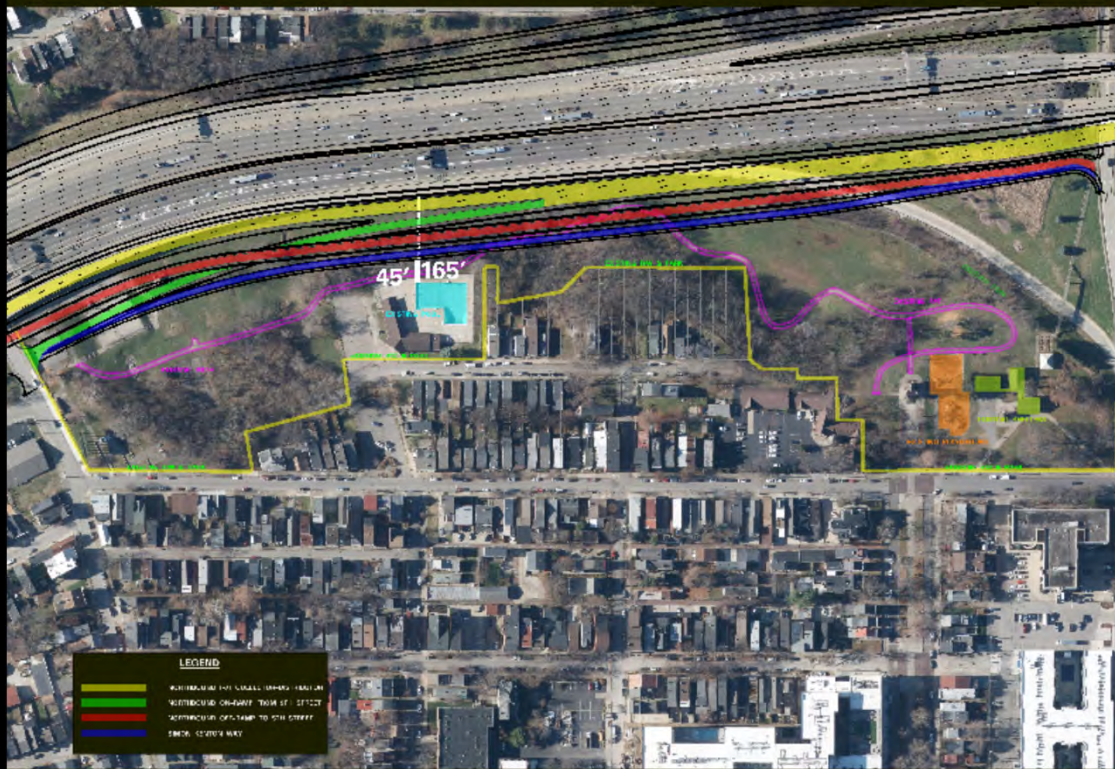


This slide shows a cross-section of the roadway and noise barriers at the clock tower to demonstrate the overall schematic of the roadway and the height of potential noise walls.



This slide shows a cross-section of the roadway and noise barriers at the Goebel pool to demonstrate the overall schematic of the roadway and the height of potential noise walls.

# GOEBEL PARK EXISTING AND PROPOSED PROPERTY MAP WITH 9th TO 5th ST. CONNECTOR



23

KYTC is evaluating two options for providing connectivity in the vicinity of Goebel Park. As currently designed, the project includes a local street connector from West 9<sup>th</sup> Street to West 5<sup>th</sup> Street in the northbound direction to provide an enhanced local street network. This roadway is indicated by the blue line.

Also on the diagram:

Red Line: West 5<sup>th</sup> Street off ramp from the collector-distributor road

Green Line: West 9<sup>th</sup> Street on ramp to the collector-distributor road

Yellow Line: Collector-distributor system

# GOEBEL PARK EXISTING AND PROPOSED PROPERTY MAP WITHOUT 9th TO 5th ST. CONNECTOR



24

This graphic shows what the project would look like without the connector between West 9th Street and West 5th Street.

Still shown on the diagram:

Red Line: West 5<sup>th</sup> Street off ramp from the collector-distributor road

Green Line: West 9<sup>th</sup> Street on ramp to the collector-distributor road

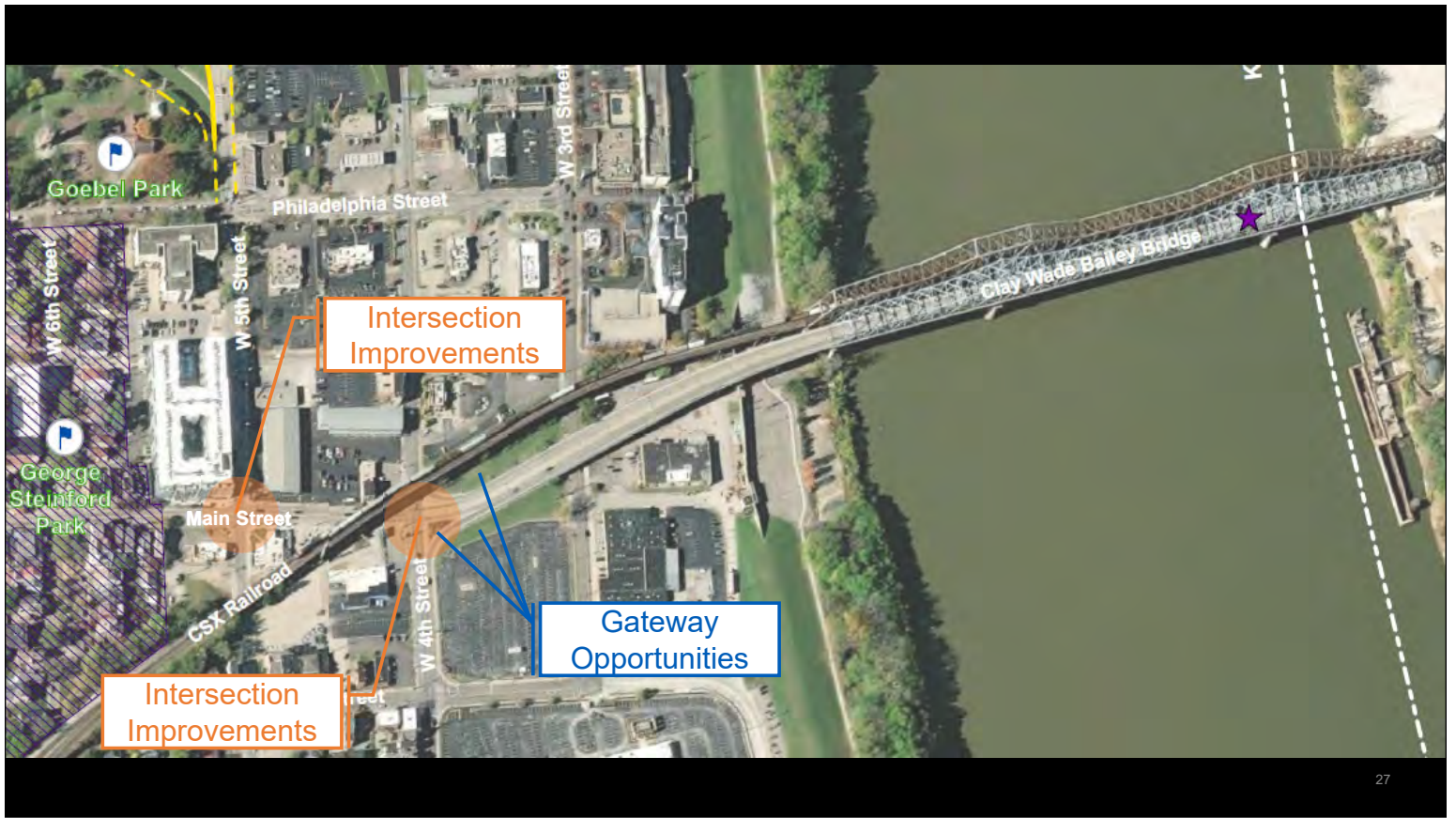
Yellow Line: Collector-distributor system

# Enhancement Measures

- Working with Covington to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, and pedestrian paths

25

In addition to coordinating work in Goebel Park, KYTC is also working with Covington to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.



KYTC is working with the City of Covington to identify improvements to the Main Street intersections with West 5th Street and West 4th Street. KYTC and Covington are also exploring gateway opportunities at the end of the Clay Wade Bailey Bridge.



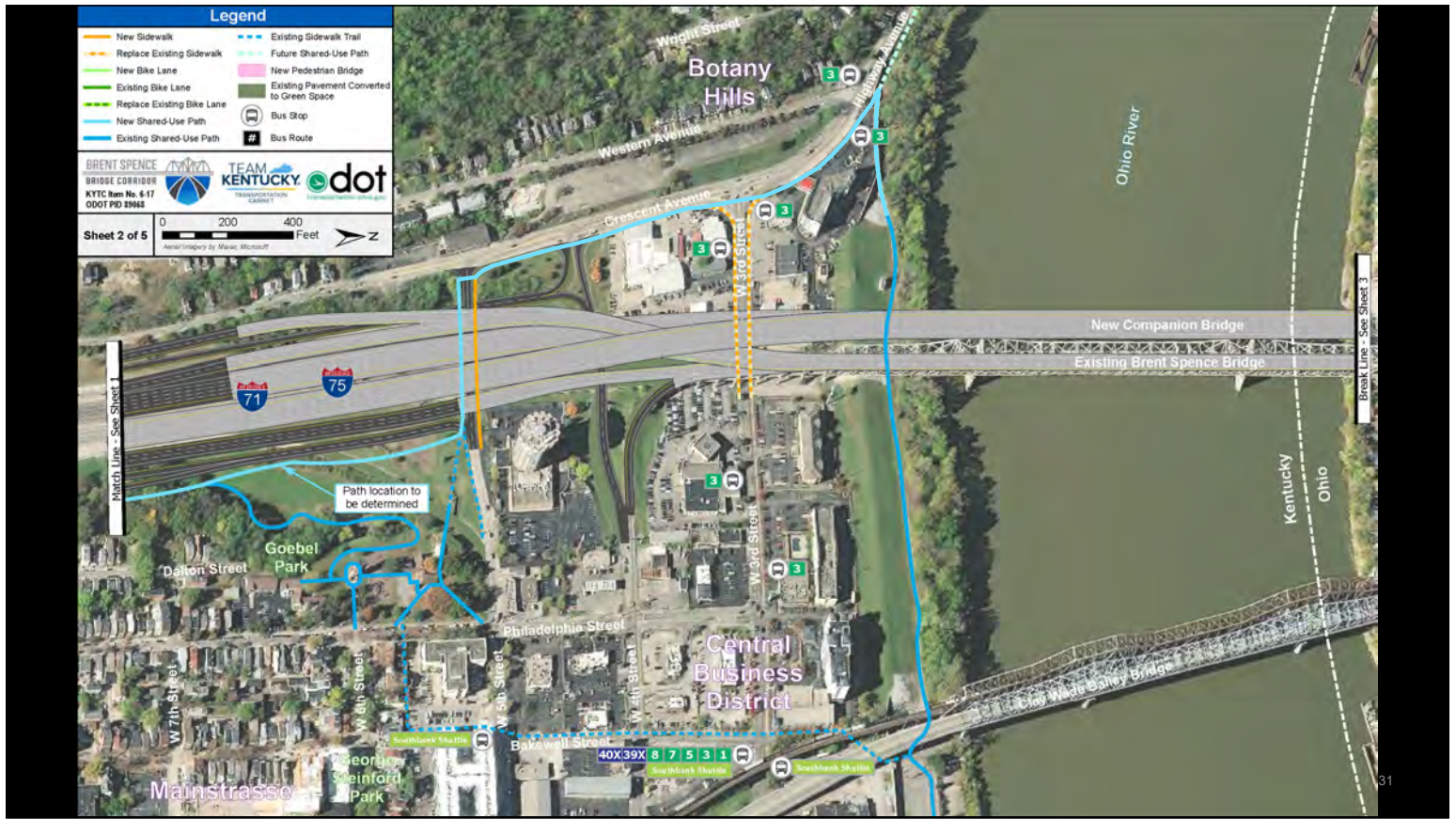
# Enhancement Measures

- Pedestrians and Bicycles
  - Connections across I-71 and I-75
  - New shared use paths
  - New and rebuilt sidewalks

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.



This exhibit shows some of the planned pedestrian and bicycle facilities near Mutter Gottes and the CBD. Note the new shared use paths and sidewalks along the frontage roads. Also notice the new and rebuilt sidewalks under the MLK/West 12th, Pike, and West 9th Street bridges. A new shared use path will be built under the West 9th Street bridge, which will tie into the shared use paths in Goebel Park.



Moving north, note the new shared use path in Goebel Park and under the West 5th Street bridge. The shared use path will be extended along Crescent Avenue to connect to the existing shared use path along the river. Also notice the new and rebuilt sidewalks along West 5th and West 3rd streets. Throughout Covington, the shared use paths and sidewalks will improve connections to existing transit stops.

# Enhancement Measures

- Willow Run Watershed Drainage Improvements
  - Construct a new interstate storm sewer system to remove stormwater runoff from the Willow Run combined sewer system
  - Reduce combined sewer overflows in Goebel Park



32

Existing KYTC is working with the City of Covington and Sanitation District No. 1 to address drainage issue in the project area. Existing highway runoff flows in the combined sewer system in Covington, and overflow events negatively affect residents, including in Goebel Park. KYTC is committed to separating the highway drainage from the combined sewer systems as part of the project. As part of the Brent Spence Bridge Corridor Project, KYTC will be a new separated storm sewer system along the reconstructed interstate. We are not just stiving to separate additional runoff from the combined sewer systems, but we are working to separate all runoff in in Covington, which will reduce overflow events in Goebel Park.

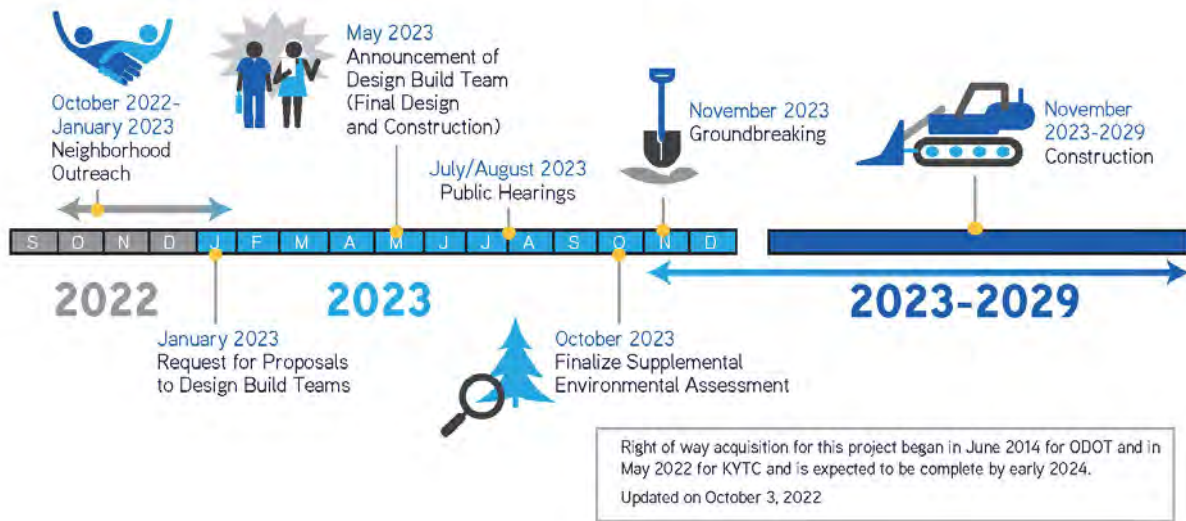


This is a view of what Goebel Park might look like once the highway improvements are constructed, the new shared use paths are built, and agreed upon landscaping is in place. The stormwater improvements should also reduce flooding from the overflows of the combined sewer system. The different roadways are also visible. Farthest left and highest is I-71/I-75. The collector-distributor road is immediately east of the highway and at a lower elevation with a retaining wall. The local street connection to West 5th Street is east of the collector-distributor road and closer to the park elevation.



This view shows how the finished project might appear when looking across I-71 and I-75 toward Mutter Gottes and the CBD.

# Schedule



This slide shows the project schedule from today to the completion of construction.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



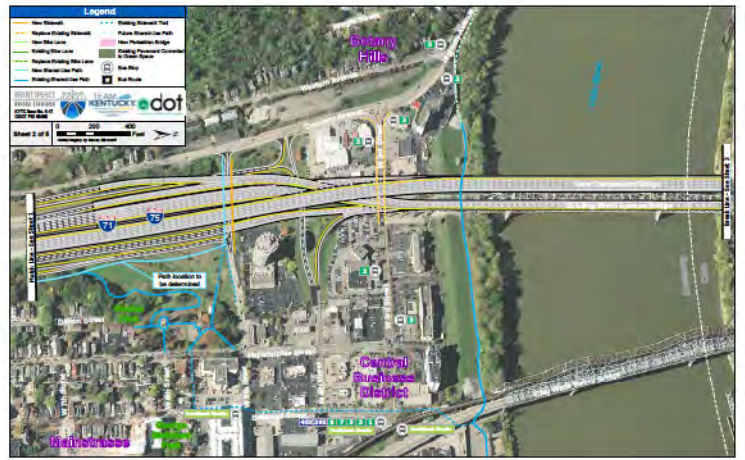
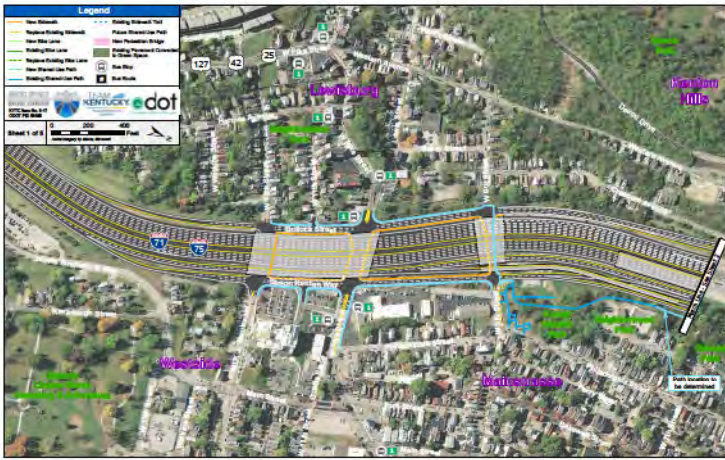
This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.





---

[Attachment 5: Exhibits](#)



**Brent Spence Bridge Corridor (Looking North from Kentucky)**  
 BREIT SPENCE TEAM CONSULTANT  
 TEAM KENTUCKY e dot  
 KYTC Section 9-17 | CDOT PG 06068

**Brent Spence Bridge Corridor - Kentucky (Looking Southeast)**  
 BREIT SPENCE TEAM CONSULTANT  
 TEAM KENTUCKY e dot  
 KYTC Section 9-17 | CDOT PG 06068



**Pike Street With Noise Walls (Looking Northwest) - Kentucky**  
 BREIT SPENCE TEAM CONSULTANT  
 TEAM KENTUCKY e dot  
 KYTC Section 9-17 | CDOT PG 06068

**Goebel Park (Looking North) - Kentucky**  
 BREIT SPENCE TEAM CONSULTANT  
 TEAM KENTUCKY e dot  
 KYTC Section 9-17 | CDOT PG 06068



---

[Attachment 6: Comment Forms](#)

# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

Area Resident     Area business owner or employee     Commuter

Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Interested in historic preservation

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: RETIRED \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

NICE PRESENTATION

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**COMMENTS DUE BY  
January 5, 2023**

KYTC Item No. 6-17 | ODOT PID 89068

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://PublicInput.com/bsbc)
  - By mail using address on back



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

Area Resident     Area business owner or employee     Commuter

Other: NEIGHBORHOOD PRESIDENT

General Comments (Please attach additional pages, if needed):

Community engagement is critical & much as practical will be key  
to adoption. What are projected project cost and tax impact?

If you wish to get in touch with Mattie Gotes Neighborhood  
MOGNA.COVK@gmail.com

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Please protect exits & entrances to & from  
North Caenloger, especially at  
4th & 5th streets

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_  
\_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

*Definitely build the sound walls!!! The*

• Is it possible to increase local bridge traffic to 55 mph?  
45 mph seems too slow for the bridge

• 9<sup>TH</sup> Street already has a speed problem, especially at 9<sup>TH</sup> + Bakewell and 9<sup>TH</sup> + Willard. Can you add stop signs at each intersection to help w/ the added traffic?  
~~Q~~ I'm worried about pedestrian safety on 9<sup>TH</sup>.

• Will the combined Kyles + Dixie exit <sup>(south)</sup> have 2 lanes?  
Kyles exit is already very slow, but maybe that's because of the trucks?

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068





# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

All Cincinnati neighborhoods are affected. Maybe reach out to other neighborhood groups

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

### How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

### What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

### What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: Trade School

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

## How many people live in your household?

- 1 - 2     3 - 5     6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

## What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

## What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

### How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

### What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

### What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Post <sup>invites</sup> in neighborhood Facebook groups ~~and~~ to the public meetings

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

## How many people live in your household?

- 1 - 2     3 - 5     6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

## What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

## What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American
- American Indian or Alaskan Native
- Asian
- Hispanic or Latino
- White
- Native Hawaiian or Other Pacific Islander
- I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English
- Spanish
- Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes
- No
- Not Applicable

### How many people live in your household?

- 1 - 2
- 3 - 5
- 6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18
- 19 - 44
- 45 - 64
- 65 +

### What is your annual household income?

- Less than \$10,000
- \$10,000 - \$24,999
- \$25,000 - \$49,999
- \$50,000 - \$74,999
- \$75,000 - \$99,999
- \$100,000 - \$149,999
- \$150,000 +

### What is the highest level of education completed by members of your household?

- No
- Elementary school
- Middle school
- High school
- College/university
- Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes
- No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518







---

# Brent Spence Bridge Corridor Project Lewisburg and Botany Hills Neighborhood Meeting Summary December 6, 2022

---

## Introduction

The Lewisburg and Botany Hills neighborhood meeting was held on December 6, 2022 from 7:00 pm to 8:30 pm. The meeting was held at the Holiday Inn located at 200 Crescent Avenue in Covington, Kentucky. Flyers advertising the meeting were posted at locations throughout the Lewisburg and Botany Hills neighborhoods. In addition, the City of Covington provided contact information for some local schools, businesses, and residents in both neighborhoods. Flyers advertising the meeting were emailed to these individuals with a request to pass the information to others in their neighborhoods. Finally, a member of the Project Advisory Committee (PAC) who is also a resident of Lewisburg distributed English and Spanish flyers to contacts throughout the neighborhoods (see Attachment 1). Attendees at the meeting included the project team, residents, and business owners. In total, 23 people attended the meeting, including 6 individuals from the project team (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). During the presentation, the project team responded to questions and comments offered by those present. Exhibits showing the project corridor and renderings of what the project will look like once it is built were available for viewing before and after the meeting (see Attachment 5). Members of the project team were available to answer questions and receive feedback while attendees reviewed the exhibits. Written comment forms, a demographic survey, and a postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. One (1) comment form and one (1) demographic survey were returned at the meeting.

## Demographics

The demographic survey that was returned indicated the individual was an older adult (over 65 years of age) but not a member of any other underserved population groups (see Attachment 6).

A write in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. No comments were provided.

## Comments

Attendees posed a number of questions during the course of the presentation/meeting. Table 1 is provided as a summary of the questions and answers.



**Table 1: Q & A from Session**

Question	Project Team Response
Will the access road to the Clay Wade Bailey Bridge be from the express lanes?	Yes, the access to the Clay Wade Bailey Bridge will be provided from I-75.
Can you get on I-75 north from the existing bridge?	Yes. Travelers can follow the collector-distributor system north through downtown Cincinnati to access I-75 north.
How high is a typical noise wall?	Noise walls vary in height. A typical wall might be 18 to 20 feet high. The proposed noise walls in Lewisburg and Botany Hills range from 14 to 24 feet in height.
If a noise wall is not built, why is a fence needed along Crescent Avenue?	A fence is needed for safety to protect pedestrians from the drop off that will be present with the retaining wall along Crescent.
Has there been consideration for transit? Has KYTC coordinated with TANK and other transit agencies for routes on either the new companion or existing Brent Spence Bridge?	TANK is on the Project Advisory Committee (PAC) and has been providing input throughout the course of the project. TANK is ultimately responsible for determining their routing.
Will the West 4 <sup>th</sup> Street ramp be opened again when the project is built? Will the Texas Turnaround be removed? If so, why is it being built?	Yes, the West 4 <sup>th</sup> Street ramp will be opened back up to vehicular traffic once the project is built. The Texas Turnaround will be removed. It is being built as an interim safety project to address safety issues at the West 4 <sup>th</sup> Street ramp. It will also help with maintenance of traffic during construction of the Brent Spence Bridge Corridor Project.
If I'm going from West 4 <sup>th</sup> Street in Covington to Ohio for shopping, what roads would I take?	One option would include taking the local (existing) bridge to I-71 or onto the collector-distributor system to access I-75. Another option would include taking the Clay Wade Bailey Bridge to I-75.

The survey form returned at the meeting did not include any comments. This individual provided contact information and will be added to the project mailing list.

The comment sheets returned at the meeting are included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.



---

Attachment 1: Advertising Materials

## **Lewisburg/Botany Hills Neighborhood Outreach Meeting Advertising**

On November 16, 2022, personnel from HNTB visited the Lewisburg/Botany Hills neighborhood to distribute flyers advertising the outreach meeting scheduled for December 6, 2022 at 7:00pm.

The following businesses/organizations agreed to post a flyer:

- West Covington Baptist Church | 1003 Highway Avenue
- Yabbi's Corner Ethiopian Market | 1255 Parkway
- Liquor City Uncorked | 501 Crescent Avenue
- Save on Cigs | 670 West 3<sup>rd</sup> Street
- DEP's Fine Wine and Spirits | 670 West 3<sup>rd</sup> Street
- Family Grocery | 940 Western Avenue
- Be Concerned: The People's Pantry and Thrift Shop | 1100 West Pike Street

Flyers were left in mailboxes at the following locations:

- St. John Congregational Church | 1235 Highway Avenue
- Covington Fire Department Company 2 | 1252 Parkway Avenue
- St. Ann Roman Catholic Church and School | 1274 Parkway Avenue
- Church of the Living God | 1279 Parkway Avenue
- VFW Post 7099 | 1201 Kelly Furnish Street

---

**From:** Sherry Kish  
**Sent:** Tuesday, November 15, 2022 2:59 PM  
**To:** Sherry Kish  
**Cc:** Seth Turner; Jodi Heflin  
**Subject:** RE: Botany Hills and Lewisburg Community Meeting  
**Attachments:** 11-08-2022\_Invitation Flyer\_Lewisburg and Botany Hills.pdf

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

Good Afternoon!

I wanted to follow back up and let you know that we have scheduled a meeting for the Botany Hills and Lewisburg neighborhoods on December 6<sup>th</sup> at the Holiday Inn Express. I've attached a flyer for the meeting in case you are able to share it with residents from these two neighborhoods. Suggested sharing is with neighborhood email lists, PTA/PTO members or posting it in popular businesses. Jodi Heflin on our project team is going to be asking a few businesses to post this flyer in their location tomorrow morning – just in case you see one 😊

Any help you can provide, especially attending, is greatly appreciated!

Thanks,  
Sherry

**Sherry R. Kish**

---

**From:** Sherry Kish  
**Sent:** Monday, October 24, 2022 12:02 PM  
**To:** Sherry Kish  
**Subject:** Botany Hills and Lewisburg Community Meeting

Good Afternoon!

I received your contact information from the Center for Great Neighborhoods as residents of either the Lewisburg or Botany Hills neighborhoods. HNTB has been contracted by the Kentucky Transportation Cabinet and the Ohio Department of Transportation and to assist with public outreach for the Brent Spence Bridge project. We are looking to schedule a meeting with your neighborhood's residents the week of December 5<sup>th</sup> to help provide an overview of the project and host a community conversation to give residents an opportunity to ask questions or share comments. We are reaching out for help as we set these meetings. Can you help us with the following areas:

1. Share if is an existing neighborhood group meeting that we can join (even if it's not during the week of December 5<sup>th</sup>)
2. Suggest an appropriate location to host a neighborhood meeting (maybe 25-50 people)
3. Identify the best ways to advertise the meeting to the Lewisburg and Botany Hills residents

We appreciate any information you can share about your neighborhood so we can make sure this important information is available to your neighbors. Finally, please let me know which neighborhood you belong to when you respond.

Thanks,  
Sherry

**Sherry R. Kish**

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



**TEAM**  
**KENTUCKY**  
TRANSPORTATION  
CABINET



**INVESTING IN LOCAL COMMUNITIES.  
GROWING AMERICA'S ECONOMY.**

## You're Invited

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of **neighborhood outreach meetings** for the Brent Spence Bridge (BSB) Corridor Project

**Purpose:** To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team

**When:** December 6, 2022 | 7:00pm

**Where:** Holiday Inn Express  
200 Crescent Avenue, Covington, KY 41011

## Hope to see you there!

If you need assistance attending this meeting, please call Stacey Hans at [\(859\) 341-2700](tel:8593412700).

View the most recent project newsletter at the following link:  
<https://mailchi.mp/aae28ef3744d/brent-spence-bridge-e-newsletter-16606655>

BRENT SPENCE  
BRIDGE CORRIDOR



TEAM  
KENTUCKY  
TRANSPORTATION  
CABINET



INVIRTIENDO EN LAS COMUNIDADES LOCALES.  
HACIENDO CRECER LA ECONOMÍA ESTADOUNIDENSE.

## Invitación

El Departamento de Transporte de Ohio (ODOT) y el Gabinete de Transporte de Kentucky (KYTC) están llevando a cabo una serie de reuniones vecinales para el Proyecto del corredor del puente Brent Spence

**Objetivo:** El propósito de las reuniones será presentar los planes actuales para el corredor del puente Brent Spence y obtener comentarios sobre cómo el proyecto puede afectar a las personas que viven cerca del área del proyecto.

**Fecha y hora:** el 6 diciembre de 2022 | 7:00pm

**Ubicación:** Holiday Inn Express  
200 Crescent Avenue, Covington, KY 41011

## Esperamos verte ahí!

Si necesita ayuda para asistir a esta reunión, llame a Stacey Hans al [\(859\) 341-2700](tel:8593412700).

Boletín electrónico del corredor del puente Brent Spence:  
<https://mailchi.mp/aae28ef3744d/brent-spence-bridge-e-newsletter-16606655>



---

Attachment 2: Sign-In Sheet





**BRENT SPENCE BRIDGE CORRIDOR PROJECT**

Neighborhood Outreach Meeting – December 6, 2022

Name	Representing	Address	Phone	Email
1. John Bealy & Tim Schuch	Lewisburg Properties			
2. Johnny Arnold	ODOT DB			
3. Scott Aeffler	AMTB			
4. Mark Becker	AMTB			
5. Mour Dior	FHWA-KY			
6. Nick Popow	Boats Charters			
7. Le Rubin	to			
8. Don Corkey Jr	CORKEN STEEL			
9. Esther Johnson	Pauline Boyd			
10. Deb + Greg Reder	DC Morrison			
11. Paul Corkey	Corkey Feed			
12. Steven Hall	Lewisburg			
13. Rums Kelly	Lewisburg			
14. Chri a Popow	Lewisburg			
15. DAVID Hoge	Lewisburg			
16. JAMES DILLON	Lewisburg			
17. Nick Wengren	Lewisburg			
18. JOSHUA MARTIN	POWERTON			
19. Julie Under	KYTC			
20. Stacey Hayes	to KYTC			
21. Gary Valentine				

Please print clearly. Contact information is optional but must be included if you wish to receive updates.



Attachment 3: Photographs





---

[Attachment 4: Presentation](#)

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



# Lewisburg and Botany Hills Neighborhood Meeting (KY)

December 6, 2022



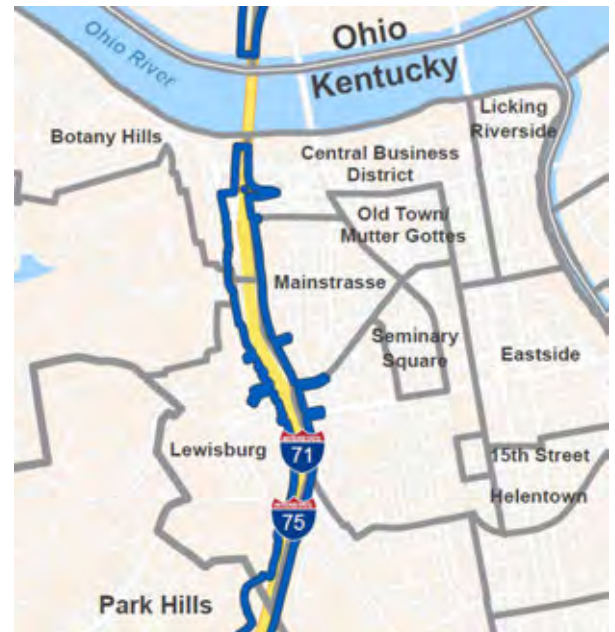
INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with remarks by the City of Covington (5 min) and introductions of the Project Team in attendance (2 min).

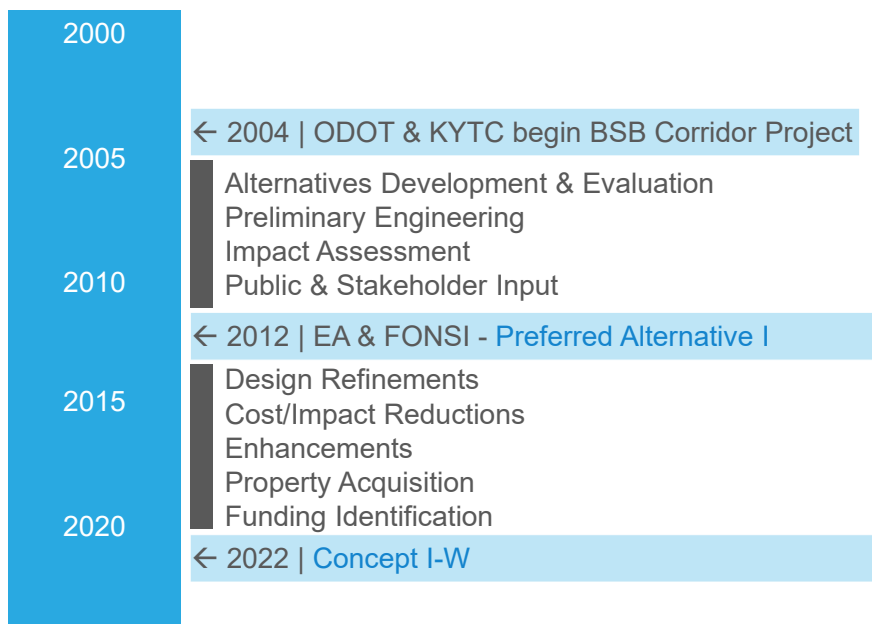
# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the Lewisburg and Botany Hills neighborhoods the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Lewisburg/Botany Hills area
  - Discussion/feedback from Lewisburg/Botany Hills residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Lewisburg and Botany Hills neighborhoods. Most importantly, tonight we're focusing on hearing from the residents of Lewisburg and Botany Hills. We're here to answer your questions and to listen to your feedback about the project.

# Project History



For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2022, KYTC has begun purchasing the land needed to build the project.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.



# Project Description

## Kentucky

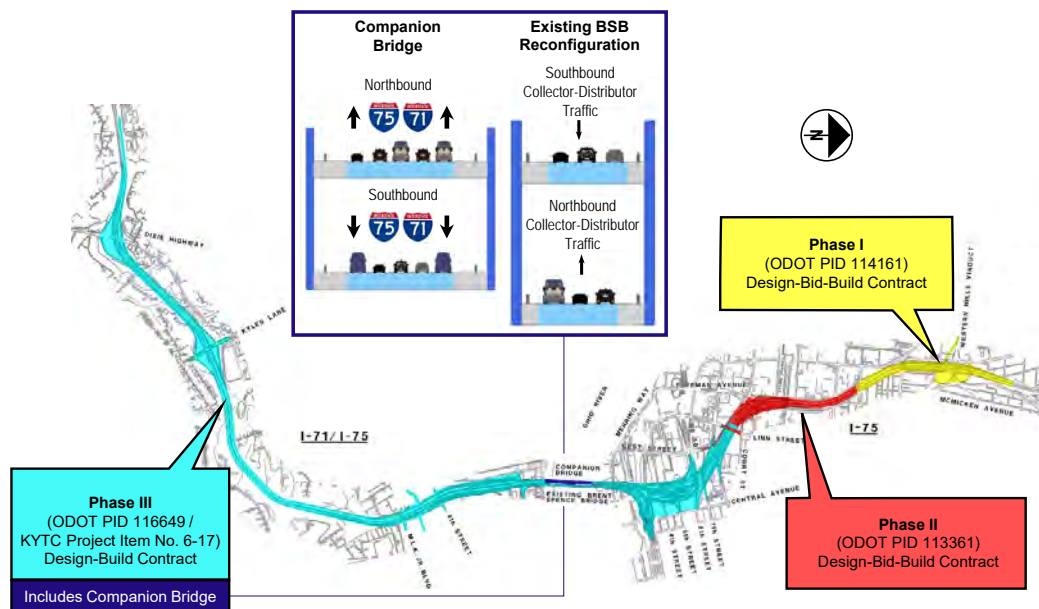
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles Lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

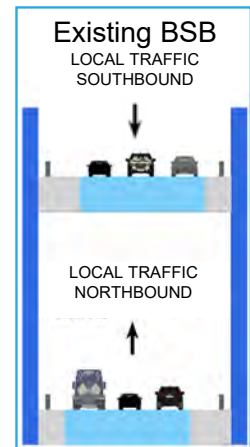
# Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

# What Has Changed?

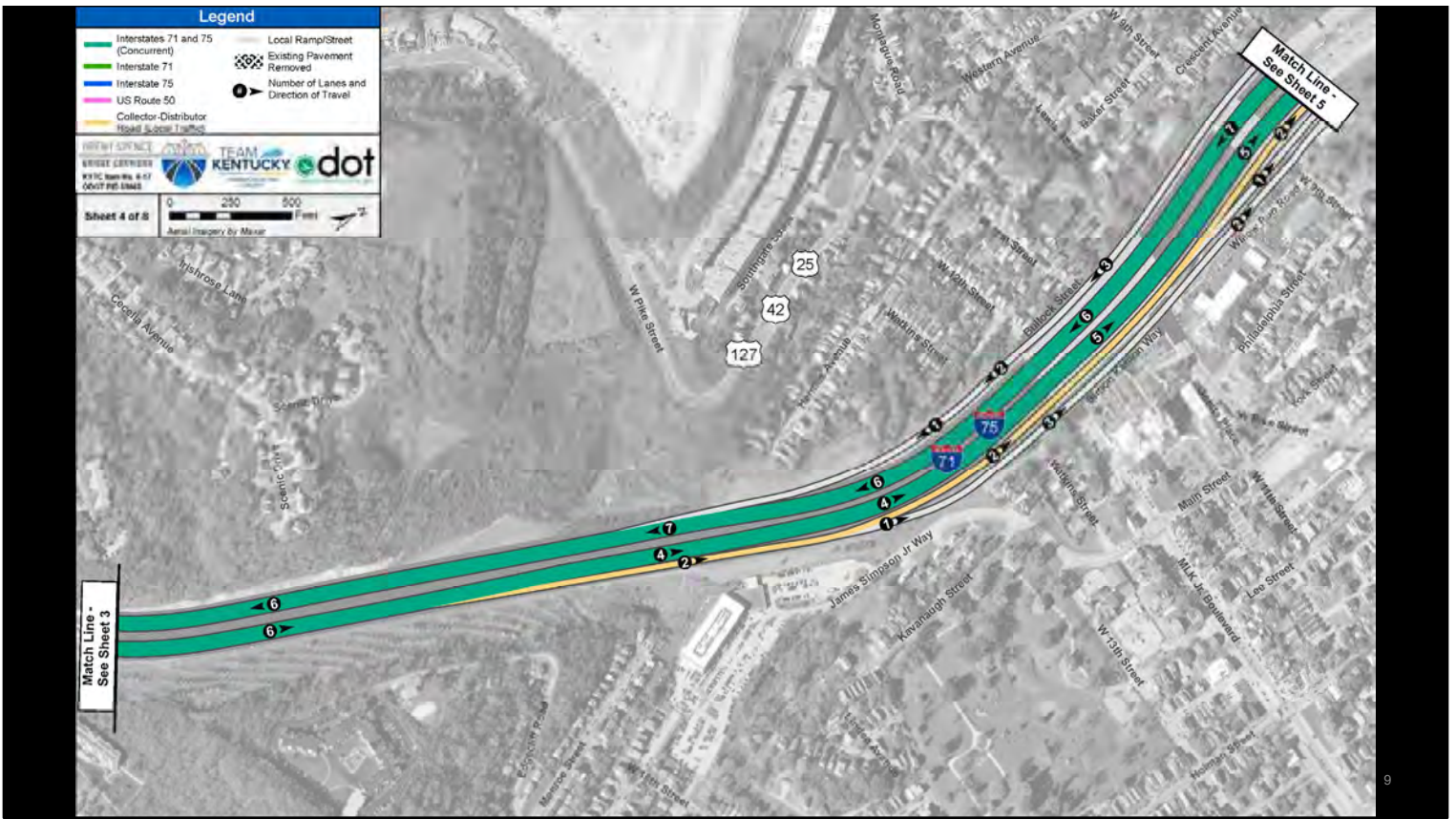
- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



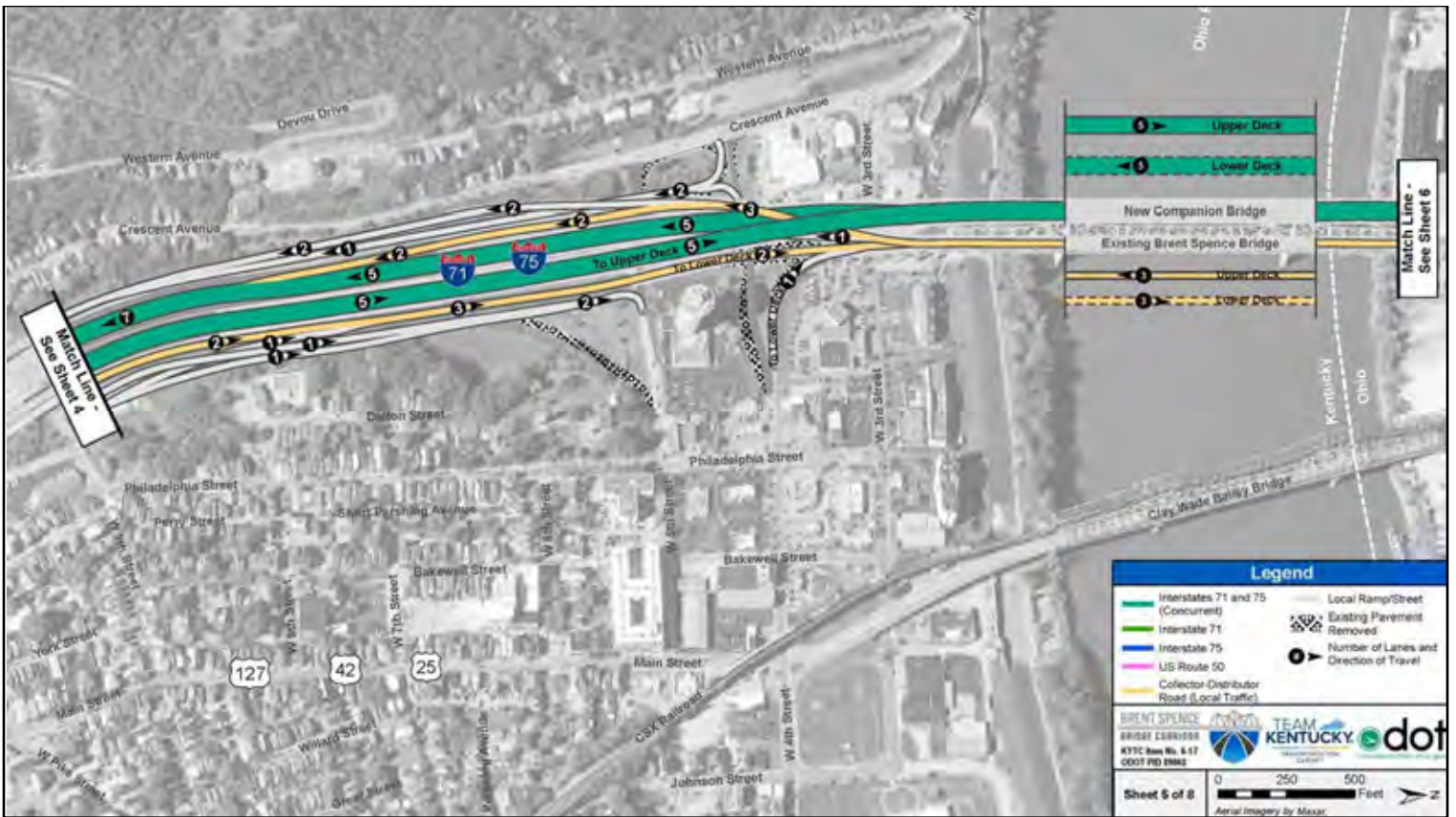
8

As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

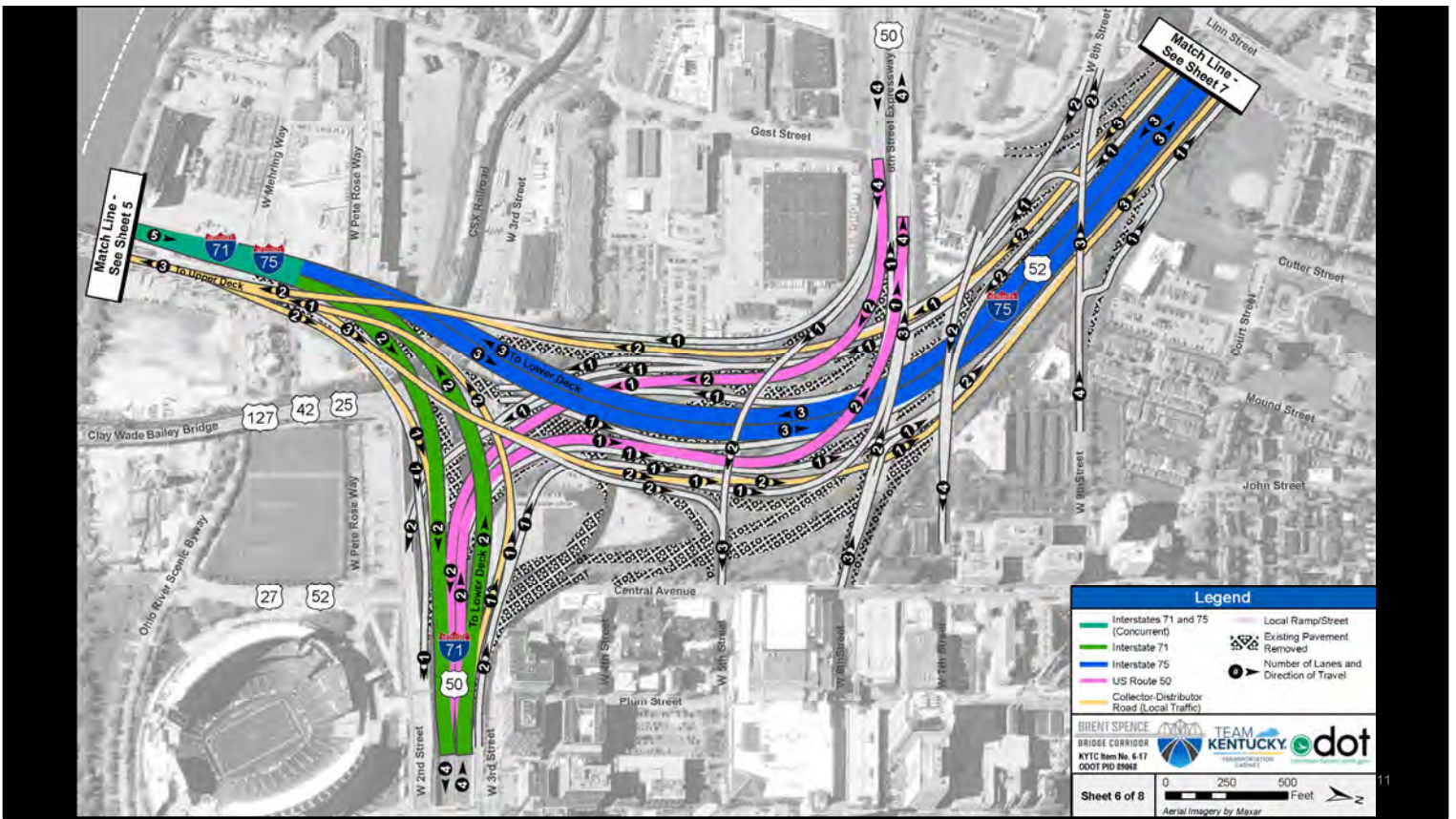
Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will move in downtown Cincinnati. The dark blue shows I-75. The dark green shows I-71. The pink shows US-50. The orange shows the collector-distributor roads. The grey shows the local access ramps.



This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

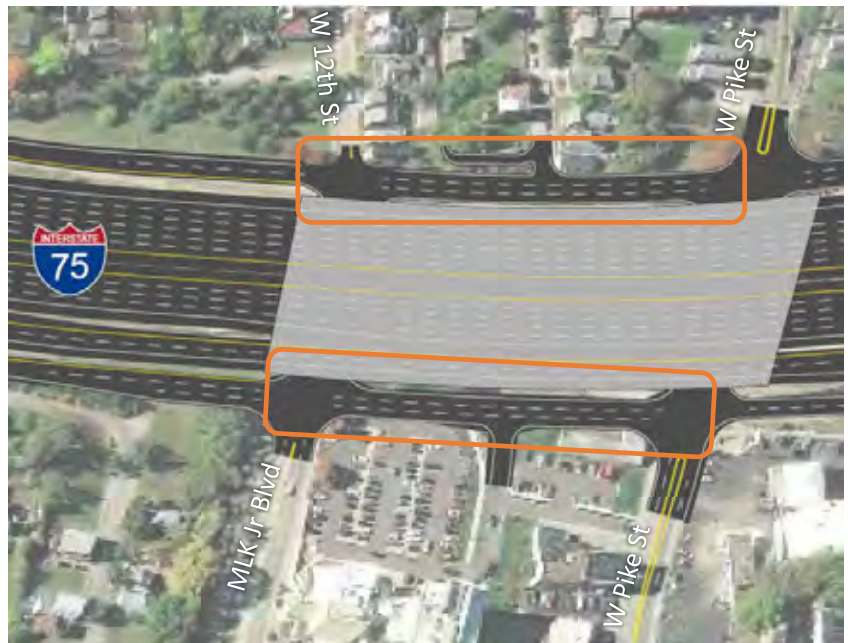


Here's another view from Covington near Lewisburg and Botany Hills.



# What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 40+ then vs. 4 now
  - Commercial | 6 then vs. 5 now

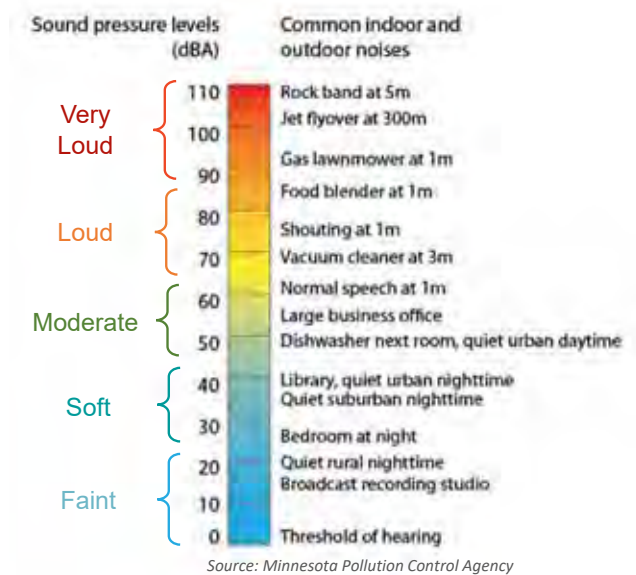


Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet. In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound



15

Now let's dig into how the project will impact Lewisburg and Botany Hills, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

## KYTC Noise Policy

- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.



16

KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dbA. We also determine if there is a substantial increase – specifically a 10 dbA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dbA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

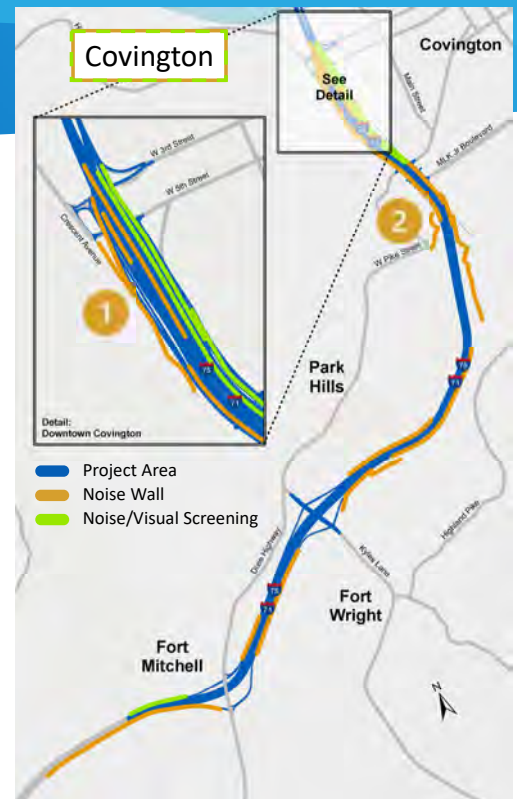
If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.

# Noise

## Noise walls reasonable/feasible per KYTC Noise Policy

- 1 • Western Ave & Crescent Ave (Botany Hills/Lewisburg)
  - Without noise wall = 70 dBA
  - With noise wall = 67 dBA
- 2 • Lewisburg Historic District (Lewisburg)
  - Without noise wall = 70 dBA
  - With noise wall = 64 dBA

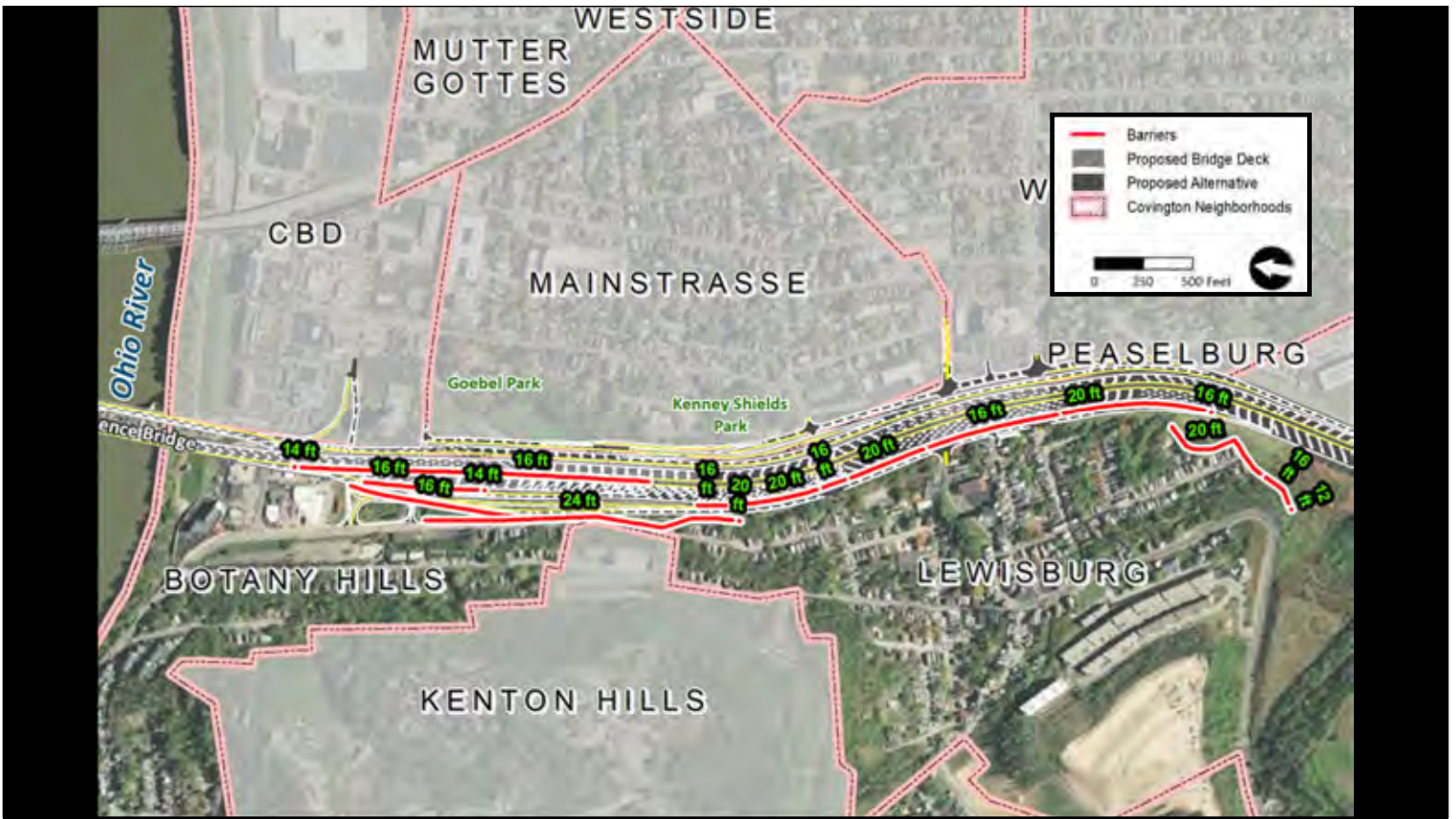
Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office



In the Botany Hills/Lewisburg neighborhoods, a noise wall system (shown by the number 1) was found to be reasonable and feasible. It includes several noise walls along Crescent Avenue, the Interstate, and a ramp to reduce noise for residencies along Western and Crescent avenues. The average exterior noise levels are predicted to be around 70 decibels without the noise walls and 67 decibels with the noise walls. These sound levels are averages for all the people who will benefit from the walls. People living closer to the highway will have higher noise levels but will also experience larger noise reductions if a noise wall is built.

The noise wall system extends further south through the Lewisburg Historic District along the Interstate (shown by the number 2). In this area, the average exterior noise levels are predicted to be 70 decibels without noise walls and about 64 decibels with noise walls.

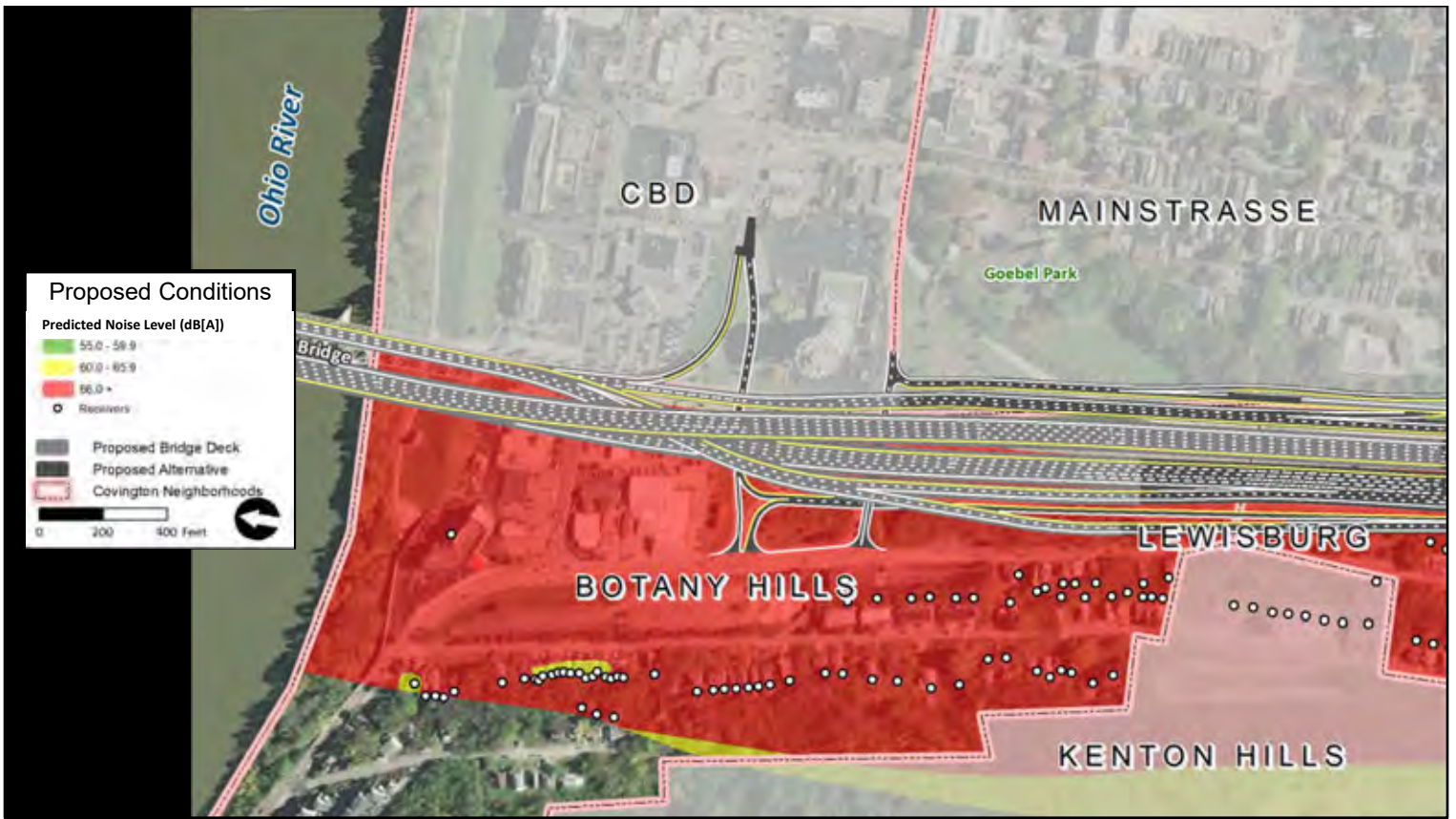
For reference, the average exterior sound levels with noise walls in west Covington are predicted to be near what you'd experience in normal conversation.



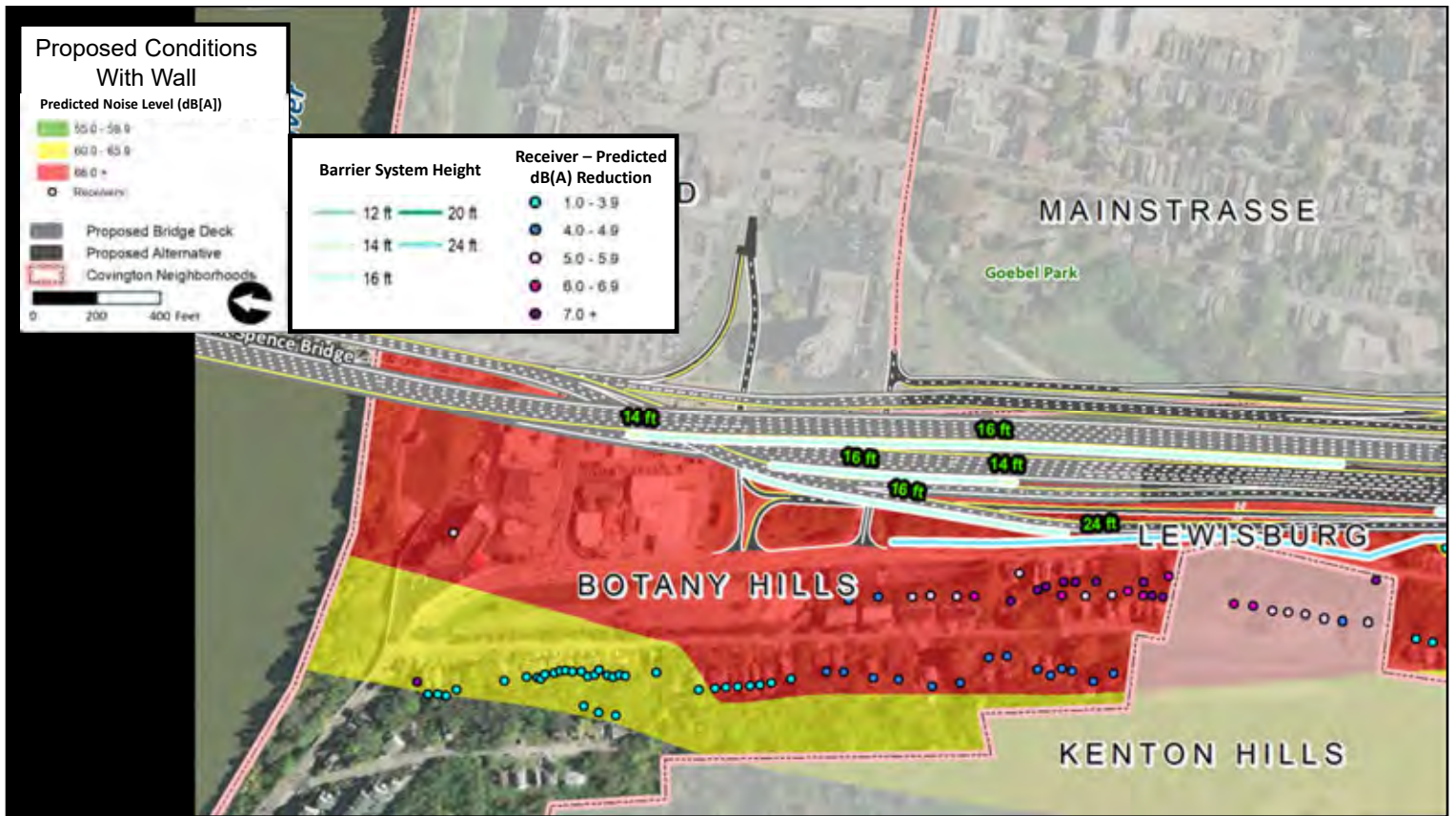
The red lines on this slide show noise walls that were evaluated as part of the noise study for northern Lewisburg and Botany Hills. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

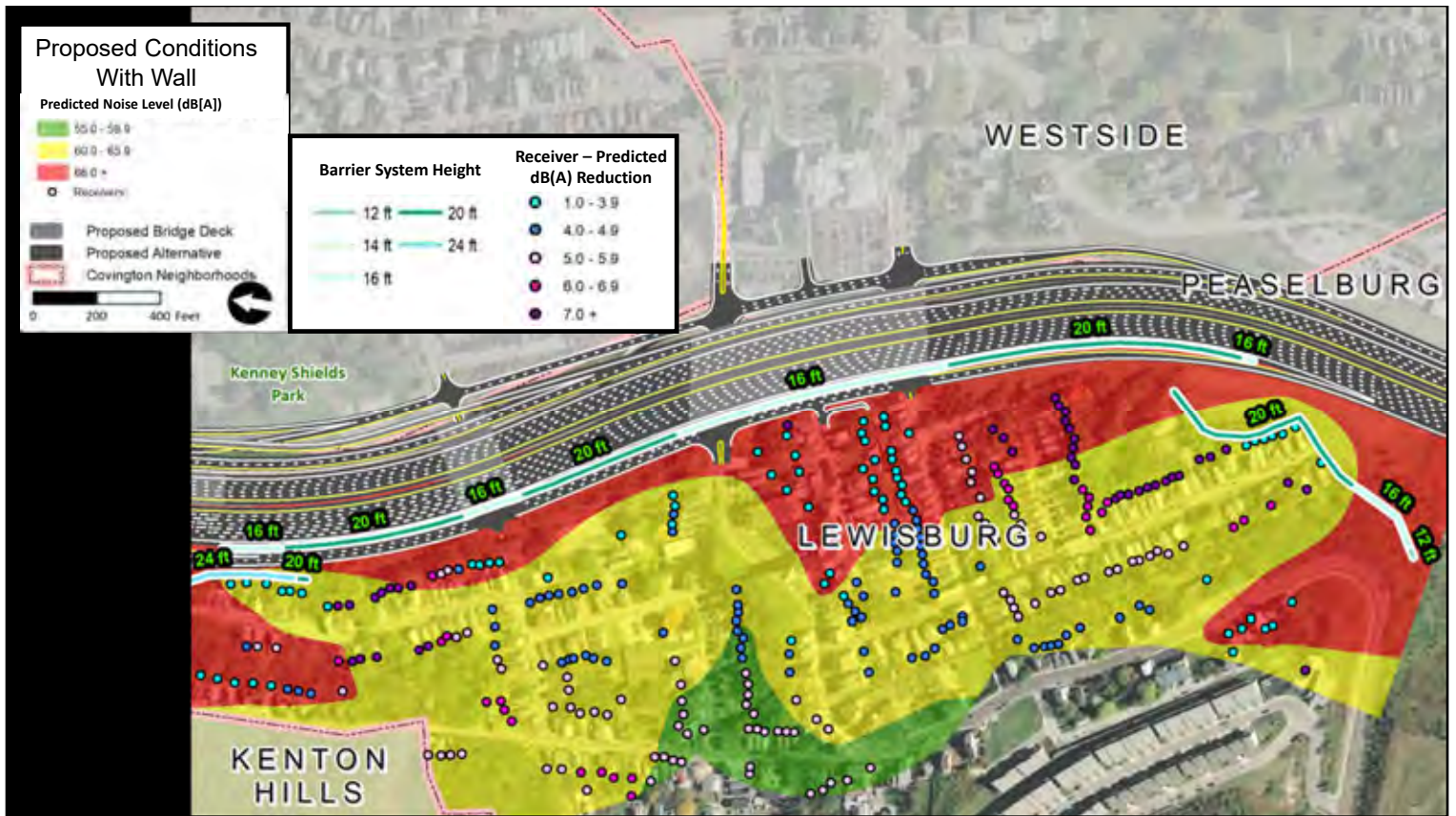




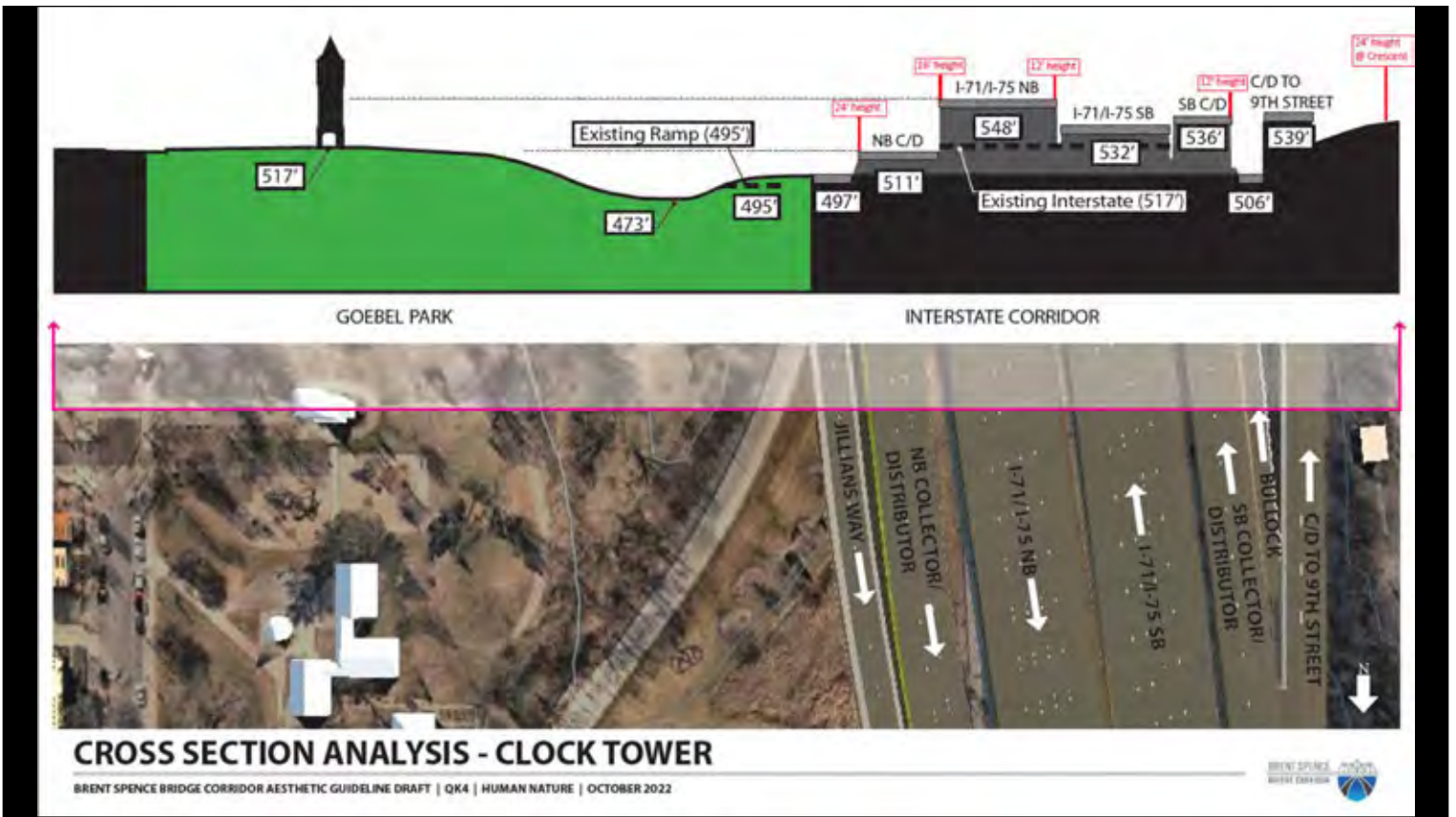
This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



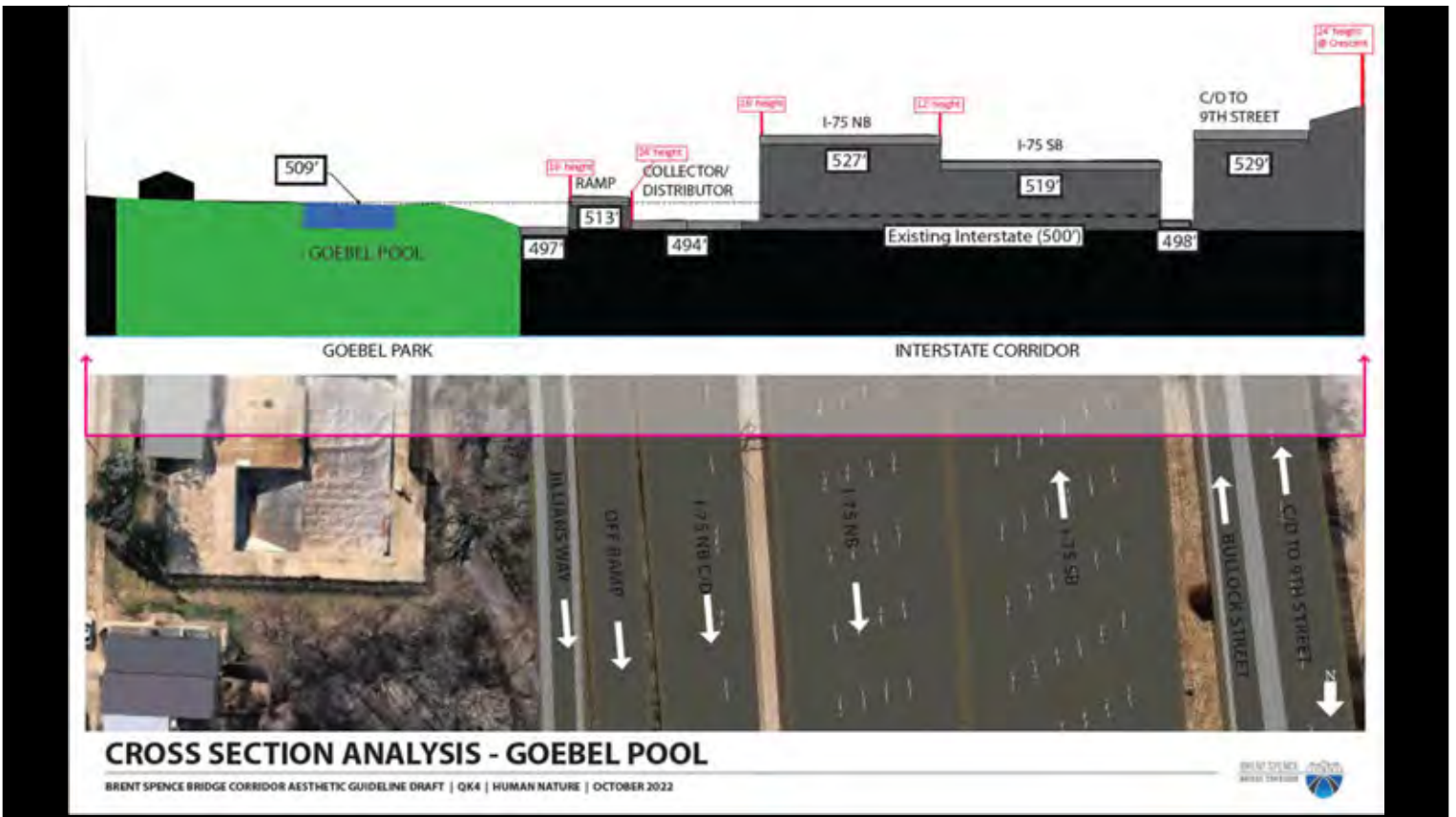
This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



This slide shows a cross-section of the roadway and noise barriers at the clock tower to demonstrate the overall schematic of the roadway and the height of potential noise walls.



This slide shows a cross-section of the roadway and noise barriers at the Goebel pool to demonstrate the overall schematic of the roadway and the height of potential noise walls.

# Noise



Pike Street Without Noise Wall (looking east)  
71 dBA (average)



Pike Street With Noise Wall (looking east)  
64 dBA (average)

To the left is a drawing showing what the finished project might look like at Pike Street if a noise wall is not built. To the right is a drawing showing what Pike Street might look like with a noise wall. These photos are looking east on Pike Street.

# Noise



Crescent Avenue Without Noise Wall  
70 dBA (average)



Crescent Avenue With Noise Wall  
67 dBA (average)

28

To the left is a drawing showing what the finished project will look like from Crescent Avenue if a noise wall is not built. To the right is a drawing showing what Crescent Avenue will look like with a noise wall. These views are looking north.

# Noise



Crescent Avenue Without Noise Wall  
70 dBA (average)



Crescent Avenue With Noise Wall  
67 dBA (average)

29

Here are more views showing what Crescent Avenue will look like with and without noise walls. These views are looking south.



# Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. As we discussed earlier, KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.

# Enhancement Measures

- Working with Covington to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, and pedestrian paths
- Pedestrians and Bicycles
  - Connections across I-71 and I-75
  - New shared use paths
  - New and rebuilt sidewalks

31

In addition to coordinating work in Goebel Park, KYTC is also working with Covington to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.



This exhibit shows some of the planned pedestrian and bicycle facilities near Lewisburg and Botany Hills. Note the new shared use paths and sidewalks along the frontage roads. Also notice the new and rebuilt sidewalks under the MLK/West 12th, Pike, and West 9th Street bridges. A new shared use path will be built under the West 9th Street bridge, which will tie into the shared use paths in Goebel Park.



Moving north, note the new shared use path in Goebel Park and under the West 5th Street bridge. Also notice the new and rebuilt sidewalks along West 5th and West 3rd streets. Throughout Covington, the shared use paths and sidewalks will improve connections to existing transit stops.



This is a view of what Goebel Park might look like once the highway improvements are constructed, the new shared use paths are built, and agreed upon landscaping is in place.



This view shows how the finished project might appear looking down along Botany Hills and Lewisburg.

# Schedule



This slide shows the project schedule from today to the completion of construction.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.





---

[Attachment 5: Exhibits](#)



Brent Spence Bridge Corridor - Kentucky  
(Looking Southeast)



KYTC Item No. 8-17 | CDOT PD 8888



Pike Street Without Noise Wall (Looking West) - Kentucky



KYTC Item No. 8-17 | CDOT PD 8888






Pike Street With Noise Wall (Looking West) - Kentucky



KYTC Item No. 8-17 | CDOT PD 8888




**Pike Street Without Noise Wall (Looking East) - Kentucky**

  
KYTC Item No. 6-17 | ODOT PD 898M




**Pike Street With Noise Wall (Looking East) - Kentucky**

  
KYTC Item No. 6-17 | ODOT PD 898M




**Crescent Avenue Without Noise Wall - Kentucky (Looking South)**


  
KYTC Item No. 6-17 | ODOT PD 898M

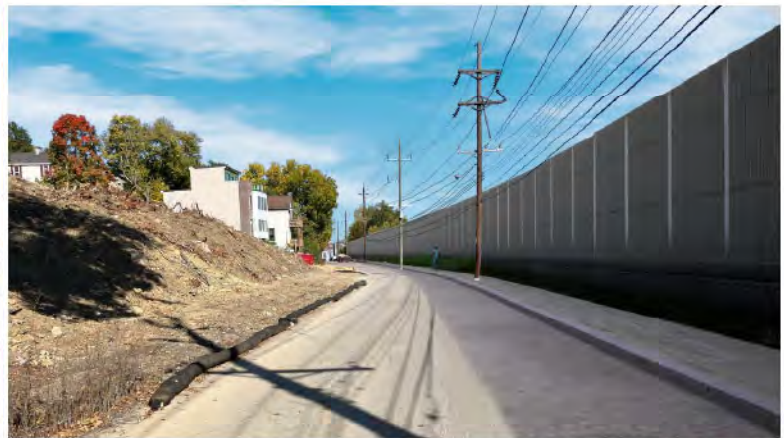



**Crescent Avenue With Noise Wall - Kentucky (Looking South)**

  
KYTC Item No. 6-17 | ODOT PD 898M




**Crescent Avenue Without Noise Wall - Kentucky (Looking North)**

  
KYTC Item No. 6-17 | ODOT PD 898M




**Crescent Avenue With Noise Wall - Kentucky (Looking North)**

  
KYTC Item No. 6-17 | ODOT PD 898M



---

Attachment 6: Comment Forms

# COMMENT FORM

BRENT SPENCE  
BRIDGE CORRIDOR



Contact information is not required but will ensure you receive a response, should one be required.

Name: DON CORKEN, SR

Mailing address (or nearest cross streets): [REDACTED]  
COV. 41011

Email address: [REDACTED] Phone: [REDACTED]

Business/Organization Name: CORKEN STEEL

Business/Organization Address: [REDACTED]

What is your interest in the proposed project? (Select all that apply.)

- Area Resident    
  Area business owner or employee    
  Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

---

---

---

---

---

---

---

---

---

---

---

---

---

---

---

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:**
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](https://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068

# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



---

# Brent Spence Bridge Corridor Project Camp Washington Meeting Summary December 12, 2022

---

## Introduction

The Camp Washington meeting was held on December 12, 2022 from 7:00 pm to 8:30 pm at the Camp Washington Urban Revitalization Corporation (2951 Sidney Avenue in Cincinnati, Ohio). It was publicized through the Camp Washington Community Council email distribution list (Attachment 1). Attendees at the meeting included the City of Cincinnati, the project team, members of the Camp Washington Community Council, and members of the public. In total, nine people from the public attended the meeting (Attachment 2), as well as multiple individuals from the Ohio Department of Transportation (ODOT), the City of Cincinnati, and the project team. The presentation was also available for virtual viewing via Zoom. Photographs are included in Attachment 3.

The meeting format included a formal presentation by ODOT (Attachment 4). After the presentation, the project team responded to questions and comments offered by those present.

Exhibits showing the renderings of Ezzard Charles Drive and the corridor were available for viewing before and during the meeting (Attachment 5). Written comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. One comment form was returned at the meeting (Attachment 6). No demographic surveys were returned at the meeting.

## Comments

The written comment received at the meeting asked whether project will leave Camp Washington roads destroyed once construction is complete and sought a commitment that these roads will be restored. The comment also asked about the tree canopy removal at Western Hills Viaduct and along the loop ramp that will be removed.

Below is a summary of comments/questions received during presentation:

- What are the traffic volumes for the project corridor?
  - The exact traffic volumes were unavailable during the presentation. ODOT is currently updating the traffic volumes for the corridor to reflect the design year 2049. That data will be made available to the public once it is complete.
- How many interchanges will there be near the neighborhood?
  - There will be one interchange, but the project will provide connection into and out of the city.
- Will there be any changes getting onto I-75 at Ezzard Charles Drive?
  - The only change is the ramp from Freeman Avenue will not be available. It will be replaced with a ramp from Ezzard Charles Drive.



- 
- Is the Bridge Forward concept dead? What are the challenges associated with this concept?
    - ODOT has reached out to Bridge Forward to schedule a working session to discuss their concepts. The primary challenge is the project cannot extend beyond the environmental footprint that has already been cleared. The original concepts showed I-75 west of its existing location. Grades on I-75 are an issue, too.
  - How will the Western Hills Viaduct at I-75 function? Will there be an elevation difference compared to current configuration? Will the on and off ramps be demolished? Will the Brent Spence Bridge Corridor project impact the Western Hills Viaduct project?
    - The Western Hills Viaduct will function much the same as it does today. There will not be much of an elevation difference with the new configuration. The existing ramps will be demolished, and the new ramps will connect to the new Western Hills Viaduct. Construction of the Brent Spence Bridge Corridor and the Western Hills Viaduct projects are being coordinated to lessen the impact to the traveling public. The Western Hills Viaduct project may begin before the Brent Spence Bridge Corridor project.
  - What are the environmental and design changes since 2012?
    - The environmental document is undergoing updates to reflect the design changes. The design changes are not significant and are more about value engineering. Changes included modifying the configuration of the I-75 Interchange with the Western Hills Viaduct, moving the northbound exit ramp to I-75 from Freeman Avenue to Winchell Avenue, reducing shoulder widths, reconfiguring the Ezzard Charles bridge over I-75, lowering design speeds, reconfiguring the existing BSB and the new companion bridge, reducing the number of lanes on frontage roads in Kentucky, and reconfiguring the ramps in downtown Cincinnati, among others.

In addition, project costs have been reduced, relocations have been reduced by over 90-percent, and land impacts in Kentucky have been substantially reduced. Land impacts in Ohio have been refined to include property owned by the City of Cincinnati which was not accounted for in the 2012 EA/FONSI.

The public hearing will cover the environmental and design changes. This material will be available at the public hearing.

- Can significant changes to the design be made?
  - The number of lanes won't change in the design; however, the collector-distributor system and some of the other details can be looked at based on the public comments.
- This seems to be a fast moving project considering there hasn't been much done since 2012.
  - We have been progressing since 2012 by studying the alternatives and updating the environmental studies.
- Can the community request changes (such as noise walls, signing, trees, and aesthetics) at the I-75 and Western Hills Viaduct interchange?
  - Noise walls are not an option in this location because they do not meet the requirements of ODOT's noise policy. The aesthetics committee is studying the other items and will present the results at the public hearing.





- 
- How is the right-of-way maintained for the parcels already acquired? There is concern about camping in right-of-way in the Camp Washington neighborhood.
    - The buildings on those parcels have been demolished. ODOT's mowing policies have been established to minimize harm to the environment and maintain its system. This limits mowing to a few times a year.
  - Can Colerain Avenue be reconnected from southeast of the Western Hills Viaduct interchange to northwest of the Western Hills Viaduct in Camp Washington?
    - There have been no discussions with the City to reconnect Colerain. Colerain is not a continuous roadway to the north. Additionally, there are historic subway tunnels near this location that could constrain construction activities. If the individual can provide specifics in a written comment, ODOT can evaluate it. Drawings can be submitted to Keith Smith at ODOT District 8.
  - How many structures will/have been demolished? Any at Ezzard Charles and Winchell Avenue?
    - A total of eight commercial properties are impacted in Ohio. Some property has been purchased along Western Avenue near the Findlay Street exit ramp. This property was limited to parking lots.
  - What is the proposed traffic pattern at Colerain and Spring Grove Avenue during construction?
    - The maintenance of traffic is currently being developed to minimize impacts to the travelling public.
  - There will be an increase of traffic during construction around Camp Washington.
    - There will be an official detour that will be provided to the public. However, we'll work with communities later in the process as the maintenance of traffic is developed. This is approximately 6 to 9 months from now.
  - Is there a way to trench I-75 after the Brent Spence Bridge?
    - The elevation of the road is fixed at this location due to the required clearances over the existing railroad north of the Ohio River. Preliminary engineering has shown at least an 8-percent grade is needed to accomplish this. For reference, the cut-in-the-hill is 5-percent.
  - Please use road tattoos and signage so the travelling public knows which lanes to use.
    - ODOT can investigate road tattoos during final design. Tattoos were used on I-71 because the tunnel obscured the sight-distance to the overhead signs.
  - What is the level of service of the roadways?
    - The traffic operations report is available for review on the project website ([brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)). ODOT is updating the traffic models, and this data will also be available on the project website. In urban areas, ODOT designs to LOS E during peak periods in the design year with the understanding that operations will be better during off-peak periods.
  - What is the percentage of local traffic across the Brent Spence Bridge?
    - The exact percentages were not available at the meeting. Generally speaking, one-third of the traffic uses local roads, one-third stays on I-75, and one-third stays on I-71. At the river crossing, the project is providing three lanes in each direction for the local connections, three lanes for I-75, and two lanes for I-71.



- 
- What other types of meetings are planned?
    - There are a series of neighborhood meetings in Ohio and Kentucky, and some have already occurred. These meetings are focused on the neighborhoods in which they are occurring. Additionally, a public hearing will occur in July/August 2023 in Ohio and Kentucky where the entire public will be invited to attend.

The comment sheet returned at the meeting is included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.



---

## Attachment 1: Advertising Materials

---

**From:** Camp Washington Community Council <communitycouncil@camp-washington.org>  
**Sent:** Saturday, December 10, 2022 7:40 AM  
**To:** Camp Washington Community Council  
**Subject:** Camp Washington Community Council Meets Monday, Dec. 12 (7pm) at CWURC Offices

Good morning,

The Camp Washington Community Council will meet **in-person\*** at the CWURC Office (2951 Sidney Ave, 45225) at 7 p.m. on Monday, Dec. 12. We encourage members to attend in-person, if possible.

[Monday's agenda](#) has several important items for the Community Council, including:

#### **Brent Spence Bridge Engagement**

A presentation on the Brent Spence Bridge from HNTB and the Ohio Department of Transportation. This will be an important opportunity for our community to engage directly on the design of the new Brent Spence Bridge.

#### **2023-2024 Community Council Board Nominating Committee**

Monday we will also be seeking three volunteers for the Nomination Committee for the 2023-2024 Camp Washington Community Council Board. Those interested in serving on the nomination committee must be Camp Washington residents. Please note, members of the nomination committee can not run for the Board of the Community Council.

After the Nomination Committee is formed on Monday, the nominating period will be open until January 3rd.

The slate of candidates will be announced at the January 9th, 2023 Community Council meeting with elections taking place at the February 13th, 2023 Community Council Meeting.

Those interested in running for a position on the Board must have attended 6 of the 12 Community Council meetings between January 2022 and December 2023.

-----  
The full agenda for Monday's meeting can be accessed here:

 [Dec. 2022 Camp Washington Community Council Age...](#)

*\*A Zoom option is provided at the bottom of this email but due to technical constraints, the ability to participate might be limited virtually. Audio and video may also be limited.*

Zoom: <https://zoom.us/j/97509884756>

Camp Washington Community Council Meeting Agenda  
Dec 12, 2022 7PM

**\*Meeting will be held in-person\***

At 2951 Sidney Ave, CWURC Offices

Please keep in mind:

- The building is not ADA accessible
- Masks are Optional



Zoom: <https://zoom.us/j/97509884756>

- Due to technical constraints, audio, video and participation via Zoom might be limited. We encourage you members to attend in-person, if possible.

Agenda:

1. Call to Order
2. Approval of Nov. Minutes
3. Community Updates
  - a. Police
  - b. River City Correction
  - c. Talbert House
  - d. Cincinnati Recreation Commission
  - e. Welcome Project/Wavepool
4. Treasury – Joyce Smith
5. CWURC Updates – Sidney Nation
6. Old Business
  - a. Pedestrian Safety - No new updates
  - b. Totems – No new updates
  - c. Neighborhood Safety Plan – No new updates
  - d. District 5 HQ – No new updates
  - e. WH Viaduct – No new updates
  - f. Crosley – No new updates
  - g. Website – No new updates
  - h. Neighborhood Gardens - No new updates
  - i. Community Budget Requests – No new updates
7. New Business
  - a. Call for Nominating Committee for 2023-2024 Board Elections
    - i. Elections will be held at January Community Council Meeting
  - b. **Voting Item:** Support for Safe and Clean Grant
  - c. Brent Spence Bridge - Stefan Spinosa - ODOT's Brent Spence Bridge Project Manager
8. Adjourn

Following Month General Council Meeting: Jan 9, 2023 7PM (tentatively zoom and in-person, location TBD)

Please register to join the community council at <https://camp-washington.org/join/>



---

Attachment 2: Sign-In Sheet



# BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting

Name	Representing	Address	Phone	Email
1. K. Fulen Fuller	ODOT DS			
2. Cratia Banta				
3. Paul Grill	CWCC			
4. Tom Manning	CWCC			
5. Mimi Rook	CWCC			
6. Adam Nixon				
7. Ryan Minnich				
8. Tom Plawa				
9. Mary Bartlett				
10. Sidney Nation	CWURC			
11.				
12.				
13.				
14.				
15.				
16.				
17.				
18.				
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



---

## Attachment 3: Photographs



## Camp Washington Neighborhood Meeting

December 12, 2022



Photograph 1. Camp Washington Neighborhood Meeting held on December 12, 2022



Photograph 2. Camp Washington Neighborhood Meeting held on December 12, 2022

## Camp Washington Neighborhood Meeting

December 12, 2022



Photograph 3. Camp Washington Neighborhood Meeting held on December 12, 2022



Photograph 4. Camp Washington Neighborhood Meeting held on December 12, 2022

---

Attachment 4: Presentation

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



# Camp Washington Neighborhood Meeting (OH)

December 12, 2022



INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with remarks by the City of Cincinnati (5 min) and introductions of the Project Team in attendance (2 min).

# Welcome

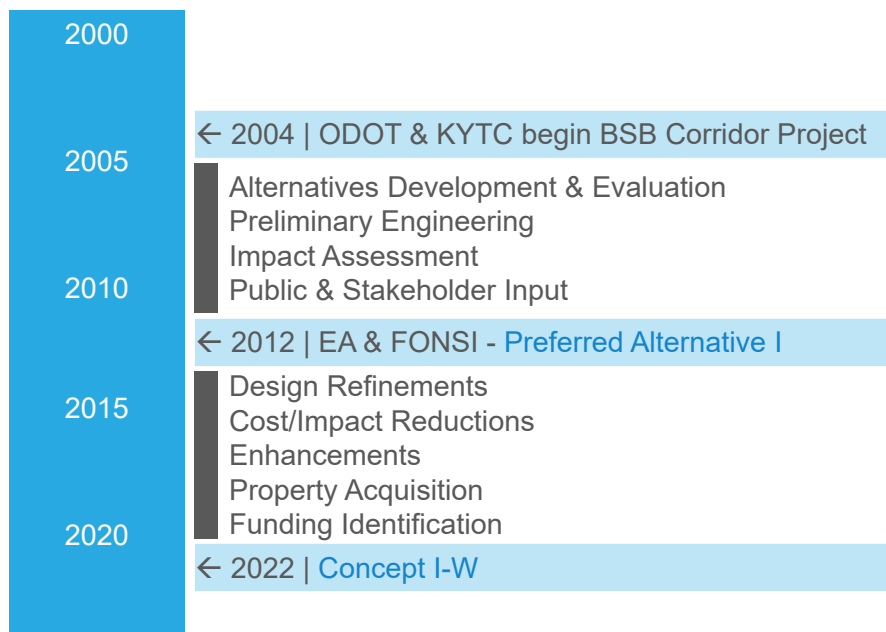
- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the Camp Washington neighborhood the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Camp Washington area
  - Discussion/feedback from Camp Washington residents



2

The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the Camp Washington neighborhood. Most importantly, tonight we're focusing on hearing from the residents of Camp Washington. We're here to answer your questions and to listen to your feedback about the project.

# Project History



3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Ohio, that includes I-75 from the Brent Spence Bridge to just north of the Western Hills Viaduct. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

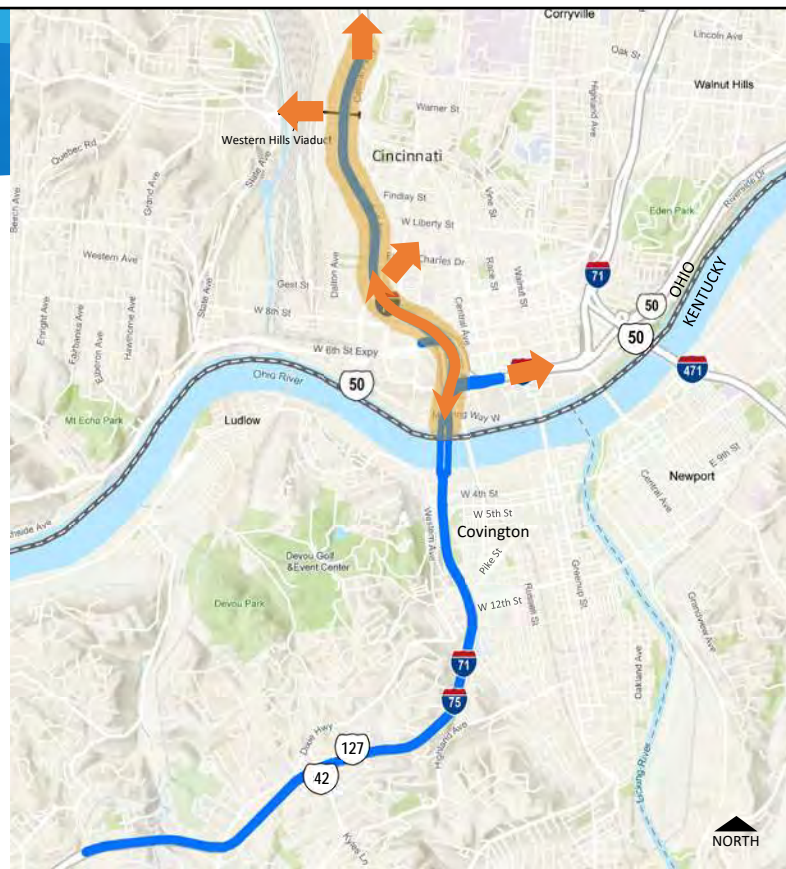
Since the approval of the FONSI in 2012, ODOT has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, ODOT has purchased most of the land needed to build the project. In addition, we've begun detailed design from Linn Street to the northern project limits.

# Project Description

## Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



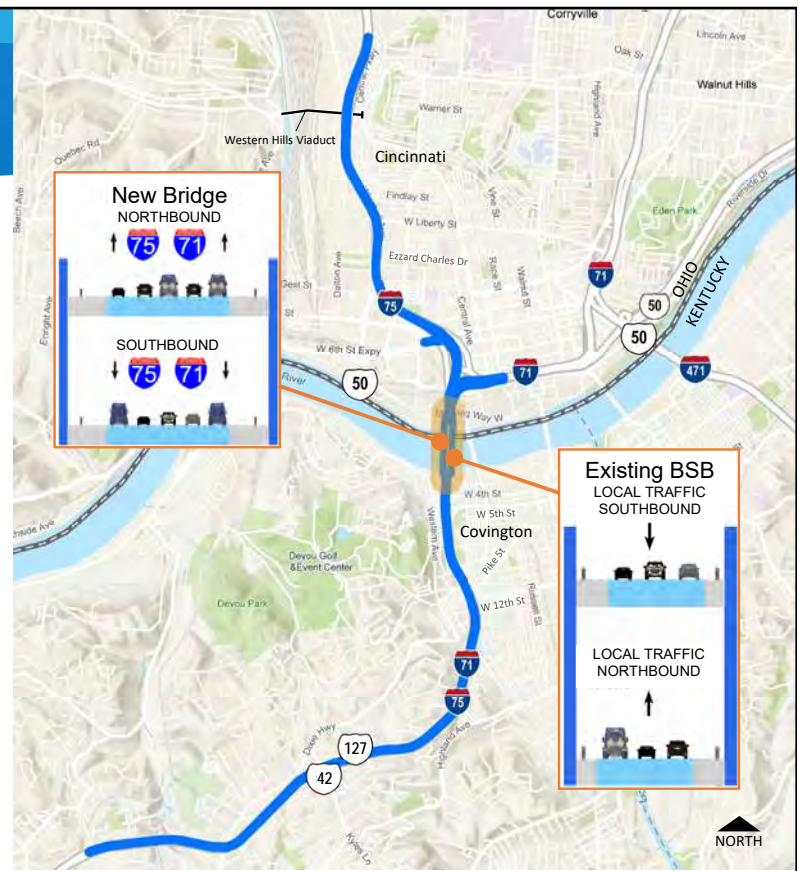
Note: Individual project elements will come in with click (marked by #).

In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. (#) Beginning near Ezzard Charles Drive, a collector-distributor system will be added to connect I-75 traffic to and from the local street network and US-50 West. (#) In the north, it will tie into the recently completed Mill Creek Expressway-Hopple Street Interchange project. (#) The project will rebuild the I-75 interchange at the Western Hills Viaduct and tie into the new bridge replacement project being developed by the City of Cincinnati and Hamilton County. (#) A new northbound exit will be built at Ezzard Charles Drive to improve access to Union Terminal, TQL Stadium, and Over-the-Rhine. Lastly, it will connect to I-71 and US-50 East.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

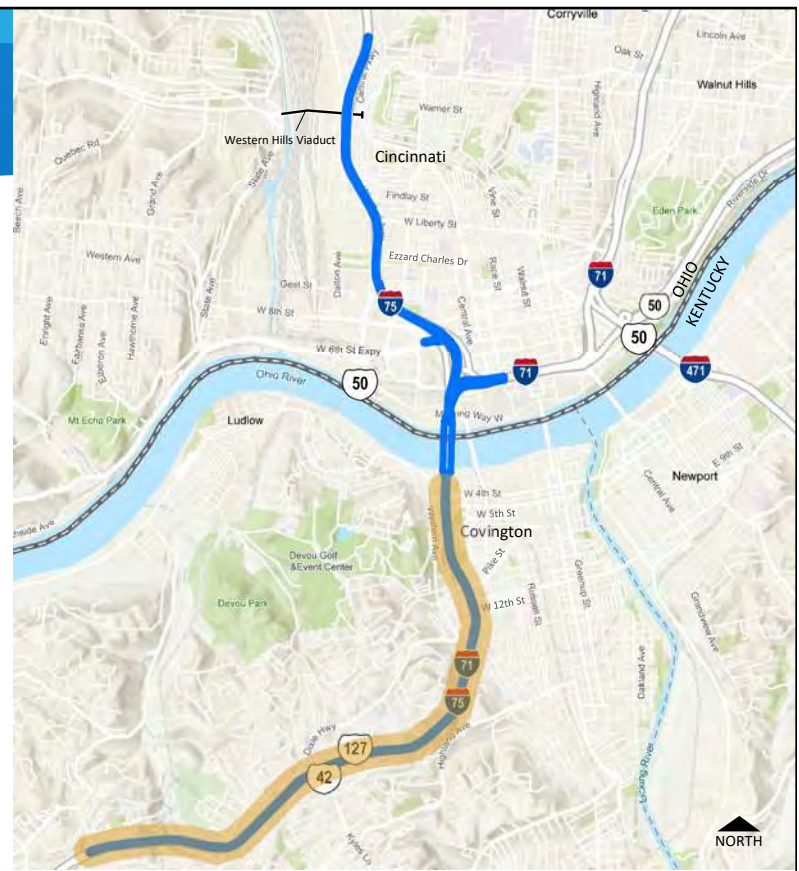
The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.



# Project Description

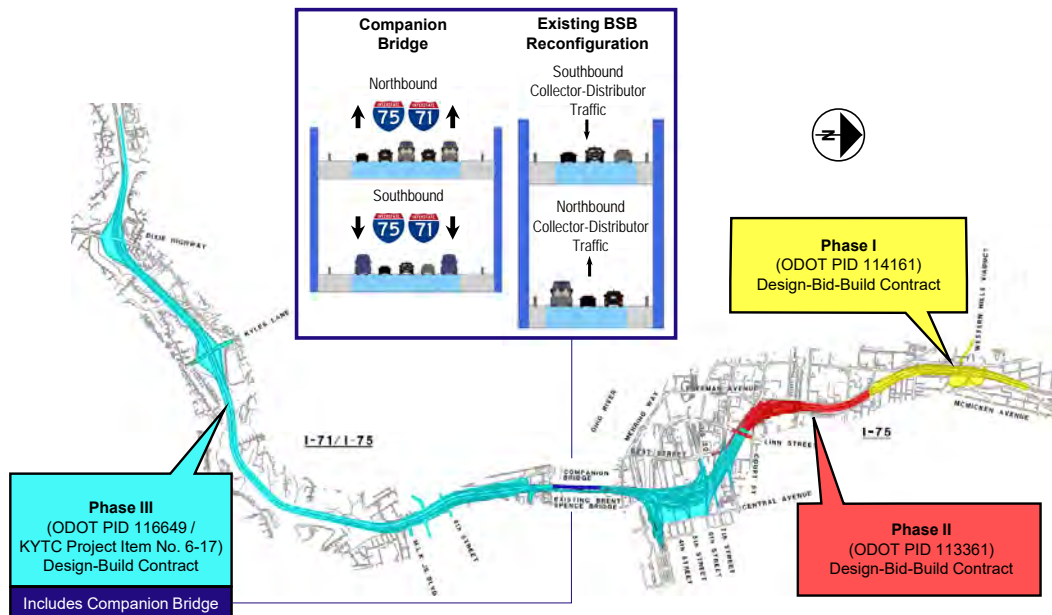
## Kentucky

- Widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct collector-distributor systems in Covington and Ft. Mitchell/Ft. Wright



In Kentucky, the project will widen I-71 and I-75 and rebuild all overpass bridges and interchanges. The project will also extend existing frontage roads to improve connectivity in Covington. A collector-distributor system will also be built beginning in Covington to connect interstate traffic to and from the local street network. Lastly, collector-distributor ramps will be built in Fort Mitchell and Fort Wright to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

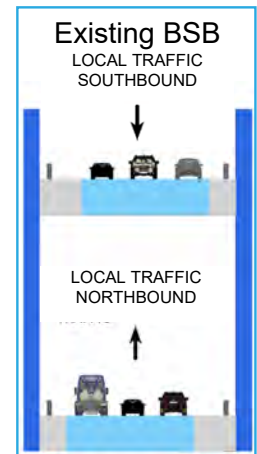
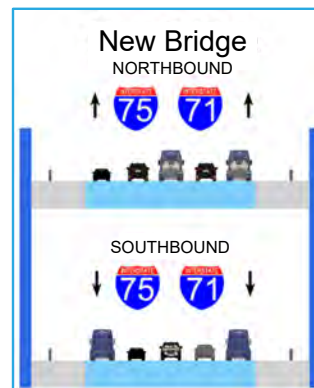
# Project Description



The project is going to be built in three phases. Phase I (shown in yellow) will stretch from Findlay Street to the north. Phase II (shown in red) will stretch from Linn Street to Findlay Street. Phase III (shown in blue) will build everything else, including the new companion bridge. Phase I, which is closest to Camp Washington is currently under design with construction expected to begin in 2028. The construction time frame may adjust based on ongoing coordination with the City's Western Hills Viaduct Project.

# What Has Changed?

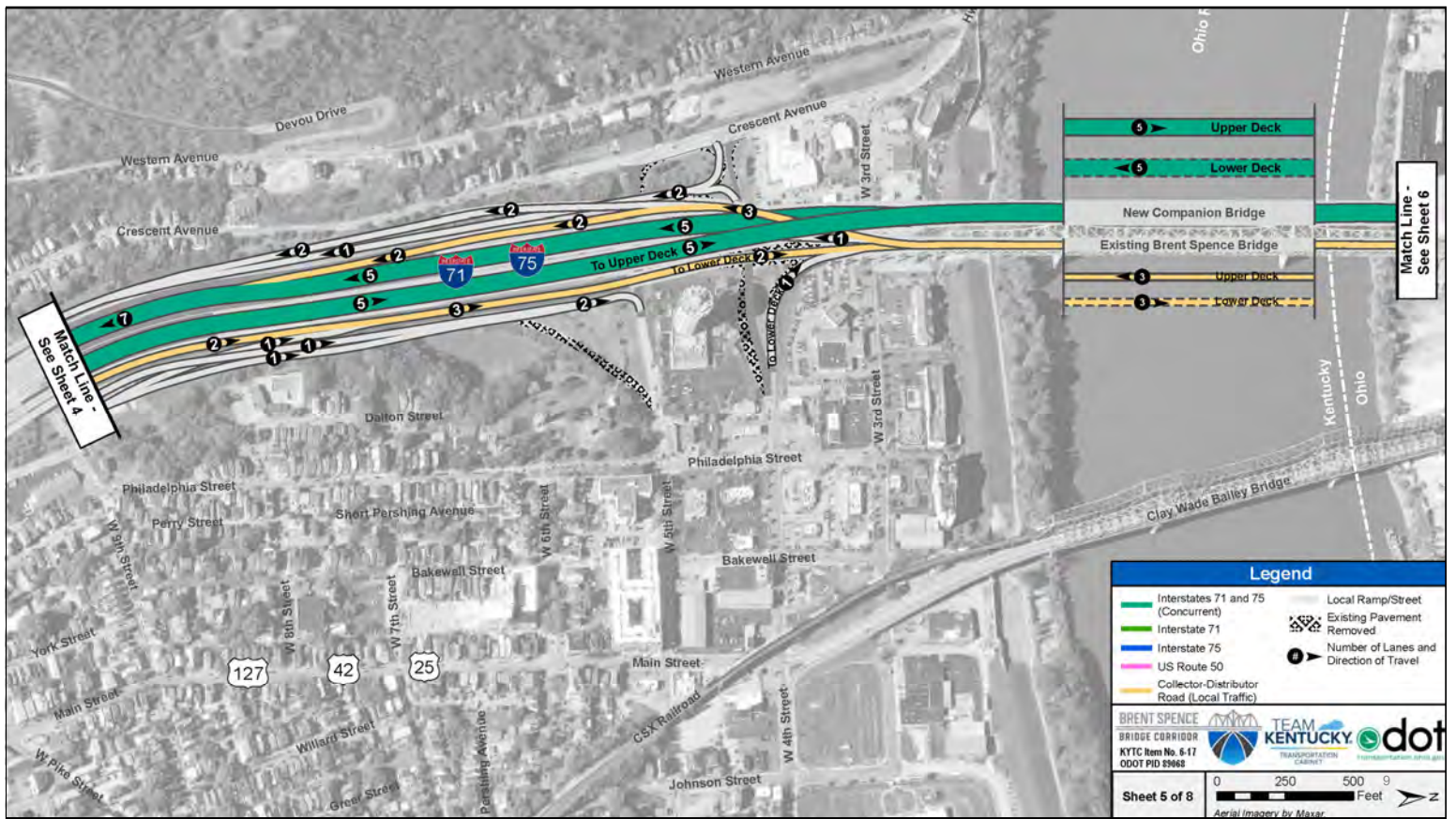
- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



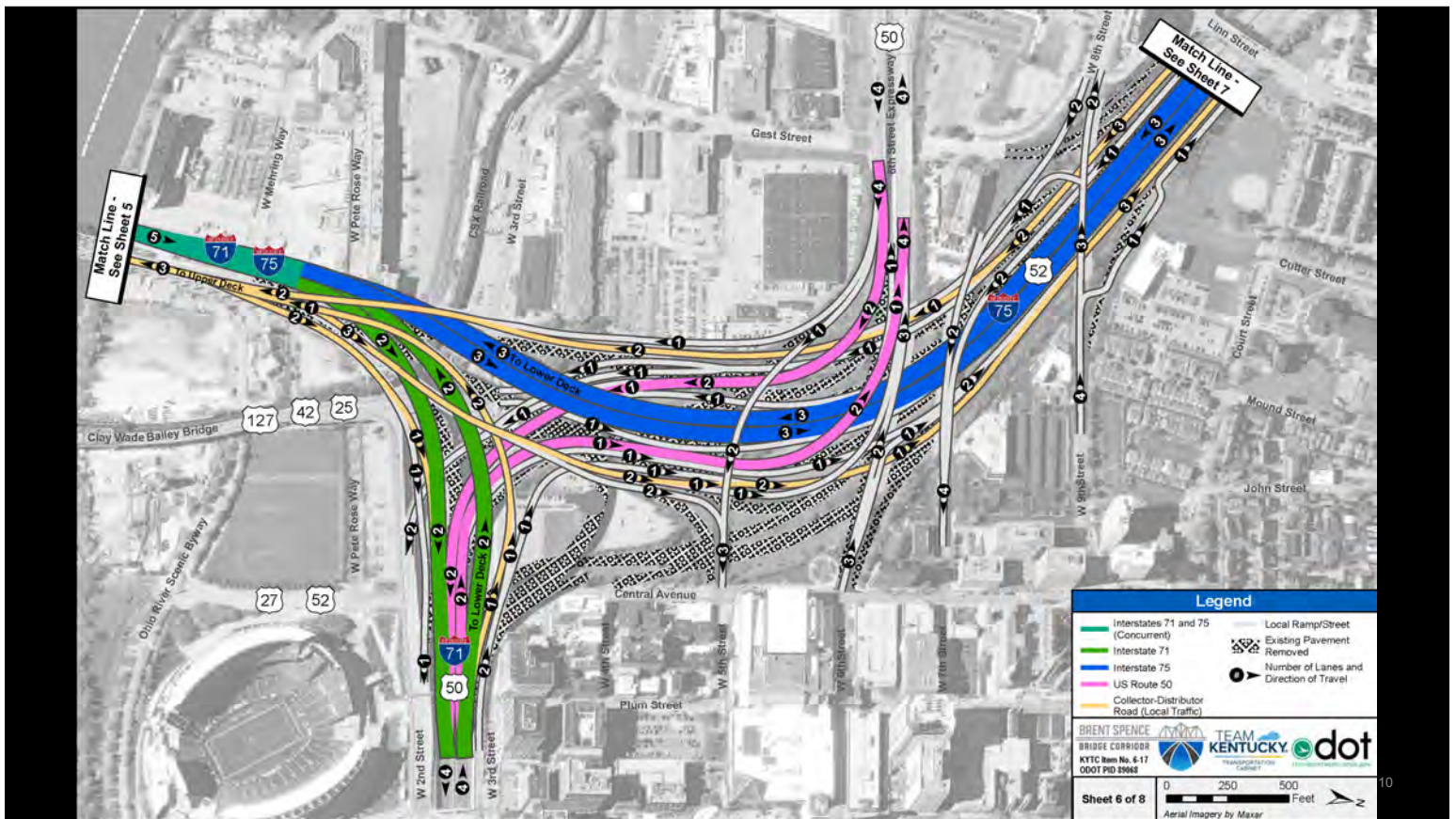
8

As mentioned earlier, since the 2012 EA and FONSI, ODOT has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



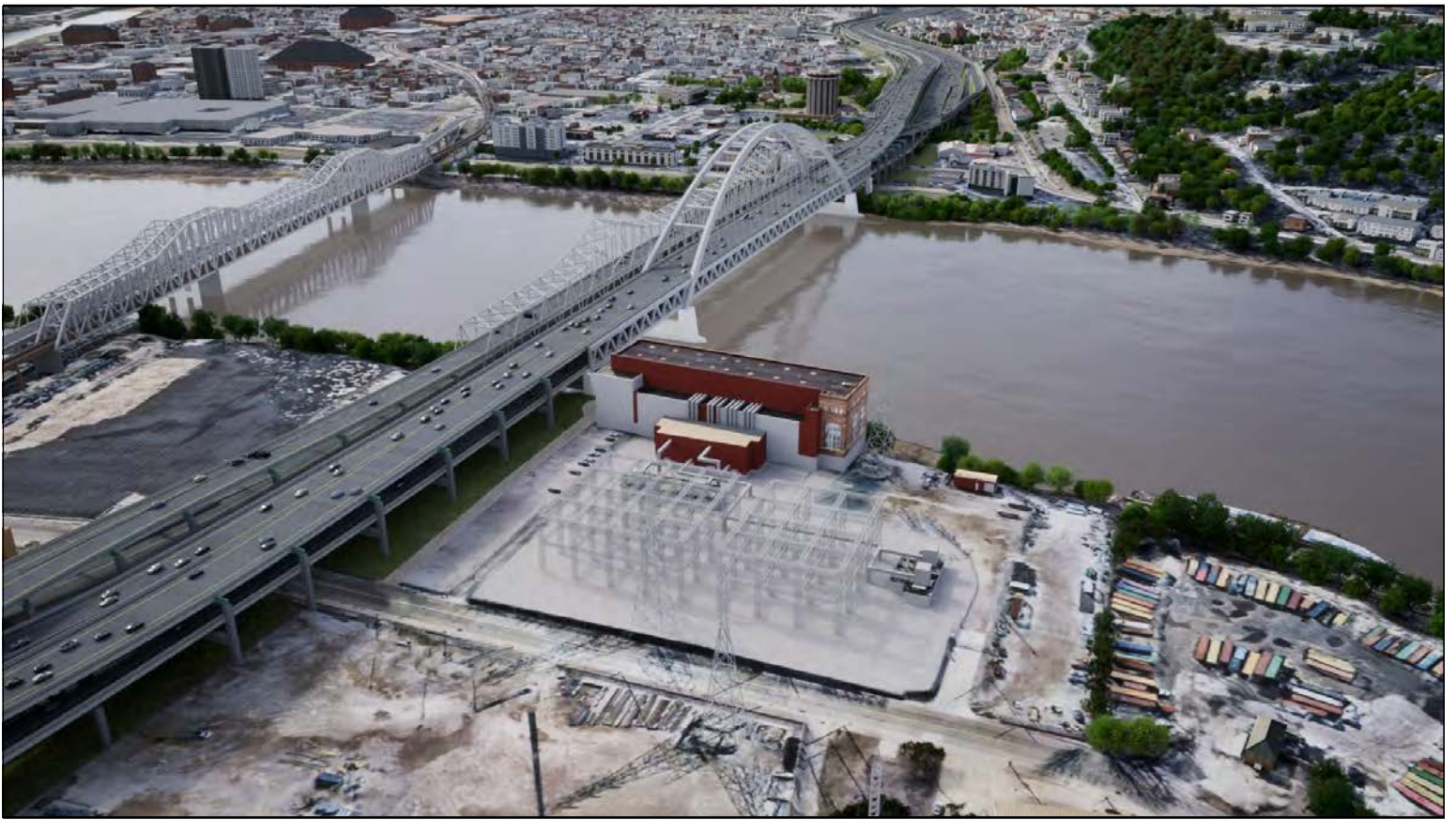
This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will through Cincinnati. The blue lines show how I-75 traffic will travel to and from the companion bridge. The green lines show how traffic will on I-71 will be routed to and from the companion bridge. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets. Finally, the pink lines show how traffic will flow on US 50.

Other changes since 2012 include reducing shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design and allows us to reduce the overall area needed to build the roadways. We also reduced the number of lanes on some of the frontage roads in Kentucky.



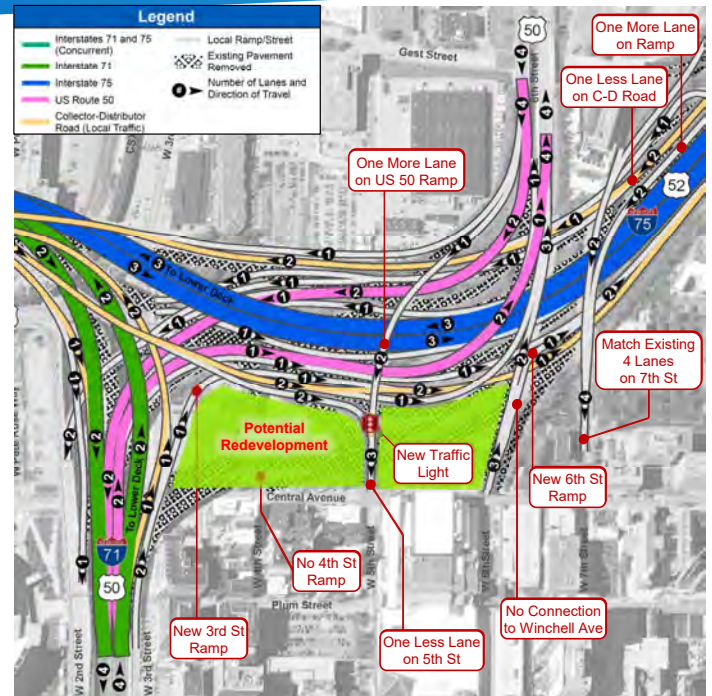
This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

# What Has Changed?

- Reconfigured Downtown Cincinnati Ramps

## Additional changes evaluated

- Depressing I-75 and extending downtown streets to form an urban street grid
  - Cannot meet current design criteria
  - Safety concerns due to steep grades
  - Increased project footprint
  - Continuity along US 50 not maintained
  - Increase traffic in Covington



**Note: Click to walk through ramp changes (marked by #)** | Another change is the layout of the ramps in Downtown Cincinnati. First, ODOT optimized the interchange layout to use land formerly occupied by the Dunnhumby building. More recently, the City asked ODOT to evaluate ramp changes to open up additional land for redevelopment. (#) Based on the City's request, the 4th street ramp to NB I-75 was removed. (#) To provide access for traffic that would have used 4th Street, a new entrance ramp to NB I-75 was added to 3rd Street. (#) The SB exit to 5th Street was removed, which also resulted in fewer lanes on the SB collector-distributor road and at the Central Avenue intersection. The 7th Street exit was also widened to provide additional lanes for traffic that would have used the 5th Street exit. (#) The NB exit to 5th Street was moved closer to the highway to intersect the US 50 ramp at a traffic light, and one more lane was added to the US 50 ramp to make sure all traffic could move smoothly through the light. (#) The 6th Street connection to Winchell Avenue was removed and replaced with a new connection between 6th Street and the NB collector-distributor road. (#) All these changes will open up about 9.5 acres of land for redevelopment, which has been a consistent comment we've been hearing from the public in the last several months.

ODOT has also received several comments about depressing I-75 through downtown Cincinnati and extending downtown streets to form an urban street grid similar to Fort Washington Way. Because I-75 first passes over the railroad, it would have to descend at very steep grades (around 8%) in order to be depressed through downtown. This doesn't meet current design standards which state the maximum grade must be 5%. In addition, such steep grades would introduce safety concerns, particularly given the large number of trucks that travel on I-75. Since I-75 cannot be lowered, local streets would need to be raised to form an urban street grid across I-75, which would increase the project footprint. Finally, building an urban street grid would require starting and ending US 50 on either side of I-75. Moving all local traffic to an urban street grid as opposed to the collector-distributor system currently included in Concept I-W would also substantially increase traffic on the local streets in Covington.



This drawing shows what the Downtown interchange area might look like once the project is built.



# What Has Changed?

- Northbound I-75 entrance ramp moved from Freeman to Winchell
- One Ezzard Charles Drive bridge
- Minimize work along Winchell



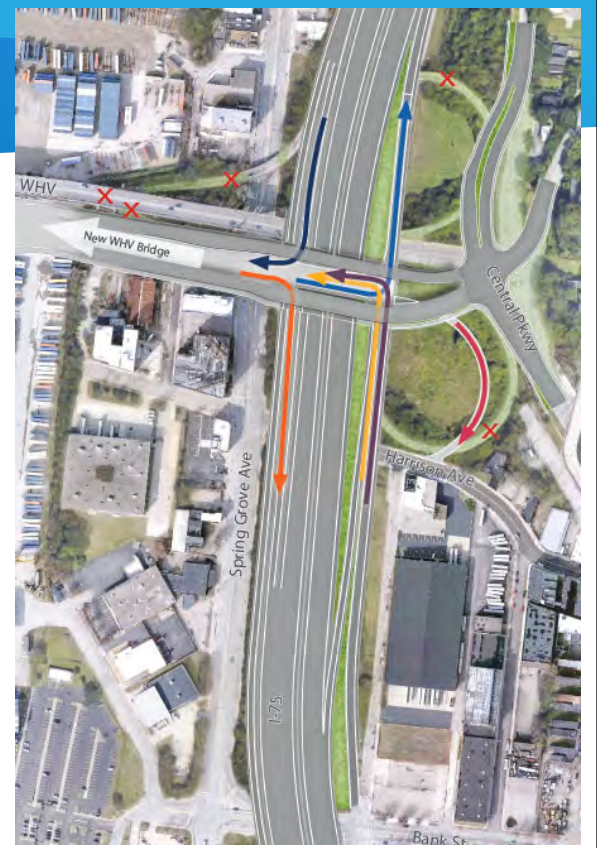
Note: Changes will come in with click (marked by #).

Another change ODOT's made since 2012 involves moving the northbound entrance ramp to I-75 from its existing location at (#) Freeman Avenue (south of Ezzard Charles Drive) (#) to Winchell Avenue (north of Ezzard Charles Drive). Also, (#) the two existing one-way bridges on Ezzard Charles Drive will be replaced with one, two-way bridge. These changes were also made in coordination with the City of Cincinnati to reduce project impacts and costs and improve local access to the interstate.

We've also refined the roadway layouts so that work will not occur along Winchell Avenue beyond what is needed to build the northbound entrance ramp shown in this drawing.

## What Has Changed?

- Interchange at the Western Hills Viaduct



Turning our focus to the areas closest to Camp Washington, the layout of the project has been refined to tie into the new Western Hills Viaduct, which is a separate, stand-alone project being developed by the City and County. The existing ramps at the I-75 interchange will be removed and replaced with new ramps that will provide direct access to and from the new Western Hills Viaduct bridge. The ramps will also connect I-75 to the local street system at Findlay Street for southbound traffic and Bank Street for northbound traffic.

# Aesthetics

- Corridor-wide aesthetic treatments
  - Ashlar stone treatments for abutments, piers, and walls
  - Decorative bridge parapets
  - Translucent screen walls and planters on bridges
  - Other features (lighting, sidewalks, etc.) to match City standards



ODOT has worked with the City and the project Aesthetic Committee to develop an Aesthetic Design Checklist that will guide what the Brent Spence Bridge Corridor will look like. For example, ODOT will add aesthetic treatments in a pattern called “Ashlar Stone” too all bridge abutments, parapets, and piers. Retaining walls and the extended traffic safety barriers we just discussed will also have ashlar stone treatments. Overhead bridge parapets will have end treatments with the bridge identification name, construction completion data, and rustification design features. Examples of similar bridge parapets are shown in the images on this slide. In addition, overpass bridges will have wide sidewalks or shared-use paths, planters, and translucent screen walls. Other features throughout the corridor will match City aesthetic standards, including lighting, sidewalks, tree lawns, and others. We will show some examples of what specific locations near Camp Washington will look like in just a few slides.

# Pedestrians and Bicycles

- Connections across I-75
  - Sidewalks
  - Shared use paths
  - Bike lanes
- New shared use path along Winchell Avenue
- Improved connections to local destinations



The project will install bicycle and pedestrian infrastructure on connections across I-75 like what is shown here for 9th Street. Pedestrian and bicycle connections will also be provided on 6th Street, 7th Street, Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue. To promote safety for bicycles and pedestrians, the ramp connections with local streets are being designed as lower-speed urban intersections in accordance with City of Cincinnati design standards. The pedestrian and bicycle infrastructure included in the project will improve connectivity to transit, employment, healthcare, cultural, recreational, and commercial destinations.



This exhibit shows some of the planned pedestrian and bicycle facilities near Camp Washington. Note the new shared use path on Harrison Avenue, the new bike lanes on Findlay Street, and the replaced sidewalks on Harrison, Bank, and Findlay. These will tie into the existing bike lanes on Western, Winchell, and Bank and connect to existing transit stops.



This is a view of what Findlay Street will look like when the project is built. Notice the rebuilt sidewalks, added bike lanes, and underpass lighting.



This view shows what the new, two-way bridge on Ezzard Charles Drive will look like. Notice the wide shared use path on the right and the sidewalk on the left. Also notice the planters, screenwall, and decorative lighting on the bridge. The bridge lights and traffic signal supports will be black and meet City aesthetic standards.



This view shows what the finished project will look like from Ezzard Charles Drive looking north toward Camp Washington. Notice the ashlar stone treatments on the retaining walls.





# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.

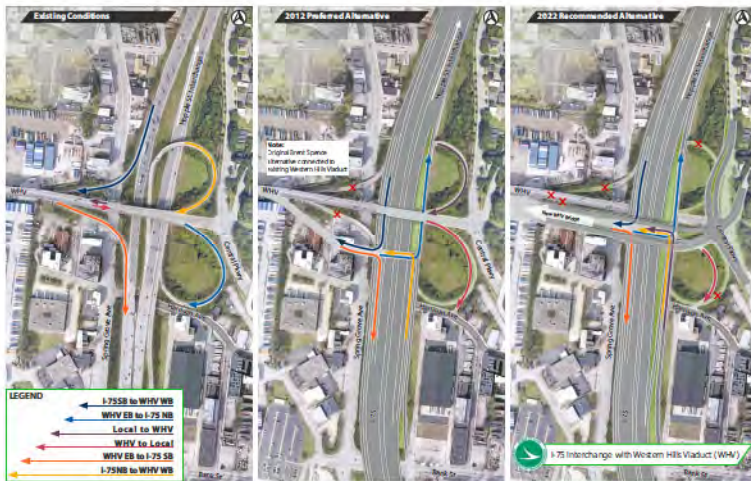


---

Attachment 5: Exhibits



Ezzard Charles Drive (Looking West) - Ohio





---

Attachment 6: Comment Form

# COMMENT FORM

BRENT SPENCE  
BRIDGE CORRIDOR



Contact information is not required but will ensure you receive a response, should one be required.

Name: Sidney Nation

Mailing address (or nearest cross streets): [REDACTED]

Email address: [REDACTED] Phone: [REDACTED]

Business/Organization Name: Camp Washington Urban Revitalization Corporation

Business/Organization Address: "same as above"

What is your interest in the proposed project? (Select all that apply.)

- Area Resident  Area business owner or employee  Commuter  
 Other: Community development corporation director

General Comments (Please attach additional pages, if needed):

• the impact of Colerain and Spring Grove when traffic gets rerouted.  
↳ will this leave Camp Washington roads destroyed once construction is complete.  
↳ can we get a commitment (in writing!) that these roads will be restored.

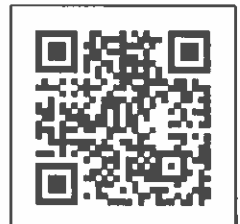
another comment:

• tree canopy @ Western Mills roadcut and along loop removal.  
↳ please continue to present to the neighborhood.

COMMENTS DUE BY  
January 5, 2023

Comments may be submitted:

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
- By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



---

# Brent Spence Bridge Corridor Project City of Park Hills Meeting Summary December 12, 2022

---

## Introduction

The City of Park Hills (COPH) meeting was held on December 12, 2022, from 7:00 pm to 8:30 pm. The meeting was held at the Garden of Park Hills located at 1622 Dixie Highway in Park Hills, KY. It was publicized through the COPH private Facebook page and the COFM email list (see Attachment 1). The city also announced the neighborhood meeting at a prior City Council meeting, posted flyers at local businesses, shared information about the meeting with local schools, and posted meeting details on a city sign on Dixie Highway. Attendees at the meeting included the Park Hills mayor and City Council members, members of the public, a representative from a local law firm, and the project team.

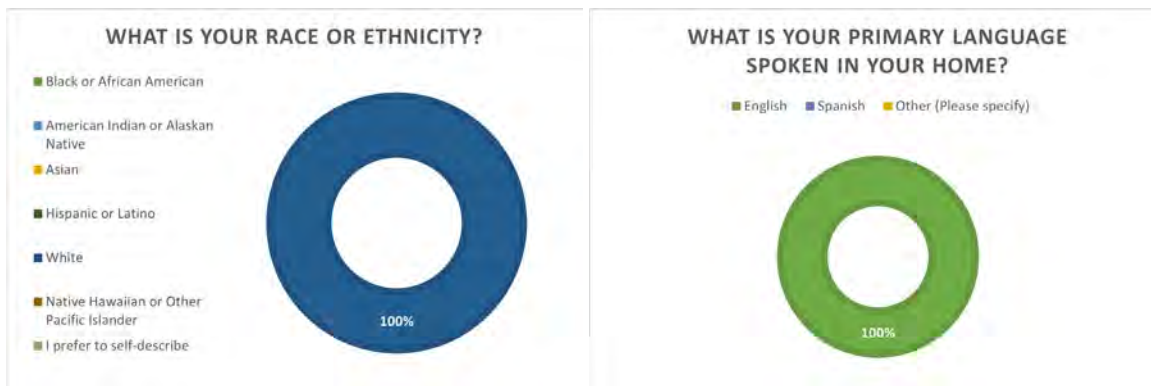
In total, forty-seven (46) people attended the meeting, including forty-three (42) members of the public and four (4) project team members (see Attachment 2). Photographs are included in Attachment 3.

The meeting format included a formal presentation by Stacey Hans (KYTC) (see Attachment 4). After the meeting, the project team responded to questions and comments offered by those present.

Exhibits showing plan views of the project through the COPH and renderings of the proposed bridge were available for viewing before and after the meeting (see Attachment 5). Written comment forms, a demographic survey, and a postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. One (1) comment form and five (5) demographic surveys were returned at the meeting.

## Demographics

The following is a breakdown of the responses received from the demographic survey.





A write-in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. The only comment received was:

- Most people do not fully understand design and construction terms & processes. A glossary of terms would be helpful. Traffic studies and input from first responders, maintenance crews, etc. would also be good to have publicly accessible.

## Comments

Attendees posed a number of questions during the course of the presentation / meeting. Table 1 is provided as a summary of the questions and answers. Common themes from the questions include:

- Concern over existing and future noise levels.
- Questions regarding access points and the proposed C-D road.
- General questions regarding the project schedule, project length, etc.

The comment sheet returned at the meeting is included in Attachment 6. All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.

Comments noted on the survey forms returned at the meeting include:

- Great presentation – great change to make the companion bridge thru traffic only!

**Table 1**

Question	Project Team Response
When did ROW decision get made? I was told my property would be relocated in 2013, but now I find out refinements were made and that I'm not longer being relocated.	We're here today to reach out to the public and inform the public of the decisions that have been made. When KYTC began the right of way process earlier this year we started to look at using retaining walls to avoid relocations.
Noise walls block the sound, but does it lift the sound up and the drop it over the other side of the wall?	The material used absorbs as much of the sound as possible. Also, in the presentation, we will show the results of noise analysis, including the predicted noise levels.
The noise problem is caused by jake brakes. Will this project do anything about them?	We cannot regulate jake brakes. They are a safety feature.
What's the intention to find funding approval? What is the plan B if it doesn't work? Are tolls still in play?	We have submitted applications for two sources of funding. We continue to move forward and plan on releasing information for contractors soon, although we will not give a contract without securing the funds. Plan B is to keep moving forward with as many options as possible. The project team is committed to looking for any type of funding solution. Also, the Bipartisan Infrastructure Bill is not a one-time option, so there will other funding opportunities in the future.





Question	Project Team Response
Are tolls still being considered?	No. The project team is committed to pursuing every available opportunity to deliver the project without tolls.
If inflation causes the project cost to keep rising, will the team look at making refinements to the project to reduce costs?	We've already pursued opportunities to reduce impacts, but nothing is off the table.
So you anticipate a 6-year construction season?	Yes, but that does not mean the entire length of the project will be impacted the whole time. We will work with the design-build team on that. As part of the progressive design-build process, public involvement is not done. When a contractor is on board, the States will work with them to determine the maintenance of traffic. Also, we have already talked to emergency services along the corridor, and we will keep coordinating with them in the future.
If I make the wrong decision at the split from the C-D road and the interstate roadway, can I get off at the first exit in Ohio and turn around?	Yes.
A new Texas turnaround is almost open. Will it stay in place with this project?	When the Texas turnaround is finished, the existing access point/ramp at West 4 <sup>th</sup> Street will be used for emergency access only. It will have a gate on it and emergency responders will have the key.  When the BSB Corridor project is finished though, it will be opened back up and provide access to all users to the local C-D road.
What is progressive design-build? Are contracts let at the same time?	On a design-build project, the contractor gives a price and a proof of concept. With progressive design-build, agencies can select a group based on qualifications. Then, once they're on board, the agencies and the design-build team discuss concepts and develop the plans. The contractor also determines buildable units, and that's when traffic, maintenance of traffic, etc. come into play.
I avoid the "cut in the hill" as much as possible when getting on the interstate. Will I still be able to get on the interstate without having to get on Kyles Lane?	Yes. You'll have very similar access to the interstate as you do today. Pike Street accesses the C-D road and then you'll be able to get onto the interstate.



Question	Project Team Response
Was there consideration for re-routing semi-trucks around I-275?	Yes. This has come up multiple times, and recently an emergency responder stated that they saw an uptick in accidents along the I-275 corridor when this was previously done. From an agency's perspective, our analysis has shown that there is a significant number of trucks with designations within the loop. There would also be an issue with enforcing a ban on through truck traffic.
Are the maps presented today going to be online as well?	Yes, the exhibits will be online ( <a href="http://PublicInput.com/bsbc">PublicInput.com/bsbc</a> ) along with tonight's presentation. While the exhibits shown here are catered to tonight's presentation, there will be exhibits from the other meetings on the site as well.



---

[Attachment 1: Advertising Materials](#)

## Julie Alig

---

**From:** Kathy Zembrodt  
**Sent:** Saturday, October 29, 2022 10:02 AM  
**To:** Julie Alig  
**Subject:** please print the attached and e mail too  
**Attachments:** 10-27-2022\_Invitation Flyer\_Park Hills.pdf

---

**From:** Sherry Kish [REDACTED]  
**Sent:** Friday, October 28, 2022 4:21 PM  
**To:** Kathy Zembrodt [REDACTED]  
**Cc:** Seth Turner [REDACTED]; Jodi Heflin [REDACTED]  
**Subject:** Brent Spence Bridge Presentation on 12/12/22

Mayor Zembrodt,

Thank you so much for working with us to get a meeting scheduled to allow the Brent Spence Bridge project team to talk with the City of Park Hills residents on December 12<sup>th</sup>. As you know, the project team wants to share information about the project and create a community conversation where your residents can share their questions and comments on the upcoming project. To ensure that all of your residents know about this opportunity, we wanted to walk through what options you have available for marketing the meeting and whether you need assistance in getting the word out. Below is a table with a list of ideas of how to communicate the opportunity – many of these come from the neighborhood representatives we are working with on these meetings. If you would be kind enough to let us know what capabilities you have and what assistance you might need, we can be helpful bolstering your existing outreach.

Our goal is to be sure all notifications about the meeting opportunity are sent out a minimum of 14 days, preferably sooner, before each meeting.

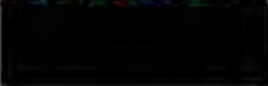
Media Outlet	Description	Can do this (mark with an X)	Completed? (Please provide date completed for Federal tracking.)
Prior Meeting Notice	Can you share BSB as an upcoming topic at a previous city/neighborhood meeting?	X	
Email list	Can you email a neighborhood wide or citywide email list with information about BSB presentation at the upcoming meeting?	X	
Facebook (private to neighborhood/city)	Can you share a Facebook message about BSB presentation?	X	
Post a sign at the local coffeehouse or gathering place	Is there a special place in your neighborhood where everyone would see this message?	X	
Share with a school PTO/PTA	Is there an elementary school with an active PTO/PTA that might be willing to share this message?	X	

Other	Is there another outlet you have available to get the word out?	Signage on Dixie Hwy X	
Do you need assistance from us with advertising?	We made a flyer for your consideration (attached to this email).	X X	

As we move forward towards the meeting, Seth Turner (copied here) will be your point of contact for outreach assistance. He has been helping on broader outreach efforts for BSB so we wanted to bring him in to assist on our neighborhood specific meetings.

Thanks!  
Sherry

Sherry R. Kish



**HNTB CORPORATION**  
88 East Broad Street, Suite 1600 | Columbus, OH 43215 | [hntb.com](http://hntb.com)

**100+ YEARS OF INFRASTRUCTURE SOLUTIONS**

[Twitter](#) | [LinkedIn](#) | [Facebook](#) | [Instagram](#)

*This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.*

\* Where else would you like?  
Kathy J.



---

Attachment 2: Sign-In Sheets



## BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 12, 2022

Name	Representing	Address	Phone	Email
1. Jim Pfaller	me			
2. Judy Pat Sullivan				
3. DAVID CROTTY	me			
4. Nolan Nicaise	self			
5. David Schlutman	Puck			
6. & Nancy				
7. Herb Black				
8. Rob Butke	Self			
9. Mary Amend	Self			
10. Susan Williamson	Self			
11. Beth Nawak	self			
12. Ginger Conway	self			
13. PETER RINDER	SELF			
14. Beth Auerbeck	Self			
15. Ken Kipperbrack	Self			
16. BART GROENEVELD	SELF			
17. Dan Lutz	Self			
18. ROBERT DACIY	SELF			
19. Kim De Sutter	PH SCENE DIR.			
20. D. Scott Smith	Self			

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



# BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 12, 2022

Name	Representing	Address	Phone	Email
1. <u>Juan Van Haverbe</u>	<u>myself</u>			
2. <u>Lynn Hemmer</u>	<u>Myself</u>			
3. <u>Celeste Gallenstein</u>	<u>myself</u>			
4. <u>RICHARD ABEL</u>	<u>''</u>			
5. <u>Anthony Zieverink</u>	<u>my self</u>			
6. <u>BOB ALBERS</u>	<u>MY SELF</u>			
7. <u>PAT FLANNERY</u>	<u>myself</u>			
8. <u>DEDE RALSTON</u>	<u>MYSELF</u>			
9. <u>Teri Morris</u>	<u>SELF</u>			
10. <u>JIM JENNING</u>	<u>Self</u>			
11. <u>Joseph Kowalski</u>	<u>-</u>			
12. <u>Marc Logan II</u>	<u>self</u>			
13. <u>Jeff Hendricks</u>				
14. <u>CHRIS CARLE</u>	<u>SELF</u>			
15.				
16.				
17.				
18.				
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.





# BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 12, 2022

Name	Representing	Address	Phone	Email
1. JAMIR DAVIS	J. Davis law firm			
2. ROB HAUTER				
3. WILLIAM BLANK				
4. CAROL BETHEL	resident			
5.				
6.				
7.				
8.				
9.				
10.				
11.				
12.				
13.				
14.				
15.				
16.				
17.				
18.				
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



# BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 12, 2022

Name	Representing	Address	Phone	Email
1. Eric Maschmeyer				
2. Lee & JOE ADAMICK	SELF			
3. Brand GARCIA MORA	SELF			
4. Michael Leathers	HMB			
5. Mark Bechnerer	HNTB			
6. Gary Valentine	KYTC			
7. Stacey Hans	KYTC			
8.				
9.				
10.				
11.				
12.				
13.				
14.				
15.				
16.				
17.				
18.				
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.



---

Attachment 3: Photographs





---

Attachment 4: Presentation

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



**Park Hills Meeting (KY)**  
December 12, 2022



INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



The meeting will open with introductions of the Project Team in attendance (2 min).

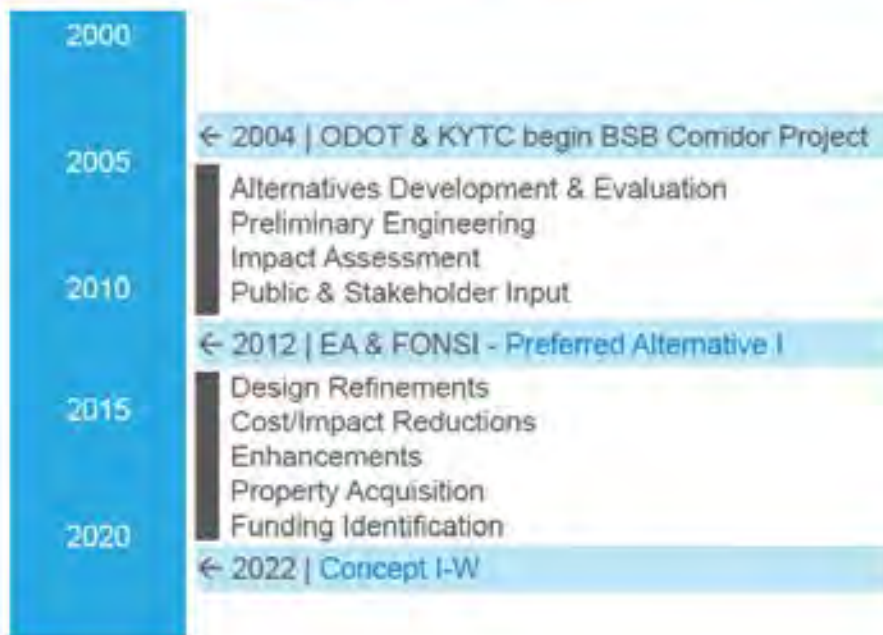
# Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in Park Hills the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the Park Hills area
  - Discussion/feedback from Park Hills residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near Park Hills. Most importantly, tonight we're focusing on hearing from the residents of Park Hills. We're here to answer your questions and to listen to your feedback about the project.

# Project History



For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2022, KYTC has begun purchasing the land needed to build the project.



## Project Description

### Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. A collector-distributor system will also be added to connect I-75 traffic to and from the local street network and US-50 West.

# Project Description

## Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

# Project Description

## Kentucky

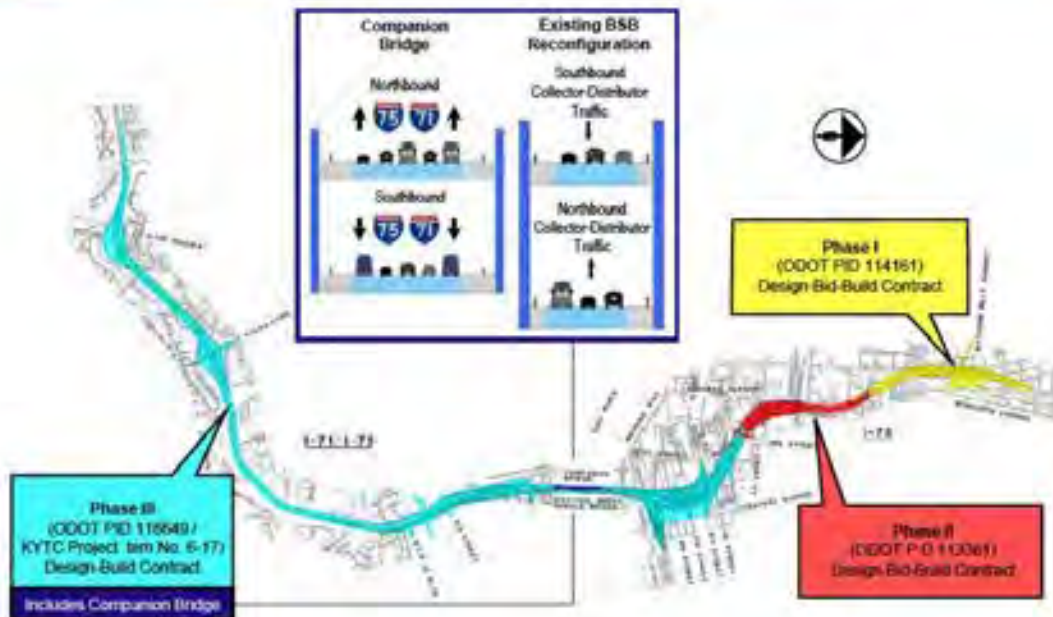
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

# Project Description



The project is going to be built in three phases. Phases I and II (shown in yellow and red) will be built in Ohio. Phase III (shown in blue) will build everything else, including the new companion bridge. The entire Kentucky corridor will be built in Phase III, which is following a progressive design-build process that will begin in 2023.

## What Has Changed?

- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move through Fort Mitchell and Forth Wright. The green shows through traffic on the interstate. As shown in the orange, a collector distributor road will start just south of Dixie Highway. The purpose of the collector-distributor road is to reduce the number of places where traffic is entering and exiting the freeway to reduce the number of times vehicles weave in and out of the through travel lanes to access local roads. As shown here, in the northbound direction, traffic traveling to Dixie Highway and Kyles Lane will exit onto the collector-distributor road south of Dixie Highway. Vehicles traveling to Dixie Highway will then leave the collector-distributor road and take a ramp to Dixie Highway (shown in grey). Traffic traveling to Kyles Lane will continue north on the collector-distributor road.



Moving further north, traffic from Dixie Highway will travel down a ramp to enter the northbound collector-distributor road. Next, traffic traveling to Kyles Lane will leave the collector-distributor road and use a ramp to reach Kyles Lane. Northbound traffic will travel through on the collector-distributor road, and more traffic will enter from Kyles Lane.



Finally, the traffic on the collector-distributor road will join with traffic from the Kyles Lane ramp and enter the interstate going northbound. Notice that there will be only one northbound exit and one entrance between Dixie Highway and Kyles Lane instead of one exit and one entrance for each road. Consolidating these access points will improve traffic flow and reduce high-speed crashes on the interstate.

The traffic movements we just walked through will be reversed in the southbound direction, providing the same access scheme and improvements in traffic flow and safety.

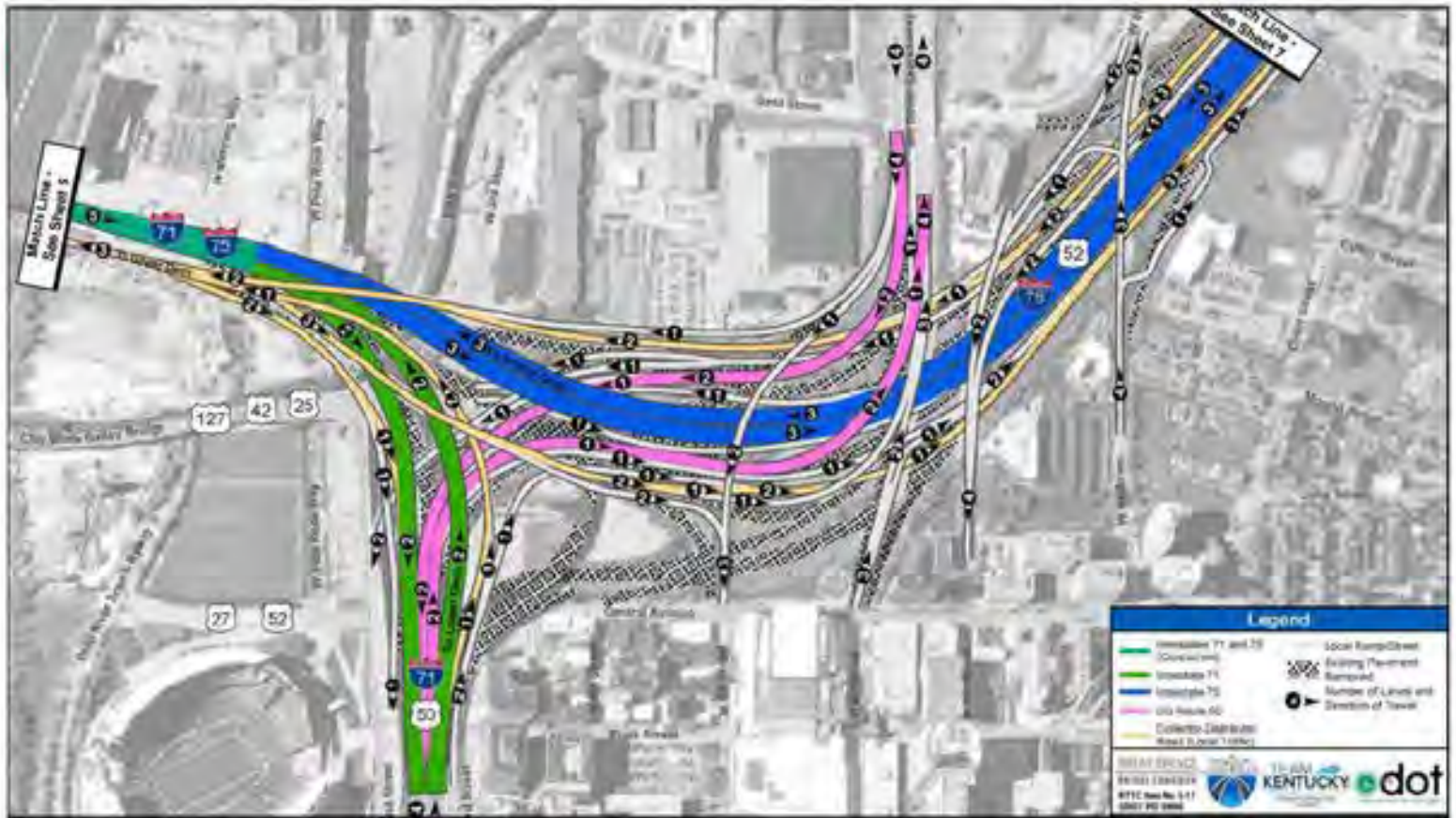




This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.



Here's another view from Covington.

## What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 40+ then vs. 4 now
  - Commercial | 6 then vs. 5 now



Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet.

In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street in Covington. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

# Noise

- Noise: Unwanted sound.
- Decibel (dB): Used to measure the intensity of a sound.
- A-weighted decibel (dBA): Gives a scale for noise levels as perceived by the human ear.
  - Change in 1 dBA = barely perceptible
  - Change in 10 dBA = doubling or halving of sound



Now let's dig into how the project will impact Park Hills, starting with noise. To give some context, let's take a moment to go over some noise fundamentals. It's fairly obvious that noise is unwanted sound, but how do we measure it? Many of us have heard of a decibel, which is the unit we use to measure the intensity of sound – similar to how we use a foot to measure distance or a pound to measure weight. For highway noise analysis, we use an A-weighted decibel, which measures how sound is perceived by the human ear. The chart to the right gives a feel for the A-weighted decibels associated with common sound sources. For example, when you are right next to a gas-powered lawn mower or near the stage at a rock concert, it will be very loud – with A-weighted decibels in the 90-110 range. When you are vacuuming or making that smoothie in the morning, you are experiencing loud sounds in the range of 70 to 85 decibels. Your normal daily activities like having a conversation and working at your desk expose you to moderate sound levels in the range of 50-65 decibels. As you can see, sound levels fall off quickly from there. An important thing to note is that sound is measured on a logarithmic scale. We won't go into all the math behind that, but it is good to know that a difference of 1 decibel is barely perceptible to the human ear, and a change of 10 decibels is about equal to a doubling or a halving of the noise you hear.

# Noise

## KYTC Noise Policy

- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.



KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dBA, while the threshold for a house receptor is 67 dBA. We also determine if there is a substantial increase – specifically a 10 dBA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dBA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.



# Noise

- Park Hills

- Noise wall reasonable/feasible per KYTC Noise Policy
- Without noise wall = 68 dBA
- With noise wall = 60 dBA

Sound pressure levels (dBA)	Common indoor and outdoor noises
70	Vacuum cleaner at 3m
60	Normal speech at 1m Large business office



*Note. Sound levels represent the average for the area benefited by the wall.*

In Park Hills, one noise wall along the Interstate was found to be reasonable and feasible. It benefits several residences north of Kyles Lane. The average exterior noise levels are predicted to be around 78 decibels without a noise wall and 60 decibels with a noise wall. These sound levels are averages for all the people who will benefit from the walls. The sound levels will vary depending on where each receptor is located. For reference, the average exterior sound levels with the noise wall in Park Hills are predicted to be about what you'd experience in normal conversation.



The red lines on this slide show noise walls that were evaluated as part of the noise study for Fort Wright. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis for Fort Wright. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a noise wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

## Enhancement Measures

- Developing aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, noise walls, and pedestrian paths

KYTC is working to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, noise walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction.

## Cohesive Interstate Treatments



This slide shows some of the potential locations of the enhancements discussed on the previous slide as it relates to Park Hills. While it is an opportunity for a unique gateway at the Kyles Lane interchange, it is important that those treatments do not conflict with those selected for the Dixie Highway interchange.



This slide shows what the finished project might look like from Kyles Lane. Notice the noise wall in Park Hills constructed at the top of the slope adjacent to Notre Dame Academy.





This view shows how the finished project might appear when looking north on I-71 and I-75 at towards Kyles Lane. Notice the opportunities for ramp buffer areas and landscaping in open areas for unique gateways.

# Schedule



This slide shows the project schedule from today to the completion of construction.



# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.



---

[Attachment 5: Exhibits](#)



Brent Spence Bridge Corridor  
(Looking Northwest from Kentucky)



Brent Spence Bridge Corridor - Kentucky  
(Looking Southeast)





---

Attachment 6: Comment Forms

# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify) \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1-2    3-5    6+

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19-44    45-64    65+

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
Other \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify) \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: 2 YEARS OF COLLEGE

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1-2    3-5    6+

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19-44    45-64    65+

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

MOST PEOPLE DO NOT FULLY UNDERSTAND DESIGN & CONSTRUCTION  
TERMS & PROCESSES. A GLOSSARY OF TERMS WOULD BE HELPFUL.  
TRAFFIC SNOOBS & INPUT FROM FIRST RESPONDERS, MAINT. CREWS ETC. WOULD  
ALSO BE GOOD TO HAVE PUBLICLY ACCESSIBLE.

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

Please submit this page only:  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

TEAM  
KENTUCKY  
TRANSPORTATION  
CABINET



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify) \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1-2    3-5    6+

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

#### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM



Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe \_\_\_\_\_

### What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

### How many people live in your household?

- 1 - 2     3 - 5     6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

### What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

### What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

#### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: \_\_\_\_\_

Mailing address (or nearest cross streets): \_\_\_\_\_

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Great Presentation - Great change to make  
Companion Bridge thru Traffic only!

COMMENTS DUE BY  
January 5, 2023

KYTC Item No. 6-17 | ODOT PID 89066

Comments may be submitted:

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](https://PublicInput.com/bsbc)
- By mail using address on back



---

# Brent Spence Bridge Corridor Project Ohio Broad Neighborhood Meeting Summary December 13, 2022

---

## Introduction

The Ohio broad neighborhood meeting was held on December 13, 2022 at two times, from 12:00 pm to 2:00 pm and 5:00 pm to 7:00 pm, at the Lincoln Recreation Center (1027 Linn Street in Cincinnati, Ohio). The Ohio Department of Transportation advertised the meeting via its District 8 Events page; the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)); the December 2022 project e-newsletter; and social media posts on Facebook, Nextdoor, and Twitter (Attachment 1). In addition, flyers advertising the meeting were emailed to representatives of the following neighborhood community councils: Lower Price Hill, Mount Auburn, Mount Auburn-Walnut Hills, Over-the-Rhine, and Pendleton. Attendees at the meeting included the Federal Highway Administration, the Kentucky Transportation Cabinet, the Ohio Department of Transportation, the City of Cincinnati, the project team, and members of the public. In total, six (6) members of the public attended the early meeting and six (6) members of the public attended the later meeting (Attachment 2). Photographs are included in Attachment 3.

These meetings followed an open-house format. The meeting format began with a 30-minute period for the public to view exhibits and speak to the agencies and the project team. A formal presentation by the Ohio Department of Transportation (Attachment 4) followed. Following the presentation, the open-house format resumed. Members of the project team were present to answer questions and respond to feedback during the open-house portions of the meetings.

Exhibits showing the corridor and various renderings were available for viewing during the meeting (Attachment 5). A noise analysis information sheet, comment forms, a demographic survey, and postcard inviting individuals to visit [PublicInput.com/bsbc](http://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Two comment forms and one demographic form were returned at the meeting (Attachment 6).

## Comments

The written comments received at the meeting included the following:

- Glad to see access to Spring Grove is addressed in the current iteration of the project.
- Have we considered moving the land bridges north (5th to 8th, 9th, 9th to Ezzard Charles)?
- The connection between Queensgate and the West End need to be strengthened, not separated. Sound walls/barriers will make this worse than it already is.

In addition, a general comment regarding a concern for all of the construction occurring in the area was raised during the presentation in the early meeting. ODOT responded they are aware of other construction activities, and the Brent Spence Bridge Corridor construction schedule was provided later during the presentation.

All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.



---















## Attachment 1: Advertising Materials

Events

Search Events

Home

Categories

-  Classics
-  Comedy
-  Crafts
-  Dance
-  Drinks
-  Fitness & Workouts
-  Foods
-  Games
-  Gardening
-  Health & Medical
-  Healthy Living & Self-Care
-  Home & Garden
-  Music & Audio
-  Parties







TUESDAY, DECEMBER 13, 2022 AT 12:00 PM – 7:00 PM EST

# Brent Spence Bridge Corridor Neighborhood Outreach

Lincoln Center

### Details

-  32 people responded
-  Event by Ohio Department of Transportation - Cincinnati District 8
-  Lincoln Center
-  Duration: 7 hr
-  Public - Anyone on or off Facebook

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are holding neighborhood outreach meetings for the Brent Spence Bridge (BSB) Corridor Pr... [See more](#)

Causes

**Lincoln Center**  
1027 Linn St, Cincinnati, OH, Cincinnati, OH





Log In

**Guests**

[See All](#)

**1**  
GOING

**31**  
INTERESTED

**Host**



Ohio Department of Transportation - Cincinnati District 8

[Privacy](#) · [Terms](#) · [Advertising](#) · [Ad Choices](#) · [Cookies](#) · [More](#) · Meta © 2022

## Facebook Event Metrics

 BSBC Dec 13 Meeting

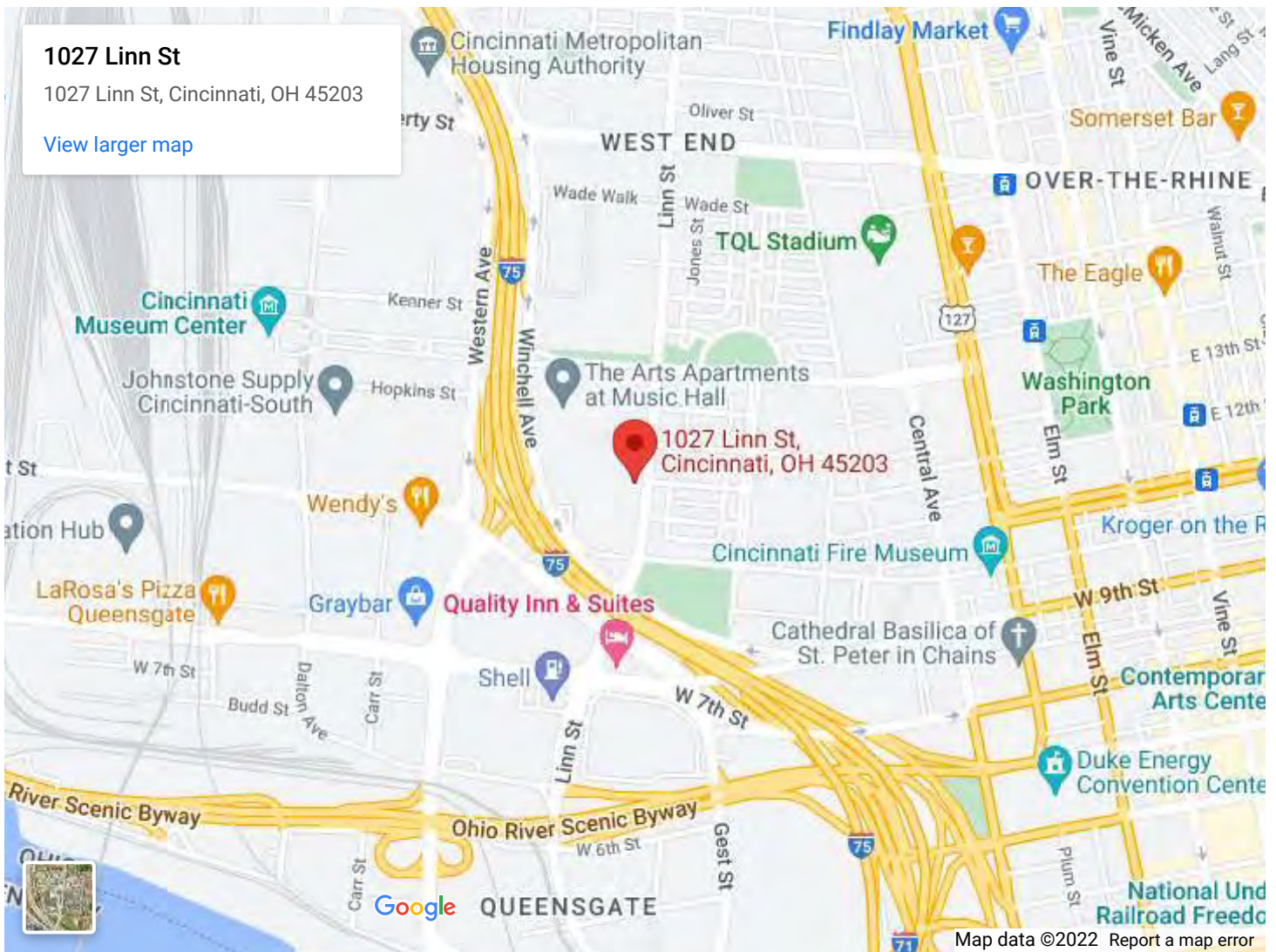
Report Period: Dec 1, 2022 - Dec 13, 2022

Gender	Age	Reach	Impressions	Reporting starts	Reporting ends
female	65+	898	2106	2022-12-01	2022-12-13
male	55-64	822	1923	2022-12-01	2022-12-13
female	55-64	786	1910	2022-12-01	2022-12-13
male	65+	716	1870	2022-12-01	2022-12-13
male	45-54	578	1637	2022-12-01	2022-12-13
female	45-54	562	1495	2022-12-01	2022-12-13
male	35-44	552	1471	2022-12-01	2022-12-13
female	35-44	512	1226	2022-12-01	2022-12-13
male	25-34	442	1175	2022-12-01	2022-12-13
female	25-34	360	1038	2022-12-01	2022-12-13
male	18-24	52	76	2022-12-01	2022-12-13
female	18-24	32	66	2022-12-01	2022-12-13
unknown	25-34	28	75	2022-12-01	2022-12-13
unknown	35-44	20	51	2022-12-01	2022-12-13
unknown	55-64	14	32	2022-12-01	2022-12-13
unknown	45-54	12	22	2022-12-01	2022-12-13
unknown	65+	4	17	2022-12-01	2022-12-13
unknown	18-24		17	2022-12-01	2022-12-13
		6390	16207	2022-12-01	2022-12-13



Please  
Enter Your  
Location

# Brent Spence Bridge Corridor Neighborhood Outreach



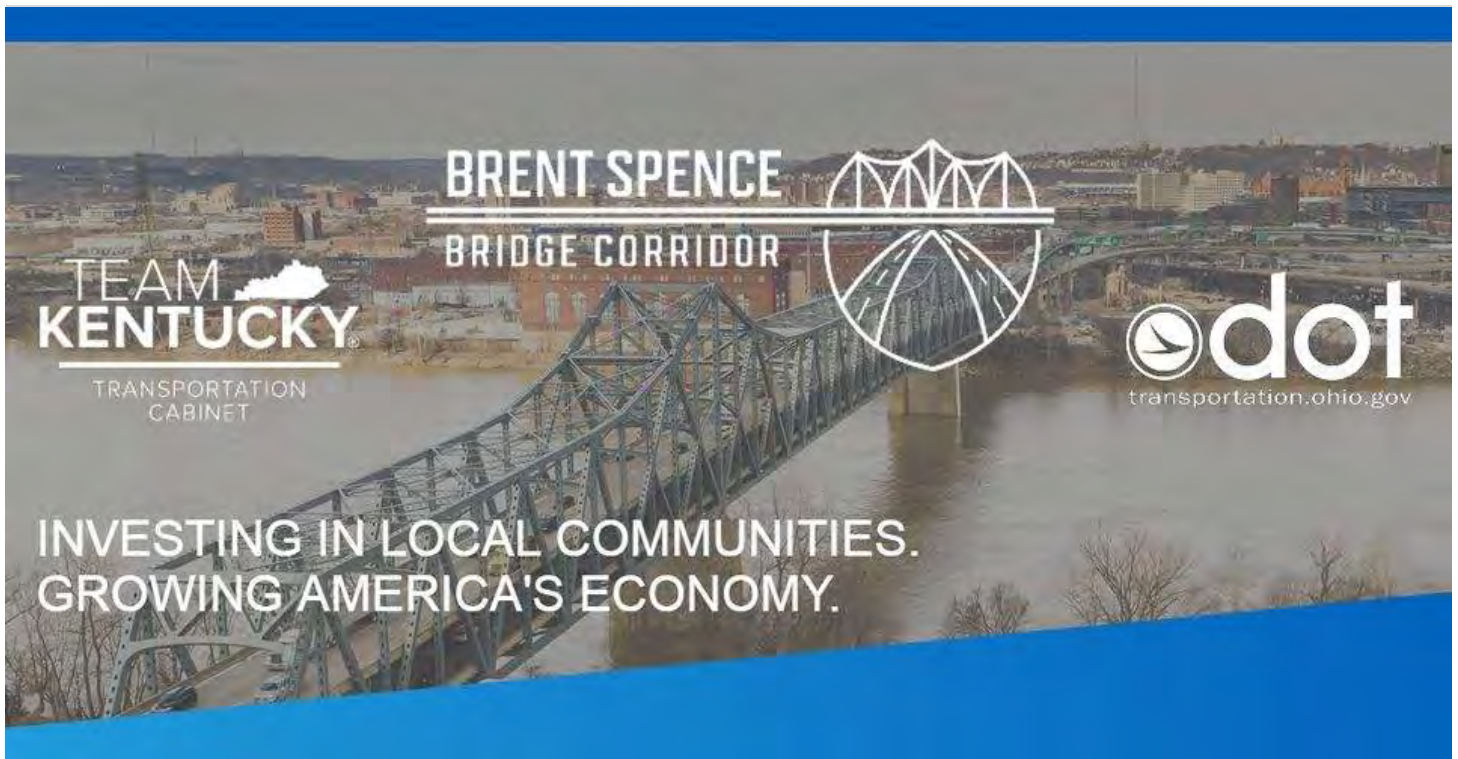
**DEC  
13**

**Tuesday, December 13, 2022**

12:00 PM - 7:00 PM

**Lincoln Recreation Center**

1027 Linn St, Cincinnati, OH 45203

 Add to Calendar

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are holding neighborhood outreach meetings for the Brent Spence Bridge (BSB) Corridor Project.

Daytime (12pm - 2pm) and evening (5pm - 7pm) open-house-style meetings allow residents to browse exhibits, review project information, ask questions, and offer feedback one-on-one with the project team.

A short presentation will take place 30 minutes after the beginning of each meeting.

You may also leave feedback by visiting <https://www.publicinput.com/bsbc>

Si desea que los materiales para esta reunión son traducidos a español, contacte a Domingo Martinez tan pronto que sea posible a [Domingo.Martinez@dot.ohio.gov](mailto:Domingo.Martinez@dot.ohio.gov) o por teléfono a (513) 933-6136.

Public participation is solicited without regard to race, color, sex, age, national origin, or disability. The Ohio Department of Transportation is committed to providing access and inclusion and reasonable accommodation in its services, activities, programs, and employment opportunities in accordance with the Americans with Disabilities Act (ADA) and other applicable laws. To request a reasonable accommodation due to a disability, or language interpretation or translation services to participate in this meeting please contact Keith Smith, (513) 933-6590 or [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov) as soon as possible.

**Brent Spence Bridge Corridor Neighborhood Outreach**  
Nextdoor.com


nextdoor

- Home
- Discover
- For Sale & Free
- Notifications
- Messages

+ Post


Search Nextdoor

S What's on your mind, neighbor?


 **Ohio Department of Transportation** ✓  
Press Secretary Matt Bruning • Edited 4 days ago



Brent Spence Bridge Corridor Neighborhood Outreach. The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are holding neighborhood outreach meetings for the Brent Spence Bridge (BSB) Corridor Project on Tuesday, December 13 at the Lincoln Recreation Center (1027 Linn St, See more...

 **Brent Spence Bridge Corridor Neighborhood Outreach**  
[transportation.ohio.gov](http://transportation.ohio.gov)

 [1027 Linn Street, Cincinnati, OH](#)

 Share

ODOT

Twitter.com



**ODOT Cincinnati** ✓  
@ODOT\_Cincinnati



Hope to see you there!

 **Brent Spence Bridge Corridor** @BSBCorridor · 17h

Hear the latest. Join us for our next Ohio meeting:

Tuesday, Dec. 13

Daytime: noon – 2pm (30-minute presentation at 12:30pm)

Evening: 5pm – 7pm (30-minute presentation at 5:30pm)

Lincoln Recreation Center  
1027 Linn Street, Cincinnati  
@ODOT\_Cincinnati





CONTACT US



# PUBLIC INVOLVEMENT AND COMMENTS



LEARN MORE ABOUT HOW THE BRENT SPENCE BRIDGE CORRIDOR PROJECT TEAM IS WORKING WITH THE COMMUNITY TO ENSURE STAKEHOLDER VOICES ARE HEARD AND COMMUNITY NEEDS ARE INTEGRATED INTO THE PROJECT PLAN.

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are fully committed to robust public involvement to gain insights and receive feedback as part of the project development process. The project team has been working closely with local partners to ensure communities surrounding the project area have the opportunity to provide feedback.

If you have a comment or question for the project team, please provide your feedback in the link at the bottom of the page.

## PUBLIC COMMENTS

Below are responses from the project team to specific public comments that have been received to date.

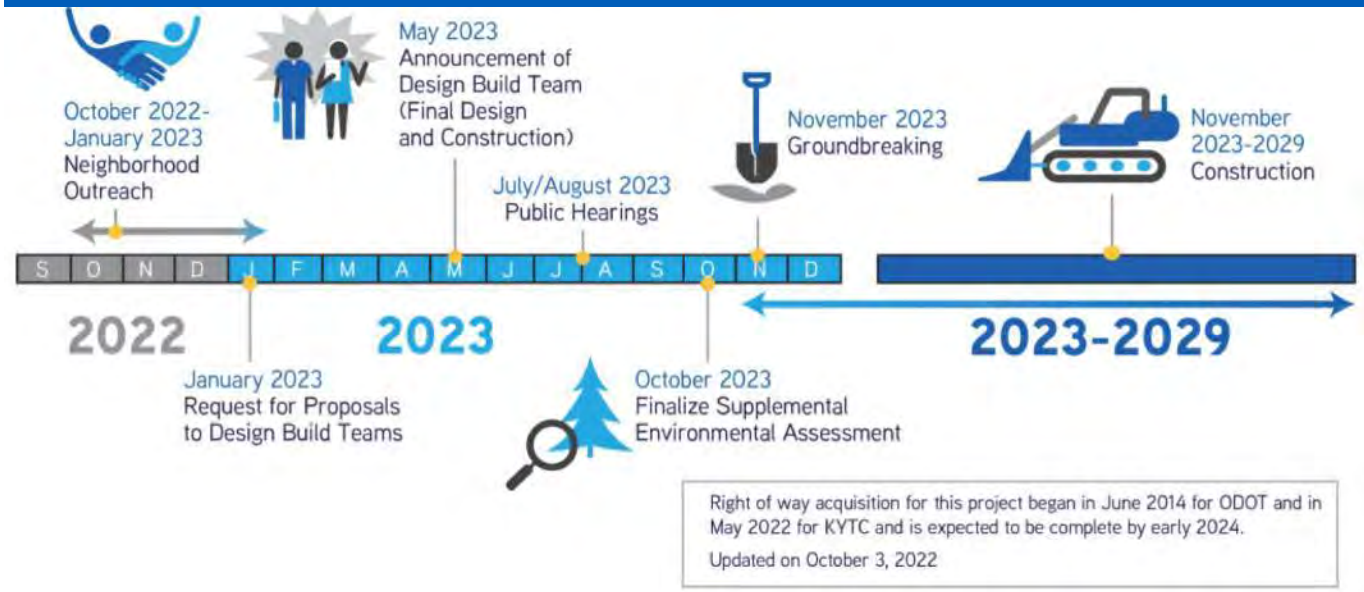
- Response to public comment – [WORKING POSITION PAPER: REDESIGN OF THE BRENT SPENCE BRIDGE PROJECT](#)
- Response to public comment – [BRENT SPENCE BRIDGE PROJECT – RECONNECTING CINCINNATI WESTWAY DESIGN IMPROVEMENTS](#)

Privacy - Terms

Below are summaries of comments received by the project team, including responses to each.

- [General Public Comment and Response Summary \(updated monthly\)](#)
- [Neighborhood Meeting Survey Comment and Response Summary \(coming in January 2023\)](#)

## PROJECT SCHEDULE



## PROJECT AREA MAPS

Below are detailed maps of the project corridor, including an overall map, a multimodal map including paths for pedestrians and bicyclists, a traffic flow map, and project renderings with illustrations of how the corridor could look when complete.

- [Project Corridor Map](#)
- [Multimodal Map](#)
- [Traffic Flow Map](#)
- [Project Renderings](#)

## NEIGHBORHOOD OUTREACH MEETINGS





To more thoroughly engage communities within the project for those residents. These events feature a presentation by survey, and offer feedback one-on-one with the project team.

[CONTACT US](#)

DOT have been holding and allow residents to

### KENTUCKY NEIGHBORHOOD MEETINGS

- Mainstrasse, 11/21/2022
- Friends of Peaselburg, 11/28/2022
- Westside Covington, 11/29/2022
- Fort Mitchell, 11/30/2022
- Fort Wright, 12/1/2022
- Mutter Gotes/CBD, 12/5/2022
- Lewisburg/Botany Hills, 12/6/2022
- Park Hills – 7-8:30 p.m., 12/12/2022, Garden of Park Hills, 1622 Dixie Highway, Park Hills, KY

### KENTUCKY BROAD NEIGHBORHOOD OPEN HOUSE MEETING

Wednesday, December 14, 2022, 12-2 p.m. and 5-7 p.m.  
 (presentations at 12:30 p.m. and 5:30 p.m.)  
 Kenton County Fiscal Court (Riedlin Schott Community Room)  
 1840 Simon Kenton Way, Covington, KY

### OHIO NEIGHBORHOOD MEETINGS

- CUF, 11/15/2022
- West End – 6 p.m., 12/20/2022, Lincoln Recreational Center, 1027 Linn St., Cincinnati
- CBD Riverfront, 11/29/2022
- Camp Washington – 7p.m., 12/12/2022, Camp Washington Urban Revitalization Corp., 2951 Sidney Ave

### OHIO BROAD NEIGHBORHOOD OPEN HOUSE MEETING

Tuesday, December 13, 2022, 12-2 p.m. and 5-7 p.m.  
 (presentations at 12:30 p.m. and 5:30 p.m.)  
 Lincoln Recreation Center  
 1027 Linn Street, Cincinnati, OH

## PUBLIC HEARINGS



Public hearings will be held in July/August of 2023, once the Supplemental Environmental Impact Statement (SEIS) is made publicly available. The Supplemental EA and information and exhibits about the project and its expected impacts on the environment, including human and natural environment, will be available for review at the hearings and in the document center on the project website.

[CONTACT US](#)



One public hearing will be scheduled in each state, and a virtual public hearing will also be offered. Dates, times, and locations for public hearings have not been determined yet. You can be notified when the meetings are scheduled by signing up for project updates.

## GET IN TOUCH

Thank you for visiting the Brent Spence Bridge Corridor website. If you have a question or comment for the project team, please complete the form below.

NAME

EMAIL

MESSAGE

ZIP CODE

## SIGN UP FOR UPDATES

Please enter your contact information to receive periodic updates, including notification of upcoming public meetings.

NAME

EMAIL

[SIGN UP](#)

protected by reCAPTCHA  
[Privacy](#) - [Terms](#)

[SUBMIT](#)



[CONTACT US](#)



Si desea asistencia traduciendo esta sitio web a español, contacte a Domingo M. Martinez@dot.ohio.gov teléfono a 513-933-6136.



Ohio Department of Transportation | 1980 W Broad St | Columbus, OH 43223



Kentucky Transportation Cabinet | 200 Mero St | Frankfort, KY 40622

© 2022 Brent Spence Bridge Corridor. All Rights Reserved.

**From:** Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>  
**Sent:** Monday, December 5, 2022 3:58 PM  
**To:**  
**Subject:** Brent Spence Bridge Corridor Project Update - DBE Matchmaker Event

Brent Spence Bridge Corridor December 5, 2022 eNewsletter



## DBE MATCHMAKER EVENT TAKING PLACE DEC. 7

*Small, Minority and Women-Owned Businesses Encouraged to Learn  
about Potential Work Opportunities*

The Brent Spence Bridge Corridor project will offer unprecedented opportunities for small, minority, and women-owned businesses in our region. To support the inclusion of Disadvantaged Business Enterprise (DBE) consultants and contractors on the Brent Spence Bridge Corridor Project, the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) will host two events Wednesday, Dec. 7 at the Anderson Pavilion, 8 Mehring Way, in Cincinnati.

From 9 a.m. to noon, representatives from ODOT and KYTC will provide an overview of the project for DBE firms. The presentation will include details about prequalification requirements for the project and support services available to DBE consultants and contractors.

From 1:30 to 4:30 p.m., ODOT and KYTC will conduct a “matchmaker” event in which DBE firms can meet one-on-one with prime contractors and firms who work directly with the government and manage subcontractors and consultants.

“This is a great opportunity for the industry,” said ODOT’s Tom Arnold of the afternoon event. “Each DBE gets 15 minutes to present their capabilities and work experience to a number of

prime contractors. Our goal is to help our prime design and construction firms understand how many great small businesses they can work with to meet our project inclusion goals.”

Arnold, a member of the project’s Diversity and Inclusion Outreach Committee, added ODOT and KYTC plan to issue a request for proposals for the project’s prime contractor early next year. Construction is scheduled to begin late next year or early 2024.

“We strongly recommend DBE firms [preregister](#) for both events because we want to help make as many connections as possible,” Arnold said. He also recommended DBE firms follow ODOT’s Outreach [webpage](#) for additional updates and events.

### Schedule of DBE Events

Wednesday, Dec. 7 | Anderson Pavilion  
8:30 to 9 a.m.: Registration  
9 a.m. to Noon: Project Overview  
1:30 to 4:30 p.m.: Matchmaker Event with Prime Contractors

## ODOT AND KYTC CONTINUE ROBUST PUBLIC OUTREACH THROUGH NEIGHBORHOOD MEETINGS

*Public Invited to Attend to Ask Questions and Share Feedback*

The joint project team from ODOT and KYTC continues to gather valuable feedback from communities surrounding the Brent Spence Bridge Corridor project area through a public meeting and listening tour. Suggestions gathered during these sessions will enhance the overall project, including opportunities to reconnect neighborhoods, improve bike and pedestrian safety, and reduce the project’s footprint.

Through the end of 2022 and into 2023, the project team will continue meeting one-on-one with directly affected communities to answer questions and gather ideas.

As the project gets under way, ODOT and KYTC will continue to explore ways to improve pedestrian access, improve safety, and support better access to business districts while reclaiming public land for local use.

During the eight meetings conducted to date, residents and local officials have identified opportunities that are now part of the plan, including enhanced streetscape amenities, aesthetic treatments, and bicycle and pedestrian infrastructure. All will improve connectivity to transit, jobs, health care, cultural, recreational and commercial destinations.

In addition to the neighborhood meetings, the public will be provided the opportunity to review project materials and provide formal comments at public hearings currently scheduled for July/August 2023.



**KENTUCKY'S UPCOMING NEIGHBORHOOD MEETING SCHEDULE**

<b>Neighborhood</b>	<b>Meeting Date</b>	<b>Meeting Time</b>	<b>Meeting Location</b>
Mutter Gottes/CBD	December 5, 2022	7:00 p.m.-8:00 p.m.	Kenton County Public Library 502 Scott Street, Covington
Lewisburg/Botany Hills	December 6, 2022	7:00 p.m.-8:00 p.m.	Holiday Inn Express & Suites 200 Crescent Avenue, Covington
Park Hills	December 12, 2022	7:00 p.m.-8:30 p.m.	Garden of Park Hills 1622 Dixie Highway, Park Hills
Broad Neighborhood Meeting	December 14, 2022	12:00 p.m.-2:00 p.m. 5:00 p.m.-7:00 p.m.	Kenton County Fiscal Court (Riedlin Schott Community Room) 1840 Simon Kenton Way, Covington
Public Hearing	TBD July/August 2023	TBD	TBD

**OHIO'S UPCOMING NEIGHBORHOOD MEETING SCHEDULE**

Neighborhood	Meeting Date	Meeting Time	Meeting Location
Camp Washington	December 12, 2022	7:00 p.m.-8:30 p.m.	Camp Washington Urban Revitalization Corporation 2951 Sidney Avenue, Cincinnati
Broad Neighborhood Meeting	December 13, 2022	12:00 p.m.-2:00 p.m. 5:00 p.m.-7:00 p.m.	Lincoln Recreation Center 1027 Linn Street, Cincinnati
West End	December 20, 2022	6:00 p.m.-8:00 p.m.	Lincoln Recreation Center 1027 Linn Street, Cincinnati
Public Hearing	TBD July/August 2023	TBD	TBD

## REVISED PROJECT DESIGN RETURNS 10 ACRES TO CINCINNATI CENTRAL BUSINESS DISTRICT

In its continuing collaboration with Cincinnati’s elected officials and community leaders, the Brent Spence Bridge Corridor Project team redesigned plans for the area immediately west of the Duke Energy Convention Center, freeing up 10 acres for future development.

Last summer the city and the Cincinnati USA Regional Chamber submitted comments and requested changes to the Ohio side of the corridor to enlarge the central business district, which were approved by the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC).

This creates “some really exciting opportunities for large-scale investments,” said Cincinnati Mayor Aftab Pureval at a news conference last month. “We want to be a city that’s growing, and these 10 acres right in the middle of our downtown” will be like pouring “gasoline on that effort.”

Changes include refinements to the entrance and exit ramps on Third, Fourth, Fifth, and Sixth streets to better integrate them into the city’s street network. The revised plan also removes one exit ramp from Interstate 75 South.

Though the city has no definite plans for the reclaimed area, Pureval said he envisions creating a place where people “can either work or enjoy retail and restaurants.” He added the 10 acres has an estimated value of \$20 million and could generate more than \$500,000 a year in taxes.

Brendon Cull, president of the chamber, said adding 10 acres in downtown Cincinnati represents a significant economic opportunity for the city and region. “Our goal all along has been to make sure that the Brent Spence Bridge gets done and gets done in a way that maximizes opportunity for our region,” he said. “This successful effort will create more economic opportunity for the people who live here and work here, and that’s what matters.”



Image from Spectrum News 1

[Read more here.](#)

## Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

[Visit our website](#)

[Sign up for updates](#)

[Submit a comment](#)

© 2022 Brent Spence Bridge Corridor. All Rights Reserved.

Brent Spence Bridge Corridor | 505 South State Route 741, Lebanon, OH 45036

[Unsubscribe jheflin@hntb.com](mailto:jheflin@hntb.com)

[Update Profile](#) | [Constant Contact Data Notice](#)

Sent by [info@brentspencebridgecorridor.com](mailto:info@brentspencebridgecorridor.com) powered by





**BRENT SPENCE**  
**BRIDGE CORRIDOR**



**TEAM KENTUCKY**  
TRANSPORTATION  
CABINET



**INVESTING IN LOCAL COMMUNITIES.  
GROWING AMERICA'S ECONOMY.**

## You're Invited

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are holding **neighborhood outreach meetings** for the Brent Spence Bridge (BSB) Corridor Project

**Purpose:** To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team

**Format:** Daytime and evening open-house-style meetings to allow residents to browse exhibits, review project information, ask questions, and offer feedback one-on-one with the project team.

*A short presentation will take place 30 minutes after the beginning of each meeting.*

**Day:** Tuesday | December 13, 2022

**Times:** Daytime | 12:00pm – 2:00pm (30-minute presentation at 12:30pm)  
Evening | 5:00pm – 7:00pm (30-minute presentation at 5:30pm)

**Where:** Lincoln Recreation Center  
1027 Linn Street, Cincinnati, OH 45203

You may also leave feedback by scanning the code at right or visiting [www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc).

Si desea que los materiales para esta reunión son traducidos a español, contacte a Domingo Martinez tan pronto que sea posible a [Domingo.Martinez@dot.ohio.gov](mailto:Domingo.Martinez@dot.ohio.gov) o por teléfono a (513) 933-6136.



Public participation is solicited without regard to race, color, sex, age, national origin, or disability. The Ohio Department of Transportation is committed to providing access and inclusion and reasonable accommodation in its services, activities, programs, and employment opportunities in accordance with the Americans with Disabilities Act (ADA) and other applicable laws. To request a reasonable accommodation due to a disability, or language interpretation or translation services to participate in this meeting please contact Keith Smith, (513) 933-6590 or [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov) as soon as possible.



---

## Attachment 2: Sign-In Sheets

(12 pm to 2 pm meeting)

BRENT SPENCE  
BRIDGE CORRIDOR



TEAM  
KENTUCKY



SIGN-IN SHEET

# BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting

Name	Representing	Address	Phone	Email
1. Dorothy Bush	SFCC			
2. Maddie Tzianus	DK Architects			
3. Margo Aug	St. Joseph Church			
4. Gregory Lang	LPH Bus. Alliance			
5. Sebastian Avila	M/SP H			
6. FATHER PET TAYLOR	GOD			
7.				
8.				
9.				
10.				
11.				
12.				
13.				
14.				
15.				
16.				
17.				
18.				
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.

page 1 of 1

(5 pm to 7 pm meeting)

BRENT SPENCE  
BRIDGE CORRIDOR



TEAM  
KENTUCKY



SIGN-IN SHEET

## BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting

Name	Representing	Address	Phone	Email
1. DAVID JONES	Cincinnati			
2. Sara Jones	Cincinnati			
3. Bridget Patton	Cincinnati			
4. Bill Shefak	Cincinnati			
5. Monica Windholtz	"			
6. Anthony Ignas	myself			
7.				
8.				
9.				
10.				
11.				
12.				
13.				
14.				
15.				
16.				
17.				
18.				
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates

page 1 of 1



---

## Attachment 3: Photographs

## Ohio Broad Neighborhood Meeting

December 13, 2022



Photograph 1. Ohio Broad Neighborhood Meeting held on December 13, 2022.



Photograph 2. Ohio Broad Neighborhood Meeting held on December 13, 2022.

## Ohio Broad Neighborhood Meeting

December 13, 2022



Photograph 3. Ohio Broad Neighborhood Meeting held on December 13, 2022.



Photograph 4. Ohio Broad Neighborhood Meeting held on December 13, 2022.

**Ohio Broad Neighborhood Meeting**

**December 13, 2022**



Photograph 5. Ohio Broad Neighborhood Meeting held on December 13, 2022.





---

Attachment 4: Presentation



The meeting will open with remarks by the City of Cincinnati (5 min) and introductions of the Project Team in attendance (2 min).

## Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in neighborhoods surrounding the project the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Changes since 2012
  - Noise
  - Aesthetics
  - Pedestrians and bicycles

The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like. Most importantly, tonight we're focusing on hearing from residents. We're here to answer your questions and to listen to your feedback about the project, which you can do one-on-one with the project team during the open house portion of this meeting, following the presentation.

## Project History

2000

2005

2010

2015

2020

← 2004 | ODOT & KYTC begin BSB Corridor Project

Alternatives Development & Evaluation  
Preliminary Engineering  
Impact Assessment  
Public & Stakeholder Input

← 2012 | EA & FONSI - Preferred Alternative I

Design Refinements  
Cost/Impact Reductions  
Enhancements  
Property Acquisition  
Funding Identification

← 2022 | Concept I-W

3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Ohio, that includes I-75 from the Brent Spence Bridge to just north of the Western Hills Viaduct. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

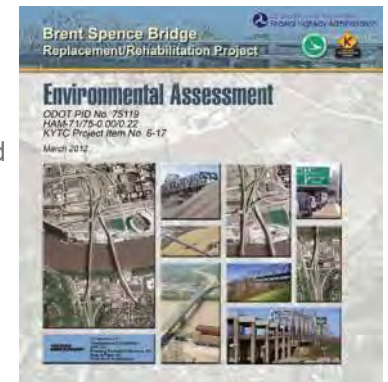
Since the approval of the FONSI in 2012, ODOT has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, ODOT has purchased most of the land needed to build the project. In addition, we've begun detailed design from Linn Street to the northern project limits.

3

## Purpose and need

- Improve traffic flow and level of service
- Improve safety
- Correct geometric deficiencies
- Maintain connections to key regional and national transportation corridors



4

The project purpose and need was established early in the project development and has not changed since the 2012 EA/FONSI.

4

## Project Description

### Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



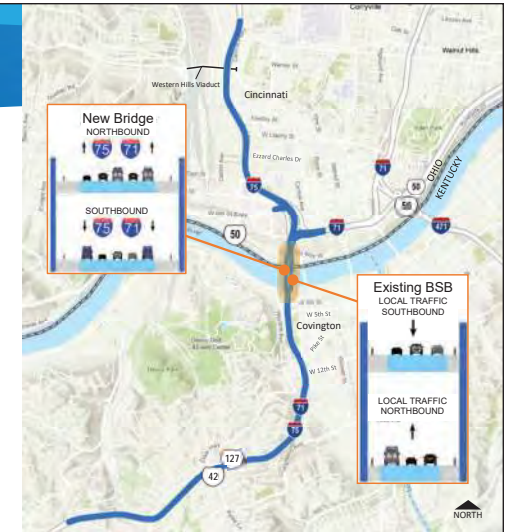
Note: Individual project elements will come in with click (marked by #).

In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. (#) Beginning near Ezzard Charles Drive, a collector-distributor system will be added to connect I-75 traffic to and from the local street network and US-50 West. (#) In the north, it will tie into the recently completed Mill Creek Expressway-Hopple Street Interchange project. (#) The project will rebuild the I-75 interchange at the Western Hills Viaduct and tie into the new bridge replacement project being developed by the City of Cincinnati and Hamilton County. (#) A new northbound exit will be built at Ezzard Charles Drive to improve access to Union Terminal, TQL Stadium, and Over-the-Rhine. Lastly, it will connect to I-71 and US-50E.

## Project Description

### Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

## Project Description

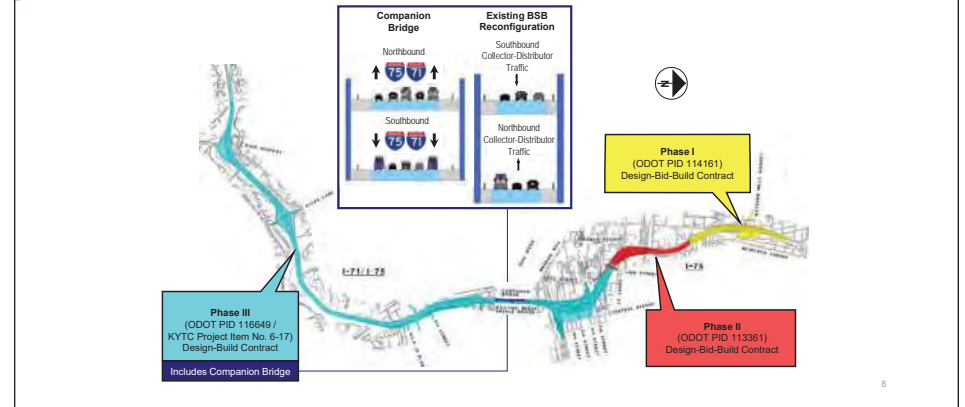
### Kentucky

- Widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct collector-distributor systems in Covington and Ft. Mitchell/Ft. Wright



In Kentucky, the project will widen I-71 and I-75 and rebuild all overpass bridges and interchanges. The project will also extend existing frontage roads to improve connectivity in Covington. A collector-distributor system will also be built beginning in Covington to connect interstate traffic to and from the local street network. Lastly, collector-distributor ramps will be built in Fort Mitchell and Fort Wright to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

## Project Description



The project is going to be built in three phases. Phase I (shown in yellow) will stretch from Findlay Street to the north. Phase II (shown in red) will stretch from Linn Street to Findlay Street. Phase III (shown in blue) will build everything else, including the new companion bridge. Phase I is currently under design with construction expected to begin in 2028. Although, the construction time frame may adjust based on on-going coordination with the City's Western Hills Viaduct Project. Phase II is also under design with construction expected to begin in 2025. Phase III will be built under a progressive design-build contract which will begin in 2023.

## What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on Kentucky frontage roads
- Reduced relocations
  - Residential | 40+ then vs. 4 now
  - Commercial | 14 then vs. 13 now

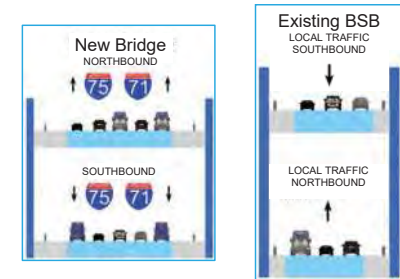


As mentioned earlier, since the 2012 EA and FONSI, ODOT and KYTC have been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. The design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet. In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55 mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design and allows us to reduce the overall area needed to build the roadways. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residences who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

## What Has Changed?

- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced

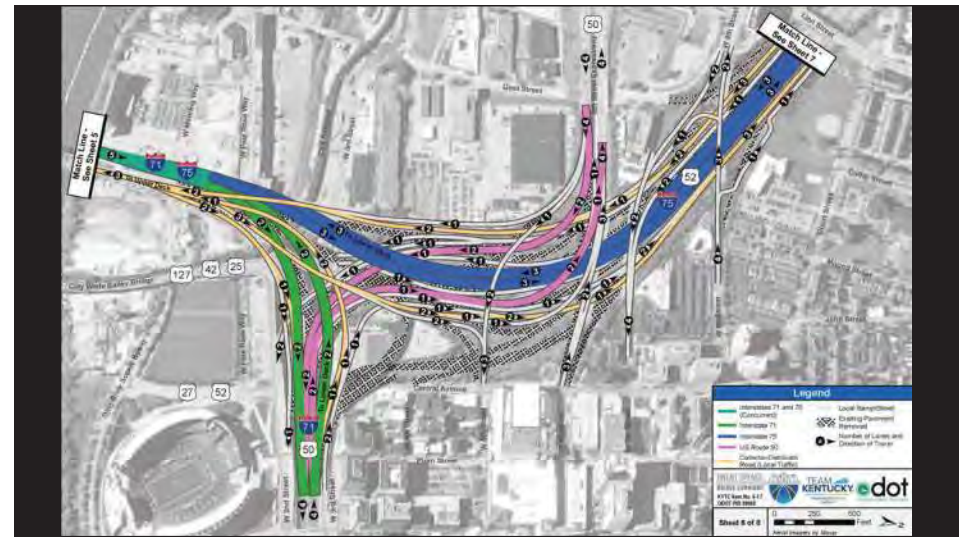


One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



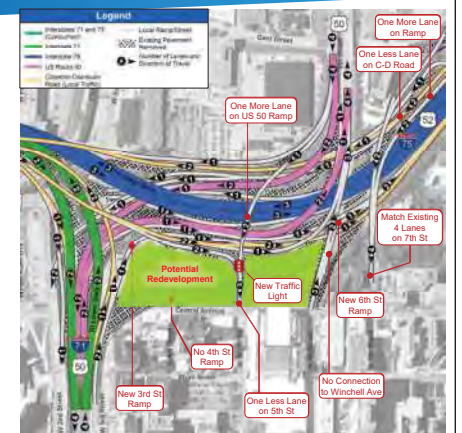
This slide shows how traffic will through Cincinnati. The blue lines show how I-75 traffic will travel to and from the companion bridge. The green lines show how traffic will on I-71 will be routed to and from the companion bridge. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets. Finally, the pink lines show how traffic will flow on US 50.



This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

## What Has Changed?

- Reconfigured Downtown Cincinnati Ramps



**Note: Click to walk through ramp changes (marked by #)** | Another change is the layout of the ramps in Downtown Cincinnati. The smaller companion bridge reduced the project footprint and ODOT optimized the interchange layout to use land formerly occupied by the Dunnhumby building, allowing more land to be given back to the City of Cincinnati. More recently, the City asked ODOT to evaluate ramp changes to open up additional land for redevelopment. (#) Based on the City's request, the 4th street ramp to NB I-75 was removed. (#) To provide access for traffic that would have used 4th Street, a new entrance ramp to NB I-75 was added to 3rd Street. (#) The SB exit to 5th Street was removed, which also resulted in fewer lanes on the SB collector-distributor road and at the Central Avenue intersection. The 7th Street exit was also widened to provide additional lanes for traffic that would have used the 5th Street exit. (#) The NB exit to 5th Street was moved closer to the highway to intersect the US 50 ramp at a traffic light, and one more lane was added to the US 50 ramp to make sure all traffic could move smoothly through the light. (#) The 6th Street connection to Winchell Avenue was removed and replaced with a new connection between 6th Street and the NB collector-distributor road. (#) All these changes will open up about 9.5 acres of land for redevelopment, which has been a consistent comment we've been hearing from the public in the last several months.





This drawing shows what the Downtown interchange area might look like once the project is built.

## What Has Changed?

- Northbound I-75 entrance ramp moved from Freeman to Winchell
- One Ezzard Charles Drive bridge
- Minimize work along Winchell



Note: Changes will come in with click (marked by #).

Another change ODOT's made since 2012 involves moving the northbound entrance ramp to I-75 from its existing location at (#) Freeman Avenue (south of Ezzard Charles Drive) (#) to Winchell Avenue (north of Ezzard Charles Drive). Also, (#) the two existing one-way bridges on Ezzard Charles Drive will be replaced with one, two-way bridge. These changes were also made in coordination with the City of Cincinnati to reduce project impacts and costs and improve local access to the interstate.

We've also refined the roadway layouts so that work will not occur along Winchell Avenue beyond what is needed to build the northbound entrance ramp shown in this drawing.

## What Has Changed?

- Interchange at the Western Hills Viaduct

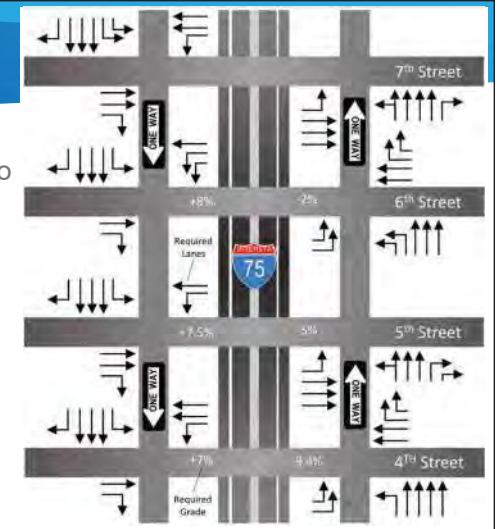


In addition, the layout of the project has been refined to tie into the new Western Hills Viaduct, which is a separate, stand-alone project being developed by the City and County. The existing ramps at the I-75 interchange will be removed and replaced with new ramps that will provide direct access to and from the new Western Hills Viaduct bridge. The ramps will also connect I-75 to the local street system at Findlay Street for southbound traffic and Bank Street for northbound traffic.

## Other Concepts

Additional changes were evaluated but not incorporated into the design

- Depressing I-75 and extending downtown streets to form an urban street grid
  - Cannot meet current design criteria
  - Introduces safety concerns due to steep grades



ODOT has also received several comments about depressing I-75 through downtown Cincinnati similar to Fort Washington Way. This graphic illustrates schematically what that concept would look like. I-75 would be depressed, and the downtown streets would be reworked so that the east-west streets connected across the highway. Two, one-way frontage roads would be built on either side of the highway to move north-south traffic. The combination of these north-south frontage roads and the east-west streets would form a grid, also called an "urban street grid."

ODOT supports concepts like this when the site conditions lend themselves to this type of design. However, the conditions on and around I-75 do not support this type of design. Just north of the Ohio River, I-75 must over a railroad, and it would require very steep grades (around 8%) in order to be depressed through downtown. This doesn't meet current design standards which state the maximum grade must be 5%. In addition, such steep grades would introduce safety concerns, particularly given the large number of trucks that travel on I-75.



Any design must accommodate a complicated system of mainline and ramp movements to pass over the railroad and provide local access and continuity along I-71, I-75, and US 50. Weaving these ramps through the project area requires a complex tiered bridge system with the highest tier rising over 50 feet above the ground. To meet these geometric constraints, the preferred alternative for the BSB Corridor Project currently descends toward downtown Cincinnati at a 4- to 5.5-percent grade. Depressing the roadway any further would require substantially steeper roadway grades (8% or more). For reference, ODOT's *Location and Design Manual, Volume 1* lists the maximum grade for urban interstates in similar terrain as 6 percent.

## Other Concepts

Additional changes were evaluated but not incorporated into the design.

- Depressing I-75 and extending downtown streets to form an urban street grid
  - Cannot meet current design criteria
  - Introduces safety concerns due to steep grades
  - Increases project footprint with steep local streets
  - Increases traffic through downtown Covington

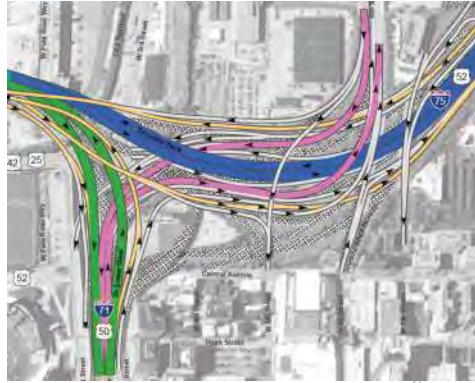
Since I-75 cannot be lowered, local streets would need to be raised to form an urban street grid across I-75. This would increase the project footprint because the east-west streets would need to be rebuilt well beyond where they intersect the highway. This additional footprint would result in new impacts to existing businesses and potential developable land. For example, the elevated cross streets would create steep slopes that do not meet design criteria for local streets (as shown in this graphic) and would negatively affect potential development opportunities for urban, street-facing development on 2.5 to 3.0 acres of land east of I-75 that is currently slated to be transferred to the City of Cincinnati once the project is completed.

Moving all local traffic to an urban street grid as opposed to the collector-distributor system currently included in Concept I-W would also substantially increase traffic on the local streets in Covington.

## Other Concepts

Additional changes were evaluated but not incorporated into the design.

- Depressing I-75 and extending downtown streets to form an urban street grid
  - Cannot meet current design criteria
  - Introduces safety concerns due to steep grades
  - Increases project footprint with steep local streets
  - Increases traffic through downtown Covington
  - Does not maintain through traffic on US 50



Finally, building an urban street grid would not allow US 50 – which is shown in pink above – to remain where it is today. To make room for the new “grid,” US 50 would have to start and stop on both sides of I-75. ODOT’s also received several comments about improving north-south connections and east-west connections across I-75. However, as illustrated by this graphic, Concept I-W maintains north-south connectivity through the collector-distributor system shown in orange. East-west connectivity across I-75 is provided by local streets shown in grey and US 50 shown in pink.

## Noise

### ODOT Noise Policy

- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds

### Noise Analysis Manual



ODOT has a formal Noise Policy that guides how traffic noise is evaluated for transportation projects, which was developed in partnership with the Federal Highway Administration. The first thing we examine is whether there are areas where people congregate outside. This could include someone’s yard, a patio, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. We also determine if there is a substantial increase in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, “Can it effectively block noise?” In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then ODOT evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

## Noise

- Project Area
- Proposed Noise Wall
- Proposed Noise/  
Visual Screening



In Ohio, noise was not evaluated for areas west of I-75, because there were too few receptors, and they are located far apart from one another. A noise wall north of the Western Hills Viaduct and east of I-75 was not feasible because it could not provide a minimum 5dBA reduction for 40% of the impacted receptors. It also could not meet the cost effectiveness criteria for reasonability. In the West End neighborhood, noise walls were not found to be feasible because there were not areas of frequent human exterior use next to the highway. For example, the existing apartment complexes and buildings don't have outdoor patios or other similar areas. In addition, a noise wall along the Queensgate Playground and Ballfields did not meet the cost effectiveness criteria for reasonability.

## Noise - West End (OH)

Noise walls not reasonable/feasible per ODOT Noise Policy

- Tire pavement noise = major traffic noise contributor
- Noise reducing measures proposed:
  - 57-inch median and bridge barrier
  - 8-foot-tall extended traffic safety barrier
- Provide substantial noise reduction
- Provide visual screening of highway



In 2012, ODOT stated that it would build noise walls between Freeman Avenue and Bank Street based on the noise analyses that were conducted at that time. Even though these noise walls are no longer warranted according to ODOT's noise policy, ODOT recognizes the I-75 corridor will remain the predominant noise source within the project area. To reduce overall traffic noise levels, ODOT intends to build barriers that will reduce tire pavement noise, which is a major contributor to traffic noise. ODOT will build 57-inch barriers in the I-75 median and on the Liberty, Findlay, and Bank street bridges. For comparison, a standard bridge barrier is over a foot shorter at 42 inches. In addition, ODOT will build 8-foot-tall extended traffic safety barriers along the I-75 shoulders from south of Ezzard Charles Drive to Bank Street. Some examples of what these walls will look like are pictured on this slide.

The 8-foot-tall barriers will provide a substantial reduction in noise levels. Research has shown that 8-foot-tall concrete traffic barriers will approximately cut traffic noise levels in half, which is what traditional noise barriers are expected to do. They will also have aesthetic treatments to help them fit into the community and will help to provide visual screening between I-75 and the West End neighborhood.

## Aesthetics

- Corridor-wide aesthetic treatments
  - Ashlar stone treatments for abutments, piers, and walls
  - Decorative bridge parapets
  - Translucent screen walls and planters on bridges
  - Other features (lighting, sidewalks, etc.) to match City standards



ODOT has worked with the City and the project Aesthetic Committee to develop an Aesthetic Design Checklist that will guide what the Brent Spence Bridge Corridor will look like. For example, ODOT will add aesthetic treatments in a pattern called “Ashlar Stone” to all bridge abutments, parapets, and piers. Retaining walls and the extended traffic safety barriers we just discussed will also have ashlar stone treatments. Overhead bridge parapets will have end treatments with the bridge identification name, construction completion data, and rustification design features. Examples of similar bridge parapets are shown in the images on this slide. In addition, overpass bridges will have wide sidewalks or shared-use paths, planters, and translucent screen walls. Other features throughout the corridor will match City aesthetic standards, including lighting, sidewalks, tree lawns, and others.

## Pedestrians and Bicycles

- Connections across I-75
  - Sidewalks
  - Shared use paths
  - Bike lanes
- New shared use path along Winchell Avenue
- Improved connections to local destinations
- Improved connections between local communities



The project will install bicycle and pedestrian infrastructure on connections across I-75 like what is shown here for 6th, 7th, and 9th streets. Pedestrian and bicycle connections will also be provided on Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue. To promote safety for bicycles and pedestrians, the ramp connections with local streets are being designed as lower-speed urban intersections in accordance with City of Cincinnati design standards. The pedestrian and bicycle infrastructure included in the project will improve connectivity to transit, employment, healthcare, cultural, recreational, and commercial destinations.



This exhibit shows some of the planned pedestrian and bicycle facilities. Note the new shared use paths on 6th, 7th, and 9th streets and Winchell Avenue and new sidewalk connections on 9th.



Continuing north, we see how the new shared use path on Winchell Avenue continues all the way to Ezzard Charles Drive. Also notice new shared use paths on Linn Street and Ezzard Charles Drive, new sidewalks along Guest Street, Court Street, Freeman, and Ezzard Charles, a new pedestrian bridge over Winchell Avenue, and new bike lanes on Liberty Street.



As we move further north, notice the new bike lanes on Findlay Street, the new shared use path on Harrison Avenue, and the replaced sidewalks on Harrison, Bank, and Findlay. These will tie into the existing bike lanes on Western, Winchell, and Bank. Throughout the corridor the new and improved pedestrian and bicycle facilities will provide connections to existing transit stops.



This is a view of what the Firefighters Memorial will look like when the project is built. Notice the highway and ramps in the background and the ashlar stone treatments on the retaining walls. Also notice the areas currently shown as green space that are opened up for potential development.





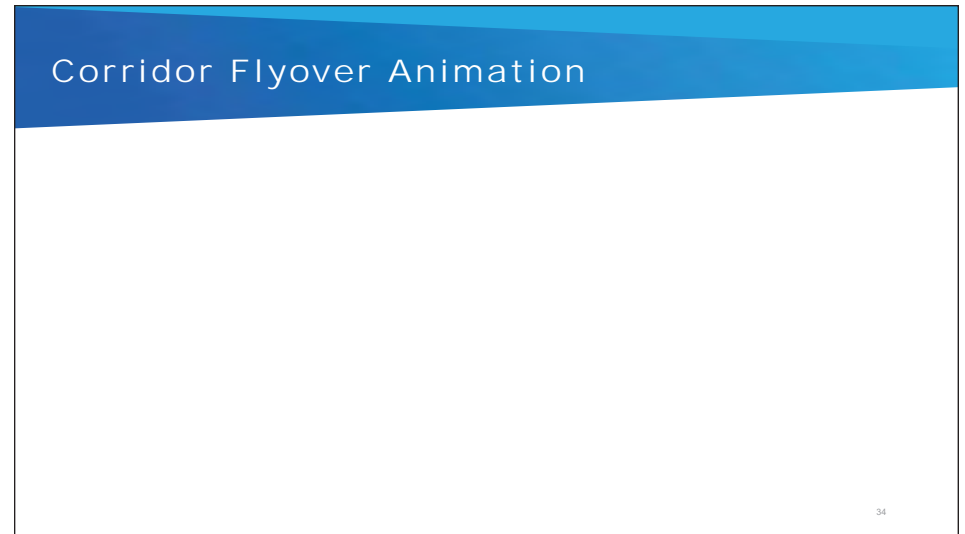
This view shows what the finished project will look like from Ezzard Charles Drive looking north. Notice the 8-foot-tall extended traffic safety barriers for noise reduction. Also notice the ashlar stone treatments on the retaining walls.



This view shows what the new, two-way bridge on Ezzard Charles Drive will look like. Notice the wide shared use path on the right and the sidewalk on the left. Also notice the planters, screenwall, and decorative lighting on the bridge. The bridge lights and traffic signal supports will be black and meet City aesthetic standards.



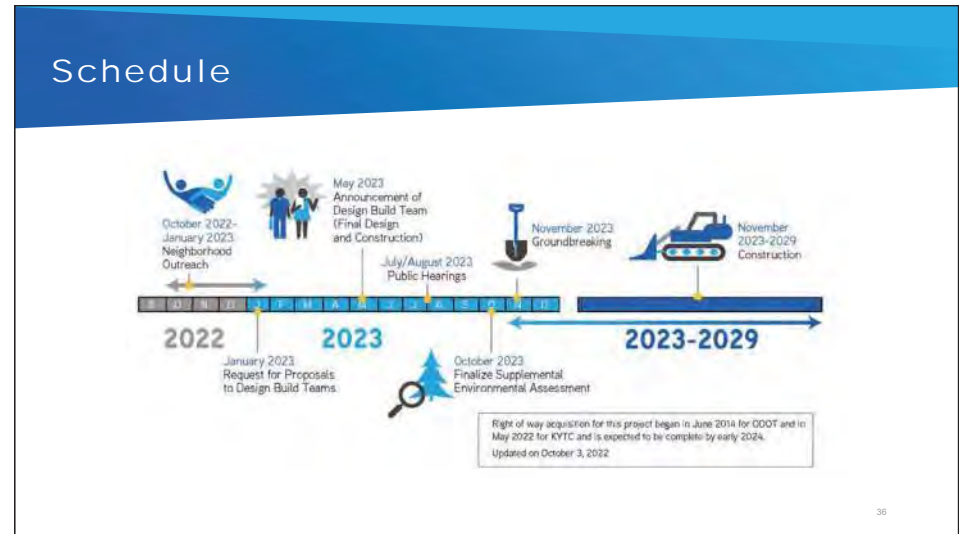
This is a view of what Findlay Street will look like when the project is built. Notice the 57-inch barrier on the I-75 bridge and the 8-foot extended traffic safety barriers on both sides of the bridge to reduce traffic noise. This also shows what the barriers and bridge walls will look like with the ashlar stone treatment. Also notice the rebuilt sidewalks, added bike lanes, and underpass lighting.



This is a flyover animation of what the finished corridor might look like from Dixie Highway in Kentucky to the Western Hills Viaduct in Ohio.



This is a flyover animation of what the finished corridor might look like from Dixie Highway in Kentucky to the Western Hills Viaduct in Ohio.



This slide shows the project schedule from today to the completion of construction.

# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.

---

[Attachment 5: Handouts and Exhibits](#)



INVESTING IN LOCAL COMMUNITIES.  
GROWING AMERICA'S ECONOMY.

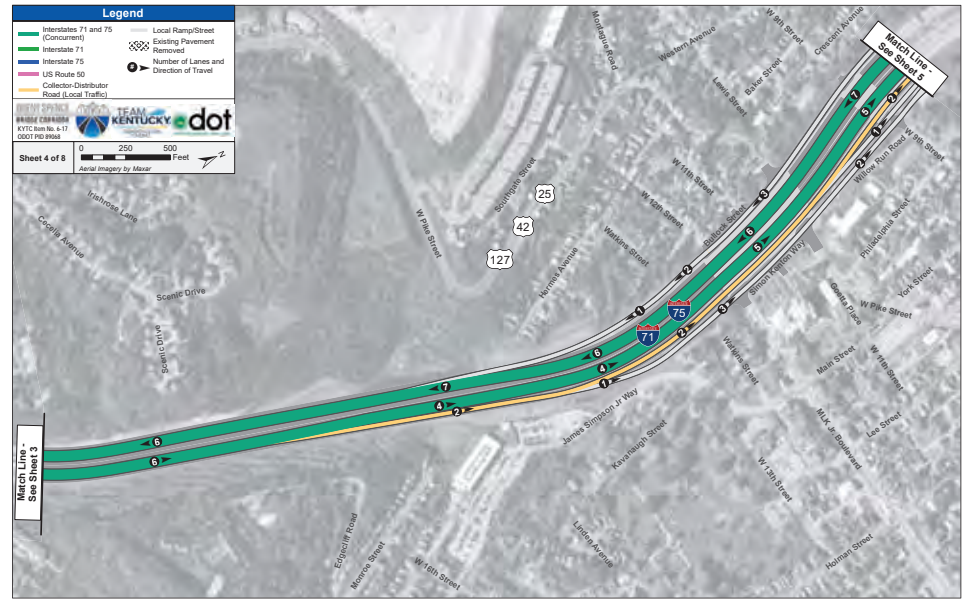
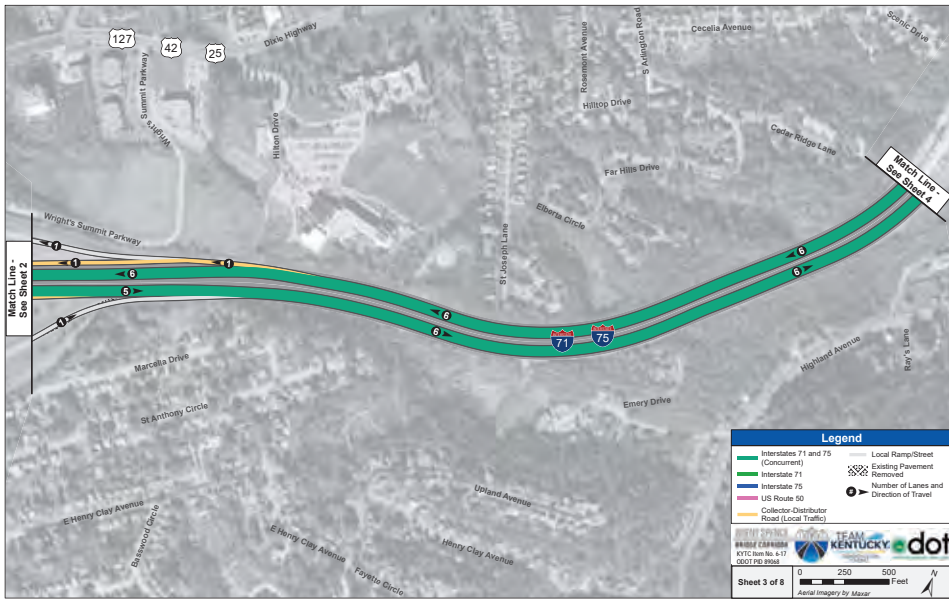
## Noise Analysis

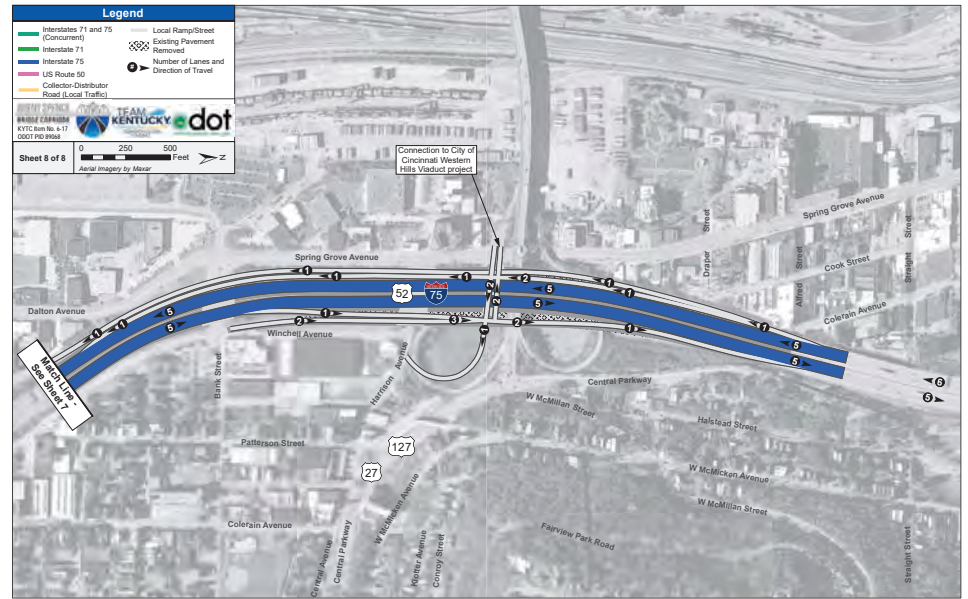
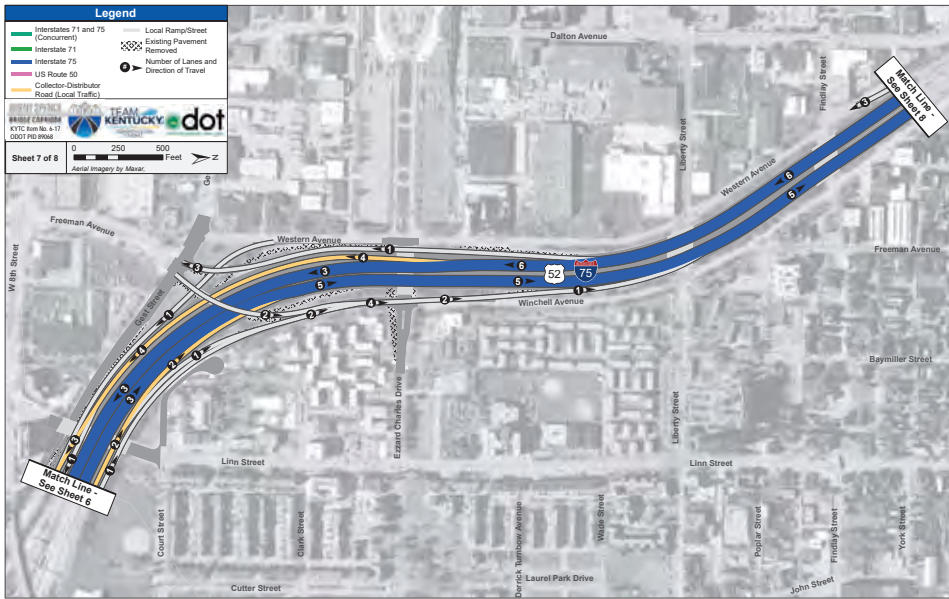
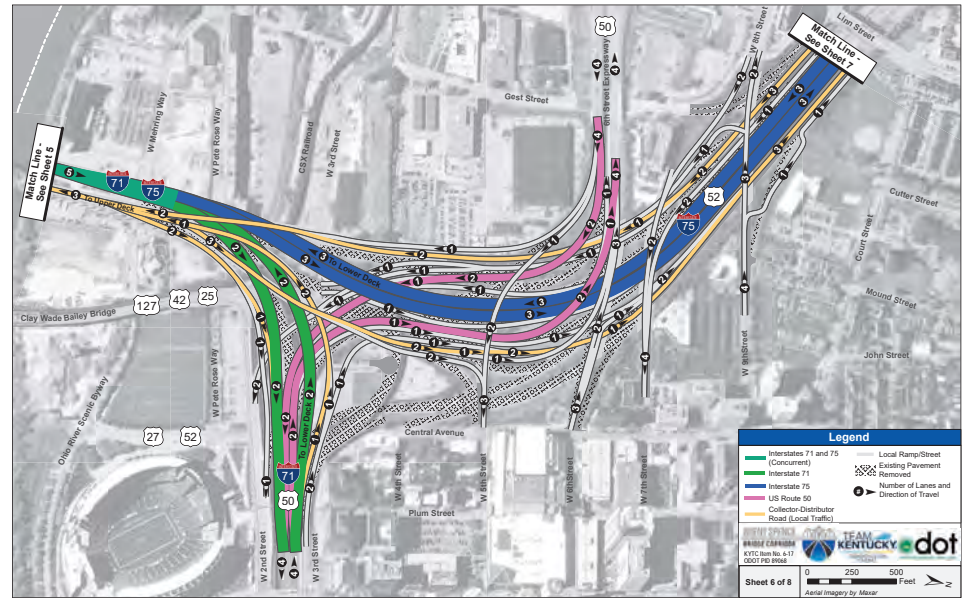
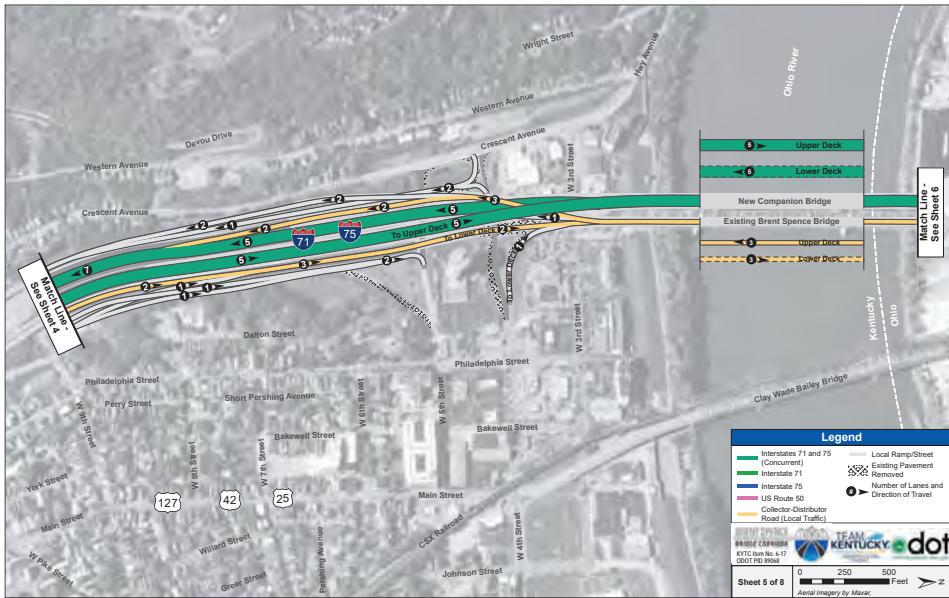
The Ohio Department of Transportation (ODOT) is currently preparing a Supplemental Environmental Assessment of the Brent Spence Bridge Project. Given the length of time since the original noise analysis was conducted, ODOT is required to prepare an updated noise analysis, which was completed in August 2022. While the 2011 noise analysis recommended noise walls along the east side of I-75, the results of the 2022 noise analysis indicated that no further noise analysis or abatement is warranted for the Ohio side of the Brent Spence Bridge project. Federal noise regulations state that noise analysis and any subsequent noise walls must be based on frequent human exterior use. Multi-family housing units with no outdoor patios or outdoor common use areas are not subject to noise analysis since they do not have a frequent exterior use. The multi-family housing along the corridor on the Ohio side does not have a frequent exterior use and is therefore, not subject to noise analysis. These criteria were inadvertently applied differently in 2011 resulting in the previous finding.

Regarding the Queensgate Playfield where a noise wall was recommended in the 2011 noise analysis, this site was reanalyzed in the 2022 analysis and it was determined that a noise wall for the site did not meet ODOT criteria because the budget is well above ODOT's threshold for cost per location that would benefit from a noise wall.

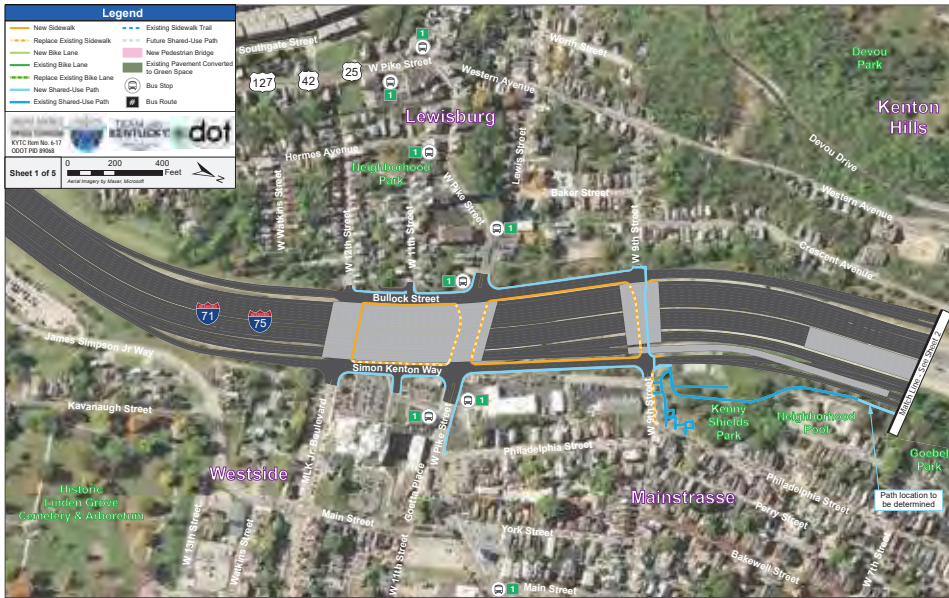
Although ODOT cannot offer noise barriers on the Ohio side, we recognize the I-75 corridor will remain the predominant noise source within the project area. To reduce overall traffic noise levels, ODOT plans to construct 4.75-ft tall concrete safety barriers wherever possible within the corridor. ODOT's standard concrete safety barriers are 3.5-ft high. The 4.75-ft tall concrete safety barriers are the tallest safety barriers available for use in Ohio and much taller than what most states use across the country. Because the majority of traffic noise comes from tire noises on the surface of the road, traffic noise research has shown that 4.75-ft tall safety barriers result in a noticeable if not substantial reduction in noise levels. ODOT proposes that the 4.75-ft tall safety barriers be constructed instead of metal guardrails and on bridges and in the center of the overall freeway. These taller safety barriers will block much of the noise from the southbound lanes of I-75.

ODOT is also planning to construct special 8-ft tall concrete traffic barriers on concrete retaining walls on the northbound side of I-75 where the apartment communities exist. The 8-ft tall concrete traffic barrier will result in a substantial reduction in noise levels. Research has shown that 8-ft tall concrete traffic barriers will approximately cut traffic noise levels in half, which is what traditional noise barriers are expected to do.

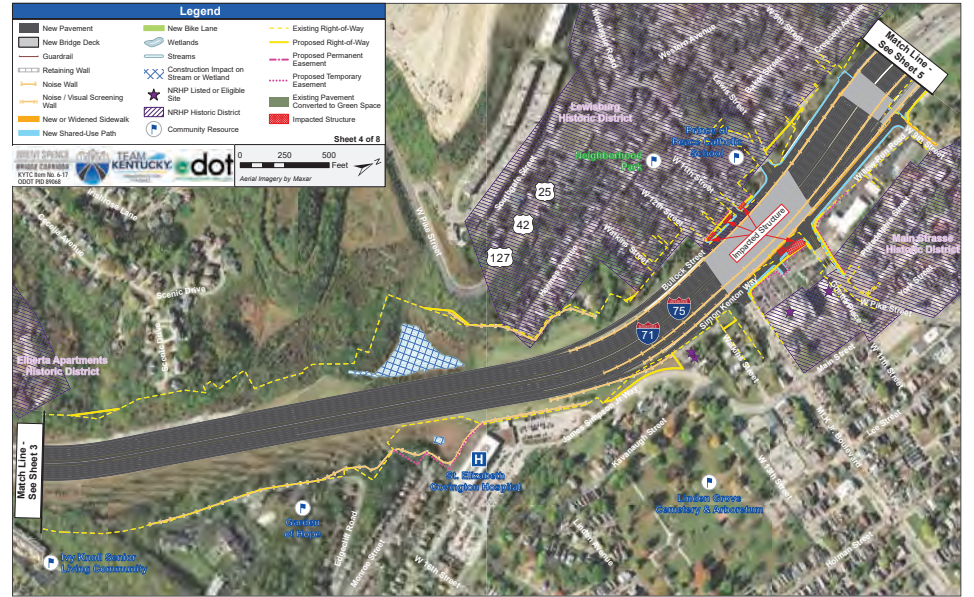
















**Brent Spence Bridge Corridor  
(Looking North from Kentucky)**



KYTC Item No. 6-17 | ODOT PID 89068



**Brent Spence Bridge Corridor  
(Looking Northeast from Kentucky)**



KYTC Item No. 6-17 | ODOT PID 89068



**Brent Spence Bridge Corridor  
(Looking Northwest from Kentucky)**



KYTC Item No. 6-17 | ODOT PID 89068



**Brent Spence Bridge Corridor  
(Looking Southeast from Ohio)**



KYTC Item No. 6-17 | ODOT PID 89068



**Brent Spence Bridge Corridor - Kentucky**  
(Looking Southeast)



KYTC Item No. 6-17 | ODOT PID 89068



**Pike Street With Noise Walls (Looking Northwest) - Kentucky**



KYTC Item No. 6-17 | ODOT PID 89068



**Pike Street Without Noise Wall (Looking East) - Kentucky**



KYTC Item No. 6-17 | ODOT PID 89068



**Pike Street With Noise Wall (Looking East) - Kentucky**



KYTC Item No. 6-17 | ODOT PID 89068



Goebel Park (Looking North) - Kentucky



I-71/I-75 Corridor Without Noise Walls – Kentucky  
(Looking West from Goebel Park Clock Tower)



I-71/I-75 Corridor With Noise Walls – Kentucky  
(Looking West from Goebel Park Clock Tower)



Crescent Avenue Without Noise Wall - Kentucky  
(Looking North)





**Crescent Avenue With Noise Wall - Kentucky  
(Looking North)**



**Firefighters Memorial (Looking Southwest) - Ohio**



**Brent Spence Bridge Corridor - Ohio  
(Looking North Near Ezzard Charles Drive)**



**Ezzard Charles Drive (Looking West) - Ohio**







Findlay Street (Looking West) - Ohio





---

Attachment 6: Comment and Demographic Forms

# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: Dorothy Bush

Mailing address (or nearest cross streets): [REDACTED]

Email address: [REDACTED] Phone: [REDACTED]

Business/Organization Name: Smith Fairmount Community Council

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident
- Area business owner or employee
- Commuter
- Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Glad to see that access to Spring Grove is addressed in the current iteration of the project.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

COMMENTS DUE BY  
January 5, 2023

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068



# COMMENT FORM



Contact information is not required but will ensure you receive a response, should one be required.

Name: Gregory Lang

Mailing address (or nearest cross streets): [REDACTED]

Email address: [REDACTED] Phone: [REDACTED]

Business/Organization Name: River West, LPH Bus. Alliance

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

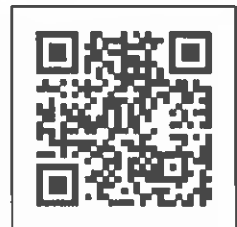
The land bridges seem to be ruled out because of elevation differences. Have you considered moving them North - 5<sup>th</sup> to 8<sup>th</sup>, 9<sup>th</sup>, 9<sup>th</sup> to Ezzard Charles.

Connection between Queensgate & the West End need to be strengthened, not further separated. Sound walls / barriers will make this worse than it already is

COMMENTS DUE BY  
January 5, 2023

Comments may be submitted:

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
- By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068

# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American     American Indian or Alaskan Native     Asian     Hispanic or Latino  
 White     Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English     Spanish     Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes     No     Not Applicable

### How many people live in your household?

- 1 - 2     3 - 5     6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18     19 - 44     45 - 64     65 +

### What is your annual household income?

- Less than \$10,000     \$10,000 - \$24,999     \$25,000 - \$49,999     \$50,000 - \$74,999  
 \$75,000 - \$99,999     \$100,000 - \$149,999     \$150,000 +

### What is the highest level of education completed by members of your household?

- No     Elementary school     Middle school     High school     College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes     No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov

Mail to:

Ohio Department of Transportation,  
District 8

ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518





---

# Brent Spence Bridge Corridor Project Kentucky Broad Neighborhood Meeting Summary December 14, 2022

---

## Introduction

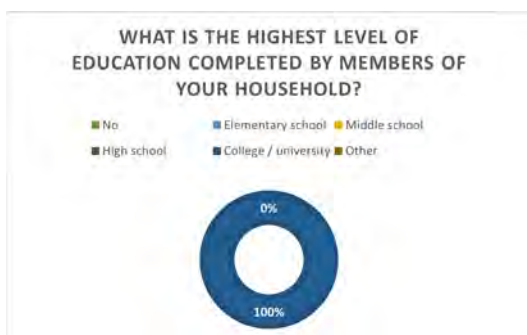
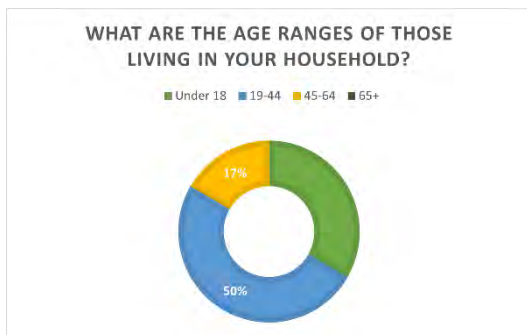
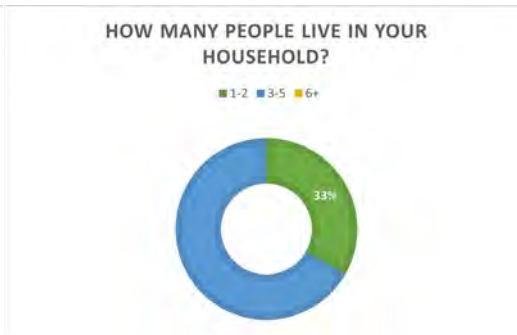
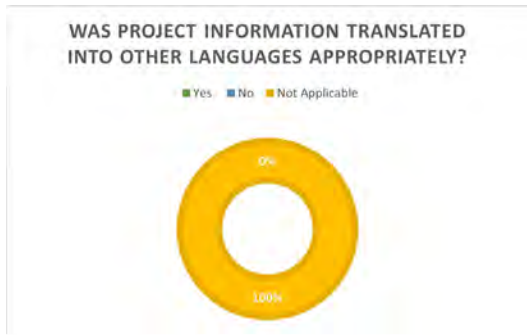
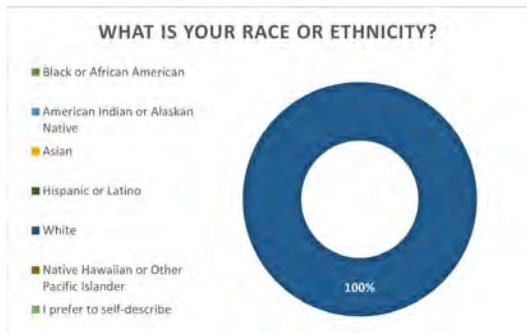
The Kentucky broad neighborhood meeting was held on December 14, 2022, at two times, from 12:00 pm to 2:00 pm and 5:00 pm to 7:00 pm, at the Kenton County Fiscal Court (1840 Simon Kenton Way, Covington, KY). The Kentucky Transportation Cabinet (KYTC) advertised the meeting via the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)); the December 2022 project e-newsletter; an article in the Northern Kentucky Tribune; social media posts on Facebook, Nextdoor, and Twitter; and local media coverage on the day of the meetings (Attachment 1). In addition, flyers advertising the meeting were emailed to representatives of the following neighborhood organizations: Licking Riverside, Helentown, Austinburg, Wallace Woodds, Monte Casino, and Newport. In total, forty-five (45) members of the public attended the early meeting, and thirty-six (36) members of the public attended the later meeting (Attachment 2). Representatives from KYTC, the Ohio Department of Transportation, the City of Covington, the Federal Highway Administration, and the project team were also in attendance at one or both meetings. Photographs are included in Attachment 3.

These meetings followed an open-house format. The meeting format began with a 30-minute period for the public to view exhibits and speak to the agencies and the project team. A formal presentation by KYTC (Attachment 4) followed. After the presentation, the open-house format resumed. Members of the project team were present to answer questions and respond to feedback during the open-house portion of the meetings.

Exhibits showing the corridor and various renderings were available for viewing during the meeting (Attachment 5). Written comment forms, a demographic survey, and a postcard inviting individuals to visit [PublicInput.com/bsbc](http://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. Four (4) comment forms and three (3) demographic forms were returned at the meetings (Attachment 6).

## Demographics

The following is a breakdown of the responses received from the demographic survey.



---

A write-in space was provided to provide suggestions for additional ways ODOT and KYTC can improve the inclusiveness of their public outreach efforts. The only comment received was:

- Door knocking of every neighbor impacted.

## Comments

The written comments received at the meeting included the following:

- The noise walls are depressing. They will be a target for graffiti. Why can't they be living walls? Why can't they have plants, vines that will enhance the living environment (birds, butterflies, etc.) and be soothing in contrast to more concrete.
- I want the noise wall in my area! It would bring the noise level down tremendously!
- Adding lanes does not solve traffic. Houston, LA have expanded highways and nothing but gridlock. Please do not spend 3 Billion on moving traffic through my city. I want people to live and enjoy it.
- Noise Barriers for ALL borders. Potential Beautification of Noise Barriers - similar to those murals along the river in Cov [Covington]. Consider impacts to local traffic and our ease of use and time. Greenup and Scott should remain 1 way unless there are plans to improve Madison prior to this project. It being convenient to get around (anywhere in 15 min) is a big part of the reason to live in Covington.

All written comments will be documented in the project public comment and response summary. All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.





---

## Attachment 1: Advertising Materials

Events

Search Events

Home

Categories

-  Classics
-  Comedy
-  Crafts
-  Dance
-  Drinks
-  Fitness & Workouts
-  Foods
-  Games
-  Gardening
-  Health & Medical
-  Healthy Living & Self-Care
-  Home & Garden
-  Music & Audio
-  Parties








WEDNESDAY, DECEMBER 14, 2022 AT 12:00 PM – 7:00 PM EST

# Brent Spence Bridge Corridor Neighborhood Outreach

1840 Simon Kenton Way, Covington, KY 41011

## Details

-  163 people responded
-  Event by KYTC District 6
-  1840 Simon Kenton Way, Covington, KY 41011
-  Duration: 7 hr
-  Public · Anyone on or off Facebook

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are holding neighborhood outreach meetings for the Brent Spence Bridge (BSB) Corridor Pr... See more

Causes

## Guests

See All


20  
GOING

143  
INTERESTED

## Host



## Brent Spence Bridge Project Neighborhood Meetings Twitter Advertisements







**Brent Spence Bridge Corridor** @BSBCorridor · 10h Dec 14 ⋮


We want to hear from you. Join us today!

Daytime: noon – 2pm (30-minute presentation at 12:30pm)  
Evening: 5pm – 7pm (30-minute presentation at 5:30pm)

Kenton County Fiscal Court (Riedlin Schott Community Room)  
1840 Simon Kenton Way, Covington, KY  
[@KYTCDistrict6](#) [@KYTC](#)

   1 

---







**Brent Spence Bridge Corridor** @BSBCorridor · Dec 13 ⋮

And tomorrow:  
Our next Kentucky meeting is Weds., Dec. 14

Daytime: noon – 2pm (30-minute presentation at 12:30pm)  
Evening: 5pm – 7pm (30-minute presentation at 5:30pm)

Kenton County Fiscal Court (Riedlin Schott Community Room)  
1840 Simon Kenton Way, Covington, KY  
[@KYTCDistrict6](#)

  3  

[Show this thread](#)



**Brent Spence Bridge Corridor** @BSBCorridor · Dec 13 ⋮

Reminder, we have another Ohio meeting tonight from 5pm - 7pm (with a 30-minute presentation at 5:30pm)

Lincoln Recreation Center  
1027 Linn Street, Cincinnati  
[@ODOT\\_Cincinnati](#)

 1   2 

[Show this thread](#)



**Brent Spence Bridge Corridor** @BSBCorridor · Dec 12



Hear the latest. Join us for our next Ohio meeting:

Tuesday, Dec. 13

Daytime: noon – 2pm (30-minute presentation at 12:30pm)

Evening: 5pm – 7pm (30-minute presentation at 5:30pm)

Lincoln Recreation Center  
1027 Linn Street, Cincinnati  
[@ODOT\\_Cincinnati](#)



↻ 4

♡ 6



**From:** Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>  
**Sent:** Monday, December 5, 2022 3:58 PM  
**To:**  
**Subject:** Brent Spence Bridge Corridor Project Update - DBE Matchmaker Event

Brent Spence Bridge Corridor December 5, 2022 eNewsletter



## DBE MATCHMAKER EVENT TAKING PLACE DEC. 7

*Small, Minority and Women-Owned Businesses Encouraged to Learn  
about Potential Work Opportunities*

The Brent Spence Bridge Corridor project will offer unprecedented opportunities for small, minority, and women-owned businesses in our region. To support the inclusion of Disadvantaged Business Enterprise (DBE) consultants and contractors on the Brent Spence Bridge Corridor Project, the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) will host two events Wednesday, Dec. 7 at the Anderson Pavilion, 8 Mehring Way, in Cincinnati.

From 9 a.m. to noon, representatives from ODOT and KYTC will provide an overview of the project for DBE firms. The presentation will include details about prequalification requirements for the project and support services available to DBE consultants and contractors.

From 1:30 to 4:30 p.m., ODOT and KYTC will conduct a “matchmaker” event in which DBE firms can meet one-on-one with prime contractors and firms who work directly with the government and manage subcontractors and consultants.

“This is a great opportunity for the industry,” said ODOT’s Tom Arnold of the afternoon event. “Each DBE gets 15 minutes to present their capabilities and work experience to a number of

prime contractors. Our goal is to help our prime design and construction firms understand how many great small businesses they can work with to meet our project inclusion goals.”

Arnold, a member of the project’s Diversity and Inclusion Outreach Committee, added ODOT and KYTC plan to issue a request for proposals for the project’s prime contractor early next year. Construction is scheduled to begin late next year or early 2024.

“We strongly recommend DBE firms [preregister](#) for both events because we want to help make as many connections as possible,” Arnold said. He also recommended DBE firms follow ODOT’s Outreach [webpage](#) for additional updates and events.

### Schedule of DBE Events

Wednesday, Dec. 7 | Anderson Pavilion  
8:30 to 9 a.m.: Registration  
9 a.m. to Noon: Project Overview  
1:30 to 4:30 p.m.: Matchmaker Event with Prime Contractors

## ODOT AND KYTC CONTINUE ROBUST PUBLIC OUTREACH THROUGH NEIGHBORHOOD MEETINGS

*Public Invited to Attend to Ask Questions and Share Feedback*

The joint project team from ODOT and KYTC continues to gather valuable feedback from communities surrounding the Brent Spence Bridge Corridor project area through a public meeting and listening tour. Suggestions gathered during these sessions will enhance the overall project, including opportunities to reconnect neighborhoods, improve bike and pedestrian safety, and reduce the project’s footprint.

Through the end of 2022 and into 2023, the project team will continue meeting one-on-one with directly affected communities to answer questions and gather ideas.

As the project gets under way, ODOT and KYTC will continue to explore ways to improve pedestrian access, improve safety, and support better access to business districts while reclaiming public land for local use.

During the eight meetings conducted to date, residents and local officials have identified opportunities that are now part of the plan, including enhanced streetscape amenities, aesthetic treatments, and bicycle and pedestrian infrastructure. All will improve connectivity to transit, jobs, health care, cultural, recreational and commercial destinations.

In addition to the neighborhood meetings, the public will be provided the opportunity to review project materials and provide formal comments at public hearings currently scheduled for July/August 2023.



**KENTUCKY'S UPCOMING NEIGHBORHOOD MEETING SCHEDULE**

<b>Neighborhood</b>	<b>Meeting Date</b>	<b>Meeting Time</b>	<b>Meeting Location</b>
Mutter Gottes/CBD	December 5, 2022	7:00 p.m.-8:00 p.m.	Kenton County Public Library 502 Scott Street, Covington
Lewisburg/Botany Hills	December 6, 2022	7:00 p.m.-8:00 p.m.	Holiday Inn Express & Suites 200 Crescent Avenue, Covington
Park Hills	December 12, 2022	7:00 p.m.-8:30 p.m.	Garden of Park Hills 1622 Dixie Highway, Park Hills
Broad Neighborhood Meeting	December 14, 2022	12:00 p.m.-2:00 p.m. 5:00 p.m.-7:00 p.m.	Kenton County Fiscal Court (Riedlin Schott Community Room) 1840 Simon Kenton Way, Covington
Public Hearing	TBD July/August 2023	TBD	TBD

**OHIO'S UPCOMING NEIGHBORHOOD MEETING SCHEDULE**

Neighborhood	Meeting Date	Meeting Time	Meeting Location
Mutter Gottes/CBD	December 5, 2022	7:00 p.m.-8:00 p.m.	Kenton County Public Library 502 Scott Street, Covington
Lewisburg/Botany Hills	December 6, 2022	7:00 p.m.-8:00 p.m.	Holiday Inn Express & Suites 200 Crescent Avenue, Covington
Park Hills	December 12, 2022	7:00 p.m.-8:30 p.m.	Garden of Park Hills 1622 Dixie Highway, Park Hills
Broad Neighborhood Meeting	December 14, 2022	12:00 p.m.-2:00 p.m. 5:00 p.m.-7:00 p.m.	Kenton County Fiscal Court (Riedlin Schott Community Room) 1840 Simon Kenton Way, Covington
Public Hearing	TBD July/August 2023	TBD	TBD

### OHIO'S UPCOMING NEIGHBORHOOD MEETING SCHEDULE

Neighborhood	Meeting Date	Meeting Time	Meeting Location
Camp Washington	December 12, 2022	7:00 p.m.-8:30 p.m.	Camp Washington Urban Revitalization Corporation 2951 Sidney Avenue, Cincinnati
Broad Neighborhood Meeting	December 13, 2022	12:00 p.m.-2:00 p.m. 5:00 p.m.-7:00 p.m.	Lincoln Recreation Center 1027 Linn Street, Cincinnati
West End	December 20, 2022	6:00 p.m.-8:00 p.m.	Lincoln Recreation Center 1027 Linn Street, Cincinnati
Public Hearing	TBD July/August 2023	TBD	TBD

## REVISED PROJECT DESIGN RETURNS 10 ACRES TO CINCINNATI CENTRAL BUSINESS DISTRICT

In its continuing collaboration with Cincinnati's elected officials and community leaders, the Brent Spence Bridge Corridor Project team redesigned plans for the area immediately west of the Duke Energy Convention Center, freeing up 10 acres for future development.

Last summer the city and the Cincinnati USA Regional Chamber submitted comments and requested changes to the Ohio side of the corridor to enlarge the central business district, which were approved by the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC).

This creates "some really exciting opportunities for large-scale investments," said Cincinnati Mayor Aftab Pureval at a news conference last month. "We want to be a city that's growing, and these 10 acres right in the middle of our downtown" will be like pouring "gasoline on that effort."





Image from Spectrum News 1

[Read more here.](#)

## Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

[Visit our website](#)

[Sign up for updates](#)

[Submit a comment](#)

© 2022 Brent Spence Bridge Corridor. All Rights Reserved.

Brent Spence Bridge Corridor | 505 South State Route 741, Lebanon, OH 45036

[Unsubscribe jheflin@hntb.com](mailto:jheflin@hntb.com)

[Update Profile](#) | [Constant Contact Data Notice](#)

Sent by [info@brentspencebridgecorridor.com](mailto:info@brentspencebridgecorridor.com) powered by





CONTACT US



# PUBLIC INVOLVEMENT AND COMMENTS



LEARN MORE ABOUT HOW THE BRENT SPENCE BRIDGE CORRIDOR PROJECT TEAM IS WORKING WITH THE COMMUNITY TO ENSURE STAKEHOLDER VOICES ARE HEARD AND COMMUNITY NEEDS ARE INTEGRATED INTO THE PROJECT PLAN.

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are fully committed to robust involvement to gain insights and receive feedback as part of the project development process. The project team has been working closely with local partners to ensure communities surrounding the project area have the opportunity to provide feedback.

If you have a comment or question for the project team, please provide your feedback in the link at the bottom of the page.

## PUBLIC COMMENTS

Below are responses from the project team to specific public comments that have been received to date.

- Response to public comment – [WORKING POSITION PAPER: REDESIGN OF THE BRENT SPENCE BRIDGE PROJECT](#)
- Response to public comment – [BRENT SPENCE BRIDGE PROJECT – RECONNECTING CINCINNATI WESTWAY DESIGN IM](#)

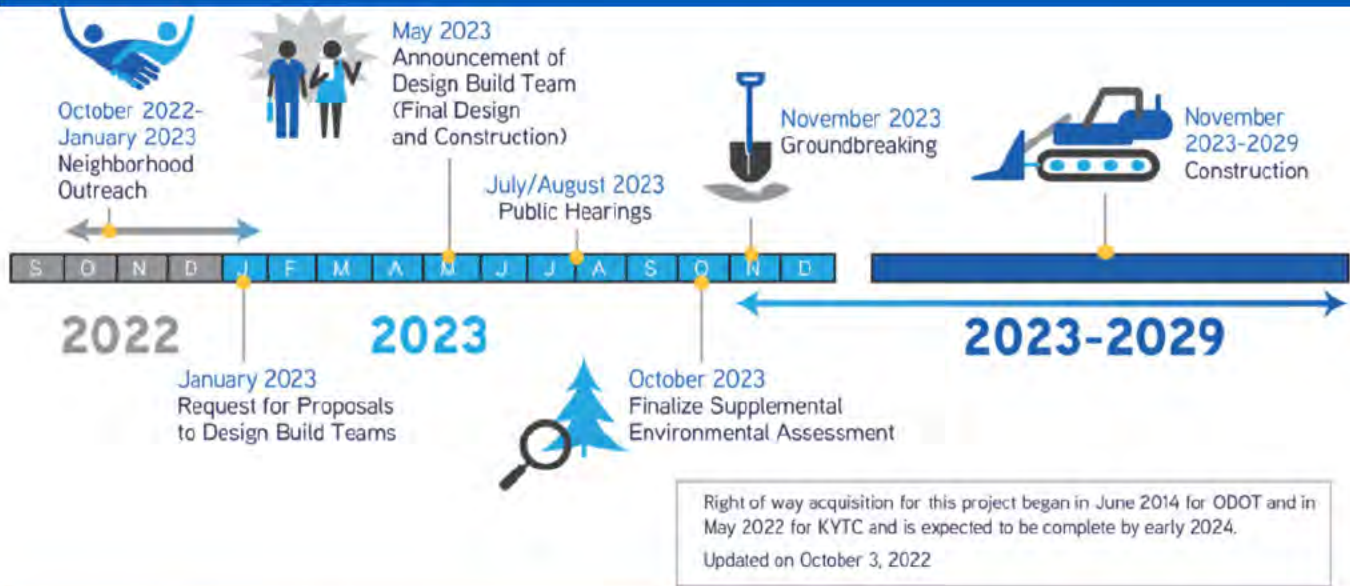
Privacy - Terms



Below are summaries of comments received by the project team, including responses to each.

- [General Public Comment and Response Summary \(updated monthly\)](#)
- [Neighborhood Meeting Survey Comment and Response Summary \(coming in January 2023\)](#)

## PROJECT SCHEDULE



## PROJECT AREA MAPS

Below are detailed maps of the project corridor, including an overall map, a multimodal map including paths for pedestrians and bicyclists, a traffic flow map, and project renderings with illustrations of how the corridor could look when complete.

- [Project Corridor Map](#)
- [Multimodal Map](#)
- [Traffic Flow Map](#)
- [Project Renderings](#)

## NEIGHBORHOOD OUTREACH MEETINGS



To more thoroughly engage communities within the project for those residents. These events feature a presentation by survey, and offer feedback one-on-one with the project team.

[CONTACT US](#)

DOT have been holding and allow residents to b

### KENTUCKY NEIGHBORHOOD MEETINGS

Mainstrasse, 11/21/2022

Friends of Peaselburg, 11/28/2022

Westside Covington, 11/29/2022

Fort Mitchell, 11/30/2022

Fort Wright, 12/1/2022

Mutter Gotes/CBD, 12/5/2022

Lewisburg/Botany Hills, 12/6/2022

Park Hills – 7-8:30 p.m., 12/12/2022, Garden of Park Hills, 1622 Dixie Highway, Park Hills, KY

### KENTUCKY BROAD NEIGHBORHOOD OPEN HOUSE MEETING

Wednesday, December 14, 2022, 12-2 p.m. and 5-7 p.m.

(presentations at 12:30 p.m. and 5:30 p.m.)

Kenton County Fiscal Court (Riedlin Schott Community Room)

1840 Simon Kenton Way, Covington, KY

### OHIO NEIGHBORHOOD MEETINGS

CUF, 11/15/2022

West End – 6 p.m., 12/20/2022, Lincoln Recreational Center, 1027 Linn St., Cincinnati

CBD Riverfront, 11/29/2022

Camp Washington – 7p.m., 12/12/2022, Camp Washington Urban Revitalization Corp., 2951 Sidney A

### OHIO BROAD NEIGHBORHOOD OPEN HOUSE MEETING

Tuesday, December 13, 2022, 12-2 p.m. and 5-7 p.m.

(presentations at 12:30 p.m. and 5:30 p.m.)

Lincoln Recreation Center

1027 Linn Street, Cincinnati, OH

## PUBLIC HEARINGS



Public hearings will be held in July/August of 2023, once the Supplemental Environmental Impact Statement (SEIS) is made publicly available. The Supplemental EA and information and exhibits about the project and its expected impacts will be available for review at the hearings and in the [document center](#) on the project website.

[CONTACT US](#)

EA) is made publicly available. The Supplemental EA and information and exhibits about the project and its expected impacts will be available for review at the hearings and in the [document center](#) on the project website.

One public hearing will be scheduled in each state, and a virtual public hearing will also be offered. Dates, times, and locations for public hearings have not been determined yet. You can be notified when the meetings are scheduled by signing up for project updates.

## GET IN TOUCH

Thank you for visiting the Brent Spence Bridge Corridor website. If you have a question or comment for the project team, please complete the form below.

NAME

EMAIL

MESSAGE

ZIP CODE

## SIGN UP FOR UPDATES

Please enter your contact information to receive periodic updates, including notification of upcoming public meetings.

NAME

EMAIL

protected by reCAPTCHA

[Privacy](#) [Terms](#)



[CONTACT US](#)



Si desea asistencia traduciendo esta sitio web a español, contacte a Domingo I. Martinez@dot.ohio.gov teléfono a 513-933-6136.



Ohio Department of Transportation | 1980 W Broad St | Columbus, OH 43223



Kentucky Transportation Cabinet | 200 Mero St | Frankfort, KY 40622

A nonprofit publication of the Kentucky Center for Public Service Journalism



Home Featured KYTC, ODOT plan events to provide information, gather feedback on Brent Spence Bridge project

Sign up for NKyTribune News Updates with Facebook and Twitter icons

# KYTC, ODOT plan events to provide information, gather feedback on Brent Spence Bridge project

Dec 9th, 2022 · 0 Comment

NewsMatch Community-funded. Community-driven. Community-focused. Click here to support the Northern Kentucky Tribune today and double your impact!



Brent Spence Bridge Corridor (Looking Northwest from Kentucky)



Are you curious how (and where) a revamped Brent Spence Bridge and its completely new companion span will separate local commuters from pass-through travelers?

Do you have thoughts and/or strong feelings about noise barriers, landscaping, pedestrian walkways, and the "look" of the piers and abutments?

Both questions and opinions will be welcome when transportation officials from Kentucky and Ohio host two community engagement events next week to gather feedback from Covington residents and businesses.

The fundamental design of the \$3 billion to \$3.8 billion Brent Spence Bridge Corridor revamp is essentially finished. But many aesthetic elements and other considerations of how the new bridge and its approaches fit within the surrounding community are still being figured out.

"KYTC values public feedback as an important part of the project development process," said Stacey Hans, project manager for the Kentucky Transportation Cabinet (KYTC). "Input from the people who live in and near the corridor will help the project team design the Brent Spence Bridge Corridor in a way that complements local communities."

Hans said the community outreach meetings are focused on a specific audience — the broader Covington community.

A series of previous and pending outreach meetings more narrowly targeted neighborhoods in Covington that about the project: MainStrasse Village, Westside, Peaselburg, Mutter Gottes/Central Business District, and Botany Hills/Lewisburg. Other events have been held or will be held in Fort Wright, Fort Mitchell, Park Hills, and north of the river in Ohio.

Covington Mayor Joe Meyer said Covington’s residents and businesses should use this opportunity to learn about the project and understand both the short-term impact of its construction and its long-term impact on the local, regional, and national transportation network.

“This is one of the biggest infrastructure projects in the history of the region, and with its proximity to our community, Covington has a vested interest in continuing to shape its size and look,” Meyer said. “I encourage every resident to familiarize themselves with the project and weigh in on issues that are still being sorted out.”

Meyer praised KYTC for being open-minded and flexible to concerns about the project, including how it’s to be funded, its size, and its impact on Covington. As explained in this June, 2022 release from the City — “State, Covington reach monumental agreement on Brent Spence Bridge” – Covington has seats on committees that are still making decisions related to noise and air pollution; impact on historic structures; traffic; and aesthetic design of retaining walls, bridge facades, landscaping, and bicycle/pedestrian amenities.

**About the meetings**

- Noon to 2 p.m. and 5 p.m. to 7 p.m.
- Wednesday, Dec. 14.
- Kenton County Building (Riedlin Schott Community Room), 1840 Simon Kenton Way, Covington.
- Open-house format, show up at any time, short presentations will be made at 12:30 p.m. and 5:30 p.m.
- Registration is not necessary.

Before and after the presentations, attendees can browse exhibits, review project information, ask questions of staff members, and give one-on-one feedback.

Anybody who can’t make the events can also review project information and leave feedback at [Brent Spence Bridge engagement](#).

**About the project**

The long-discussed Brent Spence Bridge Project aims to improve safety and alleviate highway congestion on Interstates 71-75 as they cross the Ohio River on the double-decker Brent Spence. The bridge was built in 1963 to carry 80,000 vehicles a day but currently carries about double that. Backups affect not only commuters but also trucks carrying freight, since about 3 percent of the nation’s gross domestic product crosses the bridge every year. The bridge also lacks shoulders.

Details of the project can be found at [Brent Spence Bridge Corridor](#). Generally, it involves:

- Repairs to the existing bridge will reduce the number of its lanes to three in each direction.
- Construction of a new, companion bridge to the west that would include five lanes in each direction.
- Separation of traffic based on the destination, diverting local traffic to the existing bridge and through traffic to the new bridge.
- Redesigning and upgrading 7.8 miles of I-71/75 from Fort Mitchell, Ky., to just north of the Western Hills viaduct in Cincinnati.
- Three phases, two of which are entirely in Ohio. The Kentucky phase includes the bridges themselves and will follow a design-build process that begins in 2023.

*City of Covington*

**Related Posts**



**Levi Strauss gets zoning ok to move ahead on largest building ever in Boone County — 1.26 million sq. ft.**

12/9/2022 · 0 Comment

**U.S. District Court Judge in New York denies all Michael Hild’s new trial motions; Dusing exonerated**

12/8/2022 · 0 Comment



**Build + Elevate NKY**  
 Transformation through Partnership

**Has Your Business Grown?  
 Claim Kentucky’s Small Business Tax Credit**  
[LEARN MORE](#)



**N K Y Chamber of Commerce**

**meet nky**  
 Northern Kentucky CVB

**Affiliations**

- Institute for Nonprofit News
- Online News Association
- N. Ky. Chamber of Commerce
- Covington Business Council

**Archives**

Select Month ▾





**Behringer-Crawford  
Museum to offer free  
admission for all City of  
Covington residents  
Saturday**

12/8/2022 · 0 Comment

---

**Leave a Comment**

---

Name \*

Email \*

Website

**Submit Comment**

LOCAL 12 WKRC

43° 99° 52°

Search Site

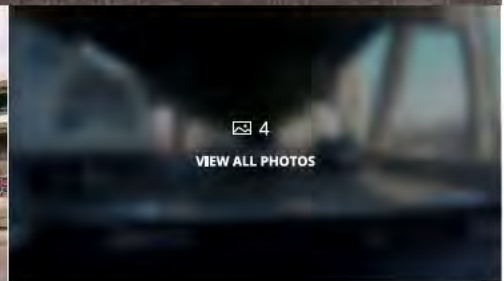


**T Mobile** **FREE SMARTPHONES FOR NEW LINES** **Get yours now**

if you cancel service before 24 credits, credits will be forfeited. See terms and conditions. Offer good while supplies last. Offer ends 12/31/22. © 2022 T-Mobile USA, Inc. All rights reserved.

## Here's how you can provide input on the Brent Spence Bridge project

by Chelsea Sick, WKRC  
Wednesday, December 14th 2022




WKRC

[Facebook](#) [Twitter](#) [Email](#)

COVINGTON, Ky. (WKRC) - Ohio and Kentucky transportation officials invite residents to attend public forums Wednesday about the Brent Spence Bridge project.

Two meetings will be held in Kenton County and those transportation officials will be asking for public input about the project. The overall design of the bridge is essentially finished, but now they need the public's help deciding on aesthetic elements, like landscaping and pedestrian walkways.

This  billion. Exactly where that funding is coming from is still being worked out, as Ohio and Kentucky government officials are still working on funding.

The goal of the project is to alleviate congestion on 71/75 as drivers cross over the bridge. It was built in 1963 to carry 80 thousand vehicles a day, construction officials say it currently carries about double that.

Sponsored Links

The major details have been decided, including reducing the number of lanes to three in each direction and a new companion bridge will be built to the west that would include five lanes in each direction.

Now it's time to make decisions about noise barriers, landscaping, pedestrian walkways, and the look of the piers and abutments.

Local 12 spoke to Covington Mayor Joe Meyer about the importance of the public's participation in these meetings.

"No transportation project in Covington history has had or will have more impact than the Brent Spence Bridge Corridor Project," Mayor Meyer said. "This is a project that will affect our community for the next 50 to 75 years and it is really time for people to take this seriously, understand it, offer their suggestions for improvement."

The meetings are from noon to 2 p.m. and 5 p.m. to 7 p.m. on Wednesday. They will be held at the Kenton County Building. Short presentations will be made at 12:30 and 5:30.

Mayor Meyer explains why what seems like small details could be big for the residents that live near the bridge.

"This is an area where popular input is going to have a lot of influence," Mayor Meyer said. "Noise barriers are big, they will block views, and so one of the decisions that our community will have to make, do we want to block the view, or do we want to eliminate the noise? And that question can be answered either way, and there's not a wrong answer, it's just what our people want."

It's an open-house format - so you can show up at any time, and you do not need to register.

If you can't attend, you can learn more about the project and tell project leaders what you think, [here](#).



---

Attachment 2: Sign-In Sheet



## BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 14, 2022

Name	Representing	Address	Phone	Email
1. JERRY VONKANDORE	SRF			
2. Matt Engelmann				
3. Chuck Seal				
4. ED RATTERMAN				
5. SHAWN KELLY				
6. GEOFF BESECKER	CC FISCAL COURT			
7. Jim Benton	CC COUNCILMAN			
8. Jim Carmichael				
9. Nate Weyand-Grise	Resident of Cov			
10. Anonymous	preferred not to sign			
11. Connie + Ray Kingsburg	Self			
12. Thomas Gamed	Self			
13. Brian Valenz	Covington Police			
14. Ryan Griffin	Self			
15. John Scroggs	Scroggs Construction			
16. Brittany Pittman	Scroggs Construction Inc			
17. Mary Lynn Sorflo				
18. MINDY OSSWALD	HLPCA Self			
19. JOHN SLOAN	Self			
20. Kenton Hornbeck	LINK nky			

Please print clearly • Contact information is optional but must be included if you wish to receive updates.

page \_\_\_ of \_\_\_

KYTC Item No. 6-17 | ODOT PID 89068



## BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 14, 2022

Name	Representing	Address	Phone	Email
1. A. Hunsley				
2. Tom Kennedy	Nash's Done Accounting			
3. John Ahlbrand	Dusty Boat's Rest			
4. Steve Hensley	Kenton County			
5. Ron Washington	CITY OF COVINGTON			
6. Matt Blount				
7. COOPER, DAVID, O.	Kenton Co., Independence, Kent			
8. Amy Clay	Palace			
9. Kareem Simpson	Covington Resident			
10. Nick Swartzell	WVXN 91.7			
11. Ken Dew				
12. John Cole	Ryland Heights City			
13. Keith	Performance Auto			
14. Don Brown				
15. Jim Southward				
16. Louis Hill	PXS			
17. Mike Emack				
18. Margaret Minzani				
19. Susan Smith	City of Covington			
20. Don Baostar				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.

page \_\_\_ of \_\_\_

KYTC Item No. 6-17 | ODOT PID 89068



### BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 14, 2022

Name	Representing	Address	Phone	Email
1. Sandy & Mike Smith	Smith Muffler	[Redacted]	[Redacted]	[Redacted]
2. Jodi Hollis	HNTB			
3. Mark Becker	HNTB			
4. Stacey Hoas	KYTC			
5. Gary Valentine	KYTC			
6. Buddy Whitley	Kentuck Horse			
7. Stefan Spinosa	CAOT			
8. Lara Boller	HNTB			
9. Pat Fra	Covington Builders Group			
10. Daniel Pugh				
11. Bill Kellan				
12. Steve Hill	Lewisburg			
13.				
14.				
15.				
16.				
17.				
18.				
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.

page \_\_\_\_ of \_\_\_\_

KYTC Item No. 6-17 | ODOT PID 89068



### BRENT SPENCE BRIDGE CORRIDOR PROJECT

Neighborhood Outreach Meeting – December 14, 2022

Name	Representing	Address	Phone	Email
1. [Redacted]		[Redacted]	[Redacted]	[Redacted]
2. Angela Feeney				
3. PJ Louneau				
4. Jeannine Bell Smith	Crescent Springs			
5.				
6.				
7.				
8.				
9.				
10.				
11.				
12.				
13.				
14.				
15.				
16.				
17.				
18.				
19.				
20.				

Please print clearly • Contact information is optional but must be included if you wish to receive updates.

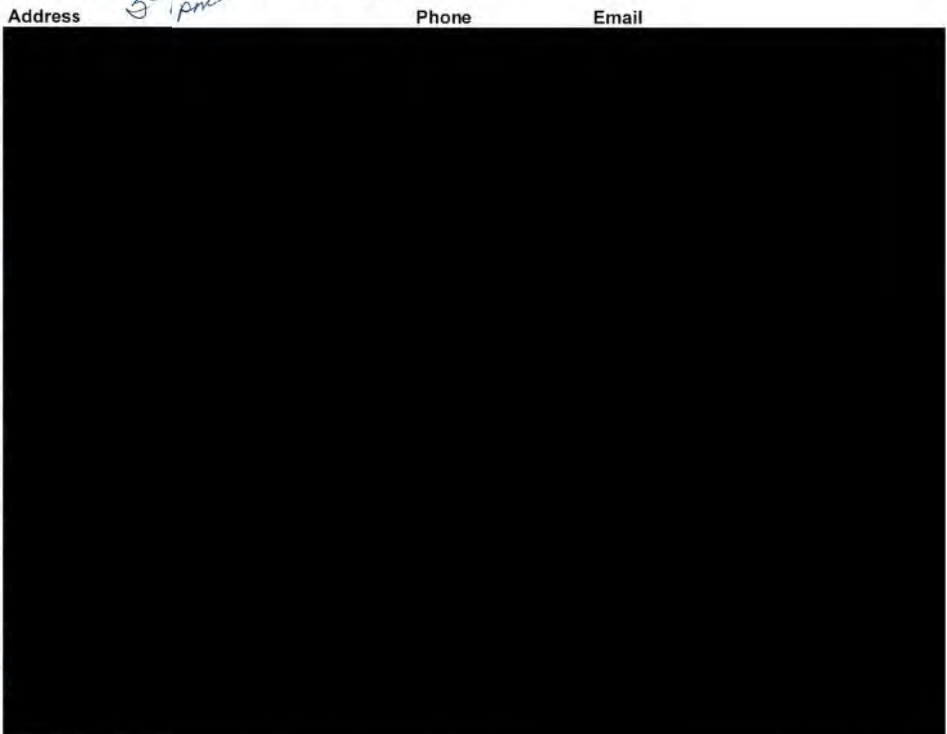
page \_\_\_\_ of \_\_\_\_

KYTC Item No. 6-17 | ODOT PID 89068

**BRENT SPENCE BRIDGE CORRIDOR PROJECT**

Neighborhood Outreach Meeting - December 14, 2022

Name	Representing
1. Mour Dior	FHWA-KY
2. Lisa McCannoch	
3. Ben Merrill	
4. Regina Estes	
5. William Taylor	
6. Lucille Danner	Hickman Properties
7. Daniel Berger	Audubon Neighborhood ACSN
8. Susan Stuber	STORM
9. DAVID GARY	RESIDENS
10. Joan FLESH	POV RESIDENS
11. Charles WOOD	resident
12. Min Orickbach	resident
13. Jade Ganote	"/ Business
14. DAVID WOODARD	AECOM
15. Amy Zoobatski	THE VIEWS
16. Ben Lehinger	Wallace Woods
17. Jeff Wallace	CT Consultants
18. Kevin BASS	
19. Silly Matthews	Senator Paul Pail
20. Beth Wood	



Please print clearly • Contact information is optional but must be included if you wish to receive updates.

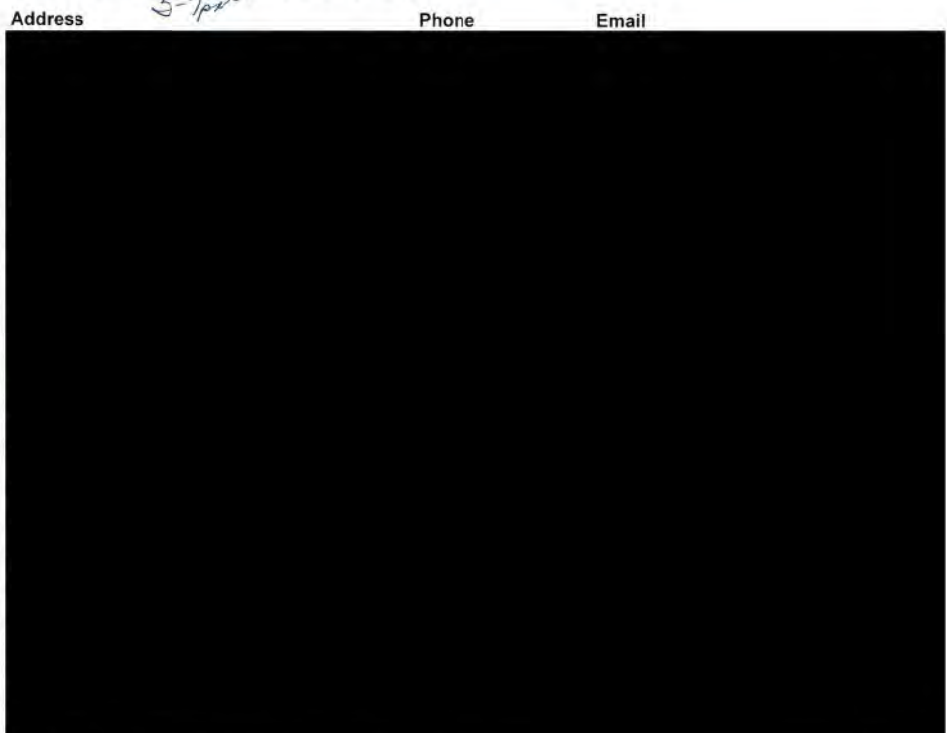
page \_\_\_ of \_\_\_

KYTC Item No. 6-17 | ODOT PID 89068

**BRENT SPENCE BRIDGE CORRIDOR PROJECT**

Neighborhood Outreach Meeting - December 14, 2022

Name	Representing
1. Bob Fluherty	Self
2. Mark C. Griffith	Self
3. Tony Mizum	Self
4. Dale Freeman	
5. Jeff Bayne	ME
6. E. St. R.	ME
7. Chuck Scheper	self
8. Julie G. Schepel	self
9. John Saxton	self
10. EDWIN CLEMENTS	self
11. Andrew Herch	Self
12. Jose Munoz	self
13. Natalie Leverone	self
14. DEVON LUCAS	self
15. Michele Wetermeeyer	self
16. Mark + Jodi	ANTB
17. Stacey + Gary	KYTC
18. Stefan	ODOT
19. Michael	AMB
20. STEVE BENDER	Self



Please print clearly • Contact information is optional but must be included if you wish to receive updates.

page \_\_\_ of \_\_\_

KYTC Item No. 6-17 | ODOT PID 89068



---

Attachment 3: Photographs







---

[Attachment 4: Presentation](#)



The meeting will open with introductions of the Project Team in attendance (2 min).

## Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in neighborhoods surrounding the project the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Changes since 2012
  - Noise
  - Stormwater drainage
  - Aesthetics
  - Pedestrian and bicycles

The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look. Most importantly, tonight we're focusing on hearing from the residents. We're here to answer your questions and to listen to your feedback about the project, which you can do one-on-one with the project team during the open house portion of this meeting, following the presentation.

## Project History

2000

2005

2010

2015

2020

← 2004 | ODOT & KYTC begin BSB Corridor Project

Alternatives Development & Evaluation  
Preliminary Engineering  
Impact Assessment  
Public & Stakeholder Input

← 2012 | EA & FONSI - Preferred Alternative I

Design Refinements  
Cost/Impact Reductions  
Enhancements  
Property Acquisition  
Funding Identification

← 2022 | Concept I-W

3

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Kentucky, that includes I-71 and I-75 from just south of Dixie Highway to the Brent Spence Bridge. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval of the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

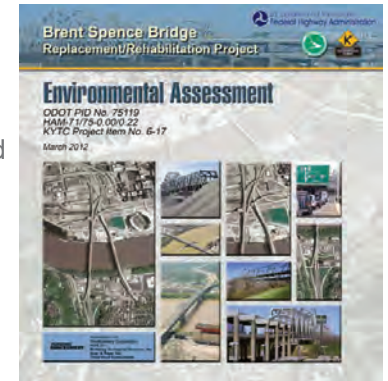
Since the approval of the FONSI in 2012, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2022, KYTC has begun purchasing the land needed to build the project.

3

## Purpose and need

- Improve traffic flow and level of service
- Improve safety
- Correct geometric deficiencies
- Maintain connections to key regional and national transportation corridors



4

The project purpose and need was established early in the project development and has not changed since the 2012 EA/FONSI.

4

## Project Description

### Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E

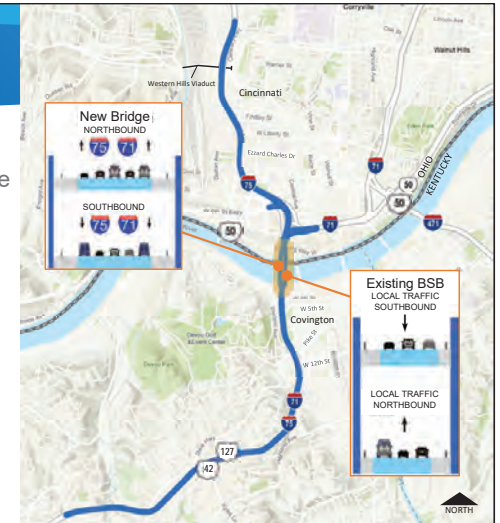


In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. (#) Beginning near Ezzard Charles Drive, a collector-distributor system will be added to connect I-75 traffic to and from the local street network and US-50 West. (#) In the north, it will tie into the recently completed Mill Creek Expressway-Hopple Street Interchange project. (#) The project will rebuild the I-75 interchange at the Western Hills Viaduct and tie into the new bridge replacement project being developed by the City of Cincinnati and Hamilton County. (#) A new northbound exit will be built at Ezzard Charles Drive to improve access to Union Terminal, TQL Stadium, and Over-the-Rhine. Lastly, it will connect to I-71 and US-50 East.

## Project Description

### Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

## Project Description

### Kentucky

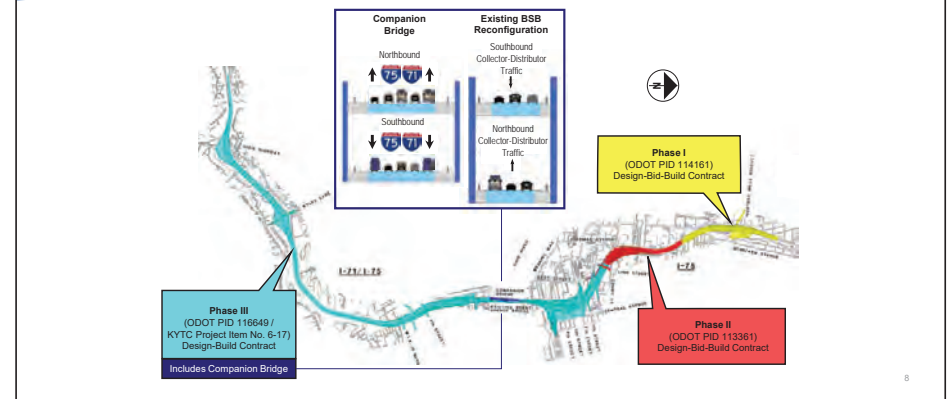
- Reconstruct and widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct a collector-distributor system from 12th Street north
- Construct collector-distributor systems between Dixie Highway and Kyles Lane



Note: Project details will come in with click (marked by #).

In Kentucky, the project will reconstruct and widen I-71 and I-75 and rebuild all overpass bridges and interchanges. (#) The project will also extend existing frontage roads connecting 5th Street and Pike Street going northbound and 4th Street and Pike Street going southbound to improve connectivity in Covington. (#) A collector-distributor system will also be built beginning northbound at 12th Street to connect interstate traffic to and from the local street network. (#) Lastly, collector-distributor lanes will be built from south of Dixie Highway and north of Kyles lane to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

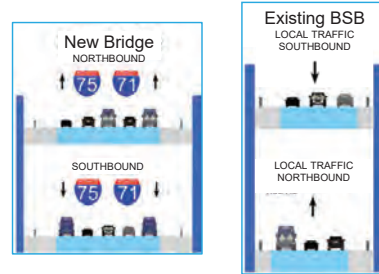
## Project Description



The project is going to be built in three phases. Phase I (shown in yellow) will stretch from Findlay Street to the north. Phase II (shown in red) will stretch from Linn Street to Findlay Street. Phase III (shown in blue) will build everything else, including the new companion bridge. Phase I is currently under design with construction expected to begin in 2028. Although, the construction time frame may adjust based on on-going coordination with the City's Western Hills Viaduct Project. Phase II is also under design with construction expected to begin in 2025. Phase III will be built under a progressive design-build contract which will begin in 2023.

## What Has Changed?

- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



As mentioned earlier, since the 2012 EA and FONSI, KYTC has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

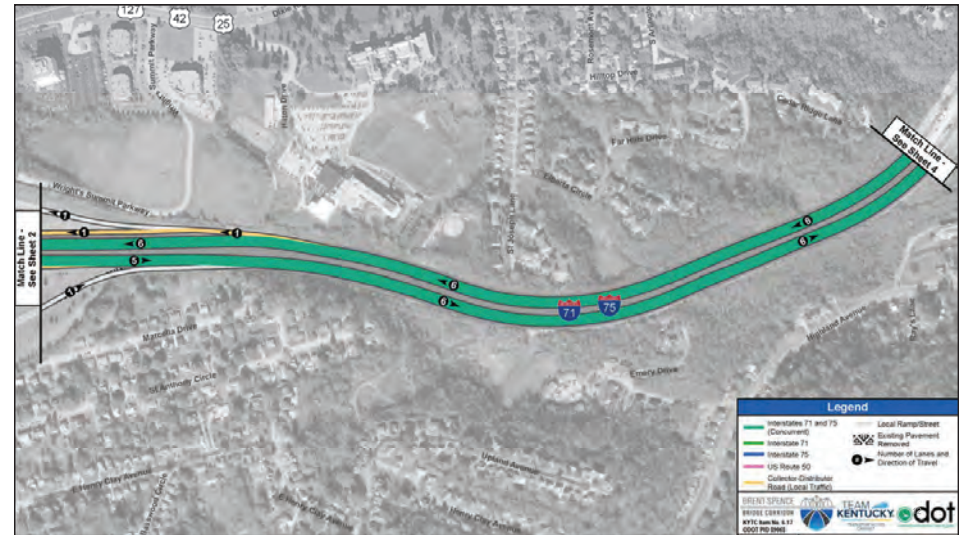
Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.



This slide shows how traffic will move through Fort Mitchell and Fort Wright. The green shows through traffic on the interstate. As shown in the orange, a collector distributor road will start just south of Dixie Highway. The purpose of the collector-distributor road is to reduce the number of places where traffic is entering and exiting the freeway to reduce the number of times vehicles weave in and out of the through travel lanes to access local roads. As shown here, in the northbound direction, traffic traveling to Dixie Highway and Kyles Lane will exit onto the collector-distributor road south of Dixie Highway. Vehicles traveling to Dixie Highway will then leave the collector-distributor road and take a ramp to Dixie Highway (shown in grey). Traffic traveling to Kyles Lane will continue north on the collector-distributor road.



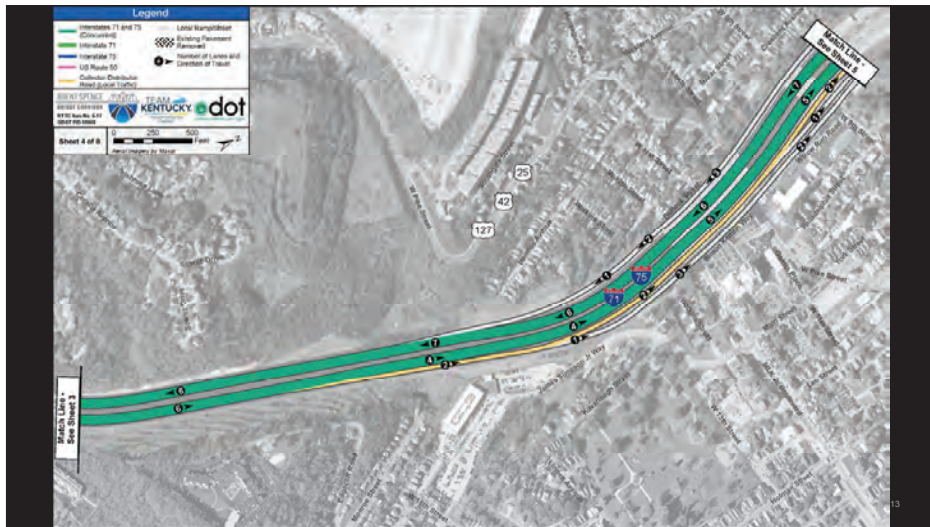
Moving further north, traffic from Dixie Highway will travel down a ramp to enter the northbound collector-distributor road. Next, traffic traveling to Kyles Lane will leave the collector-distributor road and use a ramp to reach Kyles Lane. Northbound traffic will travel through on the collector-distributor road, and more traffic will enter from Kyles Lane.



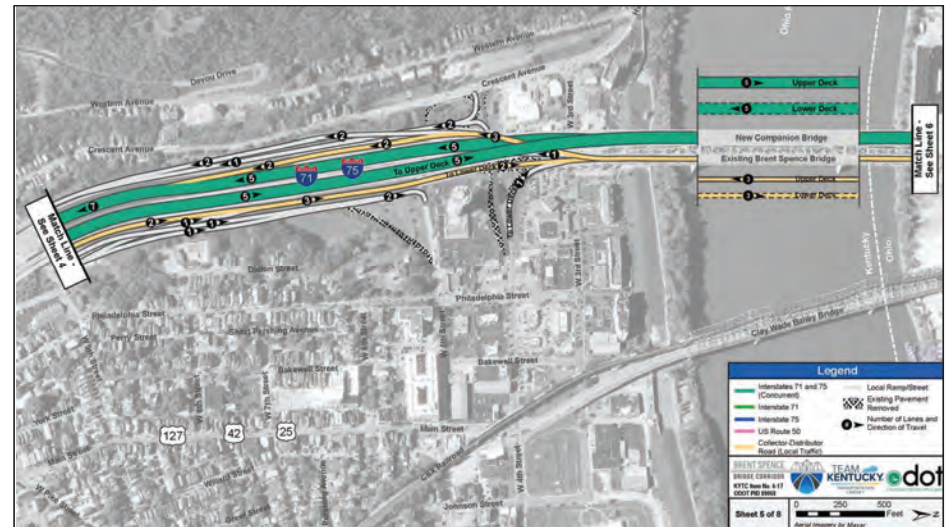
Finally, the traffic on the collector-distributor road will join with traffic from the Kyles Lane ramp and enter the interstate going northbound. Notice that there will be only one northbound exit and one entrance between Dixie Highway and Kyles Lane instead of one exit and one entrance for each road. Consolidating these access points will improve traffic flow and reduce high-speed crashes on the interstate.

The traffic movements we just walked through will be reversed in the southbound direction, providing the same access scheme and improvements in traffic flow and safety.

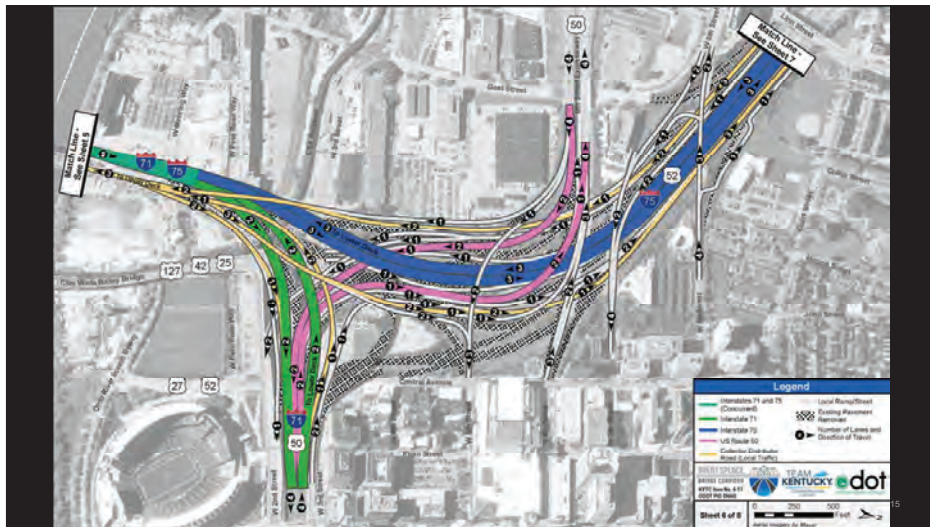




This slide shows how traffic will move in southern Covington. The teal lines show I-71 and I-75 traffic. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets.



This slide shows how traffic will move across the Ohio River. Interstate traffic (shown in green) will use the companion bridge and stay on the interstate corridor to travel through Covington and Cincinnati. Local traffic will use ramps and the collector-distributor system to travel to destinations in Cincinnati and Covington.



This slide shows how traffic will through Cincinnati. The blue lines show how I-75 traffic will travel to and from the companion bridge. The green lines show how traffic will on I-71 will be routed to and from the companion bridge. The orange lines show the collector-distributor system that will funnel traffic to and from local roadways and ramps. The grey lines show ramps connecting directly to local streets. Finally, the pink lines show how traffic will flow on US 50.



This drawing shows what the new companion bridge and the existing Brent Spence Bridge might look like after the project is built. Please note that the final design of the companion bridge will not be complete for a few years. It is possible it could be a cable-stayed design.

## What Has Changed?

- Reduced shoulders
- Reduced design speeds
- Reduced lanes on frontage roads
- Reduced relocations
  - Residential | 40+ then vs. 4 now
  - Commercial | 6 then vs. 5 now



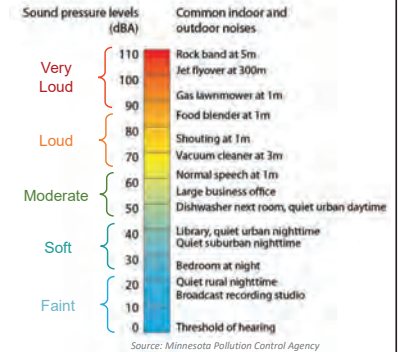
Since 2012, the design has been refined to reduce shoulder widths on I-71, I-75, and collector-distributor roads to 10 feet to match current design standards. Previous design standards were 12 feet. In addition, the design speeds on I-71, I-75, and the collector-distributor roads were adjusted to match the posted speed limits – which is 55mph for the interstate and 45 mph for the collector-distributor roads. This is 5 mph less than the 2012 design. We also completed additional traffic analyses and were able to reduce the number of lanes on the frontage roads between West 12th Street and Pike Street. There were originally 5 lanes northbound and 4 lanes southbound, and we were able to reduce both directions to 3 lanes.

These changes coupled with additional refinements to the design have substantially reduced the overall area needed to build the project in Kentucky. As a result, we've been able to substantially reduce the number of residents who will need to be relocated to build the project. In 2012, we estimated that over 40 residences would need to be relocated. Today, only 4 residences need to be relocated. In 2012, we estimated 6 businesses would need to be relocated. Now that number is 5.

## Noise

### KYTC Noise Policy

- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds.

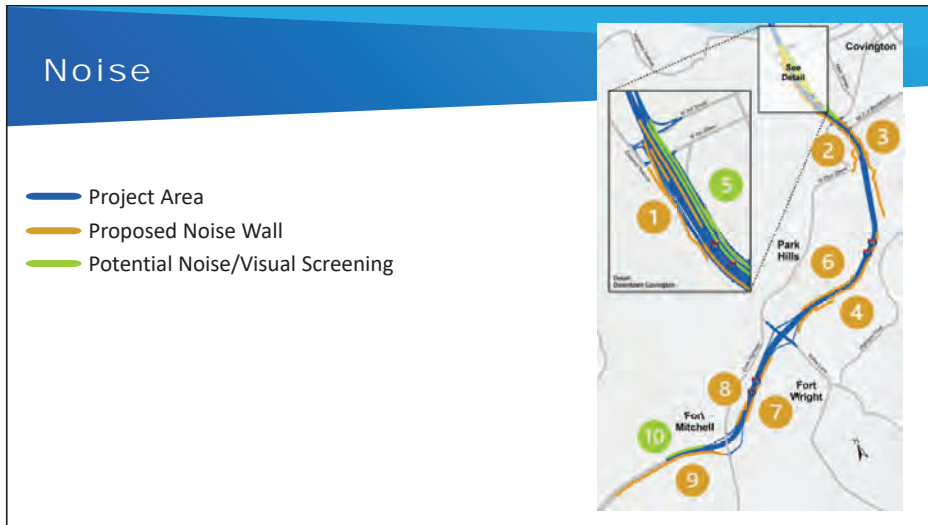


KYTC has a formal Noise Policy that was developed in partnership with the Federal Highway Administration that guides how traffic noise is evaluated for transportation projects. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a picnic area, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. For example, the threshold for a hotel or office receptor is 72 dbA, while the threshold for a house receptor is 67 dbA. We also determine if there is a substantial increase – specifically a 10 dbA increase - in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In Kentucky, that means determining if a barrier can provide a minimum 5 dbA reduction for three of the impacted receptors. In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then KYTC evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. Specific criteria for evaluating these factors are spelled out in KYTC's noise policy. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.

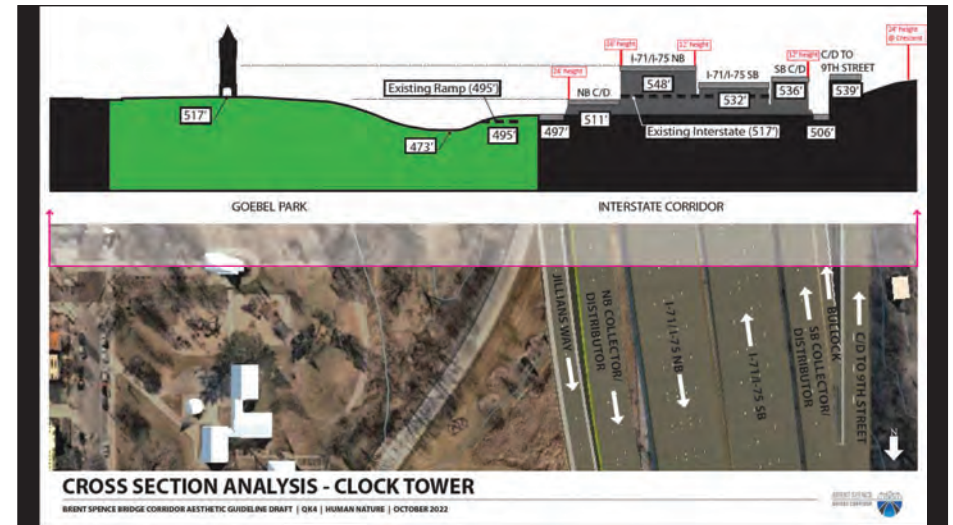
If a noise barrier is found to be both feasible and reasonable, then KYTC will ask the people who will benefit from it if they actually want it. If they agree, a noise barrier will be built.



KYTC evaluated several noise walls and noise wall systems in throughout the corridor. Some of them were warranted based on KYTC's Noise Policy. These include in Botany Hills and Lewisburg shown by #1 on this slide and the Lewisburg Historic District shown by #2. Another system of noise walls was found to be reasonable and feasible in Westside and northern Peaseburg, shown by #3. Another noise wall system shown by #4 was found to be warranted east of the highway from Highland Pike to Kyles Lane and covering portions of southern Peaseburg and Fort Wright. On the west side of the highway, another noise wall was warranted north of Kyles Lane covering portions of Park Hills and Fort Wright. Between Dixie Highway and Kyles Lane on the east in Fort Wright, see #7. In Fort Mitchell, noise walls were warranted west of I-71/I-75 north of Dixie Highway and east of the interstate south of Dixie Highway as shown in #8 and #9.

However, in Mainstrasse, a noise wall system was found to be feasible but not reasonable because it was not cost effective and did not meet design goals for overall noise reduction. This noise wall system is shown as #5. Similarly, in Fort Mitchell, a noise wall south of Dixie Highway near West Maple Avenue was not found to be cost effective, see #10.

Recognizing that traffic noise is a concern, KYTC is currently conducting a technical study to further evaluate noise/visual screening walls to help provide noise reduction for residences in Mainstrasse and along West Maple Avenue and to shield views of the highway. KYTC will document their decision about this noise wall system in a technical memo based on the technical feasibility and public comments. Comments received as a result of this meeting will be considered as part of that process. For any recommended noise walls, KYTC will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.



This slide shows a cross-section of the roadway and noise barriers at the clock tower to demonstrate the overall schematic of the roadway and the height of potential noise walls.

# Noise



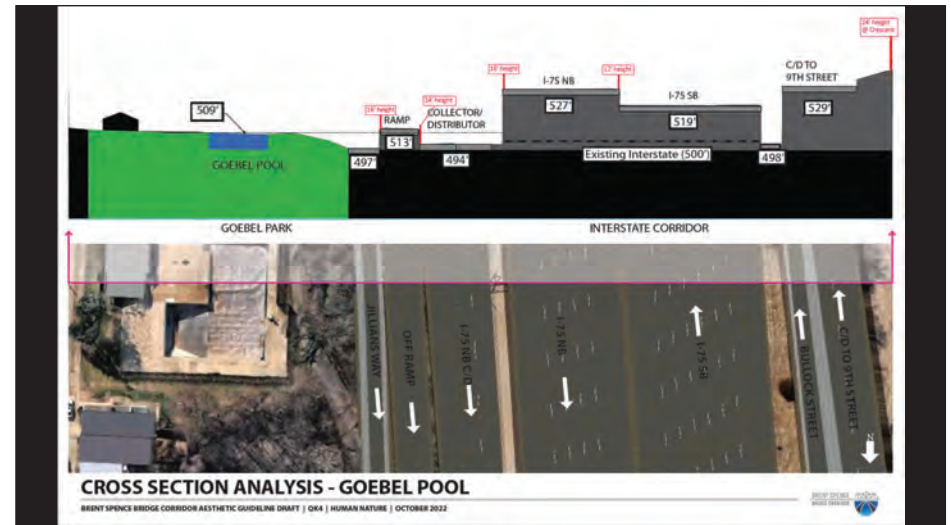
Goebel Park Clock Tower  
(looking west)



Goebel Park Clock Tower with Noise Wall  
(looking west)

21

The graphic to the left shows the finished project might look like from the Goebel Park Clock Tower without noise walls. The graphic to the right shows what the finished project might look like if noise walls are built. These views are looking west. Other exhibits available at the open-house show what other locations along the corridor might look like with and without noise walls.



This slide shows a cross-section of the roadway and noise barriers at the Goebel pool to demonstrate the overall schematic of the roadway and the height of potential noise walls.

## Willow Run Watershed Drainage Improvements

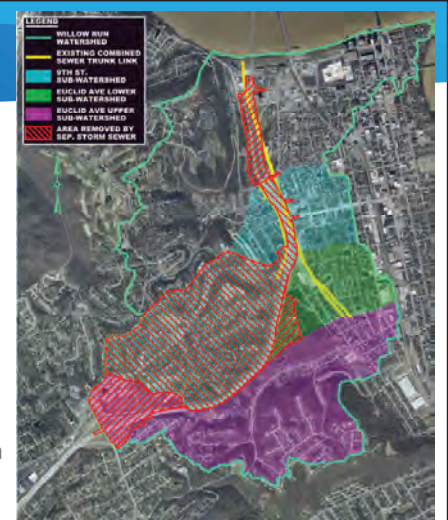
- Project team working with the City of Covington and Sanitation District No. 1
- Euclid Avenue drainage/flooding issues
- Goal
  - Construct a new interstate storm sewer system to remove stormwater runoff from the Willow Run combined sewer system
- Benefits
  - Reduce flooding problems in the Euclid Avenue area and Goebel Park
  - Reduce combined sewer overflows



KYTC is working with the City of Covington and Sanitation District No. 1 to address drainage issue in the project area. Existing highway runoff flows in the combined sewer system in Covington, and overflow events negatively affect residents, particularly in the Euclid Avenue area and also in Goebel Park. KYTC is committed to separating the highway drainage from the combined sewer systems as part of the project.

## Willow Run Watershed Drainage Improvements

- New separated storm sewer system along interstate
- Removes drainage areas from the combined Willow Run sewer in the following sub-basins:
  - Euclid Avenue upper basin – 70 acres
  - Euclid Avenue lower basin – 22 acres
  - West 9th Street basin – 375 acres
- Replace portion of Willow Run trunk line with new 120-inch pipe from hospital to W. 9th Street
- Modeling of the system shows reduction in flooding events



(Note: Project details will come in with click (marked by #))

As part of the Brent Spence Bridge Corridor Project, KYTC will be a new separated storm sewer system along the reconstructed interstate. We are not just stiving to separate additional runoff from the combined sewer systems, but we are working to separate all runoff in in Covington, which will substantially reduce overflow events.

The yellow line shows the existing combined storm sewer, and the teal line shows the Willow Run watershed which feeds the sewer. (#) The blue, green, and purple show the Euclid Avenue upper and lower and the West 9th Street sub-basins, which feed the Willow Run sewer. (#) When the new storm sewer system is built, it will remove the red hatched area from the combined Willow Run system.

In addition, portions of the Willow Run trunk line will be replaced with a larger 120-inch pipe from Saint Elizabeth Hospital to West 9th Street. Modeling of the system shows a reduction in flooding events in the Euclid Avenue and Goebel Park areas.

## Goebel Park

- Land required for roadway construction
- Additional land added near West 5th Street
- Adding shared use paths



The project will require about 3.2 acres of land from Goebel Park to widen I-71 and I-75 and to build the collector-distributor roads and ramps. As you can see in this drawing, the West 5th Street ramp will be reworked and will be much closer to the interstate. KYTC plans on giving back about 2.2 acres of land that was previously occupied by the West 5th Street ramp back to Goebel Park. In addition, KYTC has been coordinating with the Covington to determine additional enhancement measures that can be incorporated into the park to mitigate impacts from the project. As we discussed earlier, KYTC is also evaluating a system of noise walls to potentially reduce noise and provide visual screening between Goebel Park and the highway.

## Enhancement Measures

- Working with local cities to develop aesthetic guidelines
  - Landscaping
  - Streetscapes
  - Gateways
  - Treatments for piers, abutments, retaining walls, and pedestrian paths

KYTC is working with the cities of Covington, Fort Wright, and Fort Mitchell to develop plans for landscaping, streetscapes, gateways, and treatments for piers, abutments, retaining walls, and pedestrian paths throughout the Brent Spence Bridge Corridor. An aesthetics committee was formed to provide input on the design and aesthetic appearance of the Brent Spence Bridge and roadway corridor. The role of this group is to evaluate aesthetic treatments through the corridor, including structure type and corridor theme. Multiple aesthetics group meetings will be planned throughout the design process to discuss opportunities to balance aesthetic design goals with a desire to encourage innovation in design and construction of the new bridge.

KYTC is also working with Covington to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.



This is a view of what Goebel Park might look like once the highway improvements are constructed, the new shared use paths are built, and agreed upon landscaping is in place. The storm water improvements will also reduce the number of flooding events in Goebel Park. Note the interstate highway as the top tier of the roadway system to the left, the collector-distributor road as the middle tier, and the local connector road as the surface-level street.

## Enhancement Measures

- Pedestrians and Bicycles
  - Connections across I-71 and I-75
  - New shared use paths
  - New and rebuilt sidewalks

KYTC is also working to identify areas to add new shared use paths and sidewalks to improve pedestrian and bicycle connectivity across the I-71 and I-75.





This exhibit shows some of the planned pedestrian and bicycle facilities near Lewisburg and Botany Hills. Note the new shared use paths and sidewalks along the frontage roads. Also notice the new and rebuilt sidewalks under the MLK/West 12th, Pike, and West 9th Street bridges. A new shared use path will be built under the West 9th Street bridge, which will tie into the shared use paths in Goebel Park.



Moving north, note the new shared use path in Goebel Park and under the West 5th Street bridge. Also notice the new and rebuilt sidewalks along West 5th and West 3rd streets. Throughout Covington, the shared use paths and sidewalks will improve connections to existing transit stops.

## Corridor Flyover Animation

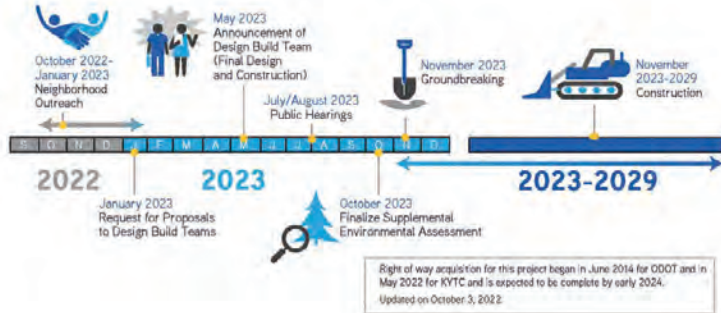
31

This is a flyover animation of what the finished corridor might look like from Dixie Highway in Kentucky to the Western Hills Viaduct in Ohio.



This is a flyover animation of what the finished corridor might look like from Dixie Highway in Kentucky to the Western Hills Viaduct in Ohio.

## Schedule



33

This slide shows the project schedule from today to the completion of construction.

# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)

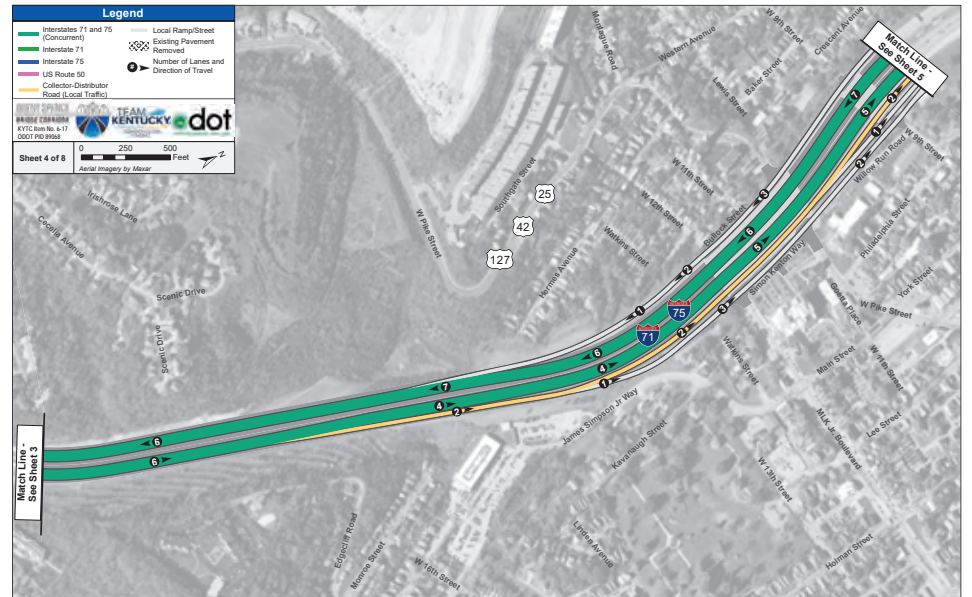
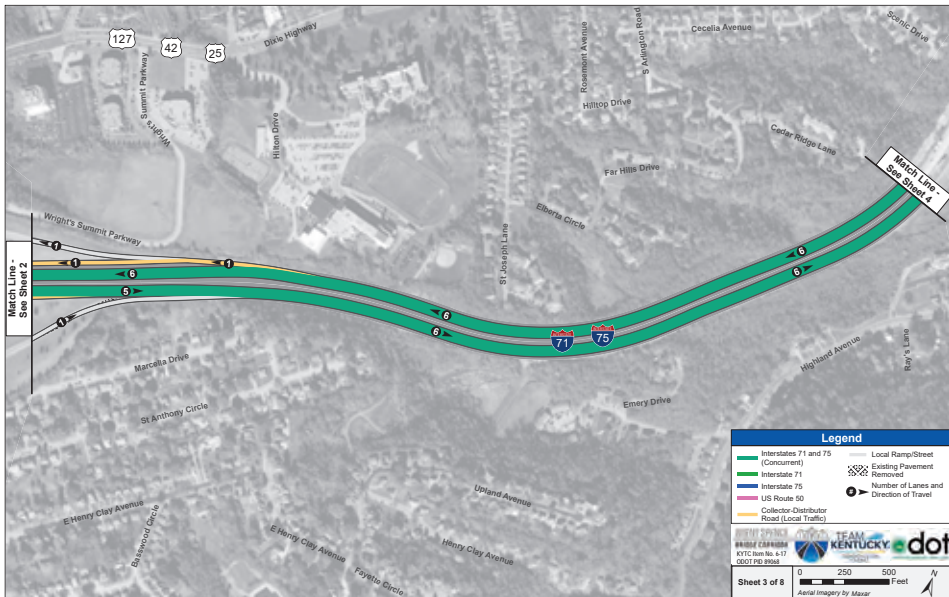
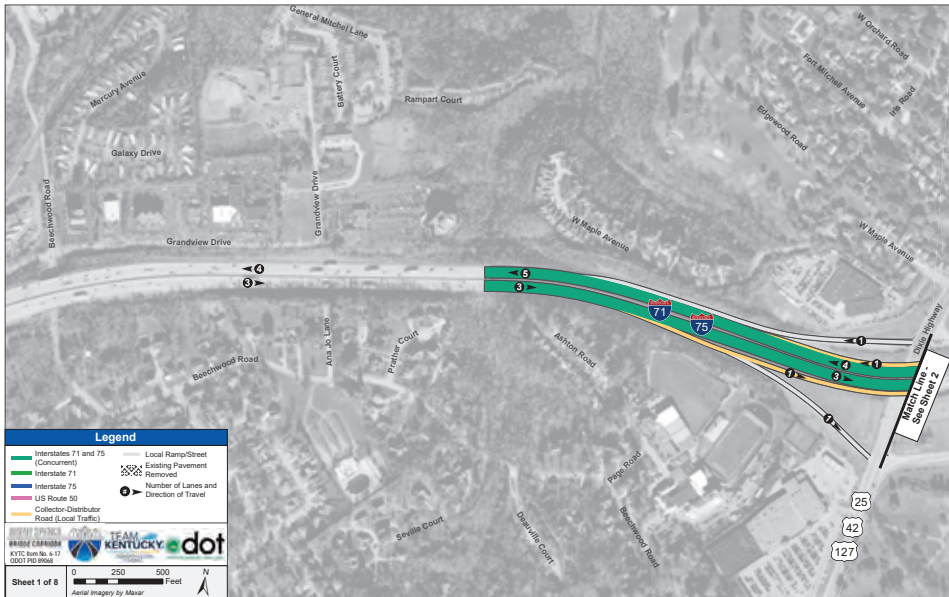


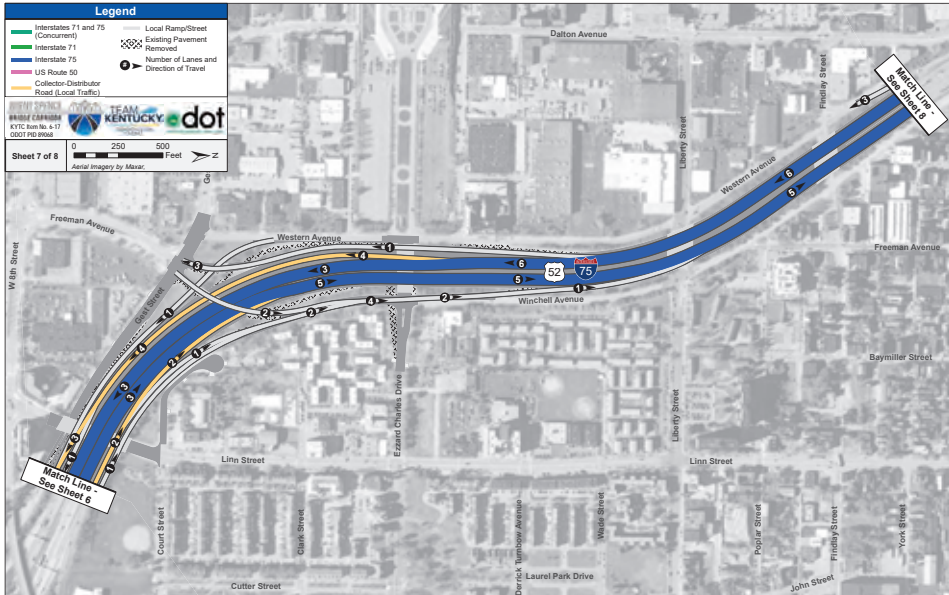
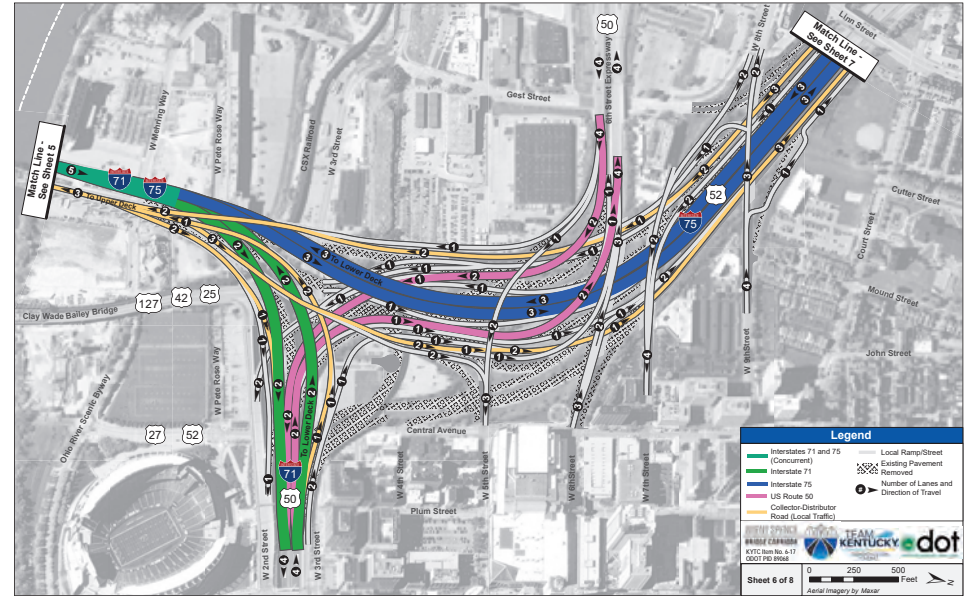
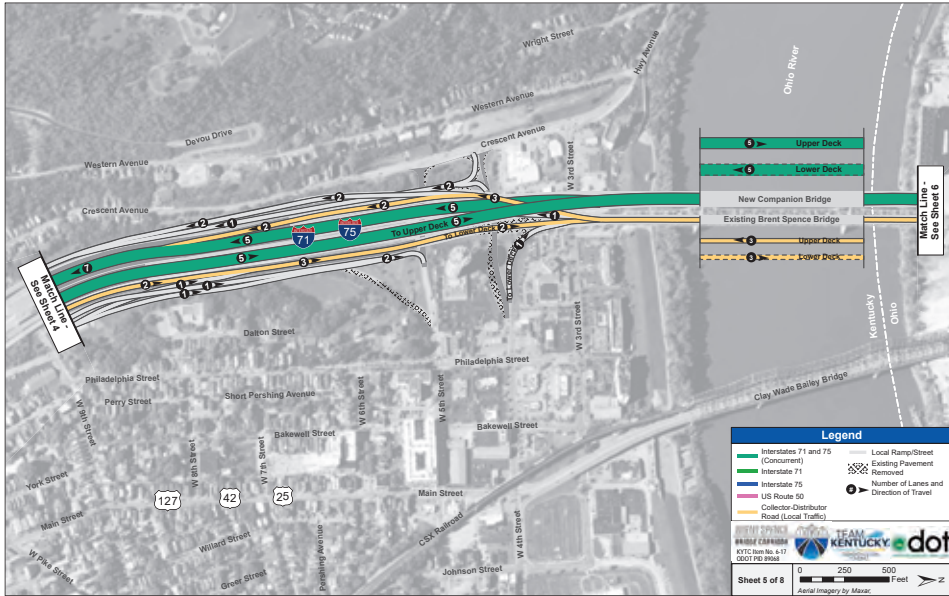
This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [PublicInput.com](http://PublicInput.com) to review information about the project and provide your feedback.

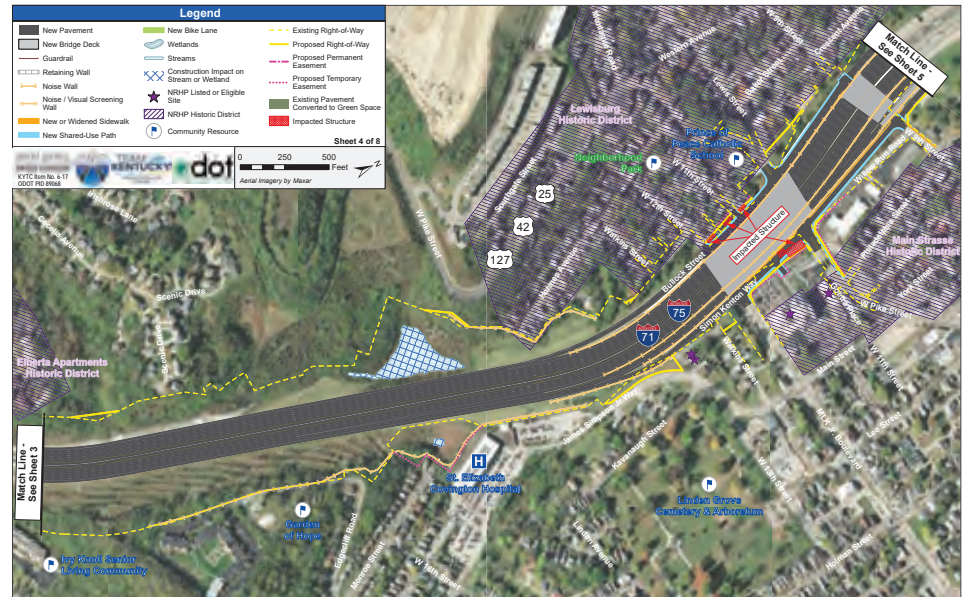


---

[Attachment 5: Exhibits](#)

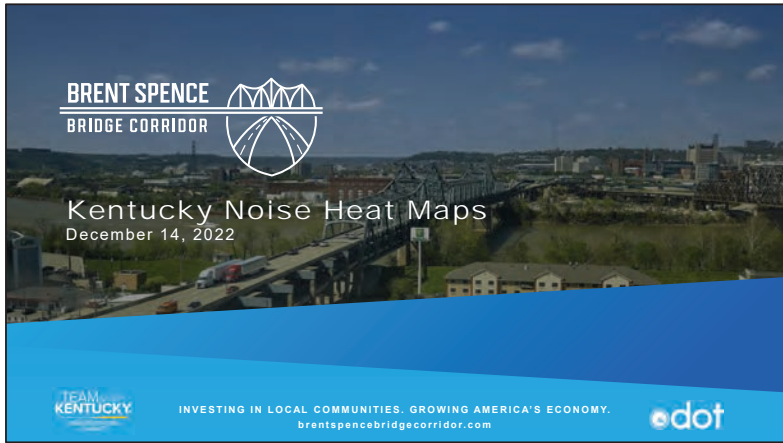














**BRENT SPENCE**  
BRIDGE CORRIDOR



**Kentucky Noise Heat Maps**  
December 14, 2022

**TEAM KENTUCKY** INVESTING IN LOCAL COMMUNITIES. GROWING AMERICA'S ECONOMY.  
[brentspencebridgecorridor.com](http://brentspencebridgecorridor.com)



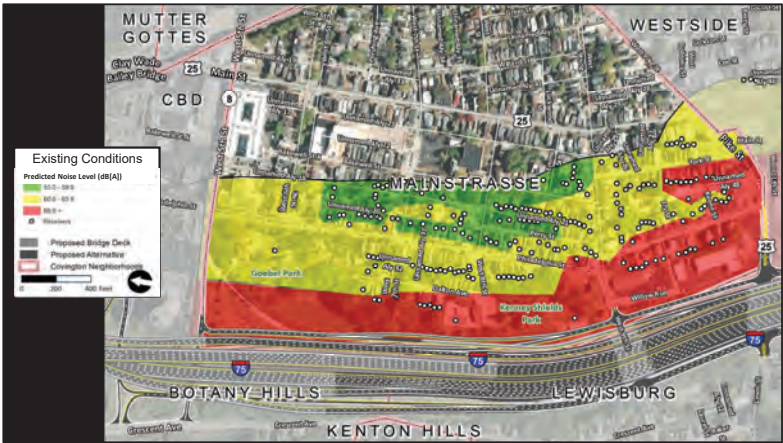
**BRENT SPENCE**  
BRIDGE CORRIDOR



**Mainstrasse Neighborhood**



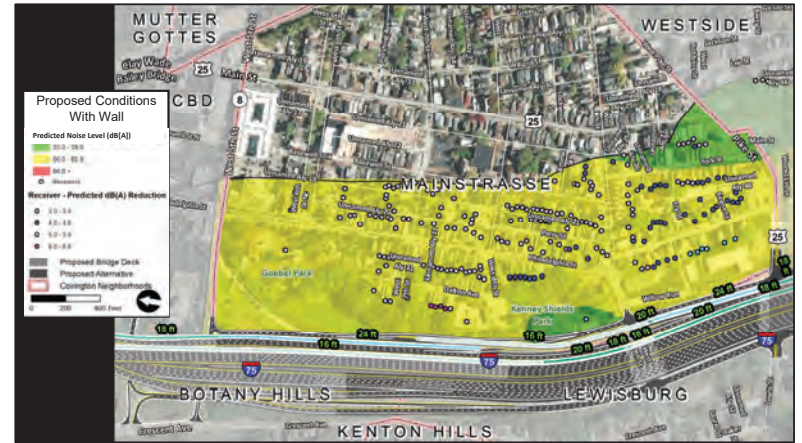
The red lines on this slide show noise walls that were evaluated as part of the noise study for Mainstrasse. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



The red lines on this slide show noise walls that were evaluated as part of the noise study for northern Peaselburg. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis for northern Peaseburg. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.





The red lines on this slide show noise walls that were evaluated as part of the noise study for in the Westside Covington area. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a noise wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.





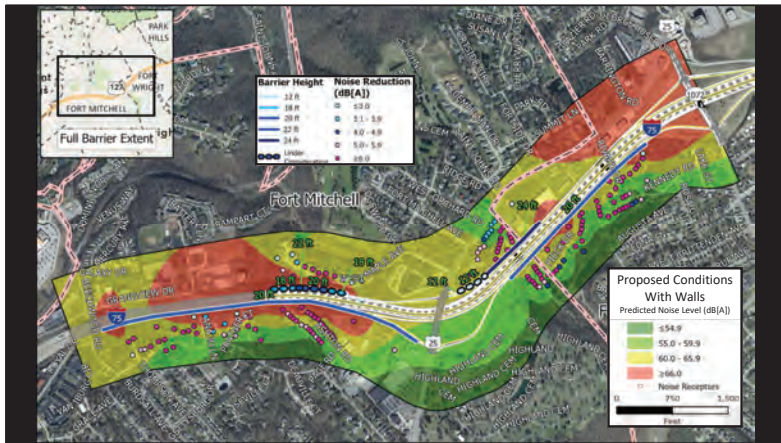
The red lines on this slide show noise walls that were evaluated as part of the noise study Fort Mitchell. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis for Fort Mitchell. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.

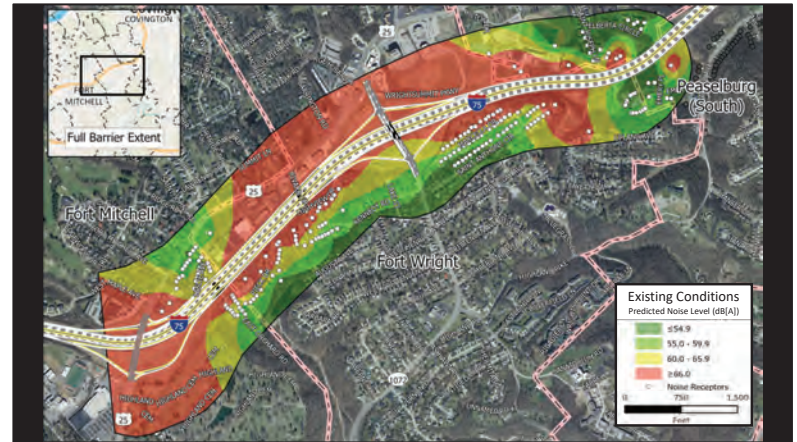


This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a noise wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

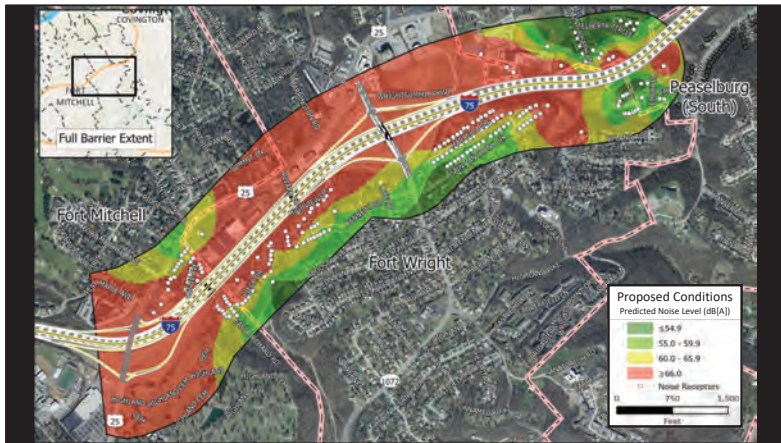




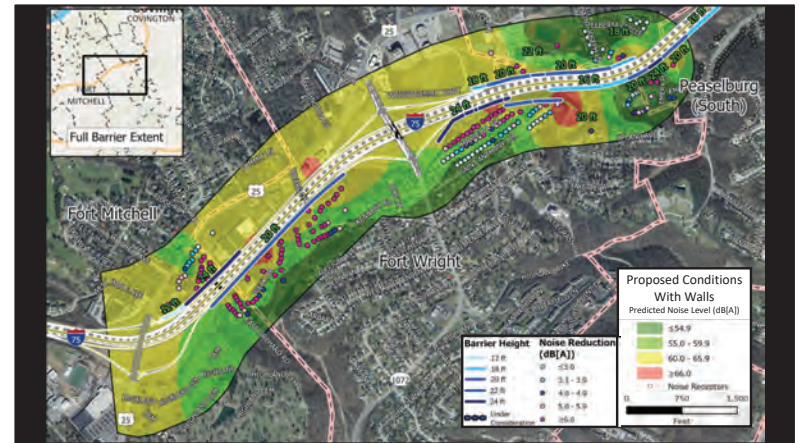
The red lines on this slide show noise walls that were evaluated as part of the noise study for Fort Wright. The green numbers show the heights of the different sections of the noise walls.



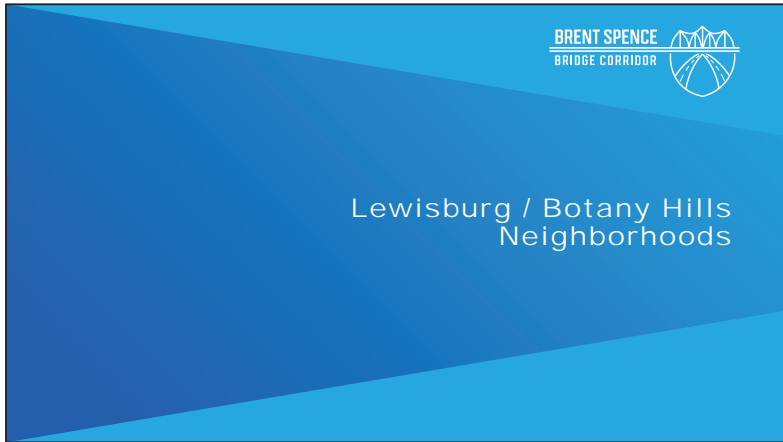
This slide highlights the receptors evaluated as part of the noise analysis for Fort Wright. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a noise wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



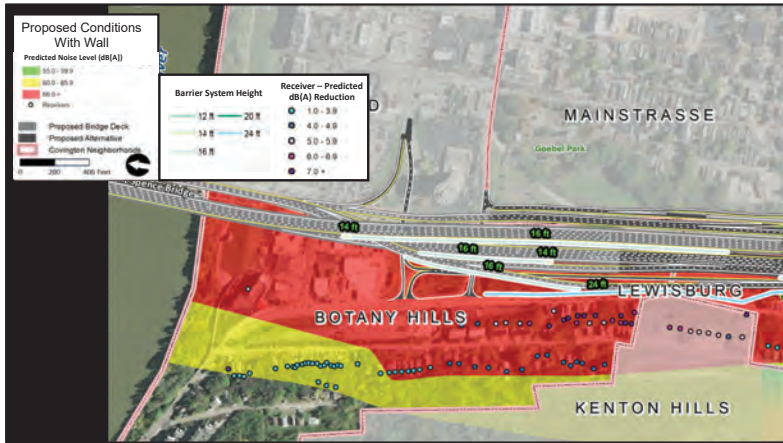
The red lines on this slide show noise walls that were evaluated as part of the noise study for northern Lewisburg and Botany Hills. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.

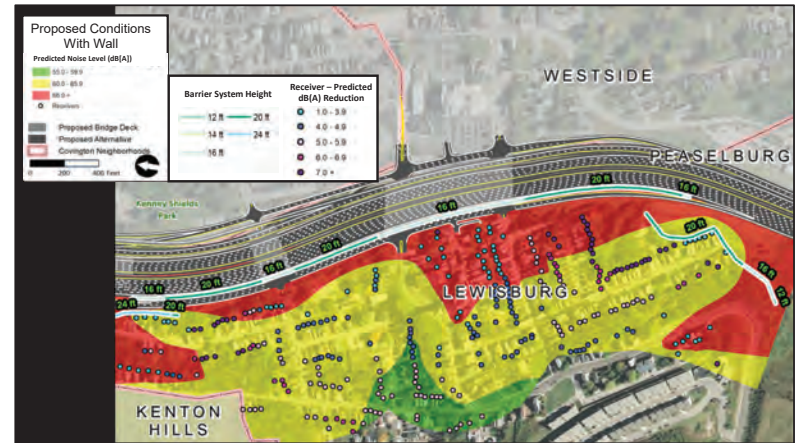


This slide highlights the receptors evaluated as part of the noise analysis. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.





This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a barrier wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



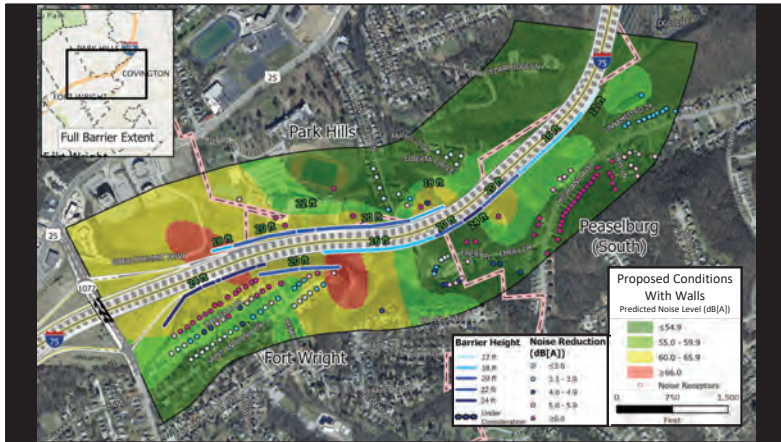
The red lines on this slide show noise walls that were evaluated as part of the noise study for Park Hills. The green numbers show the heights of the different sections of the noise walls.



This slide highlights the receptors evaluated as part of the noise analysis for Park Hills. The color gradient indicates existing noise readings, with red representing areas with higher sound levels and green representing quieter areas.



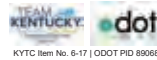
This slide shows how sound conditions are projected to change for the evaluated receptors by the year 2050 after the project is built. The color gradient indicates predicted noise levels, with red representing areas with higher sound levels and yellow representing moderate sound levels.



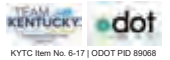
This slide shows predicted noise levels for the evaluated receptors under proposed (2050) conditions with construction of a noise wall. For the receptors, anything with light pink and dark pink have a reduction of 5 decibels and are considered benefitted receptors.



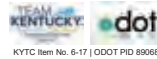
**Brent Spence Bridge Corridor  
(Looking Northwest from Kentucky)**



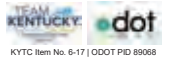
**Brent Spence Bridge Corridor - Kentucky  
(Looking Southeast)**



**Pike Street With Noise Walls (Looking Northwest) - Kentucky**

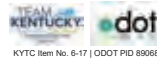


**Pike Street Without Noise Wall (Looking West) - Kentucky**

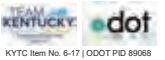




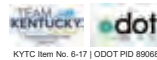
Pike Street With Noise Wall (Looking West) - Kentucky



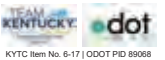
Pike Street Without Noise Wall (Looking East) - Kentucky



Pike Street With Noise Wall (Looking East) - Kentucky

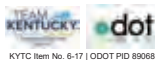


Goebel Park (Looking North) - Kentucky





**I-71/I-75 Corridor Without Noise Walls – Kentucky  
(Looking West from Goebel Park Clock Tower)**



KYTC Item No. 6-17 | ODOT PID 89068



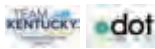
**I-71/I-75 Corridor With Noise Walls – Kentucky  
(Looking West from Goebel Park Clock Tower)**



KYTC Item No. 6-17 | ODOT PID 89068



**Crescent Avenue Without Noise Wall - Kentucky  
(Looking South)**



KYTC Item No. 6-17 | ODOT PID 89068



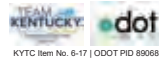
**Crescent Avenue With Noise Wall - Kentucky  
(Looking South)**



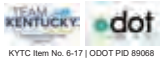
KYTC Item No. 6-17 | ODOT PID 89068



**Crescent Avenue Without Noise Wall - Kentucky  
(Looking North)**



**Crescent Avenue With Noise Wall - Kentucky  
(Looking North)**







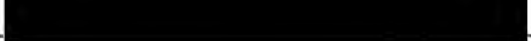
---


Attachment 6: Comment Forms

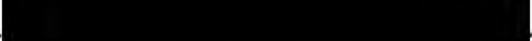
# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name 

Mailing address (or nearest cross streets): 

Email address:  Phone: \_\_\_\_\_

Business/Organization Name: 

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Noise Barriers for All borders

Potential Beautification of Noise Barriers  
↳ similar to those along the river in Covington

Consider impacts to local traffic & our ease of ~~driving~~ travel time.

↳ GreenUp of Scott should remain 1 way unless there are plans to improve Madison prior to this project.

\* It being convenient to get around (anywhere in 15 min) is a big part of the reason to live in Covington

COMMENTS DUE BY  
January 5, 2023

Comments may be submitted:

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
- By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068

# COMMENT FORM

BRENT SPENCE  
BRIDGE CORRIDOR



Contact information is not required but will ensure you receive a response, should one be required.

Name:

Mailing address (or nearest cross streets):

Email address:

Phone:

Business/Organization Name:

Business/Organization Address:

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

Adding lanes does not solve traffic.

Houston, LA have expanded highways  
and nothing but gridlock.

Please do not spend 3 Billion

on moving traffic through my city.

I want people to live and enjoy it

COMMENTS DUE BY  
January 5, 2023

Comments may be submitted:

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
- By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068

# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: 

Mailing address (or nearest cross streets): 



Email address:  one: 

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

- Area Resident     Area business owner or employee     Commuter  
 Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

I want the noise wall  
in my area! It would  
bring the noise level down  
tremendously!

**COMMENTS DUE BY  
January 5, 2023**

- Comments may be submitted:
- Verbally or in writing after the meeting
  - By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
  - By telephone at 1-800-831-2142
  - Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
  - By mail using address on back



# COMMENT FORM

Contact information is not required but will ensure you receive a response, should one be required.

Name: 

Mailing address (or nearest cross streets): 

Email address: \_\_\_\_\_ Phone: \_\_\_\_\_

Business/Organization Name: \_\_\_\_\_

Business/Organization Address: \_\_\_\_\_

What is your interest in the proposed project? (Select all that apply.)

Area Resident     Area business owner or employee     Commuter

Other: \_\_\_\_\_

General Comments (Please attach additional pages, if needed):

The noise walls are depressing. They will be a target for graffiti.

Why can't they be living walls? Why can't they have plants, vines that will enhance the living environment (birds, butterflies, etc.) and be soothing in contrast to more concrete?

COMMENTS DUE BY  
January 5, 2023

Comments may be submitted:

- Verbally or in writing after the meeting
- By email at [Keith.Smith@dot.ohio.gov](mailto:Keith.Smith@dot.ohio.gov)
- By telephone at 1-800-831-2142
- Online at [PublicInput.com/bsbc](http://PublicInput.com/bsbc)
- By mail using address on back



KYTC Item No. 6-17 | ODOT PID 89068

# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

---

---

---

Thank you for helping ODOT and KYTC improve their public involvement practices!

**Please submit this page only:**  
Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518



# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

## What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

## What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

## Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

## How many people live in your household?

- 1 - 2    3 - 5    6 +

## What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

## What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

## What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

## Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

Thank you for helping ODOT and  
KYTC improve their public  
involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of  
Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

# DEMOGRAPHICS FORM

Thank you for taking a few minutes to complete this survey. Answering the following questions is optional, but your response will be extremely helpful in ensuring the fairness and equity of ODOT's and KYTC's public involvement process. Submissions will be kept confidential and separate from any personally identifiable information so that respondents will remain anonymous. These questions are not listed in any particular order.

### What is your race or ethnicity?

- Black or African American    American Indian or Alaskan Native    Asian    Hispanic or Latino  
 White    Native Hawaiian or Other Pacific Islander  
 I prefer to self-describe: \_\_\_\_\_

### What is the primary language spoken in your home?

- English    Spanish    Other (Please specify): \_\_\_\_\_

### Was project information translated into other languages appropriately?

- Yes    No    Not Applicable

### How many people live in your household?

- 1 - 2    3 - 5    6 +

### What are the age ranges of those living in your household? (Check all that apply.)

- Under 18    19 - 44    45 - 64    65 +

### What is your annual household income?

- Less than \$10,000    \$10,000 - \$24,999    \$25,000 - \$49,999    \$50,000 - \$74,999  
 \$75,000 - \$99,999    \$100,000 - \$149,999    \$150,000 +

### What is the highest level of education completed by members of your household?

- No    Elementary school    Middle school    High school    College/university  
 Other: \_\_\_\_\_

### Do any individuals living in your home have a physical or mental impairment which substantially limits one or more major life activities?

- Yes    No

Please suggest additional ways you think ODOT and KYTC can improve the inclusiveness of their public outreach efforts.

Door knocking of every neighbor impacted.

Thank you for helping ODOT and KYTC improve their public involvement practices!

### Please submit this page only:

Email to: Keith.Smith@dot.ohio.gov  
Mail to: Ohio Department of Transportation, District 8  
ATTN: Keith Smith, P.E.  
505 South State Route 741  
Lebanon, OH 45036-9518

TEAM KENTUCKY  
TRANSPORTATION CABINET

dot  
transportation.ohio.gov





---

# Brent Spence Bridge Corridor Project

## West End Meeting Summary

### December 20, 2022

---

#### Introduction

The West End meeting was held on December 20, 2022 from 6:00 pm to 8:30 pm at the Lincoln Recreation Center (1027 Linn Street in Cincinnati, Ohio). Invitation flyers were sent to the West End Community Council. The presentation was also advertised via the agenda for the December 20<sup>th</sup> meeting, which was emailed to the members of the West End Community Council. In addition, the meeting date was published on the project website and the December project newsletter (Attachment 1). Attendees at the meeting included the City of Cincinnati, the project team, members of the West End Community Council, and members of the public. In total, 24 people attended in person, as well as multiple individuals from the Ohio Department of Transportation (ODOT), the City of Cincinnati, and the project team. Sign-in sheets are available in Attachment 2; however, many individuals opted not to formally sign in. The meeting was also available virtually via a Google Meet, and 18 people attended online. Photographs are included in Attachment 3.

The meeting format included a formal presentation by ODOT as part of the regularly scheduled meeting of the West End Community Council (Attachment 4). After the presentation, the project team responded to questions and comments offered by those present.

Exhibits showing the renderings of Ezzard Charles Drive, Findlay Street, and drawings of the planned improvements in the vicinity of West End were available for viewing before and after the meeting (Attachment 5). Written comment forms, a demographic survey, and a postcard inviting individuals to visit [PublicInput.com/bsbc](https://PublicInput.com/bsbc) to review project materials and offer additional feedback were also provided. No comment forms or demographic surveys were submitted at the meeting.

#### Comments

Below is a summary of comments/questions received during the presentation:

- How was it determined that the noise walls were not reasonable in this neighborhood?
  - ODOT's noise policy outlines criteria for analyzing noise. One requirement is that there must be locations of frequent human exterior use. A cost-benefit analysis is also completed during the noise investigation. A noise analysis has been completed, and noise walls are not recommended in West End because they do not meet the requirements of ODOT's noise policy. However, the current design will include eight-foot screening walls to reduce tire pavement noise. The noise analysis will be located online for review, and comments can be submitted on the project website.



- 
- What due diligence has been done to evaluate the Bridge Forward proposal and incorporate changes into the current project?
    - ODOT prepared a detailed response to comments submitted by Bridge Forward, which is available on the project website. In addition, ODOT has reached out to Bridge Forward to schedule a working session to discuss their concepts.
  - Why can't we cap or trench the interstate?
    - I-75 has to meet vertical height requirements over the Ohio River and the railroad, which prevents I-75 from being trenched. The first place where a trench would be feasible would be around 5<sup>th</sup> Street. ODOT looked at extending streets over I-75, but the local roads would have to be raised to go over the interstate, and it would end up creating humps on the local roads.
  - We are concerned about the dust during construction.
    - The project contract will include conditions to minimize dust during construction and contact information will be provided if there are any concerns.
  - Is the funding in place for the project? And when will construction start?
    - The state funding for the Kentucky portion is already secured. Ohio is still reviewing the budget, and a decision will be made soon. The project team is also waiting on decisions on some federal grant money. Construction is scheduled to start in late 2023 or early 2024 and will last approximately five years.
  - The residents near Linn Street and Court Street would like a noise barrier built in this section of the neighborhood and would like the Community Council to draft a resolution for the City of Cincinnati to write a formal letter requesting that ODOT construct one in this location.
    - A noise analysis has been completed and noise walls are not recommended in this location because they do not meet the requirements of ODOT's noise policy.
  - What greenery or trees will be included in this section of the project?
    - Most of the topography of the existing land in this section of the project slopes down, and there is a baseball field in one area. No trees are planned to be planted in these areas due to the existing landscape. However, ODOT is working with the city of Cincinnati to incorporate additional green spaces wherever possible.

Following the question-and-answer session, the Community Council voted to pass a resolution urging the City of Cincinnati to formally request that noise walls be included from Queensgate Playground and Ballfields to the northern limits of the West End neighborhood.

All comments and responses will be posted to the project website ([www.brentspencebridgecorridor.com](http://www.brentspencebridgecorridor.com)) in January 2023. Individuals who provide contact information will be notified when the comments and responses are made publicly available.



---

## Attachment 1: Advertising Materials

**BRENT SPENCE**  
**BRIDGE CORRIDOR**



**TEAM**  
**KENTUCKY**  
TRANSPORTATION  
CABINET



**INVESTING IN LOCAL COMMUNITIES.**  
**GROWING AMERICA'S ECONOMY.**

## You're Invited

The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are conducting a series of **neighborhood outreach meetings** for the Brent Spence Bridge (BSB) Corridor Project

**Purpose:** To share updates on the BSB Corridor Project and to offer residents in surrounding neighborhoods the opportunity to share feedback with the Project Team

**When:** December 20, 2022 | 6:00pm  
(As part of the regularly scheduled West End meeting)

**Where:** Lincoln Recreation Center  
1027 Linn Street, Cincinnati, OH 45203

## Hope to see you there!

If you need assistance attending this meeting, please call Keith Smith at [\(513\) 933-6590](tel:5139336590).

View the most recent project newsletter at the following link:  
<https://mailchi.mp/aae28ef3744d/brent-spence-bridge-e-newsletter-16606655>

Si desea que los materiales para esta reunión son traducidos a español, contacte a Domingo Martinez tan pronto que sea posible a [Domingo.Martinez@dot.ohio.gov](mailto:Domingo.Martinez@dot.ohio.gov) o por teléfono a (513) 933-6136.

AGENDA for WECC Meeting

December 20, 2022 at 6 PM

To join the video meeting, click this link:<https://meet.google.com/yto-wwzx-jki>,  
Otherwise, to join by phone, dial +1 216-930-9717 and enter this PIN: 937 555 095#  
To view more phone numbers, click this link: <https://tel.meet/inx-iziy-wjk?hs=5>

Guests: CPD Update

Guests: ODOT - Brent Spence Bridge Presentation

Guests: Cincinnati DOTE- Morgan Kolks/ Linn St Project

Guests: Chauncey Walker - CW Developments

Guest: Cincinnati Library

Guest: Joe Berta: CRC

Guest: Cincinnati Museum Center

Minutes for November Meeting

Corresponding Secretary's report

Treasurer's Report

Financial Secretary's Report

Finance Committee: Keith Blake

Standing Committee Reports

Education: Angel Griffin

Safety: Keith Blake

Beautification: Fannie Shaw

Planning and Development: Sherri Barber

Youth: Larry Collins

Business Committee: Matt King

Old Business:

New Business : Non-Resident Membership

Next WECC Meeting: January 2023

Announcements:



# PUBLIC INVOLVEMENT AND COMMENTS



LEARN MORE ABOUT HOW THE BRENT SPENCE BRIDGE CORRIDOR PROJECT TEAM IS WORKING WITH THE COMMUNITY TO ENSURE STAKEHOLDER VOICES ARE HEARD AND COMMUNITY NEEDS ARE INTEGRATED INTO THE PROJECT PLAN.

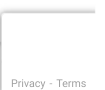
The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) are fully committed to robust involvement to gain insights and receive feedback as part of the project development process. The project team has been working closely with local partners to ensure communities surrounding the project area have the opportunity to provide feedback.

If you have a comment or question for the project team, please provide your feedback in the link at the bottom of the page.

## PUBLIC COMMENTS

Below are responses from the project team to specific public comments that have been received to date.

- Response to public comment – *WORKING POSITION PAPER: REDESIGN OF THE BRENT SPENCE BRIDGE PROJECT*
- Response to public comment – *BRENT SPENCE BRIDGE PROJECT – RECONNECTING CINCINNATI WESTWAY DESIGN IMPROVEMENTS*



**From:** Brent Spence Bridge Corridor <info@brentspencebridgecorridor.com>  
**Sent:** Monday, December 5, 2022 3:58 PM  
**To:**  
**Subject:** Brent Spence Bridge Corridor Project Update - DBE Matchmaker Event

Brent Spence Bridge Corridor December 5, 2022 eNewsletter



## DBE MATCHMAKER EVENT TAKING PLACE DEC. 7

*Small, Minority and Women-Owned Businesses Encouraged to Learn  
about Potential Work Opportunities*

The Brent Spence Bridge Corridor project will offer unprecedented opportunities for small, minority, and women-owned businesses in our region. To support the inclusion of Disadvantaged Business Enterprise (DBE) consultants and contractors on the Brent Spence Bridge Corridor Project, the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) will host two events Wednesday, Dec. 7 at the Anderson Pavilion, 8 Mehring Way, in Cincinnati.

From 9 a.m. to noon, representatives from ODOT and KYTC will provide an overview of the project for DBE firms. The presentation will include details about prequalification requirements for the project and support services available to DBE consultants and contractors.

From 1:30 to 4:30 p.m., ODOT and KYTC will conduct a “matchmaker” event in which DBE firms can meet one-on-one with prime contractors and firms who work directly with the government and manage subcontractors and consultants.

“This is a great opportunity for the industry,” said ODOT’s Tom Arnold of the afternoon event. “Each DBE gets 15 minutes to present their capabilities and work experience to a number of

prime contractors. Our goal is to help our prime design and construction firms understand how many great small businesses they can work with to meet our project inclusion goals.”

Arnold, a member of the project’s Diversity and Inclusion Outreach Committee, added ODOT and KYTC plan to issue a request for proposals for the project’s prime contractor early next year. Construction is scheduled to begin late next year or early 2024.

“We strongly recommend DBE firms [preregister](#) for both events because we want to help make as many connections as possible,” Arnold said. He also recommended DBE firms follow ODOT’s Outreach [webpage](#) for additional updates and events.

### Schedule of DBE Events

Wednesday, Dec. 7 | Anderson Pavilion  
8:30 to 9 a.m.: Registration  
9 a.m. to Noon: Project Overview  
1:30 to 4:30 p.m.: Matchmaker Event with Prime Contractors

## ODOT AND KYTC CONTINUE ROBUST PUBLIC OUTREACH THROUGH NEIGHBORHOOD MEETINGS

*Public Invited to Attend to Ask Questions and Share Feedback*

The joint project team from ODOT and KYTC continues to gather valuable feedback from communities surrounding the Brent Spence Bridge Corridor project area through a public meeting and listening tour. Suggestions gathered during these sessions will enhance the overall project, including opportunities to reconnect neighborhoods, improve bike and pedestrian safety, and reduce the project’s footprint.

Through the end of 2022 and into 2023, the project team will continue meeting one-on-one with directly affected communities to answer questions and gather ideas.

As the project gets under way, ODOT and KYTC will continue to explore ways to improve pedestrian access, improve safety, and support better access to business districts while reclaiming public land for local use.

During the eight meetings conducted to date, residents and local officials have identified opportunities that are now part of the plan, including enhanced streetscape amenities, aesthetic treatments, and bicycle and pedestrian infrastructure. All will improve connectivity to transit, jobs, health care, cultural, recreational and commercial destinations.

In addition to the neighborhood meetings, the public will be provided the opportunity to review project materials and provide formal comments at public hearings currently scheduled for July/August 2023.





**KENTUCKY'S UPCOMING NEIGHBORHOOD MEETING SCHEDULE**

<b>Neighborhood</b>	<b>Meeting Date</b>	<b>Meeting Time</b>	<b>Meeting Location</b>
Mutter Gottes/CBD	December 5, 2022	7:00 p.m.-8:00 p.m.	Kenton County Public Library 502 Scott Street, Covington
Lewisburg/Botany Hills	December 6, 2022	7:00 p.m.-8:00 p.m.	Holiday Inn Express & Suites 200 Crescent Avenue, Covington
Park Hills	December 12, 2022	7:00 p.m.-8:30 p.m.	Garden of Park Hills 1622 Dixie Highway, Park Hills
Broad Neighborhood Meeting	December 14, 2022	12:00 p.m.-2:00 p.m. 5:00 p.m.-7:00 p.m.	Kenton County Fiscal Court (Riedlin Schott Community Room) 1840 Simon Kenton Way, Covington
Public Hearing	TBD July/August 2023	TBD	TBD

**OHIO'S UPCOMING NEIGHBORHOOD MEETING SCHEDULE**

Neighborhood	Meeting Date	Meeting Time	Meeting Location
Camp Washington	December 12, 2022	7:00 p.m.-8:30 p.m.	Camp Washington Urban Revitalization Corporation 2951 Sidney Avenue, Cincinnati
Broad Neighborhood Meeting	December 13, 2022	12:00 p.m.-2:00 p.m. 5:00 p.m.-7:00 p.m.	Lincoln Recreation Center 1027 Linn Street, Cincinnati
West End	December 20, 2022	6:00 p.m.-8:00 p.m.	Lincoln Recreation Center 1027 Linn Street, Cincinnati
Public Hearing	TBD July/August 2023	TBD	TBD

## REVISED PROJECT DESIGN RETURNS 10 ACRES TO CINCINNATI CENTRAL BUSINESS DISTRICT

In its continuing collaboration with Cincinnati’s elected officials and community leaders, the Brent Spence Bridge Corridor Project team redesigned plans for the area immediately west of the Duke Energy Convention Center, freeing up 10 acres for future development.

Last summer the city and the Cincinnati USA Regional Chamber submitted comments and requested changes to the Ohio side of the corridor to enlarge the central business district, which were approved by the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC).

This creates “some really exciting opportunities for large-scale investments,” said Cincinnati Mayor Aftab Pureval at a news conference last month. “We want to be a city that’s growing, and these 10 acres right in the middle of our downtown” will be like pouring “gasoline on that effort.”

Changes include refinements to the entrance and exit ramps on Third, Fourth, Fifth, and Sixth streets to better integrate them into the city’s street network. The revised plan also removes one exit ramp from Interstate 75 South.

Though the city has no definite plans for the reclaimed area, Pureval said he envisions creating a place where people “can either work or enjoy retail and restaurants.” He added the 10 acres has an estimated value of \$20 million and could generate more than \$500,000 a year in taxes.

Brendon Cull, president of the chamber, said adding 10 acres in downtown Cincinnati represents a significant economic opportunity for the city and region. “Our goal all along has been to make sure that the Brent Spence Bridge gets done and gets done in a way that maximizes opportunity for our region,” he said. “This successful effort will create more economic opportunity for the people who live here and work here, and that’s what matters.”



Image from Spectrum News 1

[Read more here.](#)

## Stay Connected

There are several ways to stay connected with the Brent Spence Bridge Corridor Project.

[Visit our website](#)

[Sign up for updates](#)

[Submit a comment](#)

© 2022 Brent Spence Bridge Corridor. All Rights Reserved.

Brent Spence Bridge Corridor | 505 South State Route 741, Lebanon, OH 45036

[Unsubscribe jheflin@hntb.com](mailto:jheflin@hntb.com)

[Update Profile](#) | [Constant Contact Data Notice](#)

Sent by [info@brentspencebridgecorridor.com](mailto:info@brentspencebridgecorridor.com) powered by



- [Response to public comment – CITY OF CINCINNATI BSB CORRIDOR CONCEPT I-WA MINIMIZATION](#)

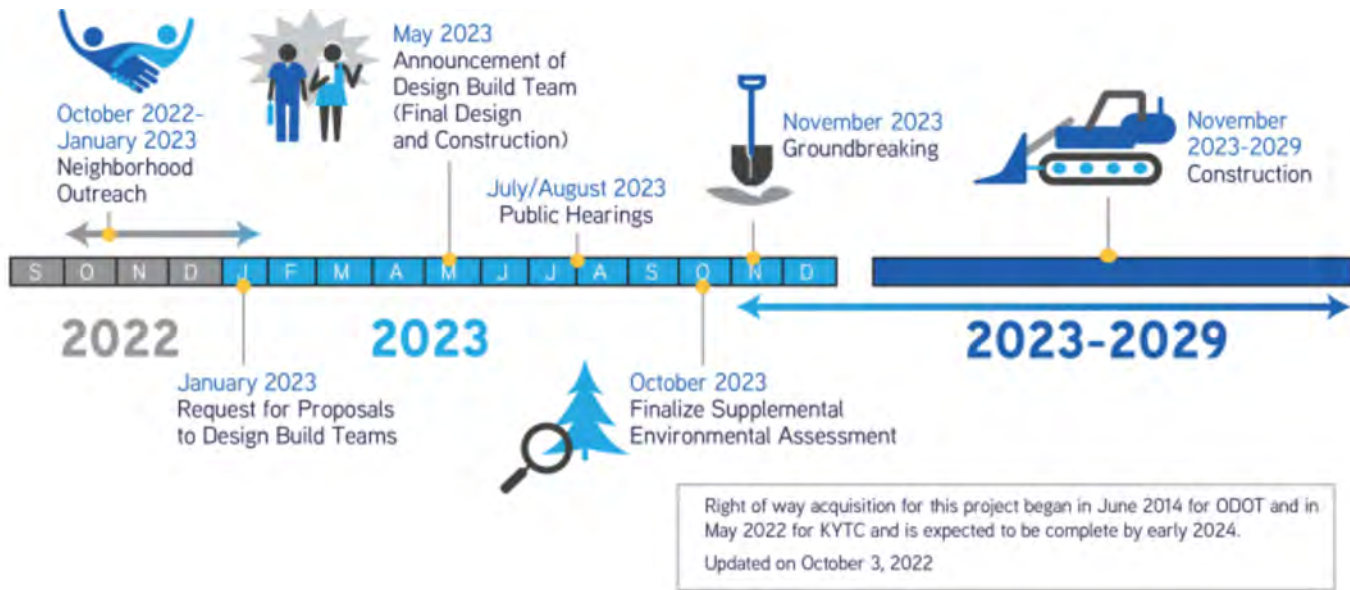
[CONTACT US](#)



Below are summaries of comments received by the project team, including responses to each.

- [General Public Comment and Response Summary \(updated monthly\)](#)
- [Neighborhood Meeting Survey Comment and Response Summary \(coming in January 2023\)](#)

## PROJECT SCHEDULE



## PROJECT AREA MAPS

Below are detailed maps of the project corridor, including an overall map, a multimodal map including paths for pedestrians a traffic flow map, and project renderings with illustrations of how the corridor could look when complete.

- [Project Corridor Map](#)
- [Multimodal Map](#)
- [Traffic Flow Map](#)
- [Project Renderings](#)

## NEIGHBORHOOD OUTREACH MEETINGS



To more thoroughly engage communities within the project for those residents. These events feature a presentation by survey, and offer feedback one-on-one with the project team.

[CONTACT US](#)

DOT have been holdin  
and allow residents to t

**KENTUCKY NEIGHBORHOOD MEETINGS**

- Mainstrasse, 11/21/2022
- Friends of Peaselburg, 11/28/2022
- Westside Covington, 11/29/2022
- Fort Mitchell, 11/30/2022
- Fort Wright, 12/1/2022
- Mutter Gotes/CBD, 12/5/2022
- Lewisburg/Botany Hills, 12/6/2022
- Park Hills, 12/12/2022

**KENTUCKY BROAD NEIGHBORHOOD OPEN HOUSE MEETING**

Wednesday, December 14, 2022 (afternoon and evening)  
View the presentation from the neighborhood meeting.

**OHIO NEIGHBORHOOD MEETINGS**

- CUF, 11/15/2022
- CBD Riverfront, 11/29/2022
- Camp Washington, 12/12/2022

West End, 12/20/2022

**OHIO BROAD NEIGHBORHOOD OPEN HOUSE MEETING**

Tuesday, December 13, 2022 (afternoon and evening)  
View the presentation from the neighborhood meeting.

**PUBLIC HEARINGS**

Public hearings will be held in July/August of 2023, once the Supplemental Environmental Assessment (EA) is made publicly available. The Supplemental EA and information and exhibits about the project and its expected impacts to the human and natural environment will be available for review at the hearings and in the document center on the project website.



One public hearing will be scheduled in each state, and a virtual public hearing will also be held. Public hearing dates, times, and locations have not been determined yet. You can be notified when the meetings are scheduled.



s, times, and locations  
g up for project update

Thank you for visiting the Brent Spence Bridge Corridor website. If you have a question or comment for the project team, please complete the form below.

Please enter your contact information to receive periodic updates, including notification of upcoming public meetings.

NAME

NAME

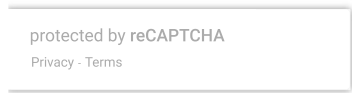
EMAIL

EMAIL

MESSAGE

SIGN UP

ZIP CODE



SUBMIT

Si desea asistencia traduciendo esta sitio web a español, contacte a Domingo Martinez a Domingo.Martinez@dot.ohio.gov o al teléfono a 513-933-6136.



CONTACT US



Ohio Department of Transportation | 1980 W Broad St | Columbus, OH 43223



Kentucky Transportation Cabinet | 200 Mero St | Frankfort, KY 40622

© 2023 Brent Spence Bridge Corridor. All Rights Reserved.





---

Attachment 2: Sign-In Sheet



# West End

Community Council

PO Box 14424  
Cincinnati, Ohio 45250

## General Body Meeting

Location: Lincoln Recreation Center

Date: 12/20/22

Time: 6:00 PM

	Name	Email	Mailing Address
1	NOSH OBSTEN	on file	on file
2	Deborah Stone		
3	Stanford Porel		
4	Fannie Shaw		
5	Don S Marshall		
6	Clare Pollock		
7	Chamy WALTER		
8	Galen Gordon		
9	Emma Norbut		
10	Kim Hale-McCarty		
11	Sharon Watkins		
12	J P Lunsol		
13	Wendy Nelson		
14	Keloni Parks		
15	Joseph Besta		
16	Tommy Arnold		
17	KIKKI APPLETON		
18			
19			
20			

▼ About this call

People

Info

Chat

Activities



Jodi Heflin (You)



West End Commun...

Meeting host



Amber Kelly



Andre Mauldin



Anita D. Robinson



Antoine Mauldin



Jason Mickey



Jemelle Howard



Jim Benedict



Keith Blake



Keith Blake

Presentation



Larry Collins



LaShonda Wright

Thumbnail for 'You' showing a profile picture and a presentation slide titled 'Bicycles and Bicycles'.

Thumbnail for 'Keith +20' showing a presentation slide with text: 'connections across I-75', 'use paths', 'use path along Winchell', 'connectors to local'.

▼ About this call

People

Info

Chat

Activities



**Keith Blake**  
Presentation



**Larry Collins**



**LaShonda Wright**



**M Mallory**



**Mark Alexander**



**NaQuia Chante**



**Nick Johnson**



**Noah O'Brien**



**+1 513-\*\*\*-\*\*\*09**



**+1 513-\*\*\*-\*\*\*37**



**+1 513-\*\*\*-\*\*\*67**



**+1 513-\*\*\*-\*\*\*91**

A call gallery showing two active call thumbnails. The first thumbnail is labeled 'You' and shows a landscape with mountains and a sun. The second thumbnail is labeled 'Keith +20' and shows a city street scene. Both thumbnails have a mute icon in the top right corner.



---

## Attachment 3: Photographs

## West End Neighborhood Meeting

December 20, 2022



Photograph 1. West End Neighborhood Meeting held on December 20, 2022



Photograph 2. West End Neighborhood Meeting held on December 20, 2022

**West End Neighborhood Meeting**  
**December 20, 2022**



Photograph 3. West End Neighborhood Meeting held on December 20, 2022



Photograph 4. West End Neighborhood Meeting held on December 20, 2022



---


[Attachment 4: Presentation](#)



The meeting will open with remarks by the City of Cincinnati (5 min) and introductions of the Project Team in attendance (2 min).

## Welcome

- Meeting purpose
  - Share updates on the Brent Spence Bridge (BSB) Corridor Project
  - Offer residents in the West End neighborhood the opportunity to share feedback with the Project Team
- Agenda
  - General project overview
  - Project specifics in the West End area
  - Discussion/feedback from West End residents



The reason we're here tonight is to give a brief overview of the Brent Spence Bridge Corridor Project both for those of you who aren't familiar with the project and for those of you who have been following the progress over the past decade. We're also here to give specific details about what the project will look like in and near the West End neighborhood. Most importantly, tonight we're focusing on hearing from the residents of West End. We're here to answer your questions and to listen to your feedback about the project.

## Project History

2000	
2005	← 2004   ODOT & KYTC begin BSB Corridor Project Alternatives Development & Evaluation Preliminary Engineering Impact Assessment Public & Stakeholder Input
2010	← 2012   EA & FONSI - Preferred Alternative I Design Refinements Cost/Impact Reductions Enhancements Property Acquisition Funding Identification
2015	
2020	← 2022   Concept I-W

For those of you who haven't been following the project for the last 15 years, let's go over a quick history.

In 2004, ODOT and KYTC formally began studying ways to improve 7.8 miles of I-71 and I-75 in Kentucky and Ohio. In Ohio, that includes I-75 from the Brent Spence Bridge to just north of the Western Hills Viaduct. Through a series of preliminary engineering and planning studies, we developed several potential alternatives for improving the corridor. We completed preliminary engineering and evaluated the impacts of each alternative. We also held several public meetings to gather feedback on the alternatives. The whole process led to the development of an Environmental Assessment (which is abbreviated "EA" on this slide) that compared the benefits and impacts of the alternatives and recommended one alternative – called the preferred alternative - to move forward into detailed design and construction. After reviewing the EA and gathering more feedback through public hearings, the Federal Highway Administration issued a decision called a "Finding of No Significant Impact" (also called a FONSI, as shown on this slide), which marked the formal approval the project and allowed the preferred alternative – Alternative I – to move into the next stages of design.

Since the approval of the FONSI in 2012, ODOT has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements into the project. These studies resulted in several refinements to Preferred Alternative I, which we are referring to as "Concept I-W" and what we will be presenting to you tonight.

Also since 2012, ODOT has purchased most of the land needed to build the project. In addition, we've begun detailed design from Linn Street to the northern project limits.

## Project Description

### Ohio

- Widen I-75
- Rebuild all overpass bridges and interchanges
- Build a collector-distributor system
- Tie into Mill Creek Expressway-Hopple Street Interchange project
- Tie into the Western Hills Viaduct project
- Add a northbound exit to Ezzard Charles Drive
- Connect to I-71 and US-50E



Note: Individual project elements will come in with click (marked by #).

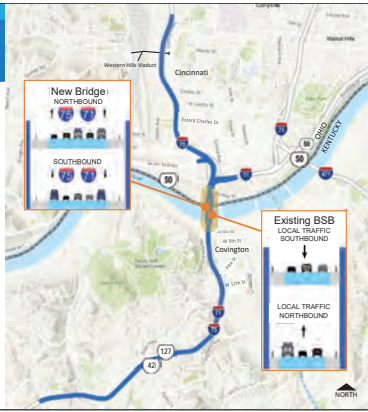
In Ohio, the project will add one lane in each direction to I-75, remove left exits, and rebuild all bridges and interchanges. (#) Beginning near Ezzard Charles Drive, a collector-distributor system will be added to connect I-75 traffic to and from the local street network and US-50 West. (#) In the north, it will tie into the recently completed Mill Creek Expressway-Hopple Street Interchange project. (#) The project will rebuild the I-75 interchange at the Western Hills Viaduct and tie into the new bridge replacement project being developed by the City of Cincinnati and Hamilton County. (#) A new northbound exit will be built at Ezzard Charles Drive to improve access to Union Terminal, TQL Stadium, and Over-the-Rhine. Lastly, it will connect to I-71 and US-50 East.



## Project Description

### Brent Spence Bridge

- New double-decker companion bridge
  - 5 lanes each deck
  - Carry through (interstate) traffic
- Rehabilitate and reconfigure existing bridge
  - Three lanes each deck
  - Increased inside/outside shoulders
  - Carry local traffic



Note: Bridge details will come in with click (marked by #).

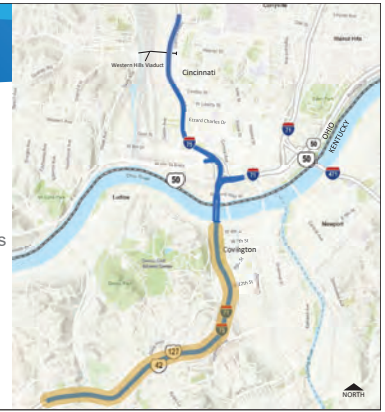
Let's move south to the Brent Spence bridge crossing. The project will build a new double decker companion bridge with five lanes on each deck west of the existing BSB. The new bridge will carry through (interstate) traffic. (#)

The existing double-decker Brent Spence bridge will be rehabilitated and reconfigured to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing bridge will carry local traffic only.

## Project Description

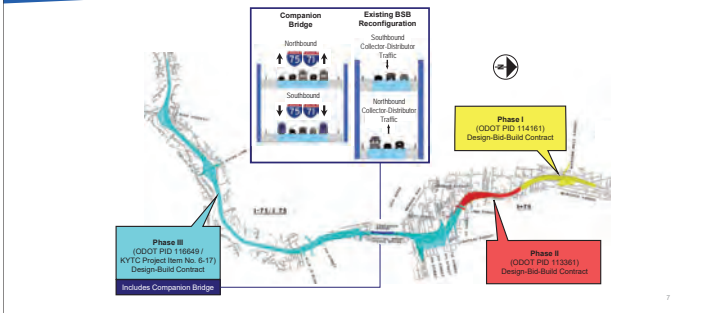
### Kentucky

- Widen I-71/I-75
- Rebuild all overpass bridges and interchanges
- Extend frontage roads in Covington
- Construct collector-distributor systems in Covington and Ft. Mitchell/Ft. Wright



In Kentucky, the project will widen I-71 and I-75 and rebuild all overpass bridges and interchanges. The project will also extend existing frontage roads to improve connectivity in Covington. A collector-distributor system will also be built beginning in Covington to connect interstate traffic to and from the local street network. Lastly, collector-distributor ramps will be built in Fort Mitchell and Fort Wright to reduce the need for traffic to weave between ramps and the through lanes on the interstate.

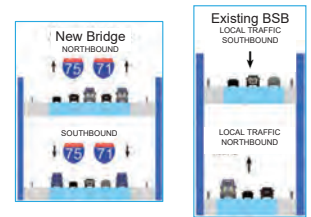
## Project Description



The project is going to be built in three phases. Phase I (shown in yellow) will stretch from Findlay Street to the north. Phase II (shown in red) will stretch from Linn Street to Findlay Street. Phase III (shown in blue) will build everything else, including the new companion bridge. Phase I, which is located in the northern portion of West End is currently under design with construction expected to begin in 2028. Although, the construction time frame may adjust based on on-going coordination with the City's Western Hills Viaduct Project. Phase II, which is located in the heart of West End is also under design with construction expected to begin in 2025. Phase III, extends into the southern portions of West End will be built under a progressive design-build contract which will begin in 2023.

## What Has Changed?

- Reconfigured how traffic travels across the Ohio River
  - Companion bridge carries through (interstate) traffic
  - Existing bridge carries local traffic
  - All northbound and southbound traffic on one deck
  - Width of companion bridge substantially reduced



As mentioned earlier, since the 2012 EA and FONSI, ODOT has been studying ways to improve the design, simplify the construction, reduce costs, reduce impacts, and incorporate additional enhancements. One of the biggest changes is how traffic will travel across the Ohio River. The Preferred Alternative identified in 2012 mixed local and interstate traffic on both bridges. The 2012 design also placed northbound and southbound traffic on the same bridge decks, which required additional width for a median to safely separate opposing traffic.

Concept I-W carries all interstate traffic on the new companion bridge and all local traffic on the existing Brent Spence Bridge. In addition, all northbound and southbound traffic is grouped on their own bridge decks. As a result, the width of the companion bridge was reduced from 172 feet to 107 feet, substantially reducing the cost of the bridge. This configuration will also improve traffic flow and safety by separating through and local traffic.





This drawing shows what the Downtown interchange area might look like once the project is built.

## What Has Changed?

- Northbound I-75 entrance ramp moved from Freeman to Winchell
- One Ezzard Charles Drive bridge
- Minimize work along Winchell



Note: Changes will come in with click (marked by #).

Turning our focus to the heart of West End, another change ODOT's made since 2012 involves moving the northbound entrance ramp to I-75 from its existing location at (#) Freeman Avenue (south of Ezzard Charles Drive) (#) to Winchell Avenue (north of Ezzard Charles Drive). Also, (#) the two existing one-way bridges on Ezzard Charles Drive will be replaced with one, two-way bridge. These changes were also made in coordination with the City of Cincinnati to reduce project impacts and costs and improve local access to the interstate.

We've also refined the roadway layouts so that work will not occur along Winchell Avenue beyond what is needed to build the northbound entrance ramp shown in this drawing.

## What Has Changed?

- Interchange at the Western Hills Viaduct



In addition, the layout of the project has been refined to tie into the new Western Hills Viaduct, which is a separate, stand-alone project being developed by the City and County. The existing ramps at the I-75 interchange will be removed and replaced with new ramps that will provide direct access to and from the new Western Hills Viaduct bridge. The ramps will also connect I-75 to the local street system at Findlay Street for southbound traffic and Bank Street for northbound traffic.

## Noise

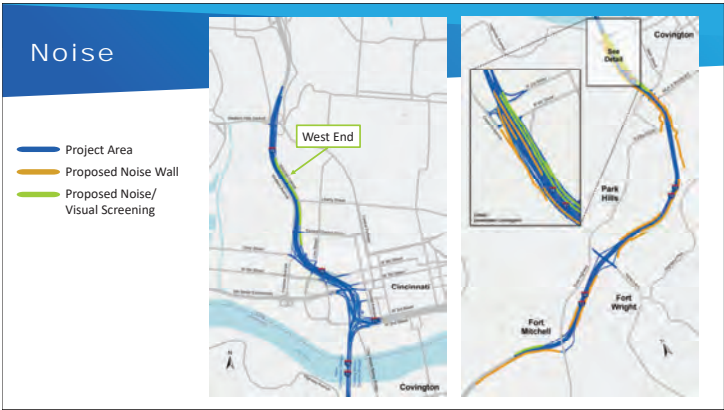
- ODOT Noise Policy
- Developed in partnership with FHWA
- Define thresholds for whether a noise barrier is feasible and reasonable
  - Are areas of frequent human exterior use present?
  - Does the noise level exceed certain established thresholds?
  - Does the noise level substantially increase?
  - Can a barrier effectively block noise?
  - Does a barrier provide enough noise reduction to justify cost?
  - Does the community want a noise barrier?
- Noise walls must meet all feasible and reasonable thresholds



Now let's dig into how the project will impact West End, starting with noise. ODOT has a formal Noise Policy that guides how traffic noise is evaluated for transportation projects, which was developed in partnership with the Federal Highway Administration. The first thing we examine is whether there are areas where people congregate outside. This could include someone's yard, a patio, or a recreation area, among others. The second thing we examine is whether the predicted noise level approaches or exceeds certain thresholds for different uses, which we call receptors. We also determine if there is a substantial increase in the predicted noise level when compared to what existed before the project. If the noise level is predicted to approach or exceed the established thresholds or to have a substantial increase, that area is considered to have a noise impact.

If a noise impact is identified, we evaluate if a barrier is feasible, or, "Can it effectively block noise?" In addition, the barrier must not pose any overriding engineering, constructability, safety, or maintenance issues.

If a barrier is found to be feasible, then ODOT evaluates whether it is reasonable to construct the barrier. When evaluating if a barrier is reasonable, we look at whether a barrier meets certain noise reduction design goals and if it is cost effective. A noise wall must be found to be BOTH feasible AND reasonable in order to be built.



In Ohio, noise was not evaluated for areas west of I-75, because there were too few receptors, and they are located far apart from one another. A noise wall north of the Western Hills Viaduct and east of I-75 was not feasible because it could not provide a minimum 5dBA reduction for 40% of the impacted receptors. It also could not meet the cost effectiveness criteria for reasonability. In the West End neighborhood, noise walls were not found to be feasible because there were not areas of frequent human exterior use next to the highway. For example, the existing apartment complexes and buildings don't have outdoor patios or other similar areas. In addition, a noise wall along the Queensgate Playground and Ballfields did not meet the cost effectiveness criteria for reasonability.

## Noise - West End (OH)

Noise walls not reasonable/feasible per ODOT Noise Policy

- Tire pavement noise = major traffic noise contributor
- Noise reducing measures proposed:
  - 57-inch median and bridge barrier
  - 8-foot-tall extended traffic safety barrier
- Provide substantial noise reduction
- Provide visual screening of highway

In 2012, ODOT stated that it would build noise walls between Freeman Avenue and Bank Street based on the noise analyses that were conducted at that time. Even though these noise walls are no longer warranted according to ODOT's noise policy, ODOT recognizes the I-75 corridor will remain the predominant noise source within the project area. To reduce overall traffic noise levels, ODOT intends to build barriers that will reduce tire pavement noise, which is a major contributor to traffic noise. ODOT will build 57-inch barriers in the I-75 median and on the Liberty, Findlay, and Bank street bridges. For comparison, a standard bridge barrier is over a foot shorter at 42 inches. In addition, ODOT will build 8-foot-tall extended traffic safety barriers along the I-75 shoulders from south of Ezzard Charles Drive to Bank Street. Some examples of what these walls will look like are pictured on this slide.

The 8-foot-tall barriers will provide a substantial reduction in noise levels. Research has shown that 8-foot-tall concrete traffic barriers will approximately cut traffic noise levels in half, which is what traditional noise barriers are expected to do. They will also have aesthetic treatments to help them fit into the community and will help to provide visual screening between I-75 and the West End neighborhood.

## Aesthetics

- Corridor-wide aesthetic treatments
  - Ashlar stone treatments for abutments, piers, and walls
  - Decorative bridge parapets
  - Translucent screen walls and planters on bridges
  - Other features (lighting, sidewalks, etc.) to match City standards

ODOT has worked with the City and the project Aesthetic Committee to develop an Aesthetic Design Checklist that will guide what the Brent Spence Bridge Corridor will look like. For example, ODOT will add aesthetic treatments in a pattern called "Ashlar Stone" to all bridge abutments, parapets, and piers. Retaining walls and the extended traffic safety barriers we just discussed will also have ashlar stone treatments. Overhead bridge parapets will have end treatments with the bridge identification name, construction completion data, and rustification design features. Examples of similar bridge parapets are shown in the images on this slide. In addition, overpass bridges will have wide sidewalks or shared-use paths, planters, and translucent screen walls. Other features throughout the corridor will match City aesthetic standards, including lighting, sidewalks, tree lawns, and others. We will show some examples of what specific locations in West End will look like in just a few slides.

## Pedestrians and Bicycles

- Connections across I-75
  - Sidewalks
  - Shared use paths
  - Bike lanes
- New shared use path along Winchell Avenue
- Improved connections to local destinations

The project will install bicycle and pedestrian infrastructure on connections across I-75 like what is shown here for 6th, 7th, and 9th streets. Pedestrian and bicycle connections will also be provided on Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue. To promote safety for bicycles and pedestrians, the ramp connections with local streets are being designed as lower-speed urban intersections in accordance with City of Cincinnati design standards. The pedestrian and bicycle infrastructure included in the project will improve connectivity to transit, employment, healthcare, cultural, recreational, and commercial destinations.



This exhibit shows some of the planned pedestrian and bicycle facilities in West End. Note the new shared use paths on 6th, 7th, and 9th streets and Winchell Avenue and new sidewalk connections on 9th.



Continuing north, we see how the new shared use path on Winchell Avenue continues all the way to Ezzard Charles Drive. Also notice new shared use paths on Linn Street and Ezzard Charles Drive, new sidewalks along Guest Street, Court Street, Freeman, and Ezzard Charles, a new pedestrian bridge connection over Winchell Avenue, and new bike lanes on Liberty Street.



As we move further north, notice the new bike lanes on Findlay Street, the new shared use path on Harrison Avenue, and the replaced sidewalks on Harrison, Bank, and Findlay. These will tie into the existing bike lanes on Western, Winchell, and Bank. Throughout the corridor the new and improved pedestrian and bicycle facilities will provide connections to existing transit stops.



This is a view of what Findlay Street will look like when the project is built. Notice the 57-inch barrier on the I-75 bridge and the 8-foot extended traffic safety barriers on both sides of the bridge to reduce traffic noise. This also shows what the barriers and bridge walls will look like with the ashlar stone treatment. Also notice the rebuilt sidewalks, added bike lanes, and underpass lighting.




This view shows what the new, two-way bridge on Ezzard Charles Drive will look like. Notice the wide shared use path on the right and the sidewalk on the left. Also notice the planters, screenwall, and decorative lighting on the bridge. The bridge lights and traffic signal supports will be black and meet City aesthetic standards.




This view shows what the finished project will look like from Ezzard Charles Drive looking north. Notice the 8-foot-tall extended traffic safety barriers for noise reduction. Also notice the ashlar stone treatments on the retaining walls.

**BRENT SPENCE**  
BRIDGE CORRIDOR




# THANK YOU!

For more detailed information or to provide feedback visit:  
[www.PublicInput.com/bsbc](http://www.PublicInput.com/bsbc)



[BRENTSPENCEBRIDGECORRIDOR.COM](http://BRENTSPENCEBRIDGECORRIDOR.COM)



This concludes our formal presentation. We would like to hear your thoughts about the project. You can also visit a [Publicinput.com](http://Publicinput.com) to review information about the project and provide your feedback.

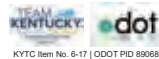


---

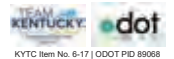
## Attachment 5: Exhibits



**Brent Spence Bridge Corridor - Ohio**  
 (Looking North Near Ezzard Charles Drive)



**Ezzard Charles Drive (Looking West) - Ohio**



**Findlay Street (Looking West) - Ohio**

